



FEASIBILITY STUDY

NORMANDALE NEIGHBORHOOD ROADWAY RECONSTRUCTION

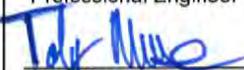
Sherwood Avenue, Ryan Avenue, Parnell Avenue,
West Shore Drive, 65th Street, and 64th Street

IMPROVEMENT NO. BA-394

November 14, 2012

**ENGINEERING DEPARTMENT
CITY OF EDINA**

I hereby certify that this feasibility study was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the State of Minnesota.

	43364	11/14/12
Toby Muse	Reg. No.	Date
Approved 		11/14/12
Chad A. Millner, PE		Date
Asst. City Engineer		

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FEASIBILITY STUDY – BA-394 ENGINEERING DEPARTMENT CITY OF EDINA

NORMANDALE NEIGHBORHOOD ROADWAY RECONSTRUCTION NOVEMBER 14, 2012

SUMMARY:

The project involves performing spot repairs to existing concrete curb and gutter and reconstructing the bituminous pavement while leaving the aggregate base layer and subgrade soils undisturbed. If poor subgrade soils are encountered, the project will remove and replace them while salvaging the aggregate base layer for reincorporation into the proposed street. The project also involves replacing deficient drainage structures and connecting sump pump drain pipe to the storm sewer system as needed. The project will also involve replacing hydrants and gate valves, trunk water main pipe extensions, water and sewer service pipe reconstruction, and rehabilitating trunk sanitary sewer pipe.

The estimated total project cost is \$3,553,000. Funding for the entire project will be from a combination of special assessment, utility and Active Living Infrastructure funds. The estimated roadway cost is \$1,095,000, and the estimated sanitary sewer service cost is \$175,000. Both of these costs will be 100 percent funded by special assessments at a rate of \$14,800 per REU. Properties not receiving new sanitary sewer services will be assessed at a rate of \$12,300 per REU. Utility improvements and repairs amount to \$1,888,000 and will be funded through respective utility funds. Multi-use path and lighting upgrades adjacent to West Shore Drive amount to \$395,000 and will be funded through the Active Living Infrastructure fund.

The project can be completed during the 2013 construction season. Staff believes the project is necessary, cost effective and feasible to improve the infrastructure as initiated by the vision of Edina's Vision 20/20 – "Livable Environment" and "A Sound Public Infrastructure".

LOCATION:

The project area includes Sherwood Avenue (66th Street to 64th Street), Ryan Avenue (Cul-De-Sac to south right-of-way of Highway 62), Parnell Avenue (Cul-De-Sac to south right-of-way of Highway 62), West Shore Drive (66th Street to south right-of-way of Highway 62), 65th Street (Sherwood Avenue to Ryan Avenue and Parnell Avenue to West Shore Drive), and 64th Street (115' west of Sherwood Avenue to West Shore Drive). Figure 1 is a detailed project location map of the Normandale Neighborhood Roadway Improvement Project.

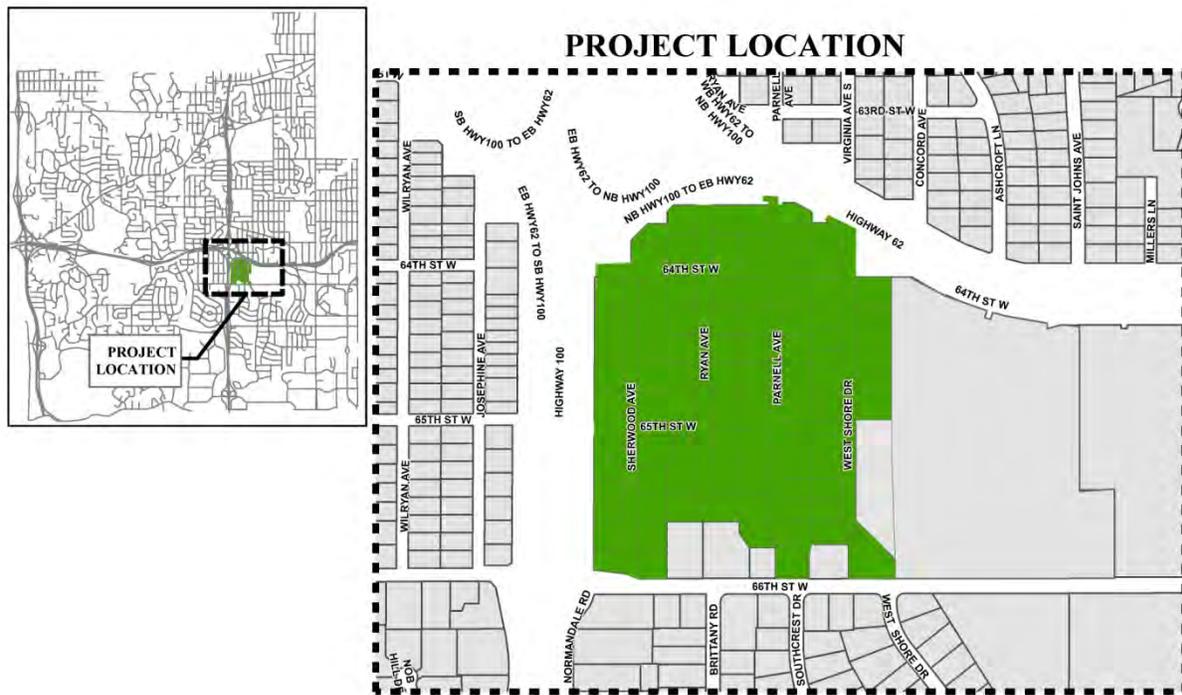


Figure 1 - Project Location Map

INITIATION & ISSUES:

The project was initiated by the Engineering Department as part of the City’s street reconstruction program and identified in the Capital Improvement Program. This project addresses updating aging infrastructure issues associated with the pavement condition, storm water, sanitary sewer and water main systems.

All Engineering projects are reviewed for compatibility with the City of Edina 2008 Comprehensive Plan Update, Comprehensive Bicycle Transportation Plan, the Comprehensive Water Resource Management Plan, and the draft Living Streets Policy Framework and the Sustainability Project Evaluation.

City of Edina 2008 Comprehensive Plan Update

Sidewalk Facilities

Chapter 7 of the plan addresses locations of proposed sidewalks facilities and funding options within the City. As shown in Figure 7.10 of Appendix G, there are no proposed sidewalk facilities indicated within the project limits.

Bicycle Facilities

Chapter 7 of the plan addresses locations of proposed bicycle facilities within the City as part of the Comprehensive Bicycle Transportation Plan. Figure 7.11 of Appendix G identifies West Shore Drive as a secondary bicycle route between 66th Street and 64th Street.

City of Edina Comprehensive Water Resource Management Plan

The project is located within the Nine Mile Creek Watershed district. The 2011 Comprehensive Water Resource Management Plan indicates no potential flood areas in the neighborhood. Further evaluation will be performed regarding drainage issues resulting from the questionnaires.

Draft Living Streets Policy Framework and the Sustainability Project Evaluation

The Edina Transportation Commission (ETC) is currently developing a Living Streets Policy Framework, which will then progress to developing a Living Street Policy and Plan for the City; see Appendix A for a Memo from HRGreen and BARR Engineering; consultants helping the ETC with this project. The vision statement expresses the need to look at projects differently in the future:

Living Streets balance the needs of motorists, pedestrians, bicyclists, and transit riders in ways that promote safety and convenience, enhance community identity, create economic vitality, improve environmental sustainability, and provide meaningful opportunities for active living and better health.

Although the Living Streets Policy and Plan has not been developed, staff has included elements that pertain to residential neighborhoods in the rehabilitation of the infrastructure and replacement of the roadways. Staff is also including a simple sustainability analysis for this project. This is the first year to include this analysis and we anticipate a more refined analysis in the future that will include review and input from a sustainability team.

Sustainability in engineering projects means delivering our services in a manner that ensures an appropriate balance between the environment, the community, and funding. This is essentially the “Triple Bottom Line” of sustainability; Equity, Environment, and Economy. We look at sustainability as maximizing our resources, creating lasting environments, improving and shaping both the present and future of our community so that future generations are not burdened by the decisions of today.

The project was evaluated based on the following key indicators to look for strengths, weaknesses, opportunities and risks.

- Equity: How well does the project provide or maintain core city services such as transportation, sanitation, clean water, emergency access, and emergency service? How does the project influence the well-being of the community?
- Environment: How does the project influence the natural environment; such as surface or ground water health, forest canopy, natural resource diversity, wildlife habitat, air quality, noise and others?
- Economy: How does the project influence the local economy, what are the short term and long term costs? Is the continued service worth the price?

The following is a summary of this evaluation:

Equity: The project maintains access to the transportation network. Updates to the fire hydrants provide public safety staff the ease of connection needed during an emergency.

Environment: The project provides for an increase in the sediment control capacity of the storm sewer network and helps to control localized flooding. The project provides homeowners a piping system to discharge ground water into; this will eliminate standing water and/or algae buildup along the street curb lines. Construction operations are required to use the smallest footprint necessary to complete the work thus protecting the existing natural environment. The project also analyzes the sanitary sewer to ensure that inflow and infiltration of clear water is kept out of the sewer system, which minimizes regional wastewater treatment.

Economy: The project is designed to reduce construction costs now and into the future. The proposed roadway section can easily be maintained in the long term with the use of mill and overlays and/or seal coating operations. These maintenance operations will extend the life of the pavement. The project will also use less intense construction methods, such as trenchless technology; i.e., lining the pipes versus removing and replacing them. This is a simplified analysis of the projects sustainability. In the future we anticipate correlating this analysis to an in-depth scoring system displaying the City's sustainability to the community.

Staff Issues

In determining the feasibility of the project, the following existing issues and/or features are addressed in this study:

- Poor condition of existing pavement surface.
- Existing landscaping and driveways.
- Sanitary sewer and water main deficiencies.
- Existing mature trees.
- Existing residential roadway lighting.

Resident Input

The Engineering Department follows a practice of notifying residents two years prior to a potential reconstruction project. The residents of this neighborhood were invited to an Open House on September 22, 2011 to discuss the City's process for street reconstruction. A copy of the presentation from this meeting is included in Appendix B.

A resident questionnaire was sent to the property owners on July 13, 2012. The questionnaires were completed and returned by 52 of the 83 property owners, a return rate of 63%.

A second questionnaire was sent to the property owners on August 17, 2012 resulting from the council's decision to amend the assessment policy to no longer include assessments for sidewalks and street lights and instead create a potential franchise fee to fund these improvements. Questionnaire No. 2

was completed and returned by 52 or 83 property owners, a return rate of 63%. Both the full questionnaire and responses are included in Appendices C & D.

The two key issues that were addressed in both of these questionnaires were the addition of new sidewalks and installation of decorative street lighting. The results from the questionnaires are shown in Table 1.

NORMANDALE IMPROVEMENT PROJECT – Results from July 13, 2012 Questionnaire Sheet					
Questionnaires Sent	Questionnaires Returned	Prefer New Sidewalk		Change Existing Lighting	
		Yes	No	Yes	No
83	52	1	51	8	41
% of Returned Questionnaires	63%	2%*	98%*	15%*	79%*
NORMANDALE IMPROVEMENT PROJECT – Results from August 17, 2012 Questionnaire No. 2 Sheet					
Questionnaires Sent	Questionnaires Returned	Prefer New Sidewalk		Change Existing Lighting	
		Yes	No	Yes	No
83	52	6	45	21	27
% of Returned Questionnaires	63%	12%*	87%*	40%*	52%*

*Percentages are based on responses of returned questionnaires and may not equal 100% if questions were not answered on questionnaire.

Table 1 – Questionnaire Results

A neighborhood informational meeting was held on September 5, 2012 to discuss the improvements planned for this neighborhood. The meeting was attended by 22 residents representing 20 properties. A copy of the presentation and public input from this meeting are included in Appendix E.

Another neighborhood informational meeting was held on November 7, 2012 to discuss West Shore Drive as a primary bicycle route and sanitary sewer service pipe reconstruction assessments. The meeting was attended by 11 residents representing 10 properties. A copy of the presentation and public input from this meeting are included in Appendix L.

EXISTING CONDITIONS: Public Utilities

Trunk Water Main and Sanitary Sewer Pipes

Trunk water main pipe material is cast iron and trunk sanitary sewer pipe material is clay and reinforced concrete pipe. The trunk water main and sanitary sewer pipes were constructed in the early 1960's.

Six-inch (6") diameter trunk water main pipes serve the neighborhood. The valves and fire hydrants along the trunk water main pipe are operable, but the

fire hydrants are original to the neighborhood and lack the STORZ nozzle fittings desired by the Edina Fire Department for quick connection of fire hoses.

The trunk water main pipe is in satisfactory condition since only four (4) repairs have been needed since 1996. Locations of the repairs are shown in Drawing 1 in the Appendix. Trunk water main pipe and hydrants were not installed at the north dead ends of Ryan Avenue and Parnell Avenue and the south Parnell Avenue cul-de-sac. While hydrants located at adjacent intersections meet fire fighting needs in these areas, they are not optimal.

Eight-inch (8") and nine-inch (9") diameter clay trunk sanitary sewer pipes serve the majority of the neighborhood. A twenty one-inch (21") diameter reinforced concrete trunk sanitary sewer pipe from north of Highway 62 enters the neighborhood at the north end of Parnell Avenue and exits east along 64th Street.

Historical records indicate there has been one (1) sewer blockage in the area. Based on the City's closed-circuit television inspections of the clay trunk sanitary sewer pipes, six (6) areas have been identified with problems including cracked pipe and offset joints. In addition to the six identified problems, multiple joint and service pipe locations along the pipe showed extensive root infiltration into the trunk main pipe. Figure 2 below shows one example of extensive root infiltration.

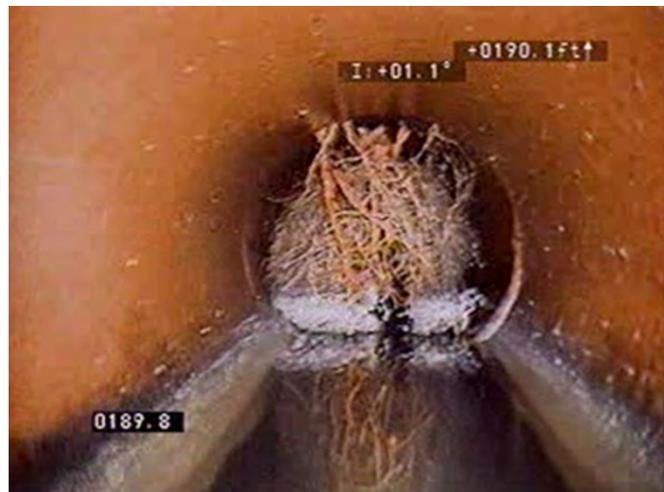


Figure 2 - Sherwood Avenue Root Infiltration

Water and Private Sanitary Sewer Service Pipes

Water service pipes within the neighborhood are copper pipes and vary in diameter. A two-inch (2") diameter water service pipe is located in a City utility easement along the south property lines of both 6325 Ryan Avenue and 6324 Parnell Avenue and connects to the trunk water main pipes on Ryan Avenue and Parnell Avenue.

Since 1992, there have been eleven (11) City-owned water service pipe break and leak repairs. Five (5) of the water service pipe repairs have

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occurred in the last five (5) years. Locations of the City-owned water service pipe repairs are shown in Drawing 1 in the Appendix.

Private sanitary sewer service pipes are likely 6-inch diameter clay pipes. Based on their age, the City should expect that cracks and separated joints have developed along these pipes similar to the trunk main pipe. These faults in the pipe facilitate tree root intrusion and the inflow and infiltration of clear ground water into the City's sanitary sewer network.

The following unique water and private sanitary sewer service pipe connections exist within the neighborhood.

- Water service pipe connections are made to trunk water main pipes located in back and side yard easements that are not located in the City street ROW.
- Private sanitary sewer service pipe connections are made to trunk sanitary sewer pipes located in back and side yard easements that are not located in the City street ROW.
- Some property owners share a portion of their water service pipe with other property owners.
- Some water service pipes are longer than recommended lengths.

Table 2 below summarizes these unique conditions by property.

Address	Back or Side Yard Easement Water Service Pipe Connection	Longer Than Recommended Water Service Pipe	Shared Water Service Connection	Back or Side Yard Easement Private Sanitary Sewer Service Pipe Connection
4850 64th Street			X	X
4860 64th Street			X	X
4728 66th Street (1)		X		
4730 66th Street (1)		X		
6501 Parnell Avenue	X			X
6509 Parnell Avenue	X			X
6517 Parnell Avenue	X			X
6525 Parnell Avenue	X			X
6526 Parnell Avenue		X		
6529 Parnell Avenue		X		
6533 Parnell Avenue		X		X
6324 Ryan Avenue		X		
6325 Ryan Avenue		X		
6500 West Shore Drive	X			X
6508 West Shore Drive	X			X

Address	Back or Side Yard Easement Water Service Pipe Connection	Longer Than Recommended Water Service Pipe	Shared Water Service Connection	Back or Side Yard Easement Private Sanitary Sewer Service Pipe Connection
6516 West Shore Drive	X			X
6524 West Shore Drive	X			X
6532 West Shore Drive	X			X
Total	9	7	2	12

(1) Property is not included in the project, but is served by a common water service pipe located in the Parnell Avenue cul-de-sac.

Table 2 – Unique Water and Private Sanitary Sewer Service Connections

Storm Sewer

The storm sewer system is located within the legal boundary of Nine Mile Creek Watershed. The neighborhood relies on its longitudinal street slopes and concrete curb and gutter to convey storm water to an extensive storm sewer network of existing pipes and inlets located at street intersections, cul-de-sac ends and mid-block of the neighborhood streets. The storm sewer network conveys storm water via pipes to outlet at three (3) separate locations that eventually drain to Lake Cornelia. Based on a hydraulic model of the existing storm sewer trunk pipes in the neighborhood, most are currently large enough to convey storm water following significant storm events. Feedback from residents and City staff indicate undersized storm sewer trunk pipes identified in the model have not caused localized flooding or surface discharges from storm sewer structures following significant storm events.

The existing topography of the site divides runoff into four (4) separate storm sewer areas as shown in Drawing 2 in the Appendix. Runoff from Area 1 is conveyed by a storm sewer system that travels north and connects to a storm sewer interceptor pipe under Highway 62 and eventually outlets into Lake Cornelia.

Runoff from the neighborhood west of Highway 100 enters the Normandale neighborhood via a trunk storm sewer pipe within the 65th Street ROW corridor. This storm water combines with runoff from Area 2 and flows east in the trunk storm sewer pipe along 65th Street, through a City utility easement along the south property lines of 6433 Ryan Avenue and 6432 Parnell Avenue to 65th Street and West Shore Drive, where it combines with storm water from Area 3. Area 3 runoff travels via trunk storm sewer pipe east along 64th Street and south along West Shore Drive, where it combines with the offsite/Area 2 storm water and drains via trunk storm sewer pipe into a pond east of West Shore Drive, which flows into Lake Cornelia.

Runoff from Area 4 is conveyed by a storm sewer trunk pipe that begins in the Parnell Avenue cul-de-sac and travels east through a City utility

easement along the south property line of 6525 Parnell Avenue and 6524 West Shore Drive into a pond east of West Shore Drive, which flows into Lake Cornelia.

Storm water runoff travels overland in the neighborhood within portions of flat longitudinally sloped curb and gutter sections that causes pooling along the edges of the streets or at select intersections. One resident reported storm water runoff pooling problems in the street in front of 6500 Parnell Avenue due to poor longitudinal drainage via the questionnaire. Several other residents mentioned storm water ponding at various catch basin locations due to blockage caused by debris.

Seven (7) property owners told us they operate sump pumps via returned questionnaires. They use these pumps to discharge ground water away from their homes. In most cases, their pumps discharge the ground water onto their yards or an adjacent street.

Private Utilities

Providers of privately owned gas, electric, communications and cable television utilities are present in the neighborhood. These utilities are either overhead or buried underground both within and outside the street ROW.

Results from the questionnaire indicate sixteen (16) residents own and operate either an underground pet containment or lawn irrigation system within the project boulevards.

Streets

The roadways in this neighborhood were originally constructed in the early 1960's. The neighborhood's existing streets are surfaced with bituminous pavement. Patches, overlays, and sections of alligator cracking are present at random locations throughout the project area as shown in Figure 3.



Figure 3 - Looking East on 64th Street at Sherwood Avenue

The streets vary in width between 29 feet and 30 feet with existing concrete curb and gutter and parking on both sides of the streets. The concrete curb

and gutter is in satisfactory condition. While the pavement condition varies throughout the neighborhood, it is generally in poor condition likely due to its age.

As part of this study, 6 soil borings were taken in the project area. The borings identified a 4- to 9-inch thick layer of sand with traces of gravel beneath the street pavement. Even though the pavement is in poor condition, the presence of this aggregate base layer provides a stable foundation beneath the street surface.

The average pavement condition index (PCI) for the City of Edina is 51 and the average PCI for Normandale as calculated in 2009 is 23. The City of Edina recently hired a consultant to evaluate all bituminous roadways within the City. The streets were graded based on a number of conditions such as sagging, alligator cracking, raveling and potholes. Streets are rated on a scale from 0 to 100; with 0 being extremely poor and 100 representing a brand new road surface. The City evaluates the PCI values of streets to determine a proper maintenance program. Streets with a PCI less than 45 are evaluated for total reconstruction, PCI's between 45 and 65 are evaluated for mill and overlays, and PCI's greater than 65 are considered for seal coats.

Street grades vary throughout the area from approximately 0.5 percent to 11 percent.

The pavement surface throughout these streets appears to be near the end of its useful life while the costs to maintain and repair the roadways are steadily increasing. Overlaying or seal coating the pavement is no longer feasible.

There are two (2) driveways featuring brick paver block edging in the project area. Figure 4 depicts a driveway with brick paver block edging found in the project area.



Figure 4 - 6525 Parnell Avenue - Paver Edge Driveway

Sidewalks and Bicycle Routes

Drawing 5 in the Appendix illustrates the segment of Rosland Park’s park pathway that is east of West Shore Drive. West Shore Drive between 66th Street and 64th Street and 64th Street east of West Shore Drive are both identified as secondary bicycle routes.

Residential Lighting

Street lighting in the project area consists of standard cobra head lights mounted on wood poles that are typically located at intersections in order to illuminate the streets. The locations of the existing street lights are shown in Appendix K.

Traffic and Crash Data

City staff measured traffic volumes and speeds at ten locations within or near the neighborhood. Average daily traffic volumes ranged from 117 to 1,259 cars per day with 85th percentile speed ranging from 24.0 to 34.6 mph. The traffic and crash data is shown in Appendix I.

Landscaping

Twenty nine (29) decorative landscaping features are located in the ROW in the project area. Many of these landscape items are located directly behind the curb or around existing fire hydrants. Some of these landscape items will need to be removed in order to complete the necessary reconstruction work. Figure 5 shows a typical landscaping feature found in the project’s ROW.



Figure 5 - 6517 Parnell Avenue - Landscaping Feature

**PROPOSED
IMPROVEMENTS:**

The project involves performing spot repairs to existing concrete curb and gutter and reconstructing the bituminous pavement while leaving the aggregate base layer and subgrade soils undisturbed. If poor subgrade soils are encountered, the project will remove and replace them while salvaging the aggregate base layer for reincorporation into the proposed street. The project also involves replacing deficient drainage structures and connecting sump pump drain pipe to the storm sewer system as needed. The sump

pump drain pipe will allow property owners to discharge ground water from their sump pumps into the trunk storm sewer pipe network. The project will also involve replacing hydrants and gate valves, trunk water main pipe extensions, water service pipe reconstruction, and rehabilitating trunk sanitary sewer pipe.

The scope of the water main, sanitary and storm sewer reconstruction should meet the following parameters.

- Protect boulevard trees.
- Provide continuous sewer and water service to residents.
- Maintain pedestrian and vehicle access in the neighborhood.
- Reconstruct private sanitary sewer service pipes between the trunk sanitary sewer pipe and ROW line.
- Encourage property owners to reconstruct their private sanitary sewer service pipe between the ROW line and their homes prior to construction.
- Reconstruct the City-owned water service pipe between the trunk water main pipe and curb stop box within the City's ROW. Reconstruction should also include replacing all of the curb stops.
- Encourage property owners to reconstruct their private water main service pipes between the curb stop and their homes.

Public Utilities

Trunk Water Main and Sanitary Sewer Pipes

Since the existing trunk water main pipe is in relatively good condition, reconstruction is not proposed as part of the project. New trunk water main pipe and hydrants will be installed at the north dead ends of Ryan Avenue and Parnell Avenue and the south Parnell Avenue cul-de-sac.

New trunk water main pipe will be installed in the City utility easement along the south property lines of both 6325 Ryan Avenue and 6324 Parnell Avenue and will connect to the trunk water main pipes on Ryan Avenue and Parnell Avenue. Drawing No. 3 in the Appendix shows the proposed combination of open cut and horizontal directional drill reconstruction methods to be deployed in these areas.

The project will replace all of the existing fire hydrants with new City standard hydrants and will replace all existing gate valves. New hydrants will also be added in locations where the distance between the existing hydrants exceeds the recommended 500-foot spacing.

All of the proposed improvements to the water main system will optimize water flow for fire fighting and improve water quality.

During improvements to the trunk water main pipe network, residents will have continuous water service. If necessary, temporary water main and service pipes will be strung out on the ground along the boulevards and front yards of the neighborhood's streets. These temporary pipes are connected to sections of the existing water main. Continuous water service is provided to each home via its outside garden hose spigot. The contractor will need access into each affected home to make a proper temporary water main connection.

The project will reconstruct the trunk sanitary sewer pipes using a trenchless reconstruction method called cured in place pipe lining. Spot repairs will be made to the trunk sanitary sewer pipes that have sagged, cracked or where offset joints occur. The City's closed circuit television inspections provide the exact locations of these repairs. Wherever necessary, the repairs will be completed using open cut repair methods. In sensitive locations, repairs will be completed using trenchless repair methods.

Block sanitary manholes will be lined with cement mortar and all sanitary manholes will have castings and adjustment rings replaced.

Sanitary Sewer Service and Water Service Pipes

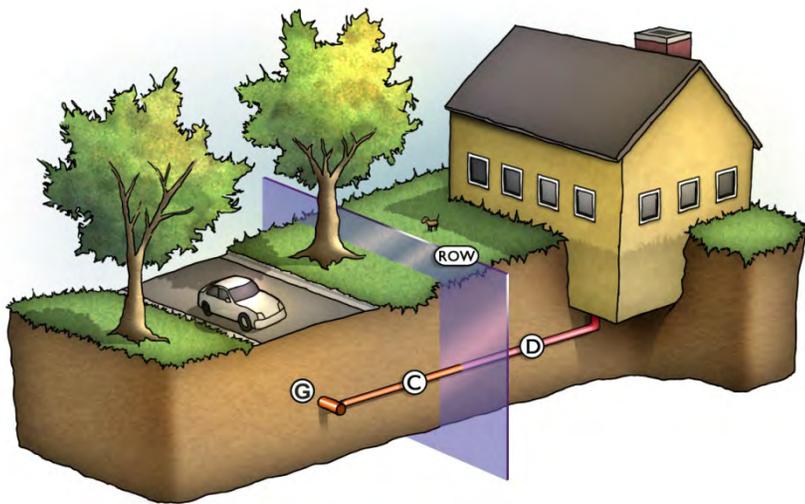
Along with upgrading the street, the City also plans to upgrade the publicly owned water main and sanitary sewer pipes that are underground. Upgrading these pipes will ensure that the water and sanitary sewer services will be safe and reliable for the foreseeable future.

Besides helping to keep safe, reliable sanitary sewer services and water services, upgrading all of the private and public underground utility lines helps ensure that the new street stays looking new and in good condition by decreasing the chances that the street will need to be cut into in order to repair old or leaking underground utilities.

Property owners will be allowed to have the cost of any sanitary sewer service or water service upgrades financed through the City's special assessment financing system. Staff will provide the interested property owners with information on how to proceed with this option.

Sanitary Sewer Services:

Because the City is upgrading the publicly owned pipes and the roadway above these pipes, we will require that property owners upgrade the privately owned sanitary sewer service line shown as segment "C" below; both segments "C & D" are privately owned. The only exception will be for properties whose pipes have been upgraded in the past 15 years.



<u>SEGMENT</u>	<u>OWNERSHIP</u>
C	Property Owner
D	Property Owner
G	City

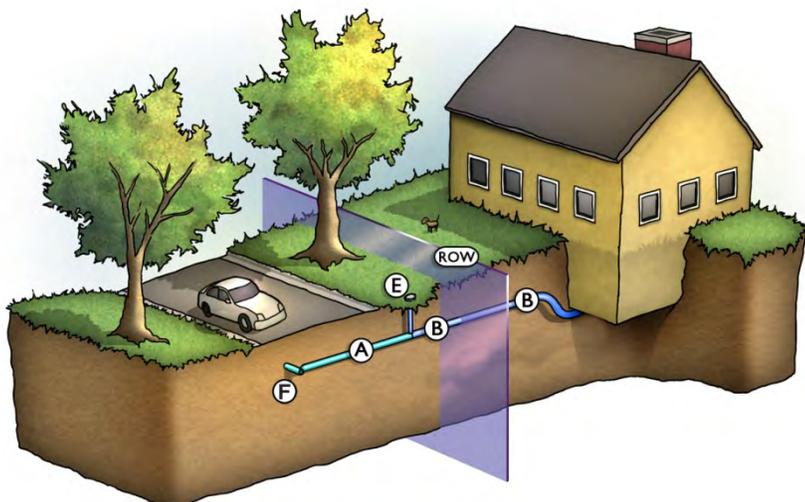
Figure 6 – Typical Section Showing Sanitary Sewer Service

A property owner will have two options to upgrade their sanitary sewer service line:

1. They may hire their own private contractor to do the upgrading work. This option allows the property owner to choose their own contractor, have some flexibility for the project schedule and to finance the cost of the work at their discretion.
2. Join a contract organized and managed by the City. The City's contractor will replace the segment of private sanitary sewer service line shown as segment "C" above for a flat cost of \$2,500.

Water Service Pipes:

The City will also be upgrading the publicly owned portion of the water service shown as segments "A & E" below.



<u>SEGMENT</u>	<u>OWNERSHIP</u>
B	Property Owner
A	City
E	City
F	City

Figure 7 – Typical Section Showing Water Service

Unique Water and Private Sanitary Sewer Service Connections

The project will:

- not reconstruct segments “A & E” if they are located in a back or side yard easement that is not located in the City’s street ROW,
- not reconstruct segment “C” if it is located in a back or side yard easement that is not located in the City’s street ROW.
- shorten individual water service connections that are too long to acceptable lengths,
- separate shared water service connections

On a per property basis, Table 3 summarizes how the project will address unique service pipe connections.

Address	No Water Service Pipe or Curb Stop Box Reconstruction	Shorten Water Service Connection	Separate Shared Water Service Connections	No Private Sanitary Sewer Service Reconstruction
4850 64th Street			X	X
4860 64th Street			X	X
4728 66th Street (1)		X		
4730 66th Street (1)		X		
6501 Parnell Avenue	X			X
6509 Parnell Avenue	X			X
6517 Parnell Avenue	X			X
6525 Parnell Avenue	X			X
6526 Parnell Avenue		X		
6529 Parnell Avenue		X		
6533 Parnell Avenue		X		X
6324 Ryan Avenue		X		
6325 Ryan Avenue		X		
6500 West Shore Drive	X			X
6508 West Shore Drive	X			X
6516 West Shore Drive	X			X
6524 West Shore Drive	X			X
6532 West Shore Drive	X			X
Total	9	7	2	12

(1) Property is not included in the project, but is served by a common water service pipe located in the Parnell Avenue cul-de-sac.

Table 3 – Unique Water and Private Sanitary Sewer Service Connections

Storm Sewer

Spot repairs will be made to the concrete curb and gutter that are no longer functioning properly. No improvements are proposed to the existing trunk storm sewer pipe since the majority of the trunk storm sewer pipes in the neighborhood are adequately sized and localized flooding of the system has not been observed following large storm events. Existing storm structures in

poor condition will be replaced. Remaining drainage structures will have castings and adjustment rings replaced. The City will consider adding a future storm water runoff treatment manhole on City property east of the West Shore Drive and 65th Street intersection as part of a separate project.

Installation of sump drains will be installed where feasible to allow the property owners to connect their sump pump discharges directly into the storm sewer system.

Private Utilities

The local gas utility company, CenterPoint Energy, has indicated that they may upgrade or replace gas mains within the project limits. CenterPoint Energy may also coordinate moving gas meters to the exterior of the homes. This work is not part of the City's project but will be coordinated to occur prior to the start of construction.

The City will coordinate other private utility relocations or upgrades prior to the start of construction.

Any damage to privately-owned pet containment and irrigation systems caused by street and City utility reconstruction activities will be repaired by the City.

Streets

The project will reconstruct the streets with a new bituminous surface to their existing widths between the existing curb and gutter. The project will recycle and mix together all of the existing bituminous street pavement and approximately 1-inch of the existing aggregate base course below it. The recycled aggregate base product will be reincorporated into the project to replace poor subgrade soils or as utility trench backfill.

Spot repairs will be made to the concrete curb and gutter in utility service reconstruction areas.

As part of the project, staff will investigate inclusion of a no parking condition along the east side of West Shore Drive between 66th Street and 65th Street based on responses from resident questionnaires and comments received at the informational meetings.

The proposed improvements acknowledge many of the comments and concerns raised by residents throughout the information gathering process while still maintaining the desired minimum standards of the engineering and public works staff.

Sidewalks and Bicycle Routes

At their October 25 meeting, the ETC recommended staff incorporate a separate bicycle lane on West Shore Drive between 64th and 66th Streets. Copies of the ETC meeting minutes are included in Appendix K.

At a neighborhood informational meeting on November 7 staff presented an option to add this bicycle lane to West Shore Drive. This option is shown in Drawing 6 of the Appendix.

Property owners at the November 7 meeting asked staff to consider adding a separate bicycle lane adjacent to Rosland Park's park pathway as shown in Drawing 7 of the Appendix. Property Owner input from this meeting is given in Appendix L.

Drawings 6 & 7 were sent to the City Park Board for their review and consideration at their November 19 meeting. Staff will provide an update of comments received from the City Park Board in their December 11, 2012 Public Hearing report.

Residential Lighting

The questionnaire asked if residents wanted to upgrade the street lights in the project area. The results from Table 1 show that property owners do not want to reconstruct the street lights. Since the lighting of the neighborhood is sufficient to delineate the intersections, no revisions to the current street lighting system are proposed.

Staff is including installation of City standard pendant style lighting units along the multi-use path shown in both Drawing 6 and 7 of the Appendix. Both Drawing 6 and 7 depict the type, style and location of pendant luminaire lighting proposed. Staff shared the locations of these lights with property owners at the November 7 neighborhood meeting.

RIGHT-OF-WAY & EASEMENTS:

Existing right-of-way on all streets in the project area is 60 feet wide. No additional right-of-way or easements are anticipated to complete the proposed improvements.

PROJECT COSTS:

The estimated total project cost is \$3,553,000. Funding for the entire project will be from a combination of special assessment, utility and Active Living Infrastructure funds. The estimated roadway cost is \$1,095,000, and the estimated sanitary sewer service cost is \$175,000. Both of these costs will be 100 percent funded by special assessments at a rate of \$14,800 per REU. Properties not receiving new sanitary sewer services will be assessed at a rate of \$12,300 per REU. Utility improvements and repairs amount to \$1,888,000 and will be funded through respective utility funds. Multi-use path and lighting upgrades adjacent to West Shore Drive amount to \$395,000 and will be funded through the Active Living Infrastructure fund.

The total estimated project cost is \$3,553,000 (2013 Dollars). The estimated total project cost is summarized in Table 4.

ITEM	ESTIMATED COST ²	
	CITY	RESIDENTS
Sanitary Sewer Trunk Pipe	\$869,000	
Sanitary Sewer Service Pipe Between the Trunk Pipe and the Right-Of-Way Line ¹		\$175,000
Water Main Trunk Pipe and Service Pipe Up To and Including Curb Stop Box	\$728,000	
Storm Sewer	\$291,000	
Street Reconstruction ¹		\$1,095,000
Multi-Use Path	\$324,000	
Lighting for Multi-Use Path	\$71,000	
Sub-total	\$2,283,000	\$1,270,000
Total	\$3,553,000	

¹ Cost to be assessed to residents

² Costs are given in 2013 dollars

Table 4 - Summary of Total Estimated Project Cost

ASSESSMENTS:

The assessments are based on the City's Special assessment policy, dated August 21, 2012.

An estimated special assessment rate of approximately \$14,800 (2013 Dollars) per residential equivalent unit (REU) is proposed to be levied against property owners in the project area who receive benefit from the proposed street improvements and have a sanitary sewer service connection in the City's ROW. There are 70 REU's in the project area that qualify for this assessment rate. The remaining 19 REU's in the project area with no sanitary sewer service connection in the City's ROW will be exempt from the sanitary sewer service assessment. The estimated total assessment rate is summarized in Table 5.

ITEM	ESTIMATED ASSESSMENT ¹	NUMBER OF REU'S
Street Reconstruction	\$12,300	89
Sanitary Sewer Service Pipe Between the Trunk Pipe and the Right-Of-Way Line	\$2,500	70
Total	\$14,800	

¹ Assessment rate is given in 2013 dollars

Table 5 - Summary of Total Estimated Assessment Rates per REU

Drawing 4 in the Appendix of this study summarizes the application of the estimated assessment rates. The methodology for calculating the REUs for City owned property along the east side of West Shore Drive is described as follows:

Park Property:

Rosland Park:

Seven (7) REUs = Layout of park property with similar size buildable lots in the neighborhood.

According to City policy, properties that abut the project but their addresses are not included in the project are either not assessed due to past assessments or assessed at either 1/3 or 2/3 of the estimated rate. No 1/3 or 2/3 assessments are proposed for this project.

The City will consider deducting that portion of the assessment rate for the sanitary sewer service pipe repair if the resident:

- Repairs, using their own contractor, their own private sanitary sewer service pipe between at least the trunk sanitary sewer pipe and ROW line before reconstruction is underway, or
- Reconstructed their sanitary sewer service pipe between at least the trunk sanitary sewer pipe and ROW line with PVC pipe or by use of trenchless methods in the last 15 years.

A copy of the preliminary assessment roll is included in Appendix F.

PROJECT SCHEDULE:

The following schedule is feasible from an Engineering standpoint:

Project Open House 2011	September 22, 2011
Neighborhood Informational Meeting	September 5, 2012
Edina Transportation Commission Meeting	October 25, 2012
Neighborhood Informational Meeting About Private Sanitary Sewer Service Reconstruction and Multi-use Path Along West Shore Drive	November 7, 2012
Receive Feasibility Report and Public Hearing	December 11, 2012
Bid Opening	March/April 2013
Award Contract	Spring 2013
Begin Construction	Spring 2013
Complete Construction	Fall 2013
Final Assessment Hearing	Fall 2014

FEASIBILITY:

Staff believes the construction of this project is necessary, cost effective and feasible to improve the public infrastructure in the Normandale Neighborhood.

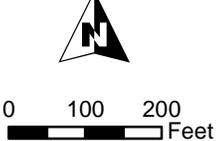
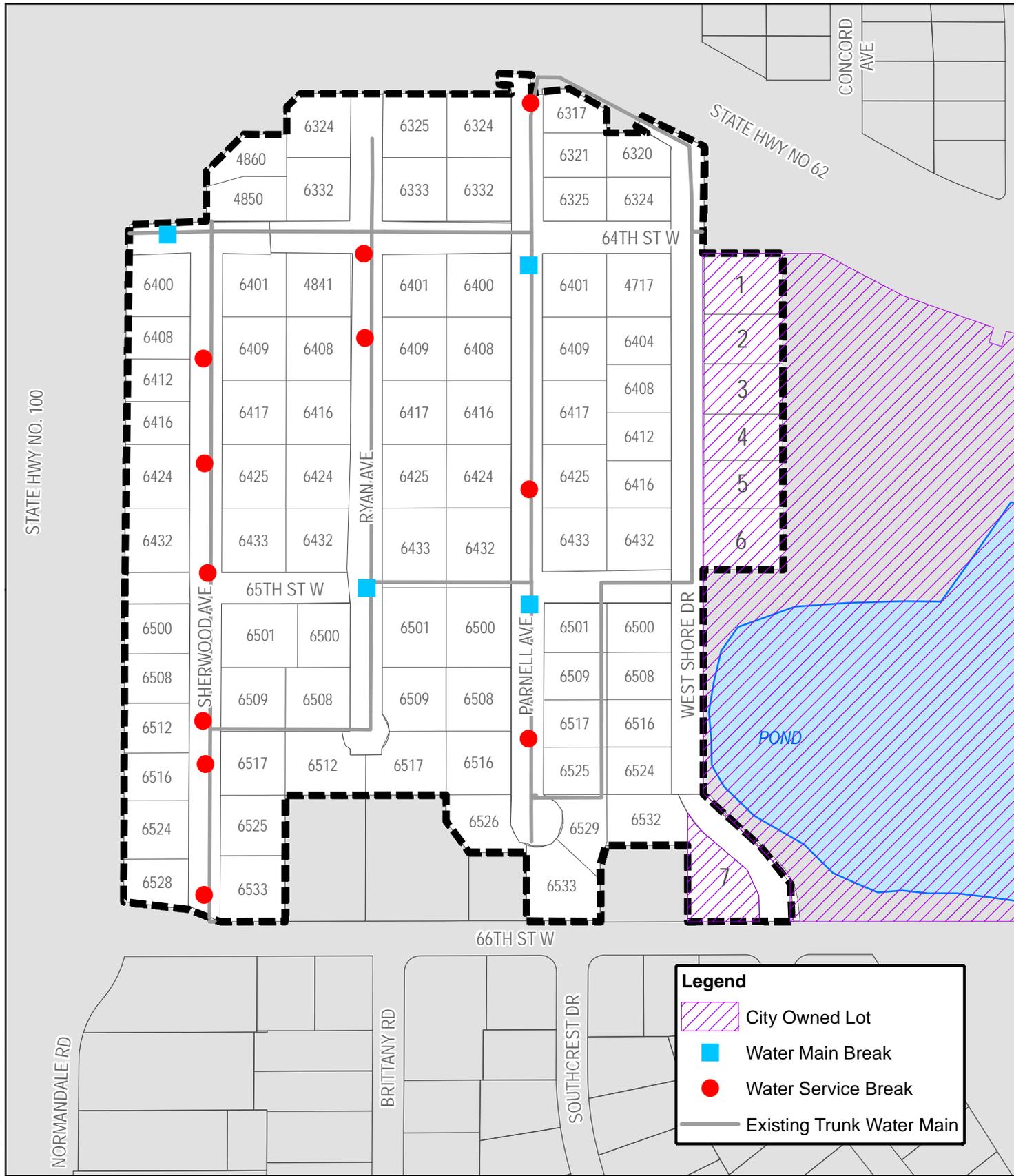
**Feasibility Study
Normandale Neighborhood Improvement No. BA-394
November 14, 2012**

APPENDIX:

- Drawing 1 – Normandale Neighborhood Water Main and Service Breaks
- Drawing 2 – Normandale Neighborhood Drainage Areas
- Drawing 3 – Normandale Neighborhood Water Main Reconstruction
- Drawing 4 – Normandale Neighborhood Assessment Summary
- Drawing 5 – Normandale Neighborhood Nearby Bicycle and Park Routes
- Drawing 6 – Normandale Neighborhood West Shore Drive Multi-use Path
Option A
- Drawing 7 – Normandale Neighborhood West Shore Drive Multi-use Path
Option B

- A. Draft Living Streets Policy Framework
- B. 2011 Open House Meeting Letter and Presentation
- C. Property Owners Questionnaire
- D. Property Owners Questionnaire Results No. 1 and No. 2
- E. September 5, 2012 Normandale Neighborhood Informational Meeting
- F. Preliminary Assessment Roll
- G. City Comprehensive Plan Update – Sidewalk and Bicycle Facilities (Fig. 7.10 and 7.11)
- H. Plumbers Contact Information Summary
- I. 2013 Normandale Traffic and Crash Data
- J. Recommendation to Set Public Hearing, Notice of Public Hearing to residents and Notice of Public Hearing Advertisement
- K. Edina Transportation Commission Review Data and Meeting Minutes
- L. November 7, 2012 Normandale Neighborhood Informational Meeting

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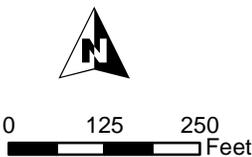
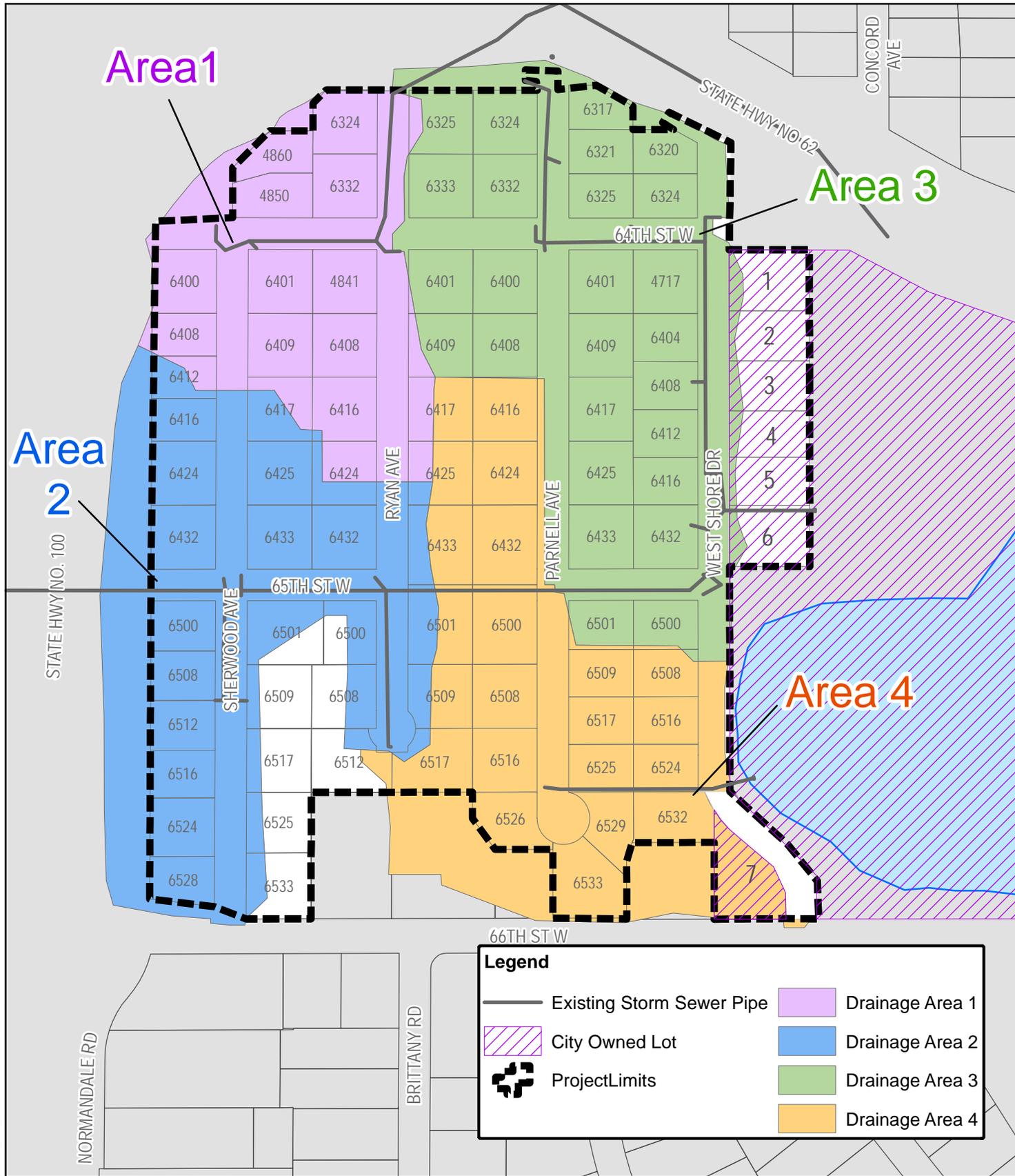
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 Source: LMIC, MnDOT

Normandale
 Neighborhood Roadway Improvements
 Edina, Minnesota

Drawing
1

This map is neither a legally recorded map nor a survey map and is not intended to be used as one. This map is a compilation of records, information, and data gathered from various sources listed on this map and is to be used for reference purposes only. SEH does not warrant that the Geographic Information System (GIS) Data used to prepare this map are error free, and SEH does not represent that the GIS Data can be used for navigational, tracking, or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features. The user of this map acknowledges that SEH shall not be liable for any damages which arise out of the user's access or use of data provided.

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Project: EDINA
 Print Date: 9/11/2012
 Map by: MSS
 Projection: Hennepin County Coords
 Source: LMIC, MnDOT

Normandale

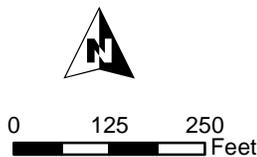
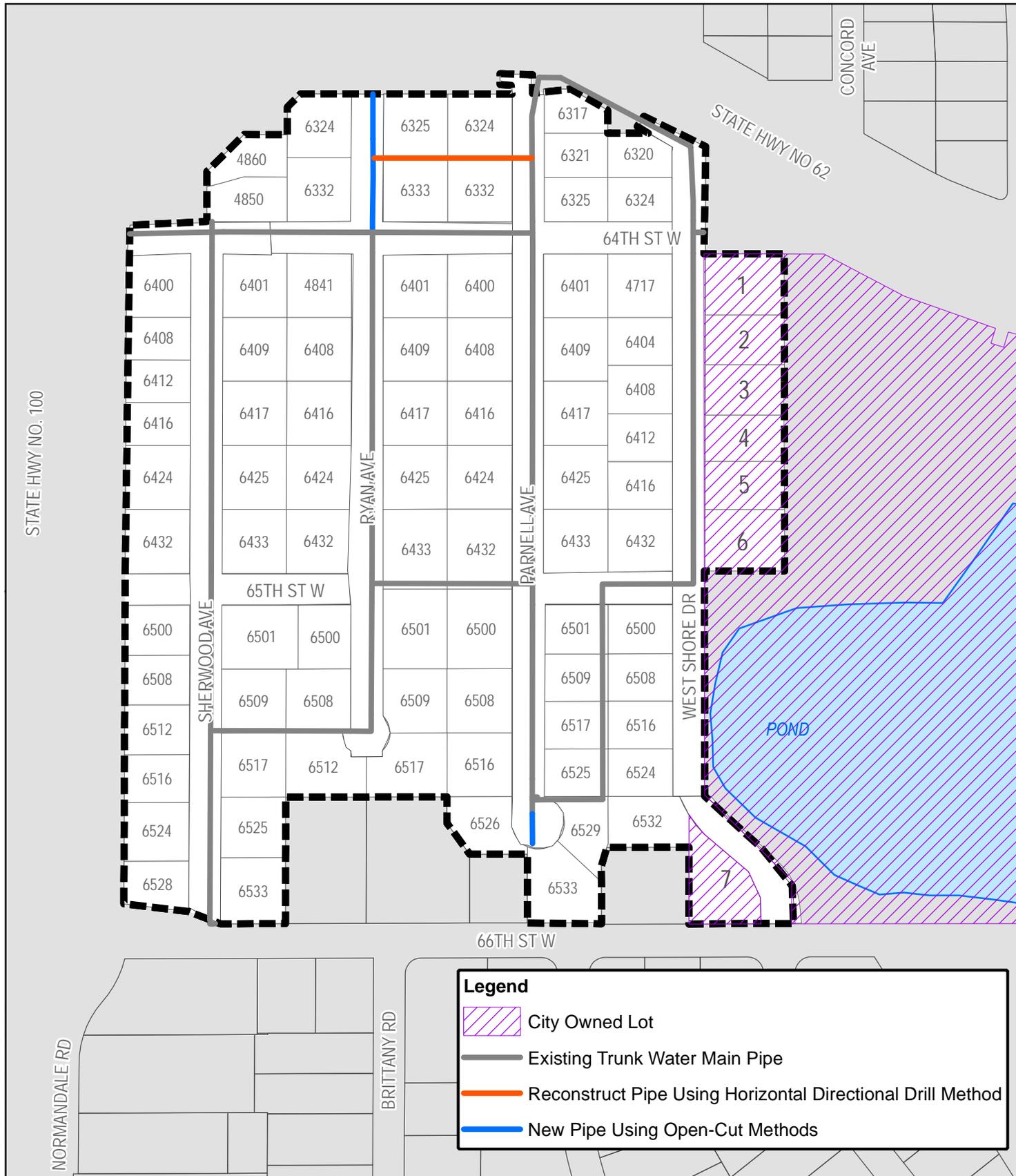
Neighborhood Roadway Improvements

Edina, Minnesota

Drawing 2

This map is neither a legally recorded map nor a survey map and is not intended to be used as one. This map is a compilation of records, information, and data gathered from various sources listed on this map and is to be used for reference purposes only. SEH does not warrant that the Geographic Information System (GIS) Data used to prepare this map are error free, and SEH does not represent that the GIS Data can be used for navigational, tracking, or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features. The user of this map acknowledges that SEH shall not be liable for any damages which arise out of the user's access or use of data provided.

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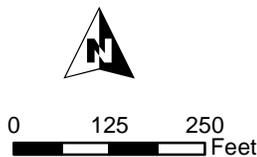
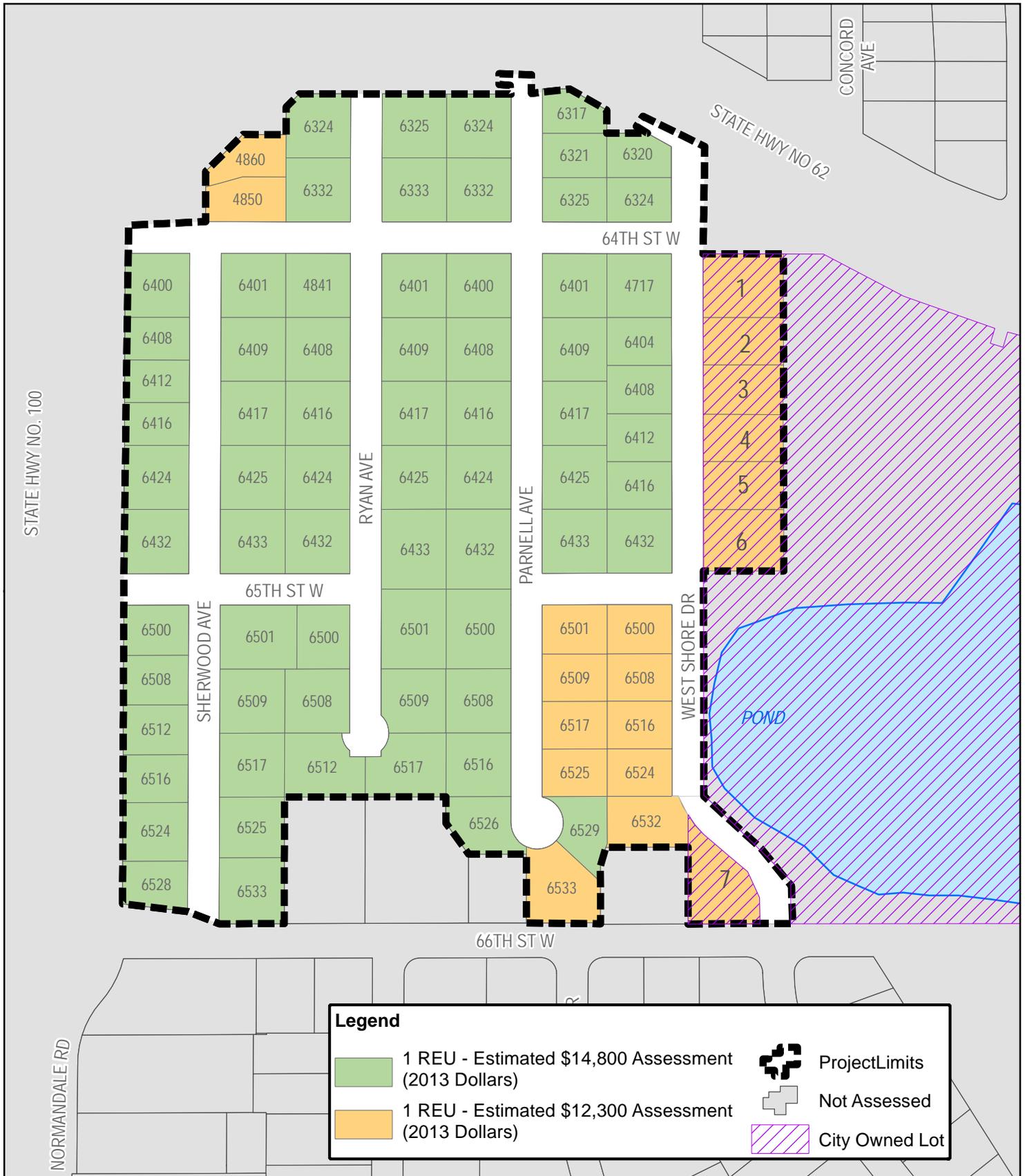
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 Print Date: 9/11/2012
 Map by: MSS
 Projection: Hennepin County Coords
 Source: LMIC, MnDOT

Normandale
 Neighborhood Roadway Improvements
 Edina, Minnesota

Drawing
3

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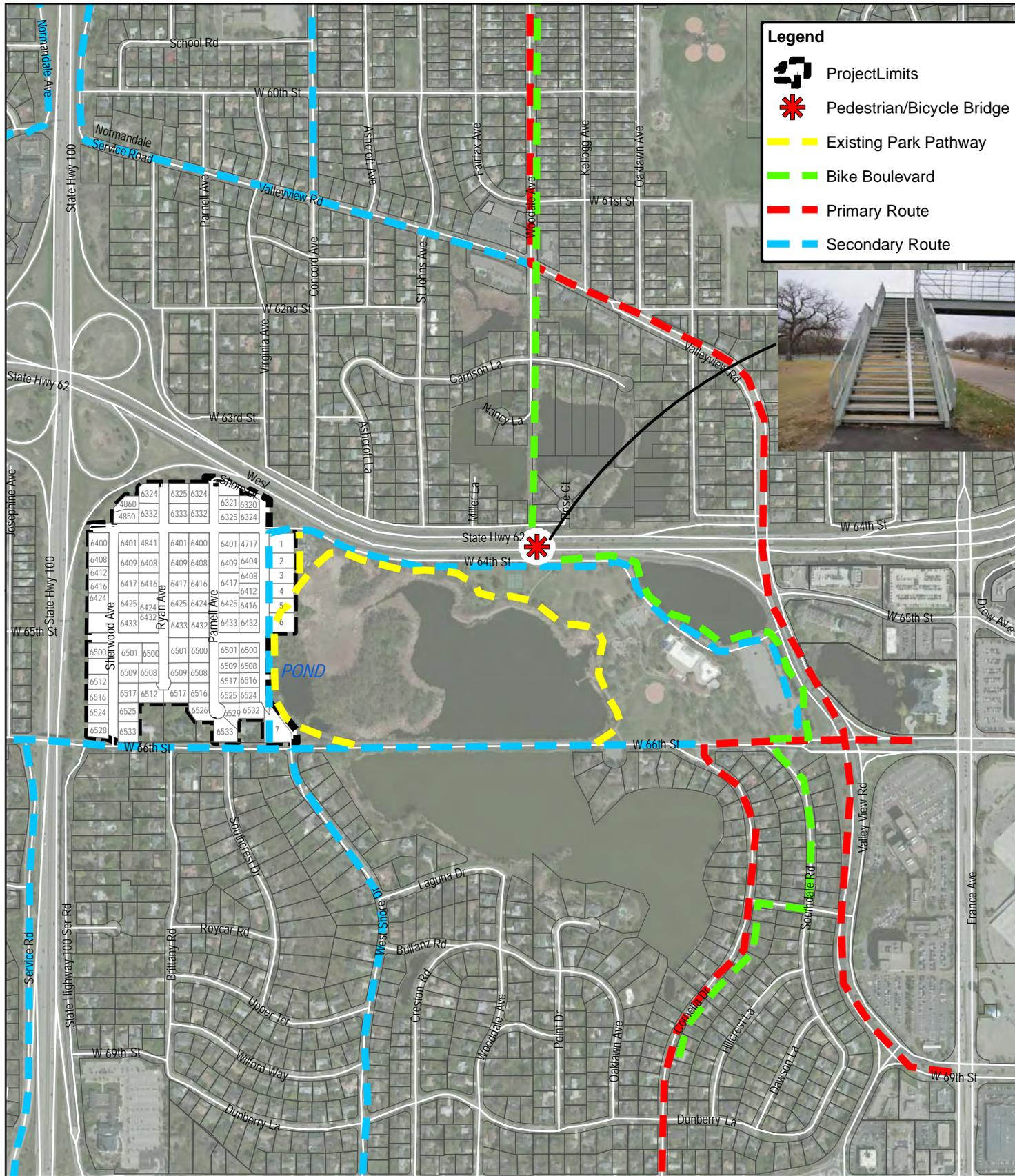


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 Source: LMIC, MnDOT

Normandale
 Neighborhood Roadway Improvements
 Edina, Minnesota

Drawing
4

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0 400 800 Feet

Project: EDINA

Print Date: 11/12/2012

Map by: MSS
 Projection: Hennepin County Coords
 Source: LMIC, MnDOT

Normandale

Neighborhood Roadway Improvements

Edina, Minnesota

Drawing

5

This map is neither a legally recorded map nor a survey map and is not intended to be used as one. This map is a compilation of records, information, and data gathered from various sources listed on this map and is to be used for reference purposes only. SEH does not warrant that the Geographic Information System (GIS) Data used to prepare this map are error free, and SEH does not represent that the GIS Data can be used for navigational, tracking, or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features. The user of this map acknowledges that SEH shall not be liable for any damages which arise out of the user's access or use of data provided.

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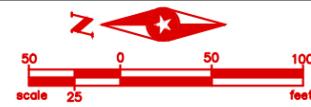
MODULAR BLOCK
RETAINING WALL WITH
FENCE



LIGHT POLE WITH FIXTURE

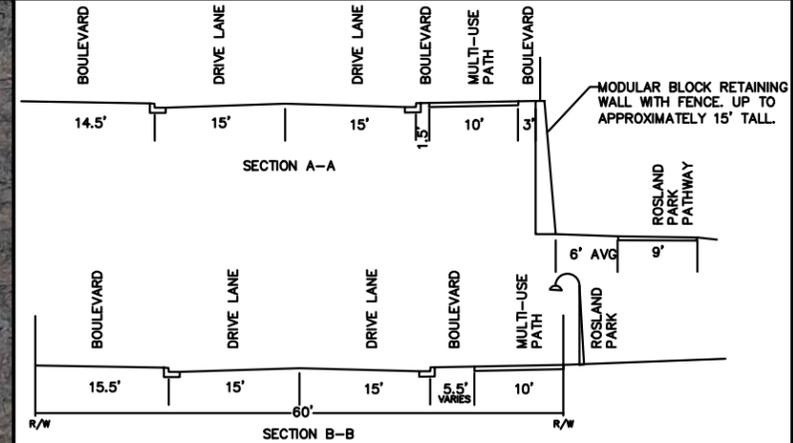
SAMPLE PHOTOS

LEGEND



- PROPOSED CONDITIONS**
 MULTI-USE PATH (BICYCLE AND WALKING)
 CONNECTION TO ROSLAND PARK PATHWAY (WALKING ONLY)
 MODULAR BLOCK RETAINING WALL WITH FENCE
 LIGHT POLE WITH FIXTURE
 CLEAR BRUSH
 REMOVE TREE
 PROTECT TREE
 TYPICAL SECTIONS

- EXISTING CONDITIONS**
 PROPERTY LINE
 RIGHT OF WAY LINE
 TREE
 LAKE CORNELIA NATURE TRAIL (WALKING ONLY)
 ROSLAND PARK PATHWAY (WALKING ONLY)
 BRUSH
 DRIVEWAY SERVICING LIFT STATION
 PARK BENCH
 PHOTO LOCATION AND DIRECTION



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Appendix A

Draft Living Streets Policy Framework

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MEMO

To: Wayne Houle, PE – Edina City Engineer
Michael Thompson, PE – Edina Living Streets Committee Chair

From: Dan Edgerton, AICP – HR Green
Jack Broz, PE – HR Green
Fred Rozumalski, RLA – Barr Engineering Co.
Matt Metzger, PE – Barr Engineering Co.

Subject: Edina Living Streets – Review of Existing Plans and Policies, Peer Review Lessons, and Draft Living Streets Policy Framework

Date: August 13 , 2012

1.0 Introduction

The City of Edina seeks to encourage the development of a more accessible and sustainable transportation system over time. To accomplish this goal, the city envisions redesigning its street network using a "complete streets" approach, guided by green streets and active living design principles. The result will be a "living streets" system which facilitates all modes of transportation, incorporates ecological services, and encourages walking and bicycling through safe and aesthetically attractive transportation corridors. The purpose of this memorandum is twofold:

1. To document the review of existing plans and policies relevant to living streets, and
2. To provide a framework for a Living Streets Policy and Plan.

2.0 Review of Existing Plans and Policies

The City of Edina has implemented a number of planning and policy documents, ordinances, and other initiatives which have a bearing on transportation system investment decisions. Attachment 1 includes a draft living streets policy background document, prepared by the city. This document summarizes the relevant plans and policies which lead to the undertaking of the current study, many of which provide the policy direction and guidance for the city's living streets efforts. In contrast, some existing policies and regulations could conflict with or create a barrier to the implementation of a living streets program.

The following is a brief summary of the documents, regulations, and programs most relevant to living streets. As the city moves toward implementation of its Living Streets Policy and Plan, each of these documents should be reviewed and policies and regulations with a direct connection to transportation design elements (i.e., right-of-way, sidewalks, trees, etc) should be amended in order to expressly support the city's living street goals.

Plans and Studies

- Edina Comprehensive Bicycle Transportation Plan (2007)
The Edina City Council adopted a Comprehensive Bicycle Plan in 2007. The guiding vision for this document is to support the gradual transformation of the City of Edina into a "progressive

bicycle-friendly community where citizens can easily integrate cycling into their daily lives.” This document compliments living street by planning for a fully integrated, safe, and inviting bicycle network for a full range of users.

- Edina Comprehensive Plan (2008)
The Edina City Council adopted an updated Comprehensive Plan in 2008. The plan promotes a vision for a city that is pedestrian, bicycle and transit friendly and inclusive of all users. A number of chapters are relevant to living streets, including Chapter 2: Visions, Goals, and Objectives; Chapter 4: Land Use and Community Design; Chapter 7: Transportation Plan; and Chapter 10: Energy and the Environment.
- Edina Comprehensive Water Resources Management Plan Update (2009)
The City of Edina developed a Comprehensive Water Resources Plan in 2009. This plan establishes the City’s goals and policies for municipal drainage systems, design criteria, and rules for development and redevelopment. The plan supports the living streets goals to reduce street water runoff into watersheds.
- Edina Capital Improvement Plan (2012)
The purpose of the Capital Improvement Plan (CIP) is to guide capital investment decisions based on a community’s needs, taking into account the best use of limited resources while providing efficient and effective municipal services. As the Edina Living Streets Policy is implemented, the CIP should be revised to reflect prioritization of capital investments in accordance with the city’s living streets goals.

Programs and Initiatives

- Edina Fit City Program (Resolution 2006-70)
In July 2006 the Edina City Council adopted a resolution expressing a commitment "to providing an activity-friendly community to its residents" and authorizing "the application for The Governor's 'Fit City' Program," which recognizes cities committed to creating and maintaining an environment supportive of active living. The goals of the Fit City program and the City's resolution supporting an activity-friendly community compliment living streets.
- Edina Green Steps Program (Resolution 2011-37)
The City of Edina adopted a resolution to participate in the Minnesota Pollution Control Agencies (MPCA) Green Step Cities program in 2011. The Green Step Cities program aims to provide Minnesota cities a clear pathway to greater sustainability based on implementing best practices specific to Minnesota cities. Several of these best practices would be supported by a Living Streets Policy, including, #11: Complete Streets (requires "complete streets policy" or "living streets," which is inclusive of "complete streets" concepts); #12: Mobility Options (walking/biking, Safe Routes to School, etc.); #16: Urban Forest; #17: Efficient Stormwater Management; #18: Green Infrastructure (porous pavement, swales, etc.); and #19: Surface Water Quality (rain gardens, etc.).
- Bloomington Public Health
Bloomington Public Health provides public health services to Bloomington, Edina, and Richfield. This includes Statewide Health Improvement Program (SHIP) and the Community Transformation Grant (CTG). SHIP is working with communities, schools, worksites, and healthcare to reduce the risk for chronic diseases such as diabetes, heart disease, stroke, and cancer. SHIP seeks to make healthy living easier where we live, work, learn, and play. CTG has similar goals and also supports the Living Streets Policy in Edina. A Living Streets Policy would complement these programs as it would promote active living and make it easier for community members to be physically active.

- Edina *do.town* Program (2011)

In September 2011, the City of Edina, in partnership with Bloomington and Richfield, launched an 18-month initiative to improve community health by making changes that support healthy eating and active living choices. The goal of *do.town* is to bring together community members and leaders to think and talk about ideas that would make the healthy choice the easy choice in Bloomington, Edina, and Richfield.

Policies and Regulations

- Edina Assessment Policy (2010)

The purpose of the Edina Assessment Policy is to provide regulations for assessments to properties for non-state aid street projects. This policy is relevant to Living Streets as it governs how traditional street improvements are funded. As the Edina Living Streets Policy is implemented, the city should seek new and innovative ways to provide additional funding for design elements related to living streets and update its assessment policy to clearly define how each street design element will be funded.

- Municipal State Aid Special Assessment Policy (2010)

The purpose of the Municipal State Aid (MSA) Assessment Policy is to establish assessment rules for MSA improvement projects. As the Edina Living Streets Policy moves forward, the city should collaborate with its partners (i.e., Hennepin County, MnDOT, etc.), to identify new ways to support the implementation of living streets in Edina.

- Edina City Code

The Edina City Code is a comprehensive document which codifies the general ordinances of the City of Edina. Several elements of the City Code will have a direct bearing on the implementation of living streets within the city, whether through supporting policies, or in some cases policies that conflict with living streets principles. The following is a listing of some of the relevant chapters (not all inclusive). Upon implementation of a Living Streets Policy in Edina, these elements of the City Code should be reviewed and updated as necessary to expressly support the city's living streets goals.

- *Chapter 4: Buildings, Construction and Signs* – Revise the following sections to support living streets principles such as creating livable neighborhoods.
 - Section 405: Landscaping, Screening and Erosion Control
 - Section 421: Right-of-way management
- *Chapter 7: Health* – Consider linking this ordinance to the Living Streets Policy to support active living goals.
- *Chapter 8: Land Use Plats and Zoning (all sections)* – Consider adopting form based zoning policies which emphasize size and position over use. This tool could be used to encourage living streets tenants such as mixed use development which promotes active living and helps to make the street a more attractive environment for non-motorized users.
- *Chapter 10: Misdemeanors and Nuisances* – Revise the following sections to support living streets principles such as reducing the total amount of paved area through modified parking and vegetation ordinances.
 - Section 1046: Parking and Storage of Vehicles and Equipment
 - Section 1050: Maintenance of Vegetation
- *Chapter 11: Public Utilities (all sections)* – Revise this chapter to support the stormwater efficiency goals of living streets.
- *Chapter 12: Streets and Parks (all sections)* – This chapter should be thoroughly reviewed and revised to support living streets goals such as allowing flexibility in street

design to implement a full range of living streets design elements and to place greater emphasis on serving the needs of non-motorized users.

- *Chapter 14: Traffic Control and Vehicles* – The following sections should be thoroughly reviewed and revised to both support living streets goals such as allowing flexibility in street design to implement a full range of living streets design elements, and to remove barriers such as excessive auto “clear zone” requirements as appropriate.
 - Section 140: Pedestrian, Bicycle, and Vehicular Traffic, Parking, Standing and Loading
 - Section 1405: Clear View Zone

In addition, as living streets implementation moves forward, the city should adopt additional policies and ordinances in support of living streets, such as traffic calming and street design guidelines. These policies should be defined as part of the future living streets planning process.

3.0 Lessons Learned

The following is a summary of key lessons learned during previous Living Streets efforts in other municipalities.

Lessons Learned in Los Angeles County, CA

(May 3, 2012 Discussion with Ryan Snyder of Ryan Snyder Associates)

Lessons learned related to policy:

- Build a policy from the ten policy elements delineated in Table 2.1 of the Los Angeles County Model Design Manual for Living Streets.
- Exceptions should be rare. The process for design and review must be clear.
- City procedures and standards need to be changed as appropriate to accommodate the new policy. For example it may be stipulated the a bike advisory committee reviews all street projects, or a pre-design check list is created for components of Living Streets (e.g. pedestrian circulation, stormwater management, street trees).
- It is important to create implementation plans. It might include plans such as Safe Routes to School (SRTS), a sidewalk plan, or an urban forest plan.

Lessons learned related to implementation:

- For each street project Ryan recommends well facilitated workshops with property owners to first educate them on Living Streets, and then to have them participate in the selection of street elements appropriate for their street.
- During the education process show lots of photographs and drawings of Living Streets as well as dysfunctional streets. Make it very visual. Explain all the benefits. Talk about the down side of keeping the street as it is.
- It is important to have all voices (supporters as well as opponents) at the table during the facilitated workshops, and to encourage the quiet people to speak their mind. It helps to break into small groups and have a balance of opinions at each table.
- In preparing for the workshops, understand any complaints about the project street beforehand in order to respond with a Living Streets solution. Create solutions from the complaints.

- Label this policy as a traffic calming and street beautification initiative rather than undefined term such as Bike Boulevard or Living Streets. This allows focus on the benefits, and prevents a target for opponents.
- The most significant opposition to Living Streets stems from confusion about traffic congestion. When proposing a Living Street project, designers and staff should be well prepared with statistics from other projects and other cities that prove that congestion does not become an issue. Ryan suggests steering the conversation away from congestion and to the life style benefits of Living Streets.
- Ryan has found that it is essential for city leadership to stand behind the plan. Staff must be fully supported in the process of implementing Living streets, and not be put in the middle of disputes between neighborhood factions.
- It will be important to provide Living Streets training for city staff.

Lessons Learned in Creating the North St. Paul, MN Living Streets Policy and Plan

(Barr Engineering Staff)

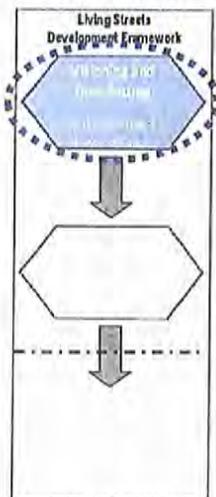
- Gather feedback about Living Streets concepts from residents prior to targeting specific neighborhood projects. Then, implement Living Street projects in areas where demographics or questionnaire feedback shows support. If there are residents who have a history of opposing proposed actions by the City, they are likely to oppose a new effort as well.
- During the N. St. Paul community engagement process, public opinions showed correlations with demographics. Much support was seen from younger families, new to the area, some with children. Much dissent was received from older, senior residents who have resided in the neighborhood for a long time.
- Every street and neighborhood is unique. The street reconstruction elements favored by residents are likely to vary. The complexity of design on different street types (state-aid vs. locally funded) will vary. Implementation should be flexible to accommodate the needs of each unique street segment.
- A useful strategy may be to not name the effort "Living Streets", but to simply make changes to street reconstructions policies, design process and focus on safe routes to school. In North St. Paul the term became a target for residents opposed to the project. Reconstructing streets using Living Street design elements under the usual capital-improvement process may prove to be a good strategy.
- In North St. Paul and Maplewood, it was vital to have a design process framework for the Ramsey-Washington Metro Watershed District to cooperate w/ the City Engineer(s). This cooperative effort facilitated design development and feedback exchange at key phases of concept and final design. The District and City worked together to develop design templates and final designs that addressed their various goals and the goals of other stakeholders. Developing a design process in Edina that engages various stakeholders early in the design process and throughout design and construction is recommended.

4.0 Living Streets Development Framework

In order to implement an effective living streets program in Edina, the city should develop a Living Streets Policy and Plan, which includes a clearly stated vision statement and goals, a set of supporting policies, and a process or plan for implementation. This development framework is illustrated in Figure 1. This document presents a draft living streets vision and a policy template developed based on the initial stakeholder engagement workshop described below. These represent the first two steps in the development framework. The third step, which includes a detailed plan for implementation of Living Streets in Edina, will be developed as part of a future effort.

A Living Streets Policy will provide the regulatory framework for living streets and a Living Streets Plan will provide the details needed for implementation, such as prioritization of projects by mode, an implementation process, and design guidelines and templates. The purpose of this framework is to facilitate discussion in order to move the City toward adoption of an Edina Living Streets Vision and the development and implementation of a Living Streets Plan. This material should be continually updated and refined as the living streets implementation process moves forward.

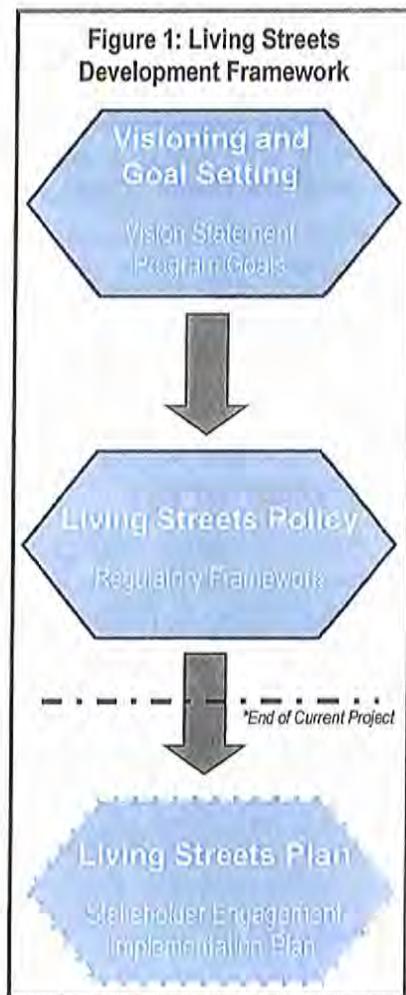
Visioning and Goal Setting



In March of 2012 the City of Edina hosted a two part Living Streets Workshop, facilitated by a nationally recognized living streets expert (refer to Attachment 2 for a summary). The purpose was to introduce participants to the living streets concept and to initiate the process of developing a Living Streets Policy in Edina. A key result of this effort was the engagement of stakeholders in the visioning, goal setting, and policy making process. The results and proceedings from this workshop served as the basis for the development of the vision and policy framework presented below.

Living Streets Vision

The purpose of the vision statement is to set the philosophical framework for living streets in Edina. It should answer the question “*why implement living streets?*”, creating a picture or vision of the community which Edina residents are striving toward. As part of the Living Streets workshop, attendees participated in a visioning exercise in which they were asked “What will be the vision behind a new living streets policy? (refer to Attachment 2)” Based on this visioning session a series of common themes emerged. These themes paint the vision for living streets in Edina.



Participants envisioned Edina as a place...

... where all modes of transportation are equally accessible



... with a cohesive community identity



... where families regularly choose walking or bicycling instead of driving



... where biking and walking are not only safe and convenient, but also enjoyable



... where public policy strives to promote sustainability through balanced infrastructure investment decisions.



... where people choose physical activity for enjoyment



... where the streets are a place you want to sit.



... where environmental stewardship and reduced energy consumption are pursued in the public and private sectors alike.



Based on the themes described above, the Edina Living Streets Committee developed the following draft vision statement for stakeholder consideration.

Draft Edina Living Streets Vision Statement

Living Streets balance the needs of motorists, pedestrians, bicyclists, and transit riders in ways that promote safety and convenience, enhance community identity, create economic vitality, improve environmental sustainability, and provide meaningful opportunities for active living and better health.

The purpose of this draft statement is to facilitate discussion in order to move the City toward an approved Edina Living Streets Vision. This vision statement should be continually updated and refined as the living streets study process moves forward.

Living Streets Goals

In order to develop a policy which moves the city toward its vision for living streets, it is important to clearly articulate the goals and priorities of the living streets program. Goals should answer the question “*what will living streets look like?*”, stating the broad, overriding outcomes the city wants to achieve. Typical goals for implementing living streets are listed below.¹

- Serve the land uses that are adjacent to the street; mobility is a means, not an end
- Encourage people to travel by walking, bicycling, and transit, and to drive less
- Provide transportation options for people of all ages, physical abilities, and income levels
- Enhance the safety and security of streets, from both a traffic and personal perspective
- Improve peoples’ health
- Create livable neighborhoods
- Reduce the total amount of paved area
- Reduce stormwater runoff and capture/treat pollutants for improved watershed health
- Maximize infiltration and reuse of stormwater
- Reduce greenhouse gas emissions and other air pollution
- Reduce energy consumption
- Promote the economic well-being of both businesses and residents
- Increase civic space and encourage human interaction
- Maintain and improve the urban forest

With these goals presented as a basis, participants of the Edina Living Streets Workshop developed the following priorities for living streets in Edina:

Living Street Priorities

- Safe and enjoyable access to all modes for all users and all abilities
- Streets will vary in character (context sensitive)
- The transportation network will enhance neighborhoods
- Streets will incorporate green management best practices

Further, the following were identified as additional goals or attributes for the Edina Living Streets System:

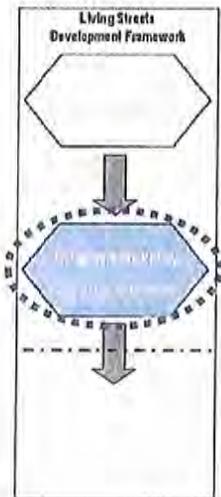
Secondary Goals

- Foster income equity
- Pedestrian-oriented design
- Connect people, not just places

¹ Los Angeles County. Model Design Manual for Living Streets. 2011.

- Foster active lifestyles
- Be inviting
- Support commerce
- Be sustainable

Living Streets Policy Development



The intent of a living streets policy is to provide policy direction in support of implementing the city's living street vision and goals. A living streets policy should answer the question "how will we achieve living streets?" It should be broad enough to cover all aspects of the transportation system and should contain sufficient detail to guide the design process in support of living streets principles. The policy should not prescribe how to design every segment of every street; rather, it should clearly define what the city wants to accomplish with its streets and refer to specific design guidance developed as part of the planning process (i.e., modal balance, prioritization, checklists, design templates, etc.).

Los Angeles County has developed a living streets policy model based on the 10 policy elements for complete streets established by the *National Complete Streets Coalition*, updated to incorporate living streets principles.¹ Any agency may adopt all or part of the Los Angeles model as is, or modify the model based on community goals, provided the Los Angeles County Design Manual is acknowledged and credited as appropriate.

The following is a draft Edina Living Streets Policy framework, which was developed using the Los Angeles County model described above as a template, and updated to reflect the values of Edina. This framework includes a range of policy elements and statements intended to provide comprehensive policy level support for living streets. The policy is intended to provide a regulatory framework for living streets and not a plan for implementation.

The draft policy framework should be reviewed and refined by the Edina Living Streets Committee throughout the living streets process, based on comprehensive stakeholder engagement and continuous public outreach. Once the Edina Living Streets Policy has been finalized, the Living Streets Committee should present the policy to the full Edina Transportation Committee for final approval and recommendation to the City Council. Once a final policy is drafted and approved, the policy can be enacted by the City through a living streets ordinance or resolution enacting the policy.

Draft Edina Living Streets Policy

Vision Statement – Why?

Living Streets balance the needs of motorists, pedestrians, bicyclists, and transit riders in ways that promote safety and convenience, enhance community identity, create economic vitality, improve environmental sustainability, and provide meaningful opportunities for active living and better health.

Goals (based on Edina Living Streets Workshop – March 2012) – What?

The goal of the Edina Living Streets Policy is to ensure the following:

- Safe and enjoyable access to all modes for all users and all abilities
- Streets will vary in character (context sensitive)
- The transportation network will enhance neighborhoods
- Streets will incorporate green management best practices

Policy Elements (guided by Los Angeles Co. model) – How?

Living Street Element *Policy Statements*

Vision The City will develop policies and practices that cause its transportation system to be designed in accordance with the Living Streets Vision and Goals described above.

All Users and All Modes The City will incorporate the full range of appropriate living streets elements in accordance with its Living Streets Policy and Plan when planning for and designing the local transportation network and in regulatory approvals for private developments, such as conditional uses and PUD.

The City will plan, design, and build high quality transportation facilities that prioritize the needs of the most vulnerable users (pedestrians, cyclists, children, elderly, and disabled) while enhancing safety and convenience for all users, and providing access and mobility for all modes.

Connectivity The City will strive to enhance community cohesion by connecting places. This will include design, operation, and maintenance of a transportation system that provides a highly connected network of streets, paths, and trails. The street network will accommodate all modes of travel.

The City will seek opportunities to overcome barriers to active transportation. This includes preserving and repurposing existing rights-of-way, and to adding new rights-of-way to enhance connectivity for pedestrians, bicyclists, and transit.

The City will prioritize non-motorized connectivity improvements to services, schools, parks, civic uses, regional connections, and commercial uses.

The City will require large, new developments to provide interconnected street networks with small blocks that connect to existing or planned streets on the perimeter of the development.

Jurisdiction The City will seek to implement living streets on all streets within Edina. The roadway network in Edina generally comprises state, county, and local routes. Policies and legislation supporting living streets implementation on state and county routes are in place.

The State of Minnesota has passed legislation in support of complete streets on state routes and MnDOT is in the process of developing a Complete Streets Policy. Hennepin County has enacted a policy supporting complete streets implementation on County routes. While there are differences, Edina’s living streets vision and the state and county complete streets efforts are complementary.

The Edina Living Streets Policy applies to all roads within the City’s jurisdiction (local, collector, and arterial).

The City will require all developers to obtain and comply with the standards and procedures of the Edina Living Streets Policy and Plan.

The City will actively seek to coordinate its Living Streets Policy and Plan with partner agencies, in order to influence decisions on roads outside of its jurisdiction (i.e., MnDOT and Hennepin County). This will include providing a copy of the Edina Living Streets Policy to partner agencies, and seeking their input throughout the Living Streets Plan development process. At a minimum, this outreach to partner agencies will include MnDOT, Hennepin County, the Metropolitan Council, and the Minnehaha Creek and Nine Mile Creek Watershed Districts.

Phases

As living streets may be achieved through single projects or incrementally through a series of smaller improvements or maintenance activities over time, the City will apply the living streets policy document to all roadway projects including those involving operations, maintenance, new construction, reconstruction, retrofits, repaving, rehabilitation, or changes in the allocation of pavement space on an existing roadway. This also includes privately built roads intended for public use.

Projects will include consideration of the logical termini by mode. For example, the logical termini for a bike lane or sidewalk may extend beyond the traditional limits of a roadway construction project, in order to ensure multimodal connectivity and continuity.

The City will draw on all sources of transportation funding to implement living streets and actively pursue new funding sources as available.

Exceptions

Living streets implementation will be a required component of ALL street construction, reconstruction, repaving, rehabilitation, and maintenance projects.

Exceptions to the guidelines set forth in the Edina Living Streets policy may be made, based on professional judgment.

The City will develop an Living Streets exception request process to set the conditions by which a project could be exempted from part of all of the Edina Living Streets Policy

Design

The City will adopt new living streets design guidelines and procedures to guide the planning, funding, design, construction, operation, and maintenance of new and modified streets while remaining flexible to the unique circumstances of different streets where sound engineering and planning judgment will produce context-sensitive designs. The City will undertake a planning effort to define this process.

The City will incorporate living street design principles of into all city plans, manuals, rules, regulations, and programs as appropriate, in accordance with the Living Streets Policy and Plan. As new and better practices evolve, these will be incorporate those as well.

The City will provide well-designed pedestrian accommodations in the form

of sidewalks or shared-use pathways on all arterial and collector streets and on local streets.

The City will provide frequent, convenient and safe street crossings. These may be at intersections designed to be pedestrian friendly, or at mid-block locations where needed and appropriate.

The City will provide bicycle accommodations along all avenues, boulevards, and connector streets. Bicycle accommodations could include but are not limited to bike lanes, trails, or other improvements.

The City will keep street pavement widths to the minimum necessary.

The City will allocate right-of-way for boulevards, plant trees, and treat stormwater associated with right-of-way harcover, including trails, transit, and pedestrian facilities as well as streets, whenever a street is constructed, reconstructed, or relocated where physical conditions warrant.

The City will prioritize right-of-way space allocation for access and mobility (all modes) improvements and include on-street residential parking only when it is not in conflict with access and mobility (all modes) and achieving other living streets goals, such as managing stormwater runoff.

Context Sensitivity

The City will seek input from local stakeholders when designing their streets with input from local stakeholders.

Street design will enhance the character of neighborhoods and be in keeping with existing land uses.

When designing streets, the City will be mindful of preserving and protecting natural features, such as waterways, trees, slopes, and ravines.

The City will design their streets with a strong sense of place. They will use architecture, landscaping, streetscaping, public art, signage, etc. to reflect the community, neighborhood, history, and natural setting.

The City will coordinate with merchants along commercial corridors to develop vibrant retail districts.

The City will inventory building and zoning codes to bring into agreement with Living Streets principals and minimum design guidelines as established by the Living Streets Policy and Plan.

The City will develop a Living Streets Plan which includes a palette of design templates for street types and minimum design guidelines, from which developers can choose.

Implementation

The City will develop, adopt, and apply a Living Streets Plan for the implementation of living streets in Edina. At a minimum, the plan should include the development of street typologies based functional classification,

multimodal traffic volumes, and land use, which are correlated to a design “toolbox.”

The City will incorporate living streets concepts into the next circulation element of their general plans (i.e., Comprehensive Plan, Bicycle Plan, Safe Routes to School Plan, etc.).

The City will update its storm management plan to include living streets principles which aim to capture streetwater runoff on site.

The City will create an urban forest management plan to protect and expand the existing urban tree canopy.

The City will review, modify, update, and delete out-dated policies included but not limited to the Assessment Policy and the City Code.

The City will prioritize CIP-prioritize by mode; based on the Living Streets Policy and addressing safety, access, and convenience.

The City will actively engage stakeholders in the street planning and design process, including property owners, residents, and other users of the transportation system. At a minimum, engagement efforts will entail extensive education campaigns, design input, public meetings, and other forms of on-going communication.

The City will actively seek out and pursue funding and grant opportunities to implement the Living Streets Policy and Plan.

Operations

The City will update right-of-way maintenance standards and practices to support living streets. As an example, on-street bicycle lanes may require more upkeep (i.e., cleaning/debris removal, striping, etc.).

The City will update its traffic enforcement practices to support living streets, such as citations for speed limit violations.

Every city agency, including public works, planning, maintenance, and others will follow the Edina Living Streets Policy and will conduct ongoing coordination in support of living streets operations.

The City will conduct on-going public engagement in order to provide public education on safe and efficient street operations and multimodal considerations. This could include targeted stakeholder education meetings (i.e., senior living facilities) a page on the city website and a living streets component at regularly scheduled public meetings and open houses.

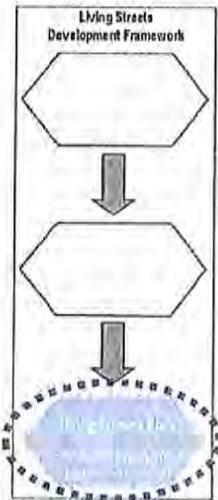
5.0 Next Steps

The purpose of the living streets vision and goals, and policy framework presented herein is to facilitate stakeholder dialog in an effort to move toward the development and adoption of an Edina Living Streets Policy and a Living Streets Plan. The Living Streets Policy will provide the regulatory framework for living streets and the Living Streets Plan will provide the details needed for implementation, such as prioritization of projects by mode, an implementation process, and design guidelines and templates.

Living Streets Policy

The next step in this process is the continued refinement of the Living Streets Policy Framework based on stakeholder engagement efforts. The purpose of the next stakeholder engagement efforts should be to present and seek input on the Living Street Vision and Goals and Policy Framework, in order to build stakeholder support for the Living Streets Policy. The policy should be a dynamic document (i.e., continuously revised, modified, and updated), which clearly articulates and supports the city's vision and goals for living streets in Edina.

Living Streets Plan



Presented above are a draft living streets vision and a policy template, which represent the first two steps in the living streets development framework. The third step includes the development of a detailed plan for implementation of Living Streets in Edina. After completion of a Living Streets Policy, the city should undertake a planning process which includes robust stakeholder engagement, identification of priority streets, and areas (neighborhoods/districts) for the implementation of living streets policies. The plan should include the development of an implementation process which includes design guidance. The implementation process should not prescribe how to design every segment of every street; rather, after clearly defining what a community wants to accomplish with its streets, designers can apply this framework along with the specific guidance developed as part of the planning process (i.e., prioritization, checklists, etc.), to meet the community's goals. A key element of the implementation process is the identification of the desired modal balance for each element of the project. This process should recognize that the needs of all roadway segments and corridors within a project are not the same and improvements should be designed to provide the level of functionality needed for

each mode, given the context of a project. For example, the bicycle accommodation needs of a high speed urban arterial road would not be the same as a residential street. The plan should also include an examination of how aboveground stormwater management can be accommodated within the right-of-way of varying street types. Living streets harvest stormwater to recharge groundwater and water street trees. Accomplishing this in narrow right-of-ways requires forethought. In addition, urban forest expansion into the right-of-way should be addressed in the plan.

At a minimum, the plan should include the development of street typologies based functional classification, multimodal traffic volumes, and land use, which are correlated to a design "toolbox." This should include an implementation process or check-list which is followed when a street project is initiated. The Edina Living Streets Committee developed the following as an example. The process would identify the functional classification and traffic volumes, such as:

- Low volume local – under 300 vehicles per day (vpd)
- Local – 300-1000 vpd
- Higher volume Local – over 1000 vpd
- Minor collector – under 3000 vpd
- Collector – 3000-6000 vpd

- Major collector – over 6000 vpd

The process should also include considerations for land use and other relevant issues, such as:

- Business segment – that part of a street adjacent to and within a set distance from a business district
- School segment – that part of a street adjacent to and within a set distance from a public or private K-12 school or public community center
- Park segment – that part of a street adjacent to and within a set distance from a public park or regional trail
- Flagged – streets or segments of streets for which a problem has been identified, such as speeding.

In this example, a project on Elm Street might have the following considerations:

Elm Street has 1300 vpd. The segment of Elm between 42nd Street and 44th Street is adjacent to the school. The school segment runs from 41st Street to 45th Street. A speeding issue has been noted on the 3900 block.

Classification: Higher volume local with school segment, flagged for speeding and volume.

This process would allow streets to be classified and correlated to an appropriate set of design tools, given the context of the street. The intent of this process is not to develop street typologies with strict design standards, but rather to facilitate an informed decision making process which will meet the needs of users while achieving the City's Living Streets goals.

Two example design Templates are included as Attachment 3. An example work plan, budget, and schedule for the proposed Living Streets Planning process are included as Attachment 4.

Attachment 1: Living Streets Background Document

LIVING STREETS

Background

Complete Streets typically refers to street design that provides for multiple modes of transportation (auto, mass transit, pedestrian, bike). *Green Streets* typically refers to street design that reduces environmental impacts by reducing impervious surface, managing stormwater, and providing shade.

Living Streets refers to streets that are not only Complete and Green but also designed to improve active living, community identity, and social connections. Cities such as Denver, Los Angeles and Tucson have Living Streets initiatives. In the Twin Cities, the Ramsey-Washington Metro Watershed District and North St. Paul have developed and are implementing a new Living Streets Plan. The City of Maplewood is also pursuing a process to adopt Living Streets.

Complete Streets Legislation

The State of Minnesota passed Complete Streets legislation in 2010. The Commissioner of Transportation has committed Mn/DOT to implement a complete street vision for the trunk highway system. In January 2010 the Edina City Council adopted a resolution expressing support for a statewide Complete Streets Policy. Inasmuch as the statewide policy will not apply to local roads, cities are encouraged to adopt policies to meet their unique needs; however this is not a mandate.

According to Mn/DOT, Complete Streets does not mean "all modes on all roads"; rather, the goal of Complete Streets should be to:

- 1) Develop a balanced transportation system that integrates all modes via planning that is inclusive of each mode (i.e., transit, freight, automobile, bicycle and pedestrian); and
- 2) Include transportation users of all types, ages and abilities.

Examples of Complete Streets goals and principles listed in the report to the legislature include:

- 1) Reduce crash rates and severity of crashes;
- 2) Improve mobility and accessibility of all individuals including those with disabilities in accordance with the legal requirements of the ADA;
- 3) Encourage mode shift to non-motorized transportation and transit;
- 4) Reduce air and water pollution and reduce noise impacts;
- 5) Increase transportation network connectivity;
- 6) Maximize the efficient use of existing facilities;
- 7) Strive for tax supported investments to provide maximum benefits to the community and all user groups;
- 8) Safely integrate intermodal connections across the transportation network; and
- 9) Promote safe and convenient access and travel for all users (pedestrians, bicyclists, transit riders) and people of all abilities as well as freight and motor vehicle drivers.

While City actions to date support the development of a Complete Streets policy for local streets, the City could go further in planning for and implementing its vision for a transportation system that also addresses environmental and active living goals and enhances community identity. The

Living Streets concept is the next step.

Minnesota Green Step City

The Minnesota Pollution Control Agency (MPCA) has launched the Minnesota Green Step Cities program. Five cities—including Edina—were selected to pilot the program. The Edina City Council asked the Energy and Environment Commission (EEC) to evaluate whether the City should apply to become a Green Step city and, if yes, which of the 28 best practices related to sustainability Edina should undertake in order to become certified. In January 2011, the EEC recommended that Edina join the Green Step Cities program and outlined the 16 best practices Edina should undertake. The City Council adopted the EEC's recommendation.

Six of the City's 16 best practices would be supported by a Living Streets Plan. These are:

- #11. Complete Streets (requires "complete streets policy" or "living streets," which is inclusive of "complete streets" concepts);
- #12. Mobility Options (walking/biking, Safe Routes to School, etc.);
- #16. Urban Forest;
- #17. Efficient Stormwater Management;
- #18. Green Infrastructure (porous pavement, swales, etc.); and
- #19. Surface Water Quality (rain gardens, etc.).

For more information on the Green Steps Cities program, please see the January 18, 2011 City Council meeting.

Edina Fit City Resolution

In July 2006 the Edina City Council adopted a resolution expressing a commitment "to providing an activity-friendly community to its residents" and authorizing "the application for The Governor's 'Fit City' Program."

The Fit City Program recognizes Minnesota cities committed to creating and maintaining an environment supportive of active living. The program requires participating cities to "adopt a resolution affirming activity-friendly commitments by City leaders and elected officials" and to "agree to advertise and display 'Fit City' designation."

The goals of the Fit City program and the City's resolution supporting an activity-friendly community provide support for a Living Streets Plan.

2008 Comprehensive Plan

The Edina City Council adopted an updated Comprehensive Plan in 2007. The Plan promotes a vision for our city that is pedestrian, bicycle and transit friendly and inclusive of all users. The following Principles from Chapter 4 provide support for a Living Streets Plan:

1. *Design public open and green linkages that bring both amenity and positive image to neighborhoods, corridors, and business precincts.*
2. *Design public streets to serve not only vehicles but also pedestrians, people with mobility disadvantages and bicycles, balancing the spatial needs of existing and future users within the right-of-way. Address both mobility and recreational needs and opportunities.*
3. *Create walkable streets that foster an active public life;*

4. *Protect and improve the urban forest, including street trees and related landscaping, in order to provide shade and shelter for pedestrians and screening for parking and service uses.*
5. *Create and promote environments that make it safe and convenient for people to integrate physical activity into their daily routines.*
6. *Recognize and integrate Edina's historic landscape features, such as its stone walls and gateways, into the design and redesign of streets, paths and pedestrian ways.*
7. *Promote a clear hierarchy of street types of distinctive and differentiated character that are defined by landscape and pedestrian amenities as well as adjacent buildings. Within larger redevelopment sites, promote a fine-grained and interconnected network of local streets and paths, encouraging pedestrian circulation and providing a choice of access points (p. 4-36. See also Guidelines of 4-37-39).*

Additional support for Living Streets can be found throughout the 2008 Comprehensive Plan. Please see the attached "Excerpts from the 2008 Comprehensive Plan" for more.

DISCUSSION

Living Streets¹

Living Streets pulls together the concepts of Complete Streets and Green Streets, and puts additional focus on quality of life aspects for city residents. Figure 1 below shows the components of Living Streets. The model balances the "green" components (environment and social factors) and the "grey" components (transportation and utilities) of the system. All too many times in the past, the "green" factors have not been highly prioritized for agencies. This mentality is starting to change in many agencies statewide.

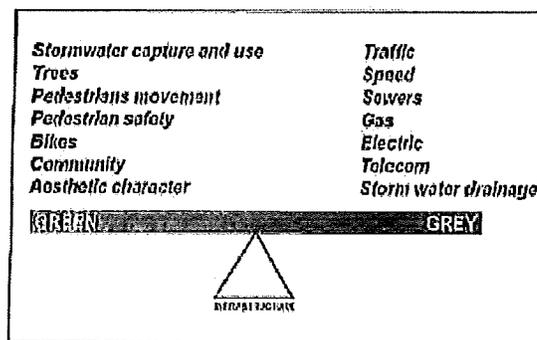


FIGURE 1 - RWMWD/BARR REPORT

A Living Streets policy should:

¹ Landscape Architect Fred Rozumalski from Barr Engineering and Ramsey-Washington Metro Watershed District (RWMWD) administrator Cliff Alchinger has championed the Living Streets concepts they developed for North St. Paul. The RWMWD administrator has stated that the North Saint Paul Living Streets policy is in the public domain and can be used as a template for others with credit giving for photos. To read the complete policy, please see http://www.ci.north-saint-paul.mn.us/index.asp?Type=B_BASIC&SEC=%7B13DF596B-ED60-4D35-B13A-6EE928B643AB%7D.

- 1) Improve stormwater quality through reducing the impervious footprint of streets, meeting or exceeding city and watershed rules through best management practices (BMP) such as rain gardens and swales.
- 2) Implement traffic calming measures through the use of techniques best suited for site conditions.
- 3) Improve biking and walking conditions along natural connector routes and collector streets through designation of bike lanes, sidewalks, or multi-purpose trails.
- 4) Create boulevard tree standards that provide environmental benefits (stormwater management, shade to reduce heating and cooling costs, filtering air pollutants, reduce urban heat island effect), enhance quality of life, and are practical and affordable.

As seen in Figure 2, a street section of 22' can accommodate parking on one side of the street with two cars passing one another on a residential street. A 22' street section with parking on one side can also accommodate larger vehicles but there may be some yielding when vehicles must pass one another near a parked vehicle. Narrowed streets could reduce costs and also provide room for sidewalk on one side that is no closer than the previous street edge once was located.

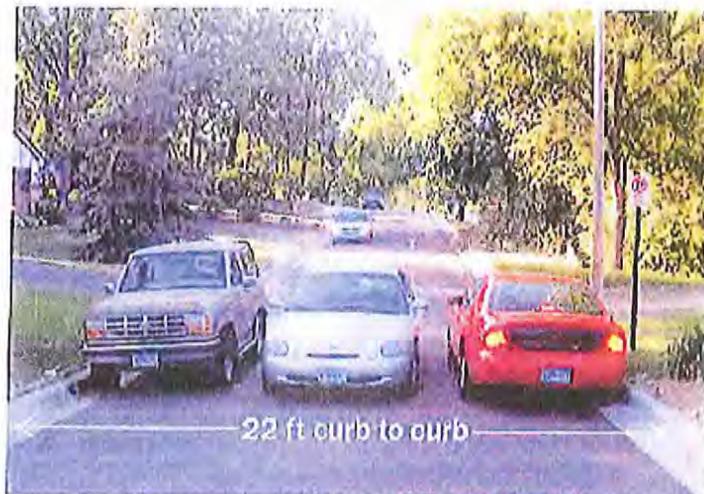


FIGURE 2 - RWMWD/BARR REPORT

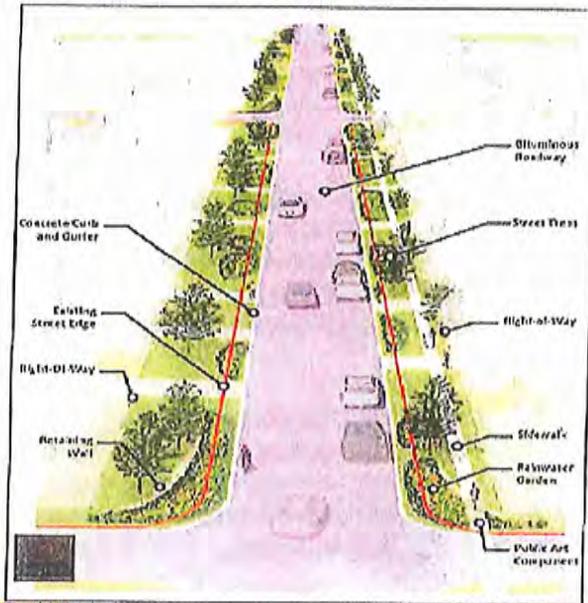


FIGURE 3 – RESIDENTIAL STREET - RWMWD/BARR REPORT

Active Living Opportunities – Providing a designated system of sidewalks, trails, and bike lanes increases the likelihood for walking and biking. Figure 4 depicts a typical layout of a collector street with biking and pedestrian facilities which could be standardized through ordinance language.

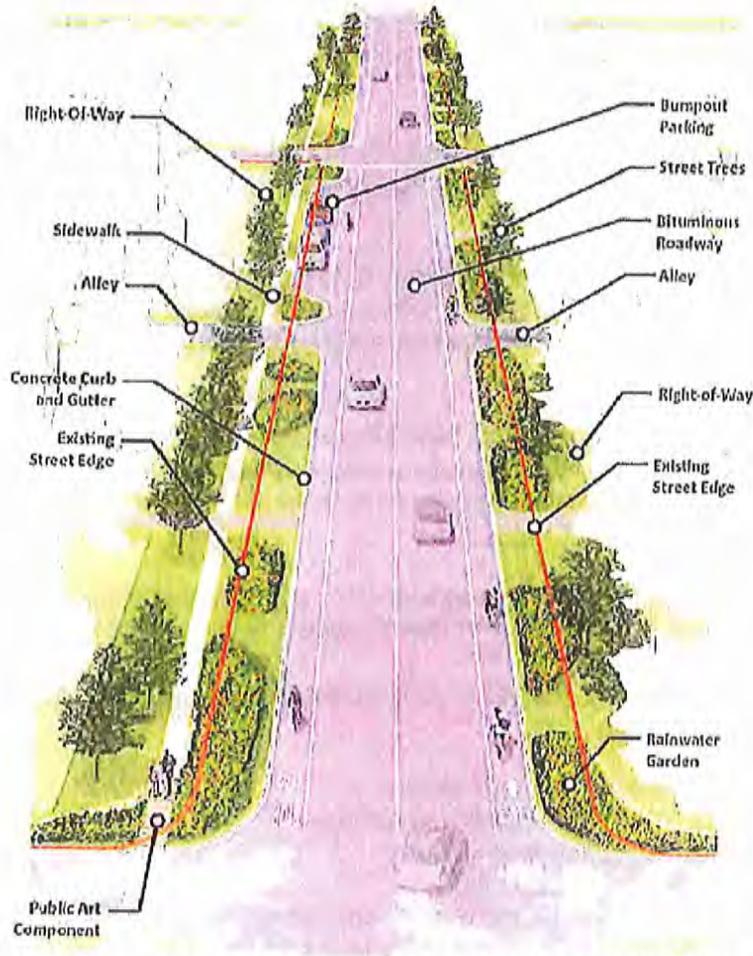


FIGURE 4 – COLLECTOR STREET - RWMWD/BARR REPORT

RECOMMENDATION

That the ETC discuss a Living Street Policy/Plan and develop a framework for working collaboratively with staff, other boards and commissions, and the City Council, with the intent to ultimately recommend a Living Street Policy/Plan to be implemented as the new vision for neighborhoods and collector streets in Edina. This Living Street Policy/Plan is important considering the large capital investment that will be occurring throughout the city to local neighborhoods over the next decade and beyond.

Attachments:

- 1) Sample Resolution
- 2) Excerpts from 2008 Comprehensive Plan

**DRAFT RESOLUTION
DECLARING SUPPORT FOR ESTABLISHING A LIVING STREETS POLICY
FOR THE CITY OF EDINA, MINNESOTA**

WHEREAS, the Living Streets concept is designed to assure safety and accessibility for all the users of our roads, trails and transit systems, including pedestrians, bicyclists, transit riders, motorists, commercial and emergency vehicles, and for people of all ages and of all abilities; and

WHEREAS, Living Streets reduce congestion by providing safe travel choices that encourage non-motorized transportation options, increasing the overall capacity of the transportation network; and

WHEREAS, Living Streets will help the City of Edina reduce greenhouse gas emissions as more people choose an alternative to the single occupant vehicle; and Living Streets is consistent with the US Mayors Climate Protection Agreement and the City's participation in the Green Steps Cities program; and

WHEREAS, Living Streets support economic growth and community stability by providing accessible and efficient connections between home, school, work, recreation and retail destinations by improving the pedestrian and vehicular environments throughout communities; and

WHEREAS, Living Streets enhance safe walking and bicycling options for school age children, in recognition of the objectives of the national Safe Routes to School program; and

WHEREAS, Living Streets provide environmental and social benefits including but not limited to:

- 1) Improving stormwater quality
- 2) Providing traffic calming measures
- 3) Enhancing community feel
- 4) Promoting active living
- 5) Improving biking and walking conditions
- 6) Protecting and enhancing the urban forest

THEREFORE, BE IT RESOLVED, that in order to develop and maintain a safe, efficient, balanced and environmentally sound city transportation system for people of all ages and abilities, transportation and development projects shall incorporate a philosophy that expands transportation choices and further incorporates the Living Streets theme and key concepts listed above; and

BE IT FURTHER RESOLVED, the ETC in collaboration with staff shall prepare a Living Streets Policy/Plan and staff shall concurrently review city ordinances, engineering standards, policies, and guidelines in order to make recommendations ultimately to the City Council on a Living Street Policy/Plan that will reduce impacts to the environment, be practical and economical, while also meeting community and stakeholder needs consistent with Living Streets concepts.

Excerpts from 2008 Comprehensive Plan

Chapter 4: Land Use Goals—

1. Protect and preserve the essential character of existing residential neighborhoods (p. 4-21)
6. Increase mixed use development where supported by *adequate infrastructure to minimize traffic congestion, support transit, and diversity tax base* (p. 4-22)
7. *Increase pedestrian and bicycling opportunities and connections between neighborhoods, and with other communities, to improve transportation infrastructure and reduce dependence on the car* (p. 4-22)
8. Ensure that *public realm corridor design is contextual, respectful of adjacent neighborhood character supportive of adjacent commercial and/or mixed use development, promotes community identity and orientation, and creates the highest quality experience for pedestrians, cyclists, and transit users* (p. 4-22)

Land Use and Transportation Interface

There is a fundamental link between land use planning and transportation planning. The transportation system must function as a network that links a diversity of uses in a manner that promotes efficient multi-modal travel (motor vehicles, bicycle, transit and pedestrian). ... Chapter 7 of this Comprehensive Plan deals with transportation, and responds to and supports the land use and community design policies presented in this chapter. (p. 4-22)

Land Use Policies—

3. Edina has evolved to a large degree into a community defined by roads. The City's roads should continue to evolve to act as connectors, rather than as barriers. *The city will incorporate amenities and infrastructure into its public corridors to make them beautiful, efficient, multi-modal public spaces.*
5. In reviewing developing proposals, the City will examine how land use and transportation are integrated to *ensure that new development and redevelopment expands non-motorized travel options.*
6. The City will maintain the current open space and wetlands acreage and seek to expand it whenever possible.
8. The City will grow and develop in a sustainable manner that will *protect its high quality natural environment, promote energy efficiency and conservation of natural resources, and minimize the impacts of buildings on the environment over the lifetime of each building.* (p. 4-34)

Staging Development: Small Area Plans—

These Small Area Plans may include planning principles such as:

- 1) Area or neighborhood land use pattern and design *that encourages walking and transit use*
- 2) *High degree of connectivity to existing and new public (and active private) spaces to encourage physical activity, social interaction, and optimal land use*
- 3) *Smaller parking footprints*
- 4) *Reduction in impervious surfaces and associated storm water runoff* (p. 4-35).

Citywide Movement Patterns and Public Spaces—

Principles:

8. *Design public open and green linkages that bring both amenity and positive image to neighborhoods, corridors, and business precincts.*
9. *Design public streets to serve not only vehicles but also pedestrians, people with mobility disadvantages and bicycles, balancing the spatial needs of existing and future users within the right-of-way. Address both mobility and recreational needs and opportunities.*
10. *Create walkable streets that foster an active public life;*
11. *Protect and improve the urban forest, including street trees and related landscaping, in order to provide shade and shelter for pedestrians and screening for parking and service uses.*
12. *Create and promote environments that make it safe and convenient for people to integrate physical activity into their daily routines.*

13. Recognize and integrate Edina’s historic landscape features, such as its stone walls and gateways, *into the design and redesign of streets, paths and pedestrian ways.*
14. Promote a clear hierarchy of street types of distinctive and differentiated character that *are defined by landscape and pedestrian amenities* as well as adjacent buildings. Within larger redevelopment sites, promote a fine-grained and *interconnected network of local streets and paths, encouraging pedestrian circulation* and providing a choice of access points (p. 4-36. See also Guidelines of 4-37-39).

Guidelines: Medium- and High-Density Design (All Uses)—

A Pedestrian-Friendly Environment: Improving the auto-oriented design pattern discussed above under “Issues” *will call for guidelines that change the relationship between parking, pedestrian movement and building placement.*

Landscaping: *Provide visual screening and privacy to buffer cars from people, provide visual relief and allow stormwater infiltration in parking lots.* Permeable hardscape, where appropriate, is preferred over blacktop or traditional paving.

Parking: *Evaluate current parking standards in order to encourage shared parking and minimize the visual impact of surface parking.*

- Design surface parking to maximize stormwater infiltration and allow for groundwater recharge, using infiltration swales, pervious pavement or similar techniques
- Where vehicle parking requirements exist, implement minimum bicycle parking standards as well (p. 4-45).

Movement Patterns:

- *Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.*
- *Limit driveway access from primary streets while encouraging access from secondary streets.*
- *Encourage enhanced transit stops, including shelters, shade and seating where feasible.*
- *Provide pedestrian amenities, such as wide sidewalks, street trees, pedestrian-scale lighting, and street furnishings (benches, trash receptacles, etc) (p. 4-47).*

Implementation—

Provision for urban forest protection and improvement consistent with Plan guidance for creating a pleasant pedestrian environment, screening parking areas, providing the benefits of landscaping, and restoring environmental processes that a tree canopy provides the biological community. These provisions may include at tree preservation ordinance. Other measures may also be considered to ensure appropriate tree replacement and management of our urban forest (4-56).

Chapter 7: Transportation

Goals and Policies: Transportation—

Goals:

1. *Maintain and enhance mobility for residents and businesses through creation and maintenance of a balanced system of transportation alternatives.*
2. *Implement a fully multi-modal transportation system that supports the land use vision and future land use plan for managing and shaping future growth.*
3. *Minimize the impacts of the transportation system on Edina’s environment and neighborhood quality of life.*
4. *Reduce the overall dependence on the use of single-occupant vehicles by promoting land use patterns that allow for shorter vehicular trips and the use of alternative travel options.*
5. *Ensure that all Edina’s residents, workers, and visitors, including those with transportation disadvantages, have viable travel options.*
6. *Promote a travel demand management program through a coordinated program of regulations, marketing, and provisions of alternative travel options.*

7. *Provide multiple travel options for transit users, pedestrians, bicyclists, and rideshare users, as well as for drivers of private automobiles.*
8. *Support attractive and high performance transit service connections.*
9. *Manage parking provision to encourage joint and shared use of facilities, ride-sharing (car pools and van pools), bicycle parking, and increased transit use (p. 7-38-39).*

Policies

Roadway Design:

5. *Design/enhance collector and arterial roadways to minimize through traffic on local streets in the functional classification system, and to be compatible with other transportation modes including transit, bicycle and pedestrian.*
6. *Use adequate transitions and buffers including, but not limited to, earth berms, walls, landscaping and distance to mitigate the undesirable impact of high volume roadways.*
8. *Encourage beautification of local roadways, where appropriate, with amenities such as boulevard streets, decorative street lighting, and monuments.*
9. *Monitor and address transportation requirements associated with demographic trends, such as an aging population (p. 7-39-40).*

Parking:

2. *Continue to limit on-street parking in and near congested commercial areas.*

Pedestrian/Bicycle:

1. *Provide accessibility to pedestrians and bicycles at major activity centers, including necessary storage facilities.*
2. *Create pedestrian and bicycle interconnections among major generators, with continuity across major roadways and other barriers.*
3. *Review and recommend construction of pedestrian and bike paths throughout Edina cooperatively with the Three Rivers Park District and Hennepin County.*
4. *Promote safe walking, bicycling and driving. Promote vehicle drivers respect for bicycles and pedestrians along with bicyclists and pedestrian observation of signs and use of designed paths for travel.*
5. *Support inclusion of pedestrian and bicycle access planning when upgrading roadways, bridges and redevelopment projects.*
6. *Provide sidewalks and safe crossings for areas of potential pedestrian/vehicle conflicts, including high-traffic streets, commercial areas, areas with transit access, and in high-density residential locations.*
7. *Provide appropriate signage in areas of potential conflict between pedestrians and automobile traffic.*
8. *Separate pedestrian traffic from bicycle traffic to ensure desired safety conditions. When a bicycle facility is provided, considerations should also be given to providing a corresponding pedestrian way where possible. This could be as a separate facility or through striping.*
9. *Support recommendations of the Comprehensive Bicycle Transportation Plan for implementation (7-43-44)*

Comprehensive Bicycle Transportation Plan—

Vision:

The guiding vision for this document is to support the gradual transportation of the City of Edina into a “progressive bicycle-friendly community where citizens can easily integrate cycling into their daily lives” (p. 8).

Guiding Principles

The following goals and principles guide the recommendations included in this Plan:

Improving Safety: proactively addressing existing hazardous conditions, assigning dedicated road space to cyclists, and alerting motorists of their presence will help improve safety and convenience for all users of Edina's streets and sidewalks.

Connecting to Local and Regional Destinations: Providing safe and convenient connections to destinations in Edina and neighboring communities will increase bicycle use and help lower demand on Edina's overall automobile transportation system. Connecting to the growing network of regional trails will expand the number of potential destinations available to Edina citizens and provide increased access to our region's recreational and transportation assets.

Safe Routes for All: Bicycling can serve the mobility needs of people over a wide range of ages and abilities. Providing a network of safe and comfortable bicycle routes to schools and parks will be an important builder of healthy life-long habits of active living and independence for children, seniors and other adults.

Bicycling As A Base For Community Health: A safe and inviting bicycle transportation network will help improve community health by increasing opportunities for active transportation and active living for all of Edina's citizens.

Bicycling As A Useful Transportation Option In Edina: A high quality bicycle network will make it easy and convenient for people in Edina to choose cycling as a way to meet at least some of their transportation and mobility needs. A network of safe, convenient and easily accessible routes will expand and use of the bicycling as a useful transportation option in Edina (p. 8-9).

Chapter 9: Parks, Open Space, and Natural Resources

Natural Resources Conservation and Management

Goals:

4. Conserve, and replace as necessary, Edina's urban forest to ensure the long-term vitality and viability of this integral part of Edina's overall identity and attractiveness (9-21)

Policies:

5. Create a program for maintaining trees throughout the City, and replacing them as necessary (p. 9-22).

Walking and Biking Trails Pathways Plan

Policies:

2. Continue to work with the Bike Edina Taskforce and Public Works staff to implement the new bike pathway master plan for the City of Edina. The objective is to identify the best routes for connecting existing and future internal park trail systems, as well as to identify the most appropriate routes for links to regional systems, as well as to identify the most appropriate routes for links to regional trails that will connect to neighboring community trails (p. 9-26).

Chapter 10: Energy and Environment

Goals and Policies—Air

4. Encourage property owners to plant trees along roadways where possible to help reduce traffic noise and absorb carbon dioxide

Chapter 11: Community Services and Facilities

Environmental Health/Public Health

Goals and Policies:

1. Ensure an Effective Local Government Public Health System.

- Inform, educate and empower people about health issues
 - Mobilize community partnerships and action to identify and solve health problems
 - *Develop policies and plans that support individual and community health efforts*
2. Reduce Behavioral Risks that are Primary Contributors to Morbidity/Mortality.

Behavioral risks that contribute to morbidity and mortality include tobacco use, alcohol and other drug use, *physical activity/inactivity*, nutrition, and *weight management*. A strategy for reducing these behavioral risk factors is:

- *Health education and promotion of health lifestyles and healthy living.*

3. Promote Health for All Children, Adolescents, and their Families.

- Work with the Park and Recreation Department to implement the requirements of the "Fit City" program.

4. Promote Well-Being of the Elderly, Those with Disability, Disease or Chronic Illness.

- Fostering healthful behaviors such as good nutrition, *physical exercise*, medication management, obtaining flu shots, *efforts to reduce isolation and promote mental health* (p. 11-12-13).

Attachment 2: Edina Living Streets Policy Development Workshop



REGULAR TRANSPORTATION COMMISSION MEETING
REPORT/RECOMMENDATION

To: Edina Transportation Commission	Agenda Item No.: <u>VI.E.</u>
From: Wayne D. Houle, PE City Engineer	ACTION:
Date: March 15, 2012	<input type="checkbox"/> Recommendation/Motion
Subject: Living Streets Workshop Recap	<input type="checkbox"/> Discussion
	<input checked="" type="checkbox"/> Information

Info/Background:

This item was continued from the February 16, 2012 ETC Meeting.

ETC Chair and ETC Members who attended the Living Streets Workshop will provide an update of the February 15 Living Streets Workshop. Attached are workshop notes compiled by ETC Chair Janovy. Also attached are the slides from the night-before workshop.

Attachments:

- Complete Street Workshop Notes
- Complete Street Night-before Workshop Presentation

How would your community benefit from a Living Streets policy?

- Safe access to parks, local and 9-mile trail
- Good for business, improve access to non-drivers
- Safety “free range kid” environment
- Age in place
- Quality of life
- Good marketing and image
- Preserve and enhance natural assets
- Connectivity – social and physical
- Creating places to gather
- Improved access to other trails
- Safe routes to schools
- Creative community with competitive edge
- More value for assessment dollars
- Looking at street projects systematically
- Equity of options
- Interconnectiveness of trails to residents
- Multi-use landscaping; environment and beauty
- Redefining streets to include social connections
- Community building
- Increased real estate values
- Model for statewide efforts
- Creating better destinations through more trans mode access
- Healthier places and people
- Reduce transportation costs for school district, less cost for roads
- Creating neighborhood meeting places
- Decision making guidelines
- Better informed community
- Serve all users in an increasingly diverse city

Exercise: decision-making process

- What current documents are relevant to development of a living streets policy?
 - Comprehensive plan, transportation chapter
 - Ordinances
 - CIP
 - Bike plan
 - Assessment policy
 - Green step cities
 - MN DOT standards
 - Local standards
- What is the process used to make decisions now?
 - Start with a study
 - Each street has a different process
 - Notices in advance, neighborhood notices
- Trans Commission and to council
- What in this process is keeping you from creating living streets?
 - Design charettes
 - Public education campaign citywide
 - Walking tour
 - Development process preceding design
 - Vision exercise
 - Baseline policy and informing people
 - Using exceptions to guidelines
 - Identifying stakeholder

3/8/123/8/12

Edina Living Streets Policy Development Workshop Notes

I. What will be the vision behind a new Living Streets policy?

Living Streets balance the needs of motorists, pedestrians, bicyclists, and transit riders in a way that promotes safety and convenience and enhances community identity, economic vitality, and opportunities for active living, better health, and environmental sustainability.

Living Streets:

- Create space for community connections
- Encourage children to walk and bike regularly
- Makes Edina a place where friends and relatives want to visit or walk
- Are tree lined
- Make walking or biking an easy choice
- Fit with land use policy and mixed use developments
- Apply to new projects and reconstruction
- Are a place where you want to walk
- Eliminate barriers to key destinations
- Provide infrastructure that encourages walking and biking in an attractive environment
- Give people options

It was noted that the vision statement should be something that residents aspire to; it should be inspirational; and it should be visual (so people can see it).

2. Which users and modes will be included?

- All modes, all users, and all abilities
- Four priorities:
 - Safe access for all users
 - Streets will vary in character (context sensitive)
 - Transportation network will enhance neighborhoods
 - Will incorporate green management
- Additional attributes:
 - Foster income equity
 - Pedestrian-oriented design
 - Connect people, not just places
 - Foster active lifestyles
 - Be inviting places
 - Support healthy commerce
 - Be sustainable

3a. How will the policy address Living Streets needs in scoping, planning, design, construction, operations, and maintenance?

Living Streets include consideration of all elements of complete streets—all modes and users. Living Streets look to create more value for the investment. Process to include property owners/residents in the design. City provides information as a factual basis for decision making. Residents should be asked for positive identification of wants and goals, not negative “problems.” Funding for Living Streets should come from shared public funds, rather than special assessments. City should pursue budget alternatives to minimize direct costs to residents.

3b. To which types of projects will the policy apply (new construction, reconstruction, resurfacing, restoration, rehabilitation, operations, retrofits, and other maintenance)?

All of the above.

4. What exceptions will the policy make, and how are the exceptions approved? Is there accountability?

- Incorporate FHWA list for exceptions
- Refer to Comp Plan and other approved documents
- Have process that includes study and analysis, staff recommendation, ETC review and City Council approval

5. How will the policy address the needs to create a network for all users? How will it encourage connectivity?

The policy will identify and then help to overcome barriers to active transportation. It was noted that the city should have a policy of not vacating transportation right of way. There was a discussion about identifying existing right of way easements that may not be obvious and considering whether these could be developed for pedestrian and/or bicycle paths.

6. Which roads would be covered by the proposed policy? How will it apply to roads under another agency's jurisdiction?

All roads within the city's jurisdiction (local and collector) will be covered. PUD will give city leverage over private roads. The city can seek to partner with other agencies to influence decisions re: roads not within the city's control.

7. Will design guidelines be specified the policy? If so, which ones? How can it address the need for design flexibility in balancing the needs of all users?

Policy should include a palette of design options for street types and should provide guidelines for minimum design.

8. How will the policy take adjoining land use/context into account?

City will inventory building and zoning codes to bring into agreement with Living

Streets principals and minimum design guidelines. Policy will include a palette of design options for street types and minimum design guidelines, from which developers can choose. Form-based zoning, which has less detail about what uses are permitted and more detail about size, shape, and features.

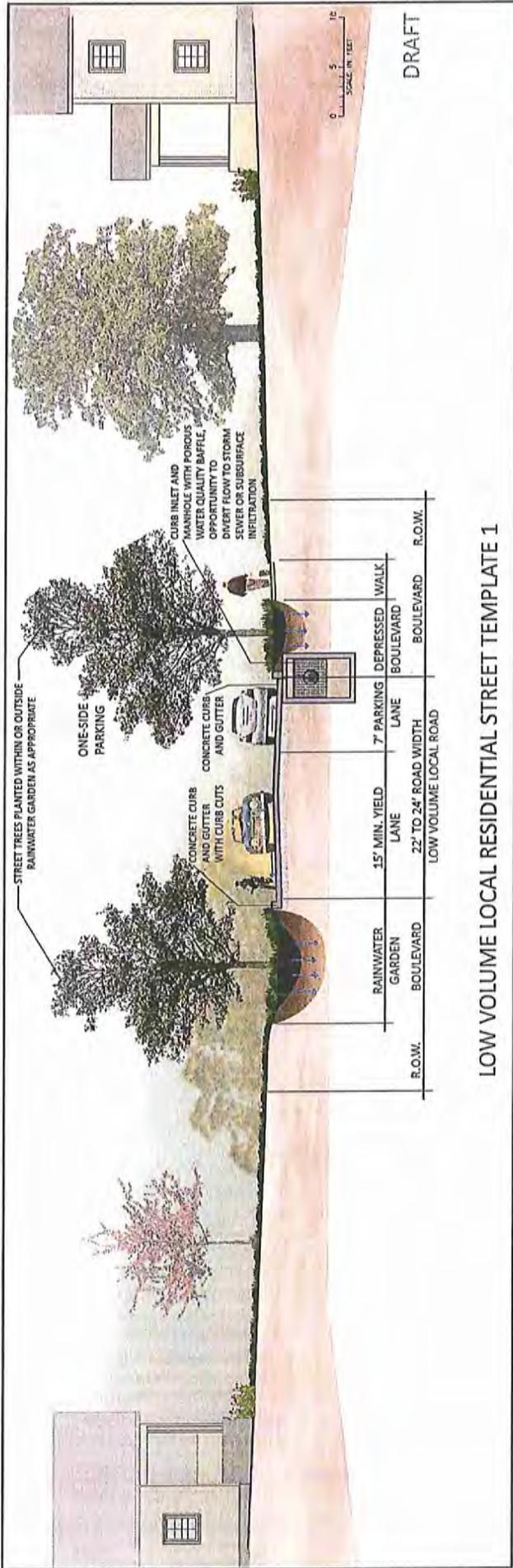
9. How will you know if the policy is working? What performance measures should be considered?

- Percent of bicycle and pedestrian network completed
- Measurement of mode shift
- Surface water management plan—reduction in impervious pavement
- Resident/community satisfaction
- Traffic volumes and speeds

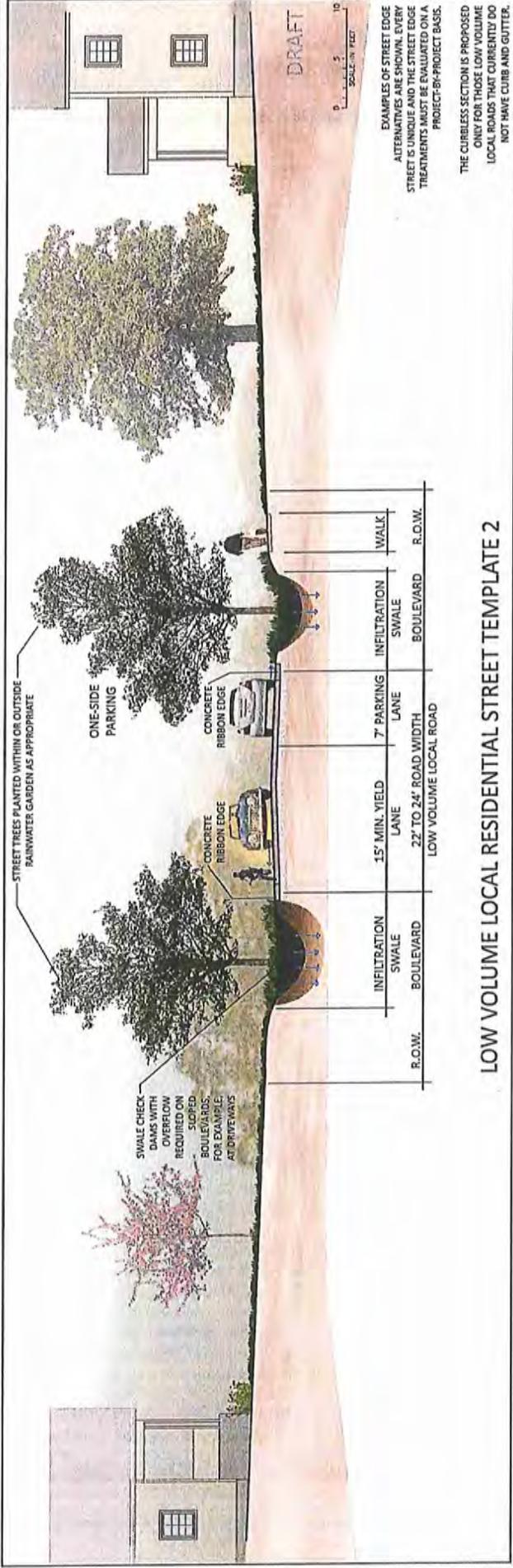
10. What implementation steps will be mentioned in the policy?

- Modify City code
- Review, modify, delete out-dated policies
- Review special assessment policy
- Educate neighborhoods—communicate
- Prioritize projects in CIP—prioritize by modes; prioritize by vulnerability (in other words, look at which users are most vulnerable and address their safety, access, convenience first)
- Develop options for design palette
- Design charrettes
- Seek out additional funding/grants

Attachment 3: Example Living Streets Implementation Templates



LOW VOLUME LOCAL RESIDENTIAL STREET TEMPLATE 1



LOW VOLUME LOCAL RESIDENTIAL STREET TEMPLATE 2

Attachment 4: Proposed Edina Living Streets Planning Process



Edina Living Streets Project – Phase 2 Proposed Work Planning Process Work Plan

As part of Phase 2 of the Edina Living Streets Project, HR Green and Barr Engineering will complete a process resulting in the development of a comprehensive Living Streets Plan for the City of Edina. As described in the Edina Living Streets policy Technical Memorandum developed as part of Phase 1, our multi-tiered approach will focus on understanding the range of users, modes, routes, and destinations within the city, as well as environmental considerations such as stormwater management. Our unique stakeholder oriented process will result in the development of an implementation process which will support the City's Living Street goals.

Task 1: Project Management

The project manager will ensure the delivery of quality products on schedule and on budget, as well as foster a cooperative spirit through strong and continuous communications. Our approach to project management includes weekly staff meetings via conference call with the local project manager and/or the Living Streets Committee chair (as needed), as well as regular Project Management Team (PMT) meetings at critical decision points throughout the study process (see Task 2).

Task 2: Stakeholder Engagement

As ongoing stakeholder engagement is inherent to the success of the Edina Living Streets Plan and implementation of Living Streets in Edina, widespread and ongoing stakeholder engagement is necessary. The study will include a unique approach to public process which follows the same steps as a traditional process, but engages stakeholders differently. The stakeholder engagement process will follow three distinct phases: 1) stakeholder goal-setting, 2) stakeholder development of measures of effectiveness to meet the goals, and 3) stakeholder evaluation of project. The development of goals and measures of effectiveness (phases 1 and 2) will occur as part of the living streets planning process, while the project evaluation (phase 3) would take place as part of the implementation of individual projects and would be used to calibrate the designs to stakeholder expectations. Stakeholder meeting content and format will facilitate stakeholders through the plan development process, leading to the development of a Living Streets Plan calibrated to the unique viewpoint of each modal group and widely supported by stakeholders and users. The following list identifies the key stakeholder groups and public engagement activities proposed:

- Project Management Team (PMT) - The Edina Living Streets Committee formed as part of phase 1 of the Edina Living Streets Project will serve as the Project Management Team (PMT) for the study. The PMT will meet regularly throughout the course of the study to review analysis, guide the overall study process, review input generated by public involvement activities, evaluate deliverables, and approve the Edina Living Streets Plan findings and recommendations. It is anticipated that the PMT will meet at Edina Public Works once per month through the duration of the study.
- Technical Advisory Committee (TAC) – The Technical Advisory Committee (TAC) will be composed of technical staff identified by the PMT. Membership is anticipated to include representatives from various City departments (i.e., public works, community development, etc.), MnDOT, Hennepin County, Three Rivers Park District, local watershed districts, as well as other agency representatives identified by the PMT. The TAC will meet three times at critical study milestones in order to provide technical input on issues and opportunities, review the study proposals and recommendations, and report back to their respective agencies.
- Neighborhood Advisory Committee (NAC) – The Neighborhood Advisory Committee (NAC) will be composed of representatives from key community groups, including residents, local

businesses, community interests, schools, seniors, and members of the PMT. The NAC will meet three times at critical study milestones in order to advise on issues and opportunities, provide input, review the study proposals and recommendations, and report back to their respective groups. NAC meetings will be held at a central location to be determined by the PMT, such as the Edina Community Center.

- Focus Groups – Individual meetings will be held with several important public/private stakeholders, including the business community, public safety (sheriff, police, and other emergency responders), environmental agencies (parks and watershed districts), school district representatives, and utility companies. The purpose of these meetings is to directly engage the key stakeholder groups early in the study process and solicit input on study issues and opportunities.
- Edina Transportation Commission – The Edina Transportation Commission will be the policy-making body governing this study and will be responsible for consideration of study recommendations, approving study products, and implementing recommendations. The study team will conduct up to two (2) meetings with the Edina Transportation Commission, one early and one late in the study process.
- Open House Public Meetings – Two public open houses will be held early and late in the process to provide opportunities for the general public to participate in the study process.
- Project Website – A project website (hosted by the City) will be developed and updated periodically throughout the study process (three-updates) in order to inform stakeholders on upcoming public meetings, provide study updates, and advise the public on key study analyses and recommendations.

Task 3: Existing System Assessment

The intent of this task is to develop a comprehensive understanding of the existing roadway system and infrastructure within the City of Edina, in order to guide prioritization of future investments. The study team will study all relevant documents and materials including the Edina Comprehensive Bicycle Transportation Plan (2007), Edina Comprehensive Plan (2008), and the Edina Comprehensive Water Resources Management Plan Update (2009). This information will be supplemented with additional data (as available), including the following: vehicle, pedestrian, and bicycle traffic volumes; existing and proposed roadways, trails, and sidewalks; existing and proposed transit routes and boardings and alightings data; pedestrian, bicycle, and transit system plans; community comprehensive and land use plans; and local development/redevelopment plans.

This task initiates the stakeholder engagement process, including meetings with the PMT, TAC, NAC, and the general public. The information collected will be vetted and expanded through ongoing stakeholder engagement, leading to a comprehensive understanding of the existing infrastructure system by mode. This task will result in the mapping of the key corridors and the identification of existing gaps, barriers, and opportunities from multiple modal perspectives. As an example, from a pedestrian perspective the creation of an enhanced pedestrian corridor along France Avenue between the 70th Street and 66th Street would provide an opportunity to overcome the barrier created by France Avenue and the cross streets within the area. From a bicyclist perspective on the same corridor, there is a need to provide enhanced connections to the wider city and regional bicycle networks, such as the Edina Promenade and the proposed regional trail along 76th Street. From a modal perspective, project termini will vary (i.e., four blocks for a sidewalk and 0.5 miles for the bike trail).

Task 4: Identification of Street Typologies

The Living Streets Plan must seek to balance the need to move traffic, encourage non-motorized transportation, accommodate differences in neighborhood character, support land use type (residential, commercial, industrial), capture and treat stormwater, expand the urban forest. To balance these

sometimes competing objectives it is important to understand the characteristics of the Edina street system. The information developed in Task 3 will be used to identify a range of street typologies within the city, based on common characteristics (i.e., traffic volumes, importance to the pedestrian and bicycle networks, land use, etc.). These street typologies will include categories for both roadway segment and intersection treatments. This process will expand upon the existing functional classification of the city's streets based on land use and context (i.e., commercial areas, schools, parks, etc.). The resulting street typologies will be used to guide the design of improvements.

Task 5: Design Guidance

The Edina Living Streets Plan will include a range of design guidelines and tools correlated to the Street Typologies described in Task 4. This task will include the development of a toolbox of best practices for pedestrian/bicycle facility improvements, stormwater management techniques, and active living practices, as well as illustrative design templates for the most common street typologies (assumes up to 12 templates). This design guidance will be used to inform the project evaluation process (see Task 6) and is intended to supplement and guide standing practices related to City of Edina streets.

The living streets toolbox and templates are intended to provide design guidance and should be implemented in a flexible manner with specific design elements developed on a project-by-project basis. The goal of this design guidance is to encourage non-motorized transportation, achieve impervious surface reduction and stormwater treatment in the right-of-way, and to create less resource consumptive streets that serve the people of Edina.

Task 6: Project Evaluation Process

The Edina Living Streets Vision sets the stage for a holistic re-invention of the City's street system. In order to implement this vision, the appropriate modal balance and functionality of each street must be evaluated prior to design of the project. In order to facilitate this understanding, the Edina Living Streets Plan will establish a unique project evaluation process. Using the street typologies (Task 4) and design guidance (Task 5) as a starting point, a process to evaluate the context, modal balance, and functionality of each project will be developed.

This checklist style approach generally begins with defining the real problem (i.e., problem statement) being addressed in a given project and understanding the varying characteristics of the project area in terms of roadway segments and intersections. Targeted technical analysis is completed to build this understanding. Included are traditional traffic analyses such as highway level of service (LOS) and safety evaluation, as well as an additional level of analyses to define modal priorities such as multimodal LOS calculations, off-peak analysis, multimodal safety review, and modal travel shed delineation.

A functional analysis is then completed to understand modal priorities and site specific issues such as land use and environmental considerations. This evaluation will allow the project to be calibrated to the unique context of a given location, rather than simply applying traditional design standards. As an example, both France Avenue and Vernon Avenue are functionally classified as A-Minor Arterial routes. As France Avenue provides vehicle access to region retail and job centers, its modal priorities would be very different than Vernon Avenue which serves neighborhood commercial and single family residential uses. Determining the appropriate modal emphasis will allow a project to be designed with the appropriate range of users in mind.

A check list of other contextual considerations will be developed to further refine the project problem statement. Considerations such as jurisdiction, land use, overlapping initiatives (i.e., is the project identified in the Bike or Safe Routes to School Plan?), and stormwater concerns should also be explored in an effort to identify project synergies or "win-win" situations.

This process will culminate in the selection of design elements based on common street typologies (Task 4) and design guidance (Task 5), but also calibrated to the individual project site based on stakeholder expectations, jurisdictional requirements, modal balance, and the functionality. This task will result in the development of a work flow process and a series of checklists to guide project implementation.

Task 7: Final Report

By starting with the end product in mind, the project team will shape the final report throughout the study process. The final report is anticipated to include the following elements:

- Introduction and background
- Existing systems inventory
- Street typologies
- Design guidance
- Project evaluation
- Identification and prioritization of projects

Appendix B

2011 Open House Meeting Letter and Presentation

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Normandale Neighborhood Roadway and Utility Improvements

September 9, 2011

RE: 2013 Roadway and Utility Improvements

Dear Resident:

The City of Edina has initiated the process of roadway and utility improvements for the summer of 2013 for your neighborhood. The first step is to have an open house to familiarize you with the City's process for street reconstruction.

You are invited to attend an open house on Thursday, September 22, 7:00 p.m. at the Public Works and Park Maintenance Facility, located at 7450 Metro Boulevard. The meeting will consist of a short presentation starting at 7:15 p.m. with question and answer to follow. The presentation will cover topics such as "How are streets selected for reconstruction; Who funds the project; Special assessment; What can I expect during construction; How can I stay informed." Please keep in mind that this meeting will not cover project specific details, that meeting will come at a later time.

The exact amount that you will be assessed for this project will not be available in time for the open house; however, based on past projects, you can expect the amount to be between \$6,000 to \$9,000.

To better communicate the overall happenings of the project, the City has introduced the City Extra e-mail notification service. This service is free and allows individuals to sign up to receive e-mail messages from the City on a variety of topics. To sign up for the service go to the City Extra website at <http://cityextra.cityofedina.com> and place a check mark in the box next to your neighborhood project name (**Normandale**). Please contact the City at 952-927-8861 if you are having trouble signing up for City Extra.

If you cannot attend the open house and have questions on the process please contact either Assistant City Engineer Jack Sullivan at 952-826-0445 or email at jsullivan@ci.edina.mn.us or Project Manager Jamie Cynor at 952-826-0440 or email jcynor@ci.edina.mn.us.

Sincerely,

Wayne D. Houle, P.E.
Director of Public Works/City Engineer

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OPEN HOUSE

for

2013 Neighborhood Roadway Improvements

9/22/2012

1



WHY ARE WE HERE?

- For Engineering staff to communicate to residents the reconstruction process directed by Council
- Staff is not here to debate the process but rather to educate and to gather input from residents
- Council determines if a street reconstruction project proceeds

2



AGENDA

Introductions

Road Reconstruction Video

Presentation

1. Process
2. Funding sources
3. Assessments
4. Communication with residents
5. What to expect during construction
6. What can I do to prepare for the project?

Questions

3



TYPICAL TIMELINE

FROM FIRST NOTIFICATION TO END OF CONSTRUCTION

April	Kick-off Informational letter to residents
May	Open House (for preceding two years)
July / August	Neighborhood Informational Meeting – draft project plans and estimated costs are provided
December / January	Feasibility Report and Public Hearing
January / February / March	Plan Preparation / Bidding
April / May	Construction Begins
October / November	Construction Complete
Following Spring	Warranty Work
Following Summer / Fall	Final Assessment Hearing

4



PROCESS FROM START TO END



5



AGENDA – change background to new style??

Introductions

Road Reconstruction Video

Presentation

1. Process
2. Funding sources
3. Assessments
4. Communication with residents
5. What to expect during construction
6. What can I do to prepare for the project?

Questions

6



FUNDING

Special Assessment pays 100% of the following (approximately \$x,000 to \$xx,000 per Residential Equivalent Unit (REU):

- Roadway – replacing entire roadbed
- Sidewalks
- Decorative Streetlights (optional)
- Traffic Management (optional)

7



FUNDING

The **Utility Fund** which covers sanitary sewer, storm sewer and watermain pays 100% of the following:

- Curb & Gutter (will be added or may be replaced)
- Sanitary Sewer – trunk pipe only within the roadway
- Storm Sewer – trunk pipe within roadway and within easements on private property
- Watermain – trunk pipe in roadway up to the curb stop of private property
- Sump Pump Pipe – for residents to connect sump pumps

8



AGENDA

Introductions

Road Reconstruction Video

Presentation

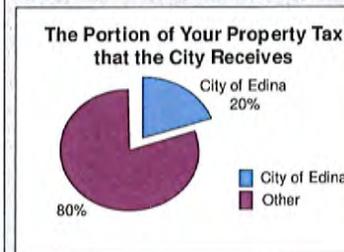
1. Process
2. Funding sources
3. Assessments
4. Communication with residents
5. What to expect during construction
6. What can I do to prepare for the project?

Questions

9



WHAT DO YOUR TAXES PAY FOR?



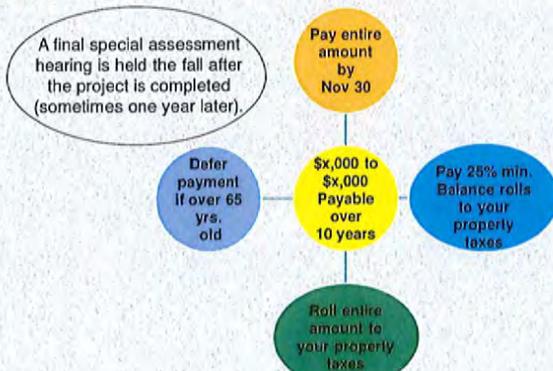
Allocation

- Police
- Fire
- Parks
- Public Works
 - Snowplowing
 - Pothole Repairs
 - Seal coating
 - Other Street Maintenance

10



SPECIAL ASSESSMENTS – PAYMENT OPTIONS



11



AGENDA

Introductions

Road Reconstruction Video

Presentation

1. Process
2. Funding sources
3. Assessments
4. Communication with residents
5. What to expect during construction
6. What can I do to prepare for the project?

Questions

12



COMMUNICATING WITH YOU



Our first contact with you is when we inform you of the informational meeting, followed by public hearings notification; open house; surveys; project plans and schedules; and construction update letters via regular mail. Our last communication to you is the notice of final special assessment hearing.



Public hearings will be published in the *Edina Sun-Current*.



Questionnaires will be sent during the early planning stage to help staff formulate a decision in areas such as street lighting and style of street lighting.



Weekly construction updates will be sent via the "City Extra" email. Stay in the loop by signing up for this free service.



Letters may be delivered to inform you of time-sensitive events such as water shut-offs 24-hrs in advance.

13



HOW CAN YOU GIVE INPUT AND STAY INFORMED?

- www.EdinaMN.gov

Visit our website regularly. Information from open houses and other communications are archived on our website.

- edinamail@EdinaMN.gov

Drop us an email

- 952-826-0371

Call us; office hours are 7:00-3:30 p.m.

- Stop by the office

Public Works and Engineering Departments
7450 Metro Boulevard

- Schedule an appointment

952-826-0371

14



RESIDENT QUESTIONNAIRE

The property owners questionnaire generally gathers information on the following topics:

- Sump Pump Discharge Service Line – do you have one and the location?
- Residential Roadway Lighting – do you want decorative street lights, style and pole location?
- Pedestrian Issues – what are the issues and do you favor sidewalks?
- Private Underground Utilities – do you have underground lawn irrigation and pet containment?
- Local Drainage Problems – have you noticed drainage issues in the neighborhood?

15



AGENDA

Introductions

Road Reconstruction Video

Presentation

1. Process
2. Funding sources
3. Assessments
4. Communication with residents
5. **What to expect during construction**
6. What can I do to prepare for the project?

Questions

16



WHAT CAN YOU EXPECT DURING CONSTRUCTION?

- That the City will keep you informed of activities
- Private utility companies updating their systems prior to City's contractor
- Dusty, noisy, muddy (watering trucks will help to keep dust down)
- May be asked to limit water use

17



WHAT CAN YOU EXPECT DURING CONSTRUCTION?

- Irrigation system and pet containment may be damaged. The irrigation system will not be fixed until the end of the project.
- Prior to construction, residents will be given an opportunity to remove plantings and other landscape features in the boulevard.
- Boulevards will be restored with grass – regardless of previous planting or landscaping.

18



WHAT CAN YOU EXPECT DURING CONSTRUCTION?

- May be connected to temporary water



19



WHAT CAN YOU EXPECT DURING CONSTRUCTION?

- May not be able to access your driveway for up to seven days



20



WHAT CAN YOU EXPECT DURING CONSTRUCTION?

- May not be able to access the roadway for a few hours. Some days it may not be accessible by vehicle. Contractor will accommodate special needs.



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AGENDA

Introductions

Road Reconstruction Video

Presentation

1. Process
2. Funding sources
3. Assessments
4. Communication with residents
5. What to expect during construction
6. What can I do to prepare for the project?

Questions

22



WHAT CAN YOU DO TO PREPARE FOR THE PROJECT?

- Begin financial planning for the assessment
- Coordinate landscaping, driveway replacement and house remodeling projects with the reconstruction schedule
- Complete questionnaire to provide feedback
- Ask questions; inform staff of your concerns
- Sign-up for "City Extra" to receive updates via email notification

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THANK YOU

24



AGENDA

Introductions

Road Reconstruction Video

Presentation

1. Process
2. Funding sources
3. Assessments
4. Communication with residents
5. What to expect during construction
6. What can I do to prepare for the project?

Questions

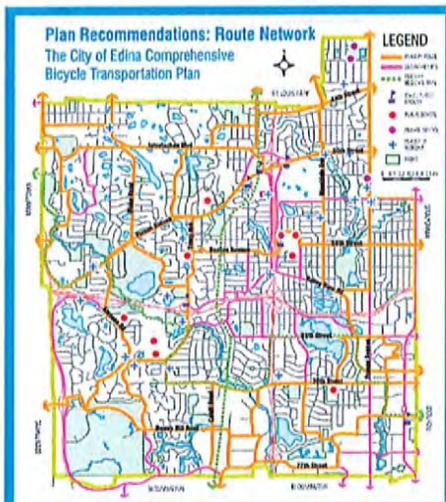
SIDEWALK FACILITIES



BICYCLE FACILITIES



Bike Edina Task Force – Bike Routes



WATERMAIN BREAKS



**SEWER
BLOCKAGES**



**Construction
Photos**



Appendix C

Property Owners Questionnaire

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Resident Questionnaire Instructions

2013 Neighborhood Roadway Reconstruction

Thank you for your time. Your responses to the attached questionnaire will help us design your neighborhood's project. Here is background information that will aid you in filling out the questionnaire. Each numeral relates to the corresponding survey question.

I. Drainage Service Connection

A typical sump pump discharges onto a homeowner's lawn. There are several sump pump drainage issues to look for. First, if your lawn drains back to your house, sump pump discharges can cause problems with your lawn, your neighbor's lawn or your basement. If the sump pump discharge runs down the gutter line, it can promote algae growth in the street. Finally, discharging the sump pump into the sanitary sewer system using floor drains or laundry tubs is against the law, both by City Ordinance and State Statute.

To prevent the issues mentioned above, your street reconstruction project could include a City sump drain system along the roadway to collect groundwater, storm water runoff, and discharges from private sump pumps, roof drains or any other runoff from private property. If the topography and final street designs favor a sump drain system, you could connect to it. That is why survey questions I.C. and I.D. ask about your sump pump preferences. Keep in mind that installation of the pipe from your house to the City sump drain system would be your responsibility, including plumbing modifications connection. However, the City sump drain system is funded through the storm sewer utility fund.

II. Local Drainage Problems

As part of the storm sewer and sump drain design process, we would like to know if storm water run-off stands in the street or sidewalk in front of your house. If this or similar situations are occurring in your area, please describe it in this section of the questionnaire. We will review for possible corrective action.

III. Private Underground Utilities

It is very important that you fill out this section. Some residents install private underground utilities in the roadway right-of-way (the area from the edge of the roadway to your property line). The most common private utilities include lawn irrigation and pet containment systems. Utility and roadway reconstruction can damage these utilities. If they are damaged, they will be restored by the City only if you indicate that you have these systems on your returned questionnaire. Additionally, if the contractor knows the location of these private utilities, crews can attempt to avoid damaging them during construction.

IV. Residential Streetlights

As part of all reconstruction projects, staff typically asks if residents favor upgrading their streetlight system. In some cases, the streetlight system is at the end of its useful life and requires upgrading. We do not know yet the condition of your specific streetlight system; an evaluation will be done later on. Funding for streetlights are currently through special assessment.

In the residential streetlight option section of the questionnaire are examples of the different styles of streetlighting that could be installed in your neighborhood. Please rank your style preferences. Please note that ranking your style preferences does not mean you are in favor of streetlights.

V. Pedestrian Issues

As part of all reconstruction projects, staff typically asks if residents see a need to add sidewalks in the neighborhood. Funding for sidewalks is split between special assessment, City of Edina and/or Edina Public Schools, depending where the sidewalk is located.

Please note if you know of any pedestrian issues such as a missing segment of sidewalk or an inadequate pedestrian crossing.

VI. Traffic Management

The Edina Transportation Commission typically reviews traffic issues on local streets. We would like to know if you feel that your roadway has any traffic issues.

VII. Construction Updates (Time Sensitive)

During construction, notifications of time sensitive work (including work that makes driveways inaccessible) is typically provided using door hangers, which are usually placed at your front door. Is there another location that works better for you?

VIII. Email Updates

One of the primary tools for communicating with you during construction is the City Extra email notification service. The City Extra service is free and allows you to sign up to receive email messages from the City regarding this project.

By signing up for City Extra email notification service, you will receive project updates as they occur. The updates will include information such as when access to your driveway might be limited, when your water may be shut off for water main replacement and when to have your contractor repair your irrigation system if it was damaged during construction.

If you do not have email access, please indicate “no” on the questionnaire and we will mail you updates that are sent out via City Extra.

To receive email updates, sign up online at http://www.edinamn.gov/index.php?section=cityextra_signup. Enter your email address and a password (new user will need to create a password). Click on **email subscriptions**. Scroll down the page until you see your neighborhood project name (**Normandale Neighborhood Roadway Reconstruction**). Place a check mark in the box next to it. Click the “**update**” button at the bottom right hand corner of the webpage.

Need Help?

If you have any questions about how to fill out the questionnaire, please contact Assistant City Engineer Chad Millner at 952-826-0318 or cmillner@EdinaMN.gov or Engineering Technician Jamie Cynor at 952-826-0440 or jcynor@EdinaMN.gov.



Resident Questionnaire

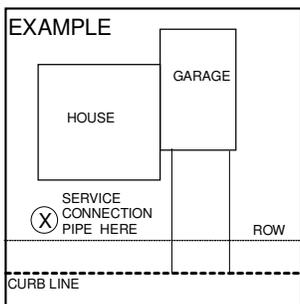
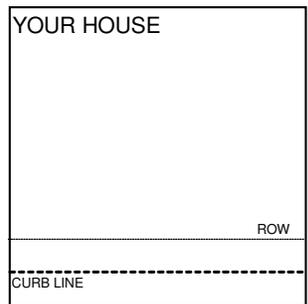
Normandale Neighborhood Roadway Reconstruction

Thank you in advance for your time. Your input is important to us. Please read the Questionnaire Instructions before completing this questionnaire.

I. Drainage Service Connection:

- A. Does your home have a drain tile/footing drain? Yes No Unknown
- B. Does your home have a sump pump? Yes No Unknown
- C. Would you be willing to connect your sump pump up to a City drain if provided (at your own cost)? Yes No
- D. Would you be willing to connect your roof drains up to a City drain if provided (at your own cost)? Yes No

Please sketch in the space to the right: your house, garage, driveway, sump pump discharge location and approximately where along the right-of-way (ROW) line you would like the service connection pipe located.



II. Local Drainage Problems

Please describe specific surface water drainage problems in your neighborhood:

III. Private Underground Utilities

- A. Do you have an underground lawn irrigation system in the City's right-of-way? (The right-of-way is typically 10' to 15' behind the roadway.)
 Yes No
- B. Do you have an underground electric pet containment system in the City's right-of-way?
 Yes No

IV. Residential Streetlights:

A. Do you favor upgrading your streetlights?

Yes

No

B. If the streetlights are upgraded, which style do you prefer? Please rank all the styles from 1 to 5, with 5 being most liked. Please rank even if you answered “No” to IV. A.

Acorn:

Style can be viewed along Concord Ave from Valley View Rd to South View Ln, Edina.



Dislike Like
1 2 3 4 5

Coach:

Style can be viewed in the Sunnyslope neighborhood, just north of Edina City Hall, Edina.



Dislike Like
1 2 3 4 5

Arlington Lantern:

Style can be viewed in the Country Club neighborhood, just north of W. 50th St along Wooddale Ave, Edina.



Dislike Like
1 2 3 4 5

Round Lantern:

Style can be viewed at County Road 101 and W. 78th St North, Maple Grove.



Dislike Like
1 2 3 4 5

V. Pedestrian Issues:

A. Do you see a need to add sidewalks in your neighborhood?

- Yes No

B. If yes, where? _____

C. Please describe specific neighborhood pedestrian issues below.

VI. Traffic Management

A. Do you feel your neighborhood or roadway has any traffic issues?

- Yes No

B. If yes, what is it and where does it occur?

VII. Construction Updates (Time Sensitive)

A. Where would you prefer to receive paper door hanger updates?

- Front Door Other

B. If other, please explain.

VIII. Email Updates

A. Do you have access to email to participate in the City Extra email notification service?

- Yes No

Thank you for completing the questionnaire. Please return it to the City in the enclosed self-addressed, stamped-envelope **by July 30.**

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Appendix D

Property Owners Questionnaire Results No. 1 and No. 2

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**NORMANDALE
NEIGHBORHOOD ROADWAY RECONSTRUCTION
RESIDENT QUESTIONNAIRE**

Data Entered By: _____
 Last Date Data Entered: _____
 Due Date: _____
 Questionnaires Sent Out: 83
 Questionnaires Returned: 52
 Percent Returned: 63%

House #	Street Name	Returned Survey	Sump Pump Discharge Service Line						Drainage				Private Underground Utilities				Residential Roadway Lighting Option					Sidewalk		Pedestrian Issues		Traffic Management			Construction Updates (Time Sensitive)		Email Updates								
			Draintile or Footing Drain			Has a Sump Pump			Willing to Connect		Local Drainage Problems		Irrigation sys. in		Pet containment		Favors Upgrading		Preferred Styles			Yes	No	Construct new sidewalks where?	Specific Neighborhood Pedestrian Issues	Yes	No	If yes, where?		Front Door	Other	Yes	No						
			Yes	No	Unknown	Yes	No	Unknown	Yes	No	Yes	No	Yes	No	Yes	No	Yes	No	Acorn	Coach	Arlington Lantern	Round Lantern	Yes	No			Yes	No											
6501	Ryan Ave	1		1		1			1	1			1		1	1	5	3	2			1				1				1									
6500	West Shore Dr	1		1		1			1	1				1								1		(Streetlights comment - "No!", sidewalk comment - "No!")	There is a paved walkway across the street in the park that parallels the street. No sidewalk is needed.							1		On the mail box by the front door	1				
6500	Ryan Ave	1		1		1			1	1				1		4	2	2	4	1			Front of house	No sidewalk															
6501	Parnell Ave	1		1		1			1	1				1								1													(city extras comment: "but I won't use it")				
6400	Parnell Ave	1		1		1			1	1				1		3	2	5	4	1		1	N/A	N/A			N/A												
6321	Parnell Ave	1	1			1			1	1				1	1	3	5	5				1																	
6509	Sherwood Ave	1		1		1			1	1				1	1	4	2	2	2			1																	
6324	Parnell Ave	1	1			1			1	1				1		2	4	3	5			1																	
6325	Parnell Ave	1		1		1			1	1				1		5	4	3				1														Exiting our neighborhood onto 66th Street is difficult in AM/PM since 70th street speed was reduced to 25 mph. A stoplight is needed.			
6516	Sherwood Ave	1		1		1			1	1				1								1																	
6433	Ryan Ave	1		1		1			1	1				1		5	1	1	1			1		(lighting comment: "Existing 65th/Ryan light hangs on utility pole is adequate")															
6433	Sherwood Ave	1		1		1			1	1				1	4	4	1	3				1																	
6325	Ryan Ave	1		1		1			1	1				1	1	3	1	5				1			There is NO pedestrian traffic as there are only 2 houses on each side of this dead-end street.														Neighborhood borders Crosstown 62. Traffic is constant and excessively loud. Personal hearing has been affected as a result of perpetual noise.
6517	Sherwood Ave	1		1		1			1	1				1	1	2	4	4				1															prefer email, front door if necessary		
6424	Parnell Ave	1		1		1			1	1				1	1	1	5	4				1																	
4717	64th St W	1		1		1			1	1				1								1																	
6526	Parnell Ave	1		1		1			1	1				1								1																	
6412	West Shore Dr	1		1		1			1	1				1	1	1	5	1				1																	
6525	Sherwood Ave	1		1		1			1	1				1								1																	
6517	Parnell Ave	1	1			1			1	1				1	1	4	4	2	3			1																	
6533	Sherwood Ave	1		1		1			1	1				1		5						1																	
6324	West Shore Dr	1	1			1			1	1				1								1																	
6432	Parnell Ave	1	1			1			1	1				1	3	4	1	3				1																	
6332	Parnell Ave	1		1		1			1	1				1	2	4	4	2				1			none														
6424	Sherwood Ave	1		1		1			1	1				1								1																	
6425	Ryan Ave	1		1		1			1	1				1	4	4	2	2				1																	
6508	West Shore Dr	1	1			1			1	1				1								1																	
6409	Parnell Ave	1		1		1			1	1				1								1		(crossed off all of the street light pictures)															
6512	Ryan Ave	1	1			1			1	1				1	1	5	2	2				1																	
6333	Ryan Ave	1		1		1			1	1				1	1	3	4	5				1																	

**NORMANDALE
NEIGHBORHOOD ROADWAY RECONSTRUCTION
RESIDENT QUESTIONNAIRE**

ADDRESS	Returned Survey	Sump Pump Discharge Service Line						Drainage				Private Underground Utilities				Residential Roadway Lighting Option						Pedestrian Issues			Traffic Management			Construction Updates (Time Sensitive)		Email Updates				
		Draintile or Footing Drain			Has a Sump Pump			Willing to Connect		Willing to Connect		Local Drainage Problems		Irrigation sys. in		Pet containment		Favors Upgrading		Preferred Styles		Sidewalk		Construct new sidewalks where?	Specific Neighborhood Pedestrian Issues			Front Door	Other	Yes	No			
		Yes	No	Unknown	Yes	No	Unknown	Yes	No	Yes	No	Yes	No	Yes	No	Yes	No	Acorn	Coach	Arlington Lantern	Round Lantern	Yes	No			Yes	No					If yes, where?		
6509 Ryan Ave	1			1		1						1		1		1	1						1	(streetlights comment: "Streetlights are only located one to a corner (intersection). I think we could use more lights in the middle of the blocks. The current design is just fine.")	None. Our neighborhood is a low traffic area		1			1				
6404 West Shore Dr	1		1			1				1				1		1	1	1	2	1	1		1	We don't have any problem (sump connection comment: "we don't have one")	The only issue is when pedestrians walk their dogs and let them run in our yard. The should be kept on the walkway across the street.	1				1				
6501 Sherwood Ave	1		1			1				1				1		1	1						1	Standing water in the street directly across from driveway in areas previously patched/repared			1			1				
6432 West Shore Dr	1	1				1				1				1		1	1			5			1	The only problem we have had is when leaves and debris collect from trees in the park and local residents between 66th and 65th don't clean out the drains. The overflow comes on down the street in those infrequent instances.	there is already a sidewalk/walking path in the park across the street from our home.		1			1				
6508 Parnell Ave	1	1				1				1				1		1	1						1	none			1			1				
6317 Parnell Ave	1		1			1				1				1		1	1	1	5	1	1		1	None that are known to me.		1				1				
6409 Parnell Ave	1		1			1				1				1		1	1	4	3	2	5		1	N/A			1			1				
6517* Ryan Ave	1		1			1				1				1		1	1	1	5	2	3		1	Please note- heavy storm water flows over curb and drains over lawn and retaining wall. Storm drain in street is easily blocked by waste in street, such as, leaves, seeds, dirt, silt, etc. A dam forms around storm drained. Also, we do have an astroturf front lawn that was professionally installed. The area will need special attention and proper reconstruction.			1			1				
6516 Parnell Ave	1	1				1				1				1		1	1	5	4	2	3		1				1			1				
6432 Ryan Ave	1		1			1				1				1		1	1	5	4	3	2		1		none		1			1				
6500 Parnell Ave	1		1			1				1				1		1	2	5	3	4			1	Water collects on the street in front of our house during spring thaw and refreezes. Hopefully this project will provide better drainage to nearby storm sewers. (irrigation comment: "(future plan)")	(street lights comment: "please note: the wire feeding our street light crosses our property outside the utility easement. We ask to have the service buried and/or confined to the easement.		1			1				
6416 Parnell Ave	1		1			1				1				1		1	2	5	3	4			1				1			1				
6532 West Shore Dr	1	1				1				1				1		1	3	3	5	2			1				1			1				
6400 Sherwood Ave	1		1			1				1				1		1	4	3	1	1			1		none		1			1				
6417 Parnell Ave	1		1			1				1				1		1	1	3	3	5			1				1			1				
6516 West Shore Dr	1	1				1				1				1		1	2	1	4	4			1	In the spring after snowmelt we get standing water on south side between our garage and neighbors garage. In winter, it is often an ice sheet. It collects from surface runoff including neighbors lawns to the west, which are higher.	(street lights comment: "We have no streetlights and we like it that way. It is better for animals and we are across from a preserve.")	W. Shore Drive has lots of walking/running/biking/baby stroller traffic. It is awkward and dangerous near 66th Street where it curves and cars are turning. Not sure how to solve, but would be good to have better way to access beach and path 1/2 block north of 66th St. Especially dangerous for young bike riders who are not allowed to ride on the path because of the ridiculous signs that say "No Bikes"		1			1			
6408 Sherwood Ave	1		1			1				1				1		1							1	None	None		1			1				
6525 Parnell Ave	1		1			1				1				1		1	1						1	None - everything works great! All we need is some tar and gravel update.	None		1			1	email			
4860 64th St W	1		1			1				1				1		1	1	1	5	2			1	The drain directly across from my driveway backs-up - does not drain properly	comment from bottom of page: "If we are to pay for a portion of this improvement - I would either suggest going to one garbage service or get them to put some \$ towards the damage they cause to our streets. They also drive through the neighborhoods at high speeds - very unsafe :(!	The lack of stop signs seems to give residents the go-ahead to take the 90 degree corners at 40 mph without looking - I have lived here for just over a year and already had several near misses with these crazy drivers - Not a great neighborhood to let your child walk around the corner to a friends house.		1			1			
6409 Ryan Ave	1		1			1				1				1		1	4	4	4	4			1				1			1	By driveway			
6425 Sherwood Ave	1		1			1				1				1		1	1	2	2	4			1	I'm not aware of any	There is too much car traffic on in our neighborhood		1			1				
6416 Ryan Ave	1		1			1				1				1		1	1	1	1	1			1				1			1				
																		</																

NORMANDALE NEIGHBORHOOD ROADWAY RECONSTRUCTION RESIDENT QUESTIONNAIRE

Data Entered By: _____
 Last Date Data Entered: _____
 Due Date: _____
 Questionnaires Sent Out: 83
 Questionnaires Returned: 52
 Percent Returned: 63%

House #	Street Name	Returned Survey	Residential Roadway Lighting Option						Pedestrian Issues			Comments
			Favors Upgrading Street Lights		Preferred Styles				Sidewalk Need		Construct new sidewalks where?	
			Yes	No	Acorn	Coach	Arlington Lantern	Round Lantern	Yes	No		
6432	West Shore Dr	1	1		2	5	1	2		1		I would favor additional mid-block lighting.
6333	Ryan Ave	1	1		1	4	1	5		1	We do <u>not need</u> sidewalks as we have very little traffic in this neighborhood.	
6432	Ryan Ave	1		1	5	4	3	2		1	<u>Absolutely not</u>	
6501	Ryan Ave	1		1	1	5	3	4		1		
6325	Parnell Ave	1	1		1	5	4	3		1		I appreciate the extended repayment terms. I am losing my job and have not had a pay raise in years. I was afraid the increase in taxes would require me to sell my home of 22 years. The five year extension helps greatly. Thank you!
6408	Parnell Ave	1	1		4	2	5	3	1		On the streets going to park.	
6433	Ryan Ave	1		1	1	1	1	1		1		Cost of 2.90/month, not an issue the neighborhood does not need it. Also the "outdate" 4 "classic" options for lights have not had tour planners input a more contemporary light would be a good addition.
6525	Sherwood Ave	1	1		1	5	1	3		1		
6509	Parnell Ave	1		1	1	1	1	1		1	Not even maybeno.	\$6k for 2" of Asphalt? I'll do it myself.
4717	64th St W	1		1						1		
6417	Ryan Ave	1	1		5	4	2	2		1		
6320	West Shore Dr	1			1	1	3	5		1		
6533	Sherwood Ave	1	1		5	1	3	4	1		West side of street	
6404	West Shore Dr	1		1	1	3	2	3		1		We appreciate our neighborhood as it is. If you really want to improve the neighborhood take a look at the houses that let them run down. Our neighbors roof is ready to fall in. The garbage is never picked up, the shrubs are just over grown weeds. It's a mess! The affects our property value.
6425	Ryan Ave	1		1	2	4	2	1		1		I have lived on this street more than 50% of my life >25 years. No sidewalks or additional lighting is necessary.
6432	Parnell Ave	1		1	3	4	3	3		1		
6500	West Shore Dr	1		1						1		
6412	West Shore Dr	1			1	1	5	1		1		Sidewalks reduce areas for precipitation to be absorbed. Sidewalks increase homeowner's need to shovel and the city's need to maintain. Both are undesirable. (Streetlights comment: "I don't care would it save energy?")
6401	Sherwood Ave	1	1		2	4	3	5				
6508	Ryan Ave	1		1						1		
6409	Ryan Ave	1	1		4	3	3	4		1		
6516	Sherwood Ave	1	1		3	4	4	4		1		
6324	West Shore Dr	1		1						1	"Double" Emphatically No! (street lights: "Emphatically No!")	This neighborhood does not lend itself to sidewalks. The only one (north side of 66th St (West Shore to HWY 100)) is just fine unless it needs to be replaced
6517	Parnell Ave	1	1		1	5	1	1		1		
6409	Parnell Ave	1		1						1	No sidewalks - I'm not cleaning them off in the winter	
6433	Sherwood Ave	1		1	5	4	1	3	1		Going down the steep hill on W 64th Street	

**NORMANDALE
NEIGHBORHOOD ROADWAY RECONSTRUCTION
RESIDENT QUESTIONNAIRE**

ADDRESS	Returned Survey	Residential Roadway Lighting Option							Pedestrian Issues		Comments	
		Favors Upgrading Street Lights		Preferred Styles				Sidewalk Need				
		Yes	No	Acorn	Coach	Arlington Lantern	Round Lantern	Yes	No	Construct new sidewalks where?		
6324	Parnell Ave	1		1	3	4	2	5		1		
4841	64th St W	1		1	1	4	4	3		1		More bike paths
6424	Parnell Ave	1		1						1		
6508	West Shore Dr	1		1						1		
6325	Ryan Ave	1	1		1	1	1	5		1	Not needed cul-de-sac & dead-end street.	
6400	Parnell Ave	1		1	1	1	1	1		1	N/A	
6332	Parnell Ave	1	1		5	5	4	2		1		I like the proposed changes in lighting.
6424	Sherwood Ave	1								1		No sidewalks - in low traffic areas people walk on the road anyway.
6525	Parnell Ave	1		1	3	1	3	5		1	In a cul-de-sac. No need	We really only need a street layover sewer & water is not that old. Sewer & waterline run really different on 65th & Parnell to Cul-de-sac. Some home of back of homes. How does that work for \$\$\$\$ assessment to each home. (streetlights: "maybe one at 65th & West Shore Dr. Maybe one at 65th & Parnell")
6501	Sherwood Ave	1	1		5	3	4	2		1		
6416	Sherwood Ave	1	1		5	3	1	2		1		
6416	Ryan Ave	1		1	1	1	1	1		1		
6501	Parnell Ave	1		1	2	3	4	1		1		
6532	West Shore Dr	1	1		1	4	5	3		1		We need lights, too many suspicious cars sit in the darkness all hours of the night!!! (street lights comment: "Too many strange cars sit in the darkness! They drink, smoke pot and who knows what else!")
6517	Ryan Ave	1		1	1	5	2	3		1		
6417	Sherwood Ave	1	1		1	5	4	2		1		
6512	Ryan Ave	1	1		1	5	4	1		1		
6408	Sherwood Ave	1		1	1	1	4	4		1		
6417	Parnell Ave	1			1			5		1		
6526	Parnell Ave	1		1						1		
6516	West Shore Dr	1		1	1	1	4	2	1		West Shore Dr. between 66th and access to walking path. Maybe a walking path on road if there was no parking on east side of W. Shore up by 66th St.	streetlights: "We don't have any & we don't want them."
6508	Sherwood Ave	1		1	1	5	4	3	1		Odd side of the street	
6321	Parnell Ave	1	1		2	4	5	3		1		
6509	Ryan Ave	1		1	1	1	1	1		1		The trees near the cobra lights at intersections need trimming so illumination is not obstructed. (streetlights comment: "The Cobra head lights work fine at intersections could the brightness (# of lumens) be increased somewhat or the trees trimmed around the lights?")
6317	Parnell Ave	1	1		1	5				1		Would nice to get a sound wall along Hwy. 62.
6412	Sherwood Ave	1	1				5		1		Sherwood & 66	
		52	21	27	89	132	115	114	6	45		

Surveys Sent	83	63%	25%	33%	107%	159%	139%	137%	7%	54%
Surveys Returned	52	100%	40%	52%	171%	254%	221%	219%	12%	87%

Appendix E

September 5, 2012 Normandale Neighborhood Informational Meeting

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July 13, 2012

2013 Neighborhood Roadway Reconstruction Normandale Neighborhood

Dear Resident:

Some streets in your neighborhood are on a list of roadway reconstruction and utility improvement projects being considered by the City of Edina for the summer of 2013. See the attached map identifying your project area.

Please save the date of **Wednesday, Aug. 8** to attend an informational meeting from 6 to 8 p.m. to learn information about how projects are funded, a typical construction timeline, how you will be impacted and how you can prepare. A meeting reminder will be mailed to you approximately two weeks prior.

Meantime, we'd like to hear from you. The City would like your input regarding key components of the project via the attached questionnaire. Please read the instructions, fill out the questionnaire and return it to us in the enclosed envelope by July 30.

How the City will use your input:

- Your responses help us design the project. Components of a project vary and are based on both the condition of the infrastructure and questionnaire responses.
- Residents pay a portion of the overall project cost in the form of a special assessment. The estimated special assessment for your neighborhood will not be determined until information is gathered from the questionnaires and a feasibility report is completed in early September. You will not be billed for the special assessment until fall 2014. The special assessment is payable over 10 years.
- The special assessment typically includes cost of the new roadway, sidewalk and streetlights. However, sidewalks and streetlights are not included with every project. The questionnaire helps us evaluate need for these items. Other utility upgrades such as water main, sanitary sewer, storm sewer and concrete curb and gutter are funded through the utility fund and are not assessed to property owners.

After we review questionnaire responses, we will continue the project planning process. We will present detailed project plans in October or November, followed by a public hearing in December or January. Construction will begin in spring/early summer and end in late fall of 2013.

If you have any questions, please contact me at 952-826-0318 or cmillner@EdinaMN.gov or Engineering Technician Jamie Cynor at 952-826-0440 or jcynor@EdinaMN.gov.

Sincerely,

A handwritten signature in black ink, appearing to read "Chad Millner", with a stylized flourish at the end.

Chad Millner, PE
Assistant City Engineer

Enc: Project Map, Questionnaire Instructions, Questionnaire, Return Envelope

ENGINEERING DEPARTMENT

7450 Metro Boulevard • Edina, Minnesota 55439
www.EdinaMN.gov • 952-826-0371 • Fax 952-826-0392

PROJECT LIMITS

6309	6308	6309	6308
6313	6312	6313	6312
		6317	6316
		6321	6320
			6324



6601

4905

6604

4809

6600

6601

4705

6600

6601

6605



Project Area

Normandale Neighborhood

Roadway Reconstruction

Improvement No: BA-394



Engineering Dept
July, 2012



August 2, 2012

2013 Neighborhood Roadway Reconstruction
Normandale Neighborhood
Informational Meeting Postponed

Dear Resident:

As you know, your neighborhood is on a list of roadway reconstruction and utility improvement projects being considered by the City of Edina for the summer of 2013. Recently you were sent a questionnaire that will assist in determining the key components of the project. Within that questionnaire letter and on a mailed postcard was a notice for an informational meeting scheduled for Wednesday, Aug. 8. This meeting date for your neighborhood had been postponed.

City staff workload is such that we are unable to complete the design of your neighborhood within the necessary timeframe. Therefore we have decided to use Short Elliot Hendrickson, Inc. (SEH) to complete the preliminary design of your neighborhood. SEH has successfully conducted engineering services for many Edina neighborhood roadway reconstruction projects.

Please tentatively save the date of Wednesday, Sept. 5 to attend an informational meeting from 6 to 8 p.m. to learn information about the project. A meeting reminder will be mailed to you approximately two weeks prior.

If you have any questions, please contact me at 952-826-0318 or cmillner@EdinaMN.gov or Toby Muse, SEH Project Manager at 952.912.2616 or tmuse@sehinc.com.

Sincerely,

A handwritten signature in black ink, appearing to read "Chad Millner".

Chad Millner, PE
Assistant City Engineer

G:\PW\CENTRAL SVCS\ENGR DIV\PROJECTS\IMPR NOS\BA394 Normandale 2013\ADMIN\CORR\LETTS & EMAILS\Info Mtg Change_Letter.docx

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Normandale Neighborhood Roadway
Reconstruction Informational Meeting
Reminder

September 5, 6-8 p.m.

Public Works & Park Maintenance Facility

7450 Metro Blvd, Edina

Join us to learn more and to provide input.



City of Edina
Engineering Department
7450 Metro Blvd
Edina, MN 55439

**Best way to keep up with project information is City Extra emails. Sign up
now at http://www.edinamn.gov/index.php?section=cityextra_signup**

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2013 Normandale Neighborhood Roadway Improvements BA-394 Informational Meeting

September 5, 2012

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Agenda

- Introductions
- Project Location
- Process & Timeline
- Project Details
- Funding
- What You Can Expect
- Communication Tools
- How to Prepare
- Q&A

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Introductions

SEH Staff

Project Engineer
Carter Schulze



Project Manager
Toby Muse



Sr. Project Manager
Paul Pasko



City Staff

Asst. City Engineer
Chad Millner



Office Manager
Sharon Allison



Environ. Engr.
Ross Bintner



Director of Engineering
Wayne Houle



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Project Location



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Process



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Typical Timeline

August/September	Feasibility report and estimates provided
December/January	Public hearing
January-March	Plan preparation and bidding
April/May	Construction begins
October/November	Construction concludes
Spring 2014	Warranty work
Summer/Fall 2014	Final assessment hearing

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Existing Roadway Details

- 89 Properties
- 82 Privately Owned
- 7 City Owned
- 1.4 miles of roads
- 21,800 square yards of street pavement

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Existing Roadway Details

- Streets constructed in the 1960's
- Streets have curb and gutter
- Existing street widths are 29-ft to 30-ft

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Existing Roadway Details

- Landscaping placed in the right-of-way
- Irrigation systems & pet containment fences in the right-of-way

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Existing Roadway Details

- Varied driveway materials.

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Existing Utility Details

- Storm Sewer

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Existing Utility Details

- Trunk Sanitary Sewer and Water Main Pipes

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Existing Utility Details

- Trunk Water Main Pipe

Legend

- Water Main Break
- Water Service Break
- Existing Trunk Water Main

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Existing Utility Details

- Trunk Sanitary Sewer Pipe

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Existing Utility Details

- Water and Sanitary Sewer Service Pipes

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Why My Street?

- Streets that meet specific standards are subject to reconstruction.
- Priority is given to streets with the highest need based on watermain breaks, sanitary sewer deficiencies, storm sewer/drainage issues, and Pavement Condition Index.
- Reconstruction is usually more cost-effective in the long-term than patching or seal-coating.
- Streets are grouped together to help prolong pavement life and maximize the economics of scale for construction.

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What is Included?

- Always included:
 - Roadway – replacing the entire pavement section
 - Curb and gutter – all or pieces
 - Utility upgrades
- Sometimes included:
 - Sump pump drainage system
 - Sidewalks
 - Streetlights
 - Traffic management
- Full project scope is based on the condition of the infrastructure and resident questionnaire responses

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Questionnaire Results

Neighborhood	% of Questionnaires Returned	Has a Sump Pump		Change Existing Lighting		Prefer New Sidewalk	
		Yes	No	Yes	No	Yes	No
Normandale	63% (52 / 83)	7 (13%)	43 (83%)	21 (40%)	27 (52%)	6 (12%)	45 (87%)

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Proposed Roadway Improvements

- Remove and replace pavement surface
- Spot repair of curb and gutter



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Proposed Roadway Improvements

- Driveway aprons and end replacements



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Proposed Utility Details

- Storm Sewer Upgrades

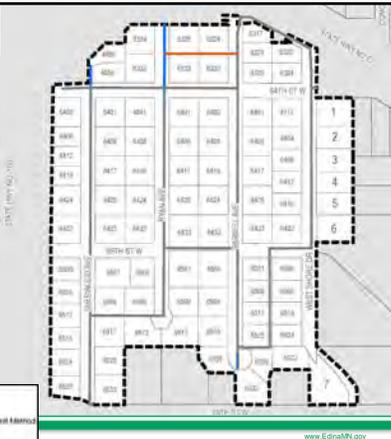


www.EdinaMN.gov

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Proposed Utility Details

- Trunk Water Main Pipe



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Proposed Utility Details

- Temporary Water System



www.EdinaMN.gov

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Proposed Utility Details

- Trunk Sanitary Sewer Pipe



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CITY OF EDINA

Proposed Utility Details

- Water Service Pipes

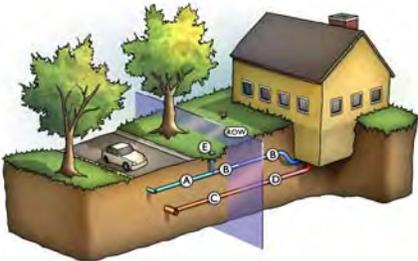


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Proposed Utility Details

- Sanitary Sewer Service Pipes



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Proposed Sanitary Sewer Service Improvements

- Homeowner can reconstruct segment C using their own contractor and
 - If the work is done per City code, you have the option to add the cost to your street assessment, or
- The City will reconstruct segment C using its contractor and the cost is automatically added to the cost of your street assessment.

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Funding

- Projects are funded by a combination of special assessments to residents and the City's Utility Fund
- Assessments are assigned to adjacent properties that stand to benefit from construction improvements

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Funding

- Special assessments to residents cover 100 percent of roadway costs.
- Sidewalks and streetlights are not included in special assessments.

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Funding

- Utility Fund covers 100 percent of:
 - Concrete curb and gutter
 - Sanitary sewer
 - Storm sewer
 - Water main
 - Sump pump pipe
- The Utility Fund is a collection of utility service charges paid to the City

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Other Impacts

- Our goal is to streamline projects and minimize neighborhood disturbance.
- The City encourages private utility companies (gas, electric, telephone and cable TV) to upgrade or repair utilities along the project area.

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Do Taxes Cover Street Projects?

- Roughly 20 percent of your property taxes go to the City for expenses such as Police, Fire, Parks and Public Works (snowplowing, pothole repairs, sealcoating, and other street maintenance).
- Your taxes do not pay for street reconstruction.

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Preliminary Assessments



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Preliminary Assessments

Neighborhood	Estimated Assessment per REU	# of REU's
Normandale - Street	\$12,300	89
Normandale - Sanitary Sewer Service	\$3,800	70
Total Preliminary Assessment	\$12,300 - \$16,100	

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Proposed Sanitary Sewer Service Improvements

- Homeowner can reconstruct segment C using their own contractor and
 - If the work is done per City code, you have the option to add the cost to your street assessment, or
- The City will reconstruct segment C using its contractor and the cost is automatically added to the cost of your street assessment.

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Preliminary Assessments City of Edina 2013 Projects

Neighborhood	Estimated Assessment Range per REU	# of REU's	SQ Yards of Paving	SQ Yards Paving per REU
Normandale	\$12,300 - \$16,100	89	21,800	245
Mendelssohn	\$13,200 - \$16,200	64.67	16,500	242
Braemar Hills B	\$8,000 - \$11,000	34.33	5,600	165
St. Patrick's Lane	\$9,000 - \$12,000	13.10	2,900	205
Gleason Circle	\$9,000 - \$12,000	7.33	1,400	187
Lake Edina	\$10,000 - \$13,000	247.40	48,800	197

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Payment Options

- You will be billed for the assessment one year after project completion
- Assessments are payable over 15 years
- Payment options:
 - Pay entire amount upon receiving bill to avoid finance charges
 - Pay 25 percent; balance rolls to property taxes
 - Roll entire amount to property taxes
 - Defer payment if 65 years old or older

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What You Can Expect

- We will keep you informed.
- You will have opportunities to provide input.
- Private utility work is to be completed before City work.
- We will do our best to minimize inconveniences, but construction does not come without pain points.

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What You Can Expect

- Dust, noise and mud.
- Localized flooding during rainfall.
- Timelines sometimes delayed due to weather.
- You will be asked to limit water use.
- Your home will be connected to a temporary water line.



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What You Can Expect

- Your driveway may be inaccessible for up to seven days.
- Roadways to your home may be periodically inaccessible.
- The contractor will accommodate special access needs.
- Irrigation and pet containment systems may be damaged.



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Property Impacts

- Items located within the City's right-of-way may be damaged
 - Private utilities will be repaired if you notify us of them in advance of the project.
 - You can remove plantings and other landscape features before the project.
 - In areas where the boulevard is disturbed, the City will plant sod in the right-of-way after the project is complete.

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Providing Input

- Public hearings and questionnaire mailed to your home
- Weigh in on:
 - Sump pump drainage options
 - Are there traffic or drainage issues in your neighborhood?

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Communication Tools

- Become a neighborhood captain to help facilitate project communication.
 - Let us know of someone in your neighborhood who might fit this role.
- You will be notified of all meetings, hearings, schedules and questionnaires via regular mail.
- Public hearing notices are also published in *Edina Sun-Current*.
- Door hangers are hung when there time-sensitive information.
- Final assessment notices are mailed one year after construction.

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City Extra

“City Extra” emails are the best way to receive regular updates once construction begins. These are free weekly email updates about your project.

- Sign up on City of Edina website, www.EdinaMN.gov.
 - Check the box next to your project name.
- If you cannot receive email, we will mail you City Extra updates upon request.
- It’s the best way to stay informed.
- Don’t delay; sign up at today’s meeting!

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How to Prepare

- Sign up for City Extra
- Begin financial planning
- Complete questionnaire
- Coordinate home and yard improvement projects around the street construction timeline
- Ask questions; stay informed

www.EdinaMN.gov

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Contact Us

Toby Muse, SEH Project Manager
Phone: 952.912.2616
Email: tmuse@sehinc.com

Chad Millner, City of Edina Assistant City Engineer
Phone: 952.826.0318
Email: cmillner@edinamn.gov

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Thank you!

Questions?

www.EdinaMN.gov

**NORMANDALE NEIGHBORHOOD
 ROADWAY RECONSTRUCTION
 INFORMATIONAL MEETING
 SEPTEMBER 5, 2012**

	NAME	ADDRESS
1	Ken Chavis	6509 RYAN AVE
2	ELVICK-SINDWICK	6517 RYAN AVE
3	Patricia Kattleman	4416 W. Shore Dr.
4	Irma Olson	6404 W. Shore Dr.
5	Roy Olson	6404 W Shore Dr
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**NORMANDALE NEIGHBORHOOD
 ROADWAY RECONSTRUCTION
 INFORMATIONAL MEETING
 SEPTEMBER 5, 2012**

	NAME	ADDRESS
1	Rob Have	6325 Parnell Avenue
2	BARB: TED STORTZ	6409 ✓ ✓
3	Pat Sommer	6417 ✓ ✓
4	Marilyn + John DeVries	6424 Sherwood
5	Phil Dupan	6324 Parnell
6	Alle C Johnson	6525 - Sherwood Ave
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**NORMANDALE NEIGHBORHOOD
 ROADWAY RECONSTRUCTION
 INFORMATIONAL MEETING
 SEPTEMBER 5, 2012**

	NAME	ADDRESS
1	Beverly Johnson	6432 Ryan Ave S
2	Barbara Voigt	6333 Ryan Ave S.
3	Delores Ahl	6325 Ryan av. S.
4	Kara Carpenter	6501 Sherwood Ave
5	MARY FORSCH	4532 WEST STORE DR
6	Ken Salberg	6424 Parnell
7	Kathleen Kohler	6332 Ryan Ave.
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NORMANDALE NEIGHBORHOOD
ROADWAY RECONSTRUCTION
INFORMATIONAL MEETING
SEPTEMBER 5, 2012

	NAME	ADDRESS
1	Robert Winder	6533 Parnell Ave. So.
2	Bob Keller	6500 Parnell Av
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QUESTION / COMMENT CARD _____

I have a sprinkler system ^{also a} garden & plantings near curb. I also have an irrigation system. Would like to see a street light in middle of block on park side.

Name PATRICIA KATTEMAN Address 6416 WEST SHORE DR.
PLEASE PRINT PLEASE PRINT

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2013 Normandale Neighborhood Roadway Improvement Project
 Summary of September 5, 2012 Informational Meeting Question and Answer Session
 Meeting Location: Public Works and Parks Maintenance Facility
 Meeting Time: 6 pm to 8 pm

Number	Question	Answer
1	Can you add a light on West Shore Drive across from the park? There is currently a safety issue.	Yes, but we would add a cobra style fixture on a wood pole, not a decorative light.
2	If my water and sanitary sewer services are located in my backyard on Parnell Avenue, will they be reconstructed?	No, the project will not reconstruct water and/or sanitary sewer services located in back yards. You will be exempt from the sanitary sewer service portion of the assessment.
3	If I recently reconstructed my sanitary sewer service, can I opt out of the program?	Yes, provided you upgraded it within the last 15 years all the way to the trunk sanitary sewer main in the street.
4	Did the street light questionnaire results shown in the presentation reflect the updated questionnaire that was sent saying street lights are not assessed?	Yes it did.
5	Why are we paying more than other neighborhoods? What happened to the economy of scale?	The preliminary assessment totals are based on the total project cost divided by the total number of REUs. Your neighborhood has a large area of paving per square yard, but compared to other neighborhoods, doesn't have as many REUs. Preliminary costs are calculated using an economy of scale price because the project will be constructed as a neighborhood instead of one street at a time.
6	Can I still report that I have an irrigation system?	Yes, feel free to write down your information on a comment card or talk to one of us after the meeting.
7	If curb and gutter is not going to be removed, why would you damage my boulevard?	Small amounts of curb and gutter removal and boulevard impacts will occur due to water and sanitary sewer service work.
8	What is the City's response time to a resident question?	We try to get back to you within the same day or within 24 hours.
9	I have a friend who lives on Yvonne Terrace in the Richmond Hills neighborhood who says the City's communication has been poor during construction. What will the City do to ensure good communication on this project.	We will update you during construction through a series of mailings and door hangers depending on what type of work is occurring. Also, as mentioned, you can sign up for the City Extra to receive weekly email updates. Please ask your friend in Richmond Hills to call me and I will try to resolve her issues.
10	What about trees where utility work is occurring? Will trees be damaged?	Every effort will be made to protect existing trees. There are utility technologies available that limit tree damage from utility work. These technologies will be implemented where feasible. From past experience, tree roots tend to root away from the roadbed due to the type of material and compaction of material under the roadway.
11	Will construction affect my fire/emergency protection?	No, emergency access is provided at all times during construction. The contractor's scope of work includes providing any necessary measures to allow emergency vehicle access at all times throughout construction.
12	How will the City ensure the project doesn't drag on across the entire summer?	The general time frame of construction is April/May to October/November if the project is approved. If feasible, the project will be broke into Stages so that construction is only occurring in one area of the neighborhood for a maximum timeframe of approximately 10 weeks.

Number	Question	Answer
13	Will the City hire a construction manager?	Yes, the City would provide a representative from SEH to manage the project during construction with City staff oversight.
14	Why doesn't City staff manage this project to save money instead of using a consultant?	City staff workload is such that we are unable to complete the design of your neighborhood within the necessary timeframe. We have found it costs the City the same amount to manage these types of projects using City staff as it does if the City hires a consultant. SEH has conducted engineering services for many Edina neighborhood roadway reconstruction projects.
15	The phrase 'if the project moves forward' was used several times tonight. Is it a given this project is going to move forward?	City staff will recommend that this project move forward but ultimately it will be the City Council's decision at the Public Hearing.
16	Have there been previous City projects that have not moved forward?	Yes, there are a few projects that did not move forward: The Sunny Slope, Ridge Road and Country Club neighborhood projects. Eventually, all of these neighborhoods were approved and reconstructed. Due to the delay in constructing the projects, residents paid significantly more in assessments than what was originally proposed.
17	Does the City bid out services for engineering?	The City uses multiple consultants and selects them depending on their expertise. The City has hired SEH for multiple street reconstruction projects, including the Country Club project. There, they used their trenchless utility expertise to design the project so that no historic trees were removed as part of construction.
18	Do City projects require workers to be paid the prevailing wage rates identified in the Davis-Bacon Act?	No. There is no funding from federal or state agencies for these types of projects. Local contractors usually belong to organized unions that meet the Davis/Bacon requirements.
19	During the presentation, it was mentioned that localized flooding was a possibility during construction. Do we need to buy flood insurance prior to the project starting?	No. Localized flooding typically occurs at existing catch basins following significant rain events. This is because the project will require inlet protection or filter fabric installations at all existing storm sewer catch basins to catch/trap sediment so it does not enter the storm sewer system. The filter fabric does not allow water into the catch basins as quickly as normal. Eventually, these small ponded areas will drain away.
20	We have a yard light approximately 8-feet off of the curb in our front yard. Will it be impacted by construction?	Likely not, since we are only removing concrete curb and gutter in select locations. In the event that the light was damaged during construction, the City would pay the contractor to repair it.

Appendix F

Preliminary Assessment Roll

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City of Edina
Normandale Street Reconstruction
Improvement No. BA-394
Preliminary Assessment Roll
9/12/2012
Revisions: 10/24/12, 11/13/12

STREET	PID	HOUSE NO.	OWNER	ASSESSABLE STREET REU	STREET RECONSTRUCTION ASSESSMENT AMOUNT	ASSESSABLE SANITARY SEWER SERVICE PIPE REU	SANITARY SEWER SERVICE PIPE ASSESSMENT AMOUNT	ESTIMATED TOTAL ASSESSMENT RATE
64th St W								
	3002824230003	4717	Scott & Constance Cullen	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230089	4841	David & Mary Quello	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220001	4850	Jesse Oachs	1	\$12,300.00		(1)	\$12,300.00
	3002824220002	4860	Sara Ann Harkess	1	\$12,300.00		(1)	\$12,300.00
Parnell Ave								
	3002824220049	6317	Andrew & Eva Carcione	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220048	6321	Marc & Patricia Wertwijn	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220037	6324	Philip & Thais Dugan	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220047	6325	Robert J Have	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220039	6332	Faye S Rood	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230071	6400	Paul & Mary Turney	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230005	6401	Todd & Nancy Cullen	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230016	6408	Patrick Ryan & Rebecca Greig	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230004	6409	Theodore & Barbara Stortz	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230018	6416	Aleksandar Bekric & Violeta Rflog-Bekric	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230012	6417	Donald & Patricia Lennes	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230020	6424	Kenneth & Jill Salberg	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230073	6425	Mark Helgren & Kelly Leinfelder-Helgren	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230021	6432	Vincent & Ramona Settergren	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230013	6433	Daniel & Debra Moffatt	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230047	6500	Robert Keller & Ann Orchard	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230069	6501	Gary & Kirsten Cheezig	1	\$12,300.00		(2)	\$12,300.00
	3002824230086	6508	Mark & Nancy Lawrence	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230068	6509	Ralph & Mary Peterson	1	\$12,300.00		(2)	\$12,300.00

STREET	PID	HOUSE NO.	OWNER	ASSESSABLE STREET REU	STREET RECONSTRUCTION ASSESSMENT AMOUNT	ASSESSABLE SANITARY SEWER SERVICE PIPE REU	SANITARY SEWER SERVICE PIPE ASSESSMENT AMOUNT	ESTIMATED TOTAL ASSESSMENT RATE
	3002824230050	6516	Kunyuan Yang & Yuming Neng	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230067	6517	Anthony & Allison Rubin	1	\$12,300.00		(2)	\$12,300.00
	3002824230066	6525	John & Deborah Thomas	1	\$12,300.00		(2)	\$12,300.00
	3002824230083	6526	Gary & Deborah Hildebrand	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230060	6529	Wayne & Darlene Swanson	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230061	6533	Robert & Helen Winder	1	\$12,300.00		(2)	\$12,300.00
Ryan Ave								
	3002824220034	6324	John G Fassbender	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220057	6325	Richard & Delores Ahl	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220035	6332	John & Kathleen H Kohler	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220040	6333	Barbara J Vogt	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230070	6401	Kenneth & Nagwa Ohaeri	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230025	6408	Michael Elko & Elizabeth Delap	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230015	6409	Robert M Rolschau	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230026	6416	Thomas & Mary Pavak	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230017	6417	Peter & Kathleen Snell	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230029	6424	Kouatli A Walid	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230019	6425	Roberta Jo Swenson	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230030	6432	Donald & Beverly Johnson	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230022	6433	Jack L Ovick	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230041	6500	Mark Schiefelbein	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230048	6501	Michael & Ellen Dobies	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230043	6508	Robert & Joan Sherwin	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230085	6509	Kenneth & Geraldine Chavis	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230046	6512	John & Diane Opsahl	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230081	6517	Charles & Bette Sedgwick	1	\$12,300.00	1	\$2,500.00	\$14,800.00
Sherwood Ave								
	3002824230032	6400	Lillian C Carteng	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230088	6401	Mohamed Ibrahim	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230033	6408	Leonard & Audre Gaer	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230024	6409	Josheph & Luella Maley	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230034	6412	Mohammad Hassanzadeh	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230035	6416	Claudia P Biancaiana	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230027	6417	Mark & Cindy Fretland	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230072	6424	John & Marilyn Devries	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230028	6425	Rebecca J Burman	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230002	6432	Sherwood House Llc	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230031	6433	Nicholas & Jennifer Moody	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230078	6500	Eugene & Ruth Anderson	1	\$12,300.00	1	\$2,500.00	\$14,800.00

STREET	PID	HOUSE NO.	OWNER	ASSESSABLE STREET REU	STREET RECONSTRUCTION ASSESSMENT AMOUNT	ASSESSABLE SANITARY SEWER SERVICE PIPE REU	SANITARY SEWER SERVICE PIPE ASSESSMENT AMOUNT	ESTIMATED TOTAL ASSESSMENT RATE
	3002824230042	6501	Joseph Carpenter & Kara Norland	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230079	6508	William & Carol Behning	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230044	6509	Gary Matson	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230080	6512	Harold & Marguerite Lundin	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230037	6516	Harold & Gail Huwe	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230045	6517	Reid R Thompson	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230038	6524	Lisa K Boone	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230057	6525	Allen Johnson	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230087	6528	Branko & Vera Kastratovich	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230059	6533	Marc & Lisa Ungerman	1	\$12,300.00	1	\$2,500.00	\$14,800.00
West Shore Dr								
	3002824220045	6320	Donald & Sandra Stang	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824220046	6324	Thomas & Constance Schoening	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230074	6404	Roy E Olson	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230075	6408	Nancy A Kermath	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230076	6412	Randall Pratt	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230077	6416	Dale & Patricia Kattleman	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230014	6432	Michael & Beth Montgomery	1	\$12,300.00	1	\$2,500.00	\$14,800.00
	3002824230062	6500	Gary & Diana Rosen	1	\$12,300.00		(2)	\$12,300.00
	3002824230063	6508	Rosemary Kostick	1	\$12,300.00		(2)	\$12,300.00
	3002824230064	6516	Scott Forbes & Barbara Kunz	1	\$12,300.00		(2)	\$12,300.00
	3002824230065	6524	Christopher & Stephanie Grimes	1	\$12,300.00		(2)	\$12,300.00
	3002824230090	6532	Wesley & Mary Torsch	1	\$12,300.00		(2)	\$12,300.00
	3002824230009		City of Edina	7	\$86,100.00		(3)	\$86,100.00
				89	\$1,094,700.00	70	\$175,000.00	\$1,269,700.00

Notes:

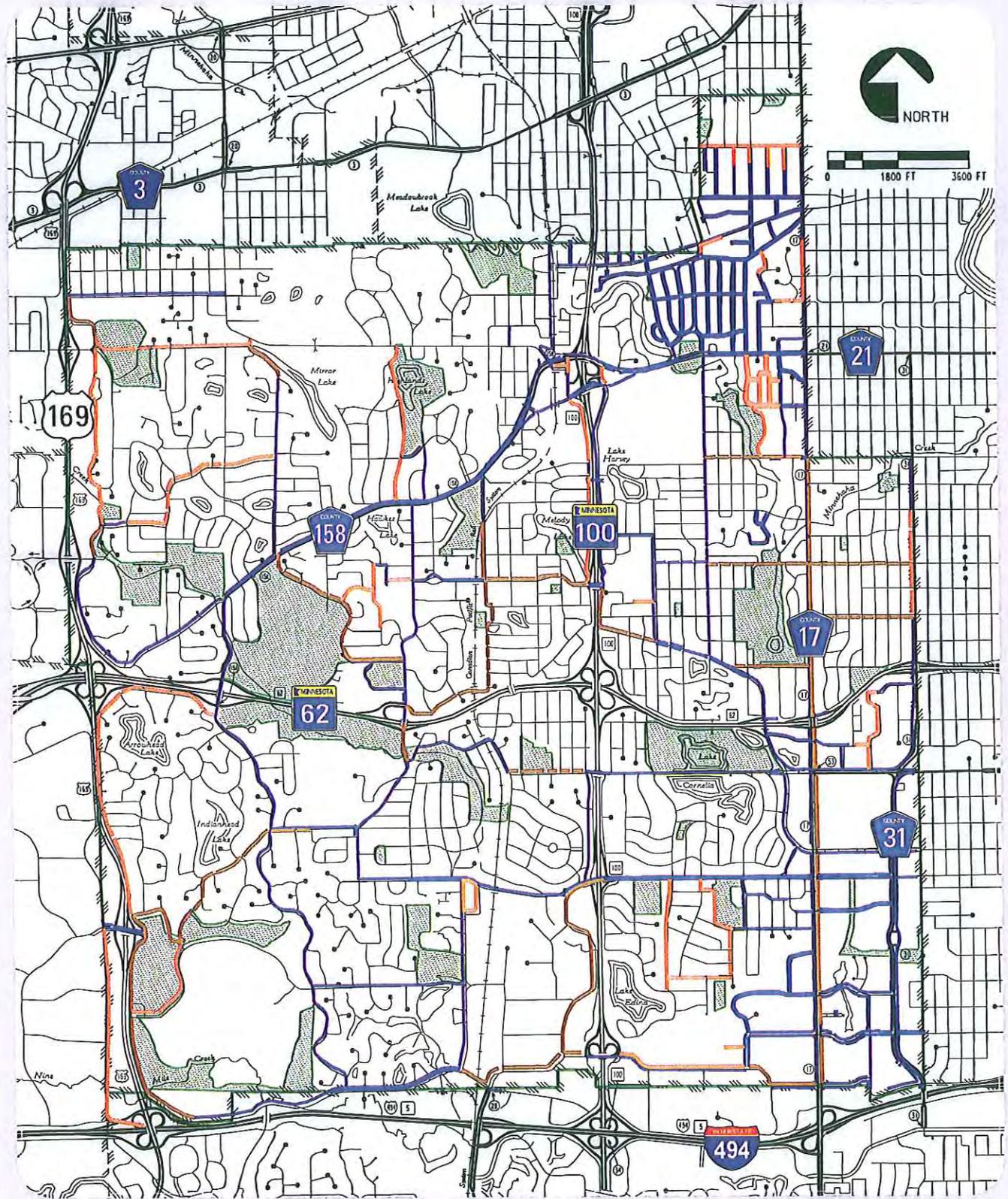
- (1) No sanitary sewer service pipe assessment due to side yard service connection outside of City right-of-way.
- (2) No sanitary sewer service pipe assessment due to backyard service connection outside of City right-of-way.
- (3) No City sanitary sewer service pipe connection.

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Appendix G

City Comprehensive Plan Update – Sidewalk and Bicycle Facilities (Fig. 7.10 and 7.11)

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LEGEND:

- Existing Sidewalk
- Proposed School / Business Sidewalk
- Proposed State-Aid Sidewalk

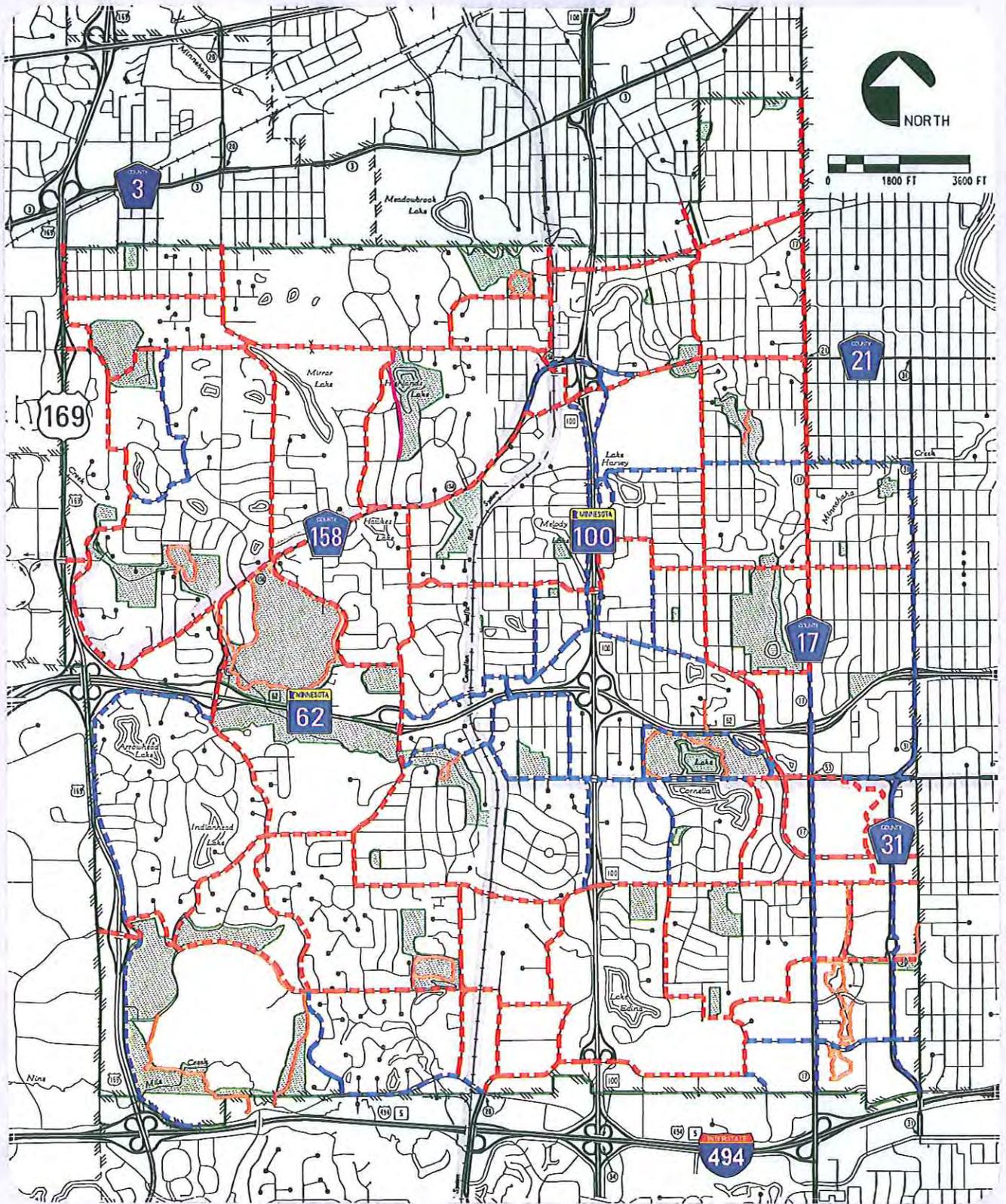
Note: Park Pathways are included on Figure 7.11



**City of Edina
2008 Comprehensive Plan Update**

Sidewalk Facilities

Figure 7.10



LEGEND: Existing Park Pathway Existing Hennepin County Corridors Proposed Park Pathway

From the 2007 Edina Comprehensive Bicycle Transportation Plan
 - - - Primary Route - - - Secondary Route



**City of Edina
 2008 Comprehensive Plan Update**

Bicycle Facilities

Figure 7.11

Appendix H

Plumbers Contact Information Summary

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City of Edina
Normandale Street Reconstruction
Open House Plumbers Contact Information

September 5, 2012

Revisions:

P:\AE\Edina\121719\1-gen\16-meet\090512 Neighborhood Mtg\Plumber Quest Rev 082412.xlsx]Sheet1

DISCLAIMER:

The City of Edina does not endorse any contractor listed on this list. The list is being provided to you only as a beginning reference in your search for a contractor.

Contractor	Private Service Pipe Reconstruction Services	Trenchless Methods for Sanitary Sewer Service Pipe Reconstruction				Trenchless Methods for Water Service Pipe Reconstruction		Contact Information		
		Slip Line Method	Cured in Place Pipe Method	Pipe Bursting Method	Other Trenchless Methods	Directional Drilling Method	Other Trenchless Methods	Contact Name	Day Phone	Email Address
Aqua City Plumbing	Yes	No	Yes	No		No	NUMA Gopher	Dave Vogelgesang	612.827.2871	dave@aquacityplumbing.com
Ellingson Companies	Yes	Yes	Yes	Yes		Yes		Todd Hoffman	888.527.2294 507.251.4372	thoffman@ellingsondrainage.com
Benjamin Franklin	Yes	Yes	Yes	Yes	Yes - Directional Drilling and Pilot Tube Pullback	Yes	Tunneling w/ Pneumatic Gopher. With pipe bursting equipment can pull water service at same time.	Paul Gavic	763.755.6468	paulgavic@gmail.com
Gene's Water & Sewer	Yes	Yes	Yes	Yes	Yes - Drop hole -tunnel	Yes	Drop hole - tunnel	Craig Peterson	612.781.3737	craig@geneswaterandsewer.com
Highview Plumbing	Yes	Yes	Yes	Yes	Yes Drop hole -tunnel	Yes	Drop hole - tunnel	Dan Swanson	952.933.8600	highplmg@aol.com
Minneapolis & Suburban Sewer & Water	Yes	Yes	Yes	No	Yes - Directional Drilling and Pilot Tube Pullback	Yes	Handtunneling, underground piercing tool (Gopher)	Kevin Hoyhtya	612.861.6076	website: MSSW-INC.com; Contact Us; connects to email
Mr Rooter Plumbing	Yes	Yes	Yes	Yes	Yes - Directional Drilling and Pilot Tube Pullback		Horizontal Directional Boring	John Wilder	763.551.0555	dispatch@mrrootermn.com
Olson Construction	Yes	No	No	No	Yes - Directional Drilling and Pilot Tube Pullback	Yes		Ron Olson	952.448.6674	
Ouverson Sewer & Water	Yes	Yes	Yes	No	Yes - Directional Drilling	Yes	NUMA Gopher	Troy Ouverson	612.751.6888	troyo@linemysewer.com
Parrott Contracting, Inc.	Yes	Yes	No	No	Yes - Directional Drilling and Pilot Tube Pullback	Yes	Directional Boring	Dave Parrott	507.248.9697	dparrott@frontiernet.net
Roto Rooter Services	Yes	Yes	Yes	Yes	Yes - Directional Drilling	Yes	With pipe bursting equipment can pull water service at same time. Handtunneling, underground piercing tool (Gopher)	Blaine Weiss	763.519.3928	blaine.weiss@rrsc.com
Valley-Rich Co., Inc.	Yes	Yes	Yes	Yes	Directional Drilling	Yes	Pneumatic Gopher	John Miklya	952.448.3002	john@valley-rich.com
Westonka Mechanical	Yes	Yes	Yes	Yes	The yes answers above are done by us and subcontractors.	YES	None	Brian Clark	612.741.9779	westonkasw@frontier.com
Dasen Contracting	Yes	No	No	No		Yes		Gordy Larsen or Todd Larsen	952-461-2100	dasencontracting@yahoo.com
Widmer Const.	Yes	No	No	No		Yes subcontract	No	Tony Vanderlinde	952-955-5062	vanderlinde5@frontiernet.net

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Appendix I

2013 Normandale Traffic and Crash Data

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2013 Normandale Traffic and Crash Data



Traffic Data

Location	Description	Year	Average Daily Traffic	85th Speed, MPH
1	Sherwood Avenue	2012	350	27.9
2	Sherwood Avenue	2012	121	29.5
3	Ryan Avenue	2012	40	21.8
4	Parnell Avenue	2012	37	22.8
5	65th Street West	2012	118	22.2
6	West Shore Drive	2012	413	26.5
7	West Shore Drive	2012	261	27.6
8	64th Street West	2012	238	24.4

Crash Data

Location	Severity	Year	Month	Time
A	Property Damage - No Apparent Injury	2007	Oct.	2200

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Appendix J

Recommendation to Set Public Hearing, Notice of Public Hearing to residents and
Notice of Public Hearing Advertisement

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REPORT / RECOMMENDATION



To: MAYOR AND COUNCIL

Agenda Item #: IV. C.

From: Wayne D. Houle, PE, Director of Engineering

Action

Discussion

Date: November 5, 2012

Information

Subject: Resolution No. 2012-145 Setting Public Improvement Hearings To Be Held On December 11, 2012

Action Requested:

Approve Resolution No. 2012-145 Setting Public Improvement Hearings To Be Held On December 11, 2012 for the following neighborhoods:

- Mendelssohn A Neighborhood Roadway Reconstruction, Improvement No. BA-393
- Normandale Neighborhood Roadway Reconstruction, Improvement No. BA-394
- Braemar Hills B Neighborhood Roadway Reconstruction, Improvement No. BA-395
- St. Patrick's Lane Reconstruction, Improvement No. BA-396
- Gleason Circle Reconstruction, Improvement No. BA-397
- Lake Edina Neighborhood Roadway Reconstruction, Improvement No. BA-398

Information / Background:

City staff is requesting public hearings for six neighborhood street reconstruction projects to be held on a special City Council Meeting date of December 11, starting at 6 PM. All projects are initiated by staff. Staff is hosting two additional informational meetings for the Normandale Neighborhood and the Lake Edina Neighborhood on November 7 due to comments from the Edina Transportation Commission meeting on October 25. Any comments from these meeting will be incorporated into the Feasibility Reports that will be submitted to the City Council on November 20.

Attachments:

Resolution No. 2012-145 Setting Public Improvement Hearings To Be Held On December 11, 2012

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RESOLUTION NO. 2012-145
SET PUBLIC IMPROVEMENT HEARINGS FOR DECEMBER 11, 2012
FOR NEIGHBORHOOD RECONSTRUCTION FOR:
MENDELSSOHN A, IMPROVEMENT NO. BA-393
NORMANDALE, IMPROVEMENT NO. BA-394
BRAEMAR HILLS B, IMPROVEMENT NO. BA-395
ST. PATRICK'S LANE, IMPROVEMENT NO. BA-396
GLEASON CIRCLE, IMPROVEMENT NO. BA-397
LAKE EDINA, IMPROVEMENT NO. BA-398

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF EDINA, MINNESOTA, that a public hearing shall be held on the 11th day of December, 2012, in the Council Chambers at City Hall at 6:00 p.m. to consider street reconstruction in the neighborhoods of:

- Mendelssohn A, Improvement BA-393;
- Normandale, Improvement No. BA-394;
- Braemar Hills B, Improvement No. BA-395;
- St. Patrick's Lane, Improvement No. BA-396;
- Gleason Circle, Improvement No. BA-397;
- Lake Edina, Improvement No. BA-398.

BE IT FURTHER RESOLVED that the City Clerk shall give mailed and published notice of such hearings and improvements as required by law.

Dated: November 5, 2012

Attest: _____
Debra A. Mangen, City Clerk

James B. Hovland, Mayor

STATE OF MINNESOTA)
COUNTY OF HENNEPIN) SS
CITY OF EDINA)

CERTIFICATE OF CITY CLERK

I, the undersigned duly appointed and acting City Clerk for the City of Edina do hereby certify that the attached and foregoing Resolution was duly adopted by the Edina City Council at its Regular Meeting of November 5, 2012, and as recorded in the Minutes of said Regular Meeting.

WITNESS my hand and seal of said City this _____ day of _____, 20__.

City Clerk

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November 21, 2012

*NOTICE OF PUBLIC HEARING
ROADWAY IMPROVEMENTS
PROPOSED IMPROVEMENT NO. BA-394
NORMANDALE NEIGHBORHOOD ROADWAY RECONSTRUCTION*

The Edina City Council will meet at Edina City Hall, on Tuesday, December 11, 2012, at 6:00 p.m., to consider the public hearing on roadway improvements for Normandale Neighborhood. This hearing is being conducted under the authority granted by Minnesota Statutes, Chapter 429.

This hearing has been called as a recommendation from staff. The proposed project would be constructed in the summer of 2013 with the assessment hearing occurring in the fall of 2014. The estimated project cost is \$1,270,000. The cost of the project will be funded by special assessment. The estimated cost per assessable lot for street reconstruction and sanitary sewer service pipe between the trunk pipe and the right-of-way line is \$12,300 and \$2,500, respectively, per residential equivalent unit. The assessments can be divided over a fifteen-year period with interest accumulating on the unpaid balance.

The area proposed to be assessed the cost of the proposed improvement includes the following:

4717 to 4860 64th St. W., 6317 to 6533 Parnell Ave, 6324 to 6517 Ryan Ave,
6400 to 6533 Sherwood Ave, and 6320 to 6532 West Shore Dr.

Your receipt of this notice is an indication that property whose ownership is listed to you is among those properties which are considered to be benefited by the improvement.

The City Council can authorize the proposed project immediately upon the close of the hearing.

Read the feasibility study online at:

http://edinamn.gov/index.php?section=construction_normandale_construction

To comment, you may:

- **Write** to City of Edina, Attention Engineering, 7450 Metro Boulevard, Edina, MN 55439
- **Email to** mail@EdinaMN.gov, attention City Council and Engineering
- **Attend the public hearing** and offer comments.

ENGINEERING DEPARTMENT

7450 Metro Boulevard • Edina, Minnesota 55439
www.EdinaMN.gov • 952-826-0371 • Fax 952-826-0392

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PID: 3002824220001
JESSE OACHS
4850 64TH ST W
EDINA MN 55435

PID: 3002824220002
SARA ANN HARKESS
4860 64TH ST W
EDINA MN 55435

PID: 3002824220034
JOHN G FASSBENDER
6020 HALIFAX AVE S
EDINA MN 55424

PID: 3002824220035
JOHN H KOHLER
6332 RYAN AVE
EDINA MN 55435

PID: 3002824220037
PHILIP A DUGAN
6324 PARNELL AVE
EDINA MN 55435

PID: 3002824220039
FAYE S ROOD
6332 PARNELL AVE
EDINA MN 55435

PID: 3002824220040
BARBARA J VOGT
6333 RYAN AVE
EDINA MN 55435

PID: 3002824220045
DONALD STANG
6320 WEST SHORE DR
EDINA MN 55435

PID: 3002824220046
THOMAS E SCHOENING
CONSTANCE B SCHOENING
6324 WEST SHORE DR
EDINA MN 55435

PID: 3002824220047
ROBERT J HAVE
6325 PARNELL AVE
EDINA MN 55435

PID: 3002824220048
PATRICIA J WERTWIJN
MARC R WERTWIJN
6321 PARNELL AVE
EDINA MN 55435

PID: 3002824220049
DWIGHT C LARSON
6317 PARNELL AVE
EDINA MN 55435

PID: 3002824220057
DELORES L AHL
6325 RYAN AVE
EDINA MN 55435

PID: 3002824230002
SHERWOOD HOUSE LLC
5025 KNOX AVE S
MINNEAPOLIS MN 55419

PID: 3002824230003
SCOTT L CULLEN
CONSTANCE L CULLEN
4717 64TH ST W
EDINA MN 55435

PID: 3002824230004
BARBARA A STORTZ
6409 PARNELL AVE
EDINA MN 55435

PID: 3002824230005
NANCY ROSS CULLEN
6401 PARNELL AVE
EDINA MN 55435

PID: 3002824230012
PATRICIA A LENNES
6417 PARNELL AVE
EDINA MN 55435

PID: 3002824230013
DEBRA A MOFFATT
DANIEL W MOFFATT
6433 PARNELL AVE
EDINA MN 55435

PID: 3002824230014
MICHAEL S MONTGOMERY
BETH MONTGOMERY
6432 WEST SHORE DR
EDINA MN 55435

PID: 3002824230015
ROBERT M ROLSCHAU
6409 RYAN AVE
EDINA MN 55435

PID: 3002824230016
PATRICK S RYAN
6408 PARNELL AVE
EDINA MN 55435

PID: 3002824230017
PETER J SNELL
KATHLEEN S SNELL
6417 RYAN AVE
EDINA MN 55435

PID: 3002824230018
VIOLETA RFLOG-BEKRIC
ALEKSANDAR BEKRIC
6416 PARNELL AVE
EDINA MN 55435

PID: 3002824230019
ROBERTA JO SWENSON
6425 RYAN AVE
EDINA MN 55435

PID: 3002824230020
KENNETH L SALBERG
JILL L SALBERG
6424 PARNELL AVE
EDINA MN 55435

PID: 3002824230021
VINCENT SETTERGREN
6432 PARNELL AVE
EDINA MN 55435

PID: 3002824230022
JACK L OVICK
6433 RYAN AVE
EDINA MN 55435

PID: 3002824230024
JOSEPH J MALEY
6409 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230025
MICHAEL N ELKO
ELIZABETH L DELAP
6408 RYAN AVE
EDINA MN 55435

PID: 3002824230026
THOMAS E PAVEK
MARY G PAVEK
6416 RYAN AVE
EDINA MN 55435

PID: 3002824230027
MARK C FRET LAND
CINDY K FRET LAND
6417 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230028
REBECCA J RORKE
6425 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230029
KOUATLI A WALID
5604 BENTON AVE
EDINA MN 55436

PID: 3002824230030
BEVERLY JOHNSON
6432 RYAN AVE
EDINA MN 55435

PID: 3002824230031
NICHOLAS MOODY
JENNIFER MOODY
6433 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230032
LILLIAN C CARTENG
6400 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230033
LEONARD J GAER
6408 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230034
MOHAMMAD HASSANZADEH
8440 XYLON CIR
BLOOMINGTON MN 55438

PID: 3002824230035
CLAUDIA P BIANCAIANA
6416 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230037
GAIL HUWE
6516 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230038
LISA K BOONE
6524 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230041
MARK SCHIEFELBEIN
6500 RYAN AVE
EDINA MN 55435

PID: 3002824230042
KARA C CARPENTER
JOSEPH S CARPENTER
6501 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230043
ROBERT D SHERWIN
6508 RYAN AVE
EDINA MN 55435

PID: 3002824230044
GARY MATSON
6509 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230045
REID R THOMPSON
6517 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230046
JOHN A OPSAHL
DIANE J OPSAHL
6512 RYAN AVE
EDINA MN 55435

PID: 3002824230047
ROBERT M KELLER
ANN L ORCHARD
6500 PARNELL AVE
EDINA MN 55435

PID: 3002824230048
MICHAEL S DOBIES
ELLEN S DOBIES
6501 RYAN AVE
EDINA MN 55435

PID: 3002824230050
YUMING NENG
KUNYUAN YANG
6516 PARNELL AVE
EDINA MN 55435

PID: 3002824230057
ALLEN JOHNSON
6525 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230059
MARC A UNGERMAN
LISA A UNGERMAN
6533 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230060
WAYNE R SWANSON
DARLENE J SWANSON
6529 PARNELL AVE
EDINA MN 55435

PID: 3002824230061
ROBERT G WINDER
HELEN A WINDER
6533 PARNELL AVE
EDINA MN 55435

PID: 3002824230062
GARY R ROSEN
6500 WEST SHORE DR
EDINA MN 55435

PID: 3002824230063
ROSEMARY KOSTICK
6508 WEST SHORE DR
EDINA MN 55435

PID: 3002824230064
SCOTT J FORBES
BARBARA J KUNZ
6516 WEST SHORE DR
EDINA MN 55435

PID: 3002824230065
STEPHANIE A GRIMES
CHRISTOPHER P GRIMES
6524 WEST SHORE DR
EDINA MN 55435

PID: 3002824230066
JOHN C THOMAS
DEBORAH A THOMAS
6525 PARNELL AVE
EDINA MN 55435



PID: 3002824230067
ANTHONY S RUBIN
6517 PARNELL AVE
EDINA MN 55435

PID: 3002824230068
RALPH H PETERSON III
MARY B PETERSON
6509 PARNELL AVE
EDINA MN 55435

PID: 3002824230069
KIRSTEN L CHEEZIG
GARY L CHEEZIG
6501 PARNELL AVE
EDINA MN 55435

PID: 3002824230070
NAGWA R OHAERI
KENNETH O OHAERI
6401 RYAN AVE
EDINA MN 55435

PID: 3002824230071
PAUL A TURNEY
MARY H TURNEY
6400 PARNELL AVE
EDINA MN 55435

PID: 3002824230072
JOHN M DEVRIES
6424 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230073
MARK I HELGREN
6425 PARNELL AVE
EDINA MN 55435

PID: 3002824230074
ROY E OLSON
6404 WEST SHORE DR
EDINA MN 55435

PID: 3002824230075
NANCY A KERMATH
6408 WEST SHORE DR
EDINA MN 55435

PID: 3002824230076
RANDALL PRATT
6412 WEST SHORE DR
EDINA MN 55435

PID: 3002824230077
PATRICIA KATTLEMAN
DALE KATTLEMAN
6416 WEST SHORE DR
EDINA MN 55435

PID: 3002824230078
RUTH S ANDERSON
6500 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230079
WILLIAM R BEHNING
CAROL J BEHNING
6508 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230080
MARGUERITE LUNDIN
6512 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230081
CHARLES SEDGWICK
6517 RYAN AVE
EDINA MN 55435

PID: 3002824230083
GARY T HILDEBRAND
6526 PARNELL AVE
EDINA MN 55435

PID: 3002824230085
KENNETH M CHAVIS
6509 RYAN AVE
EDINA MN 55435

PID: 3002824230086
NANCY J LAWRENCE
MARK B LAWRENCE
6508 PARNELL AVE
EDINA MN 55435

PID: 3002824230087
VERA K KASTRATOVICH
BRANKO J KASTRATOVICH
6528 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230088
MOHAMED IBRAHIM
6401 SHERWOOD AVE
EDINA MN 55435

PID: 3002824230089
MARY BRECKENRIDGE QUELLO
DAVID QUELLO
4841 64TH ST W
EDINA MN 55435

PID: 3002824230090
WESLEY H TORSCH
MARY S O TORSCH
6532 WEST SHORE DR
EDINA MN 55435

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(OFFICIAL PUBLICATION)
CITY OF EDINA
4801 W 50TH STREET
EDINA, MINNESOTA 55424
HENNEPIN COUNTY, MINNESOTA

NOTICE OF PUBLIC HEARING

NORMANDALE NEIGHBORHOOD ROADWAY RECONSTRUCTION

IMPROVEMENT NO. BA-394

The Edina City Council will meet at the Edina City Hall on Tuesday, December 11, 2012, at 6:00 p.m. to consider the following proposed improvements to be constructed under the authority granted by Minnesota Statutes, Chapter 429.

<u>Normandale Neighborhood Roadway Reconstruction</u>	<u>Estimated Total Assessment Cost</u>
Improvement No. BA-394	\$1,270,000

The area proposed to be assessed by the proposed improvements includes all properties between and including:

4717 to 4860 64th St. W., 6317 to 6533 Parnell Ave, 6324 to 6517 Ryan Ave,
6400 to 6533 Sherwood Ave, and 6320 to 6532 West Shore Dr.

These properties are proposed to be assessed at approximately \$12,300 for street reconstruction and \$2,500 for sanitary sewer pipe between the trunk pipe and the right-of-way line, per residential equivalent unit;

Debra Mangen
City Clerk

PLEASE PUBLISH IN THE EDINA SUN ON November 29 and December 7, 2012

PLEASE SEND TWO (2) AFFIDAVITS OF PUBLICATION. BILL TO CITY OF EDINA.

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Appendix K

Edina Transportation Commission Review Data and Meeting Minutes

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**MINUTES OF
CITY OF EDINA, MINNESOTA
TRANSPORTATION COMMISSION
COUNCIL CHAMBERS
OCTOBER 25, 2012
6:00 P.M.**

CALL TO ORDER

Director Houle called the meeting to order in the absence of the chair and vice-chair. He explained that the bylaws require the commission to nominate an acting-chair under this circumstance. **Member Janovy was nominated as acting-chair by member Franzen and the nomination was seconded by member Surya. No other nominations were made and the nomination closed. All voted aye. Motion carried.**

APPROVAL OF MEETING AGENDA

Motion was made by member Franzen and seconded by member LaForce to approve the meeting agenda.

APPROVAL OF MEETING MINUTES

REGULAR MEETING OF SEPT. 20, 2012

The following corrections were made: Page 2, Traffic Safety Committee, 3rd paragraph, delete '30 mph' and replace with '25 mph;' delete 'pick the speeds that 85% of drivers are traveling' and replace with 'the 85th percentile speed is the speed at which 85% of drivers are traveling at or under;' and page 3, delete 'None' and add 'Receive minutes of Aug. 14, 2012.' **Motion was made by member LaForce and seconded by member Surya to approve the amended minutes of Sept. 20, 2012. All voted aye. Motion carried.**

COMMUNITY COMMENT – None.

REPORTS/RECOMMENDATIONS

do.town Update

Mr. Flynn Rico-Johnson explained the do-town initiative is a partnership with the cities of Bloomington, Edina and Richfield and Blue Cross Blue Shield of Minnesota which aims to address unhealthy and inactive lifestyles and make residents healthier. They are doing this by conducting walking audits. Residents volunteer to walk certain streets and evaluate the infrastructure to see what is wrong with it. He said the point is not to avoid certain streets but to identify what can be done to make improvements, e.g. no sidewalk, no streetlights, etc. The audits are for people of all ages and focus on places most often traveled such as schools, parks, major shopping districts, etc. He said Edina is a vibrant city and they are looking for ideas and help with getting the information out so that they can do more audits and help the City address the concerns when projects are done.

Member Janovy said the City Council and the ETC received letters from residents that have done the walking audit. She asked if the audit is better done person by person or in a group. Mr. Rico-Johnson said the 50th/Halifax Ave area audit included different families and this brings different perspectives because each family may have different reasons for walking and because of this, they prefer to have as many people involved. He said they would like to do walking audits during the winter too and volunteers can contact him at flynn@do-town.org or info@do.town.org.

Member Janovy asked if they are doing similar audits for biking and he said it is more difficult to do but they are working on it. He said the biking audit can be done by an individual riding his/her favorite route and evaluating it.

Hennepin County Pedestrian & Bicycle Plan

Ms. Rose Ryan, pedestrian and bicycle planner with Hennepin County said they are working on their first ever county-wide pedestrian plan funded by a five year grant from the Centers of Disease Control called Communities Transformation Grant. She said the purpose is to prevent chronic disease and since walking is an easy physical activity with great benefits, they are using pedestrian/ bicycle planning to set up a framework for a healthy community design.

To accomplish their goals of increasing the rates of walking, safety of walking and improved health of county resident, Ms. Ryan said they will be identifying sidewalk gaps, difficult crossings, prioritize improvements to the pedestrians system and coordinate with other agencies/organizations to reach their goals.

Ms. Ryan said planning began this past summer with 10 workshops in various communities and the final plan will be submitted to the County Board next summer. She asked for feedback and said the draft plan would be available for review.

Mr. Houle noted that the map of existing sidewalks in Edina that Ms. Ryan was working from was inaccurate as it reflected proposed sidewalks and not actual sidewalks. Some areas suggested were missing gaps on France Avenue, Xerxes Avenue, Vernon Avenue by Schaefer Road and View Lane, W. 66th from France to York, and York Avenue.

Xerxes Avenue Restriping – from 60th Street West to TH62

Director Houle said this was initiated by a resident through the TSC. The issues were congestion and lanes that are not clearly marked by TH62. This was referred to Hennepin County and they drafted a restriping plan. Area residents and city of Minneapolis are supportive of the plan and staff would like feedback from the ETC.

Two suggestions were made that Director Houle will take back to Hennepin County. They were to taper the line where parking ends to make it clear; and add no parking sign where the full width of parking ends because drivers tend to park over the white line where tapering ends.

Pedestrian and Cyclist Safety Fund Policy

Member Janovy said Manager Neal is seeking feedback from the ETC on his draft Pedestrian and Cyclist Safety Fund Policy. The franchise fee would come from Edina customers of Xcel Energy and CenterPoint Energy to be used for non-motorized transportation.

After discussion, which included the reasons for implementing the franchise fee and the lack of funding for sidewalks, the ETC suggested the following:

1. Use funds for construction only, not for staffing;
2. Change the name from Pedestrian and Cyclist Safety Fund to Active Living Infrastructure Fund to emphasize active living;
3. Identify a certain percentage that will be used for construction vs staffing;

4. Chair Nelson's suggestions which is attached;
5. Clarity on the meaning of 'maintenance' and consider changing it;

2013 Work Plan

Member LaForce likes the revised plan. Member Janovy suggested changing Living Streets Policy: Implementation to Living Streets Policy and Implementation Plan; Director Houle will check to see if Safe Routes to School Comprehensive Plan is the correct name; and change Ordinance Review for Policy to Review Ordinances for Conformance to Policy. Member Janovy asked if the \$150,000 would be in the budget for the Living Streets Policy and Director Houle said he did not know.

Motion was made by member Iyer and seconded by member LaForce for members Thompson and Iyer to write an advisory communication to the City Council to consider putting \$150,000 in the budget to fund Living Streets Policy development.

Member Franzen suggested laying out exactly how the \$150,000 would be spent. Member LaForce concurred and said he is not sure what the amount is or should be. Director Houle said the amount was identified by the consultant and that he could contact him for a breakdown.

All voted aye. Motion carried.

2013 Neighborhood Reconstruction Projects

Director Houle presented the four 2013 Neighborhood Reconstruction Projects with the primary focus being on transportation-related reconstruction and not utility. He said a second survey was sent to residents to see if they would like sidewalks and streetlights now that they are to be funded by the franchise fees and majority of respondents said no. A special public hearing meeting is scheduled for Dec. 11 for the City Council to consider approvals.

Director Houle said the feasibility studies were also distributed to fire, police, public works, parks and recreation, the school district, and BETF for feedback, but to date, have not received any feedback. Member Janovy suggested identifying for each group how the feasibility study relates to them, e.g. identify the walking zone for the school district.

Director Houle said all of the streets in all four neighborhoods are in similar condition - - constructed in the 1960's and have alligator cracks, exposed base course and utility issues.

Mendelssohn A

Proposed improvements are new bulkhead curb and gutter, except for Spruce Road which is 14 ft. wide and will get rainbow curbs instead, and new pavement. The proposed assessment is \$16,150. Member Janovy said she submitted feedback to staff on the feasibility studies. She asked if Director Houle had considered splitting this neighborhood in two and he said he hadn't but will take a look at it.

Normandale

Proposed improvements are spot repair to curb and gutter, new pavement, and a sidewalk on W. 66th Street. Proposed assessment is \$16,100, including private sanitary sewer upgrades; and \$12,300 for a few residents whose sanitary sewer is not in the street and therefore not required to upgrade.

Member Janovy said the BETF said West Shore Drive is a primary bike route and recommended separated bike lanes or road markings and signage. Member Iyer concurred and recommended dedicated bike lanes. He said parking should not be an issue because there is a parking lot nearby. Member Janovy suggested an off-road trail for consideration. Director Houle said he will schedule an informational meeting with residents to share the suggestions and gather feedback.

Braemar Hills B, St. Patrick's Lane, Gleason Circle

These three neighborhoods will be presented in one feasibility study but they will be approved separately by City Council. Proposed improvements are spot repair to curb and gutter and new pavement. Proposed assessments are: Braemar Hills B: \$9,325, St. Patrick's Lane: \$11,300, and Gleason Circle: \$11,600.

Lake Edina

Proposed improvements are spot repair to curb and gutter and new pavement, plus narrowing the curb line at Hibiscus Avenue & West Shore Drive to reduce storm water run-off. Proposed assessment is \$11,900.

Member Janovy recommended a sidewalk on Normandale Road to Hibiscus Avenue to Kellogg Avenue because of speed and volume and to connect with the Nine Mile trail that is coming soon. Director Houle said he will schedule an informational meeting with residents to share the suggestion and gather feedback.

Traffic Safety Committee Report of Oct. 3, 2012

There were no action items in this report. Director Houle will follow up on the counts that were done in member Whited's neighborhood and on W. 44th Street.

Updates

Student Members

Member Schwieger noted that 'fascism' was still painted on the stop sign. Director Houle will follow up on this.

Bike Edina Task Force – Sept. 13, 2012 Minutes

A new member was added to the BEFT.

Living Streets Working Group

No update.

Transportation Options Working Group

Received minutes of Sept. 18, 2012. Member Whited reported that John Dillery and Jill Hentges from Metro Transit attended the Sept. 18 meeting and talked about Metro Transit's transportation options in Edina.

Member Janovy said the time limit is up for this working group and asked if the ETC would like it to continue for another year.

Motion was made by member Franzen and seconded by member LaForce for the working group to continue for another year. Member Whited said they need new members. Director Houle will work with

communications director Bennerotte to do a press release for new members. Member Iyer suggested notifying residents who have applied for boards and commissions positions.

All voted aye. Motion carried.

Communications Committee

Member LaForce said they met with communications director Bennerotte and they now have a good sense of how the committee will work with her. First, the committee will not produce communications or set budget, but instead will provide ideas, tactics, strategies, etc. Secondly, they will provide feedback on the educational material for the Wooddale bike lanes that staff is working on. And lastly, they are planning to have online forums instead of regular meetings which are new for them so they will evaluate its effectiveness and maybe other groups could do the same.

CORRESPONDENCE AND PETITIONS

Received correspondences from City Council packet relating to transportation issues

CHAIR AND COMMISSION MEMBER COMMENTS

Member Janovy said she received a traffic safety request concerning the traffic on South View Lane that she forwarded to Director Houle. Director Houle said the request for flashing lights has been forwarded to the TSC. He said the area will not meet warrants for flashing lights and suggested reviewing the Crosswalk Policy when they create the Pedestrian Plan. In the meantime, they will be rotating the in-road pedestrian signs to this area and they should be there this week (there are 10-12 that are rotated throughout the city).

Member Iyer said he's noticed some errors on the bicycle facility map that he will forward to Director Houle.

Regarding the bike lanes on Wooddale Avenue, Member LaForce said he is hopeful that they will be able to figure out a way to help residents but he said they should be thinking about ways to reach those who are coming to the city for the first time.

STAFF COMMENTS

The biking on sidewalks ordinance will be on the Nov. 20 City Council meeting.

The Southwest Transit Environment Impact Statement is available for review in Community Development-Planning in City Hall.

Continue to observe the Wooddale Avenue advisory bike lanes and the number of signage may be reduced. Staff is considering installing yellow tabs as a temporary measure to delineate the center line. Director Houle said City of Minneapolis did the same striping on 14th Street and it did not generate the kind of responses that Edina's is getting; he also said W. 54th Street was restriped similarly and it has not generated the kind of responses either. Member Iyer asked if Edina's communication was the same or different from Minneapolis and Director Houle said it was very similar. Member LaForce asked if there have been any accidents since it was restriped and Director Houle said there have been two – one with a parked car and the other was a rear-ended.

Other thoughts and suggestions were: removal of signs that are not required; W. 54th Street has symmetry with parking lanes on both sides; Minneapolis too has symmetry with parking lanes on both sides that are

often filled with parked cars while the parking lane on Wooddale is usually empty and gives the impression that it is a driving lane; adding yellow tabs could add more confusions and expectation of adding a permanent line.

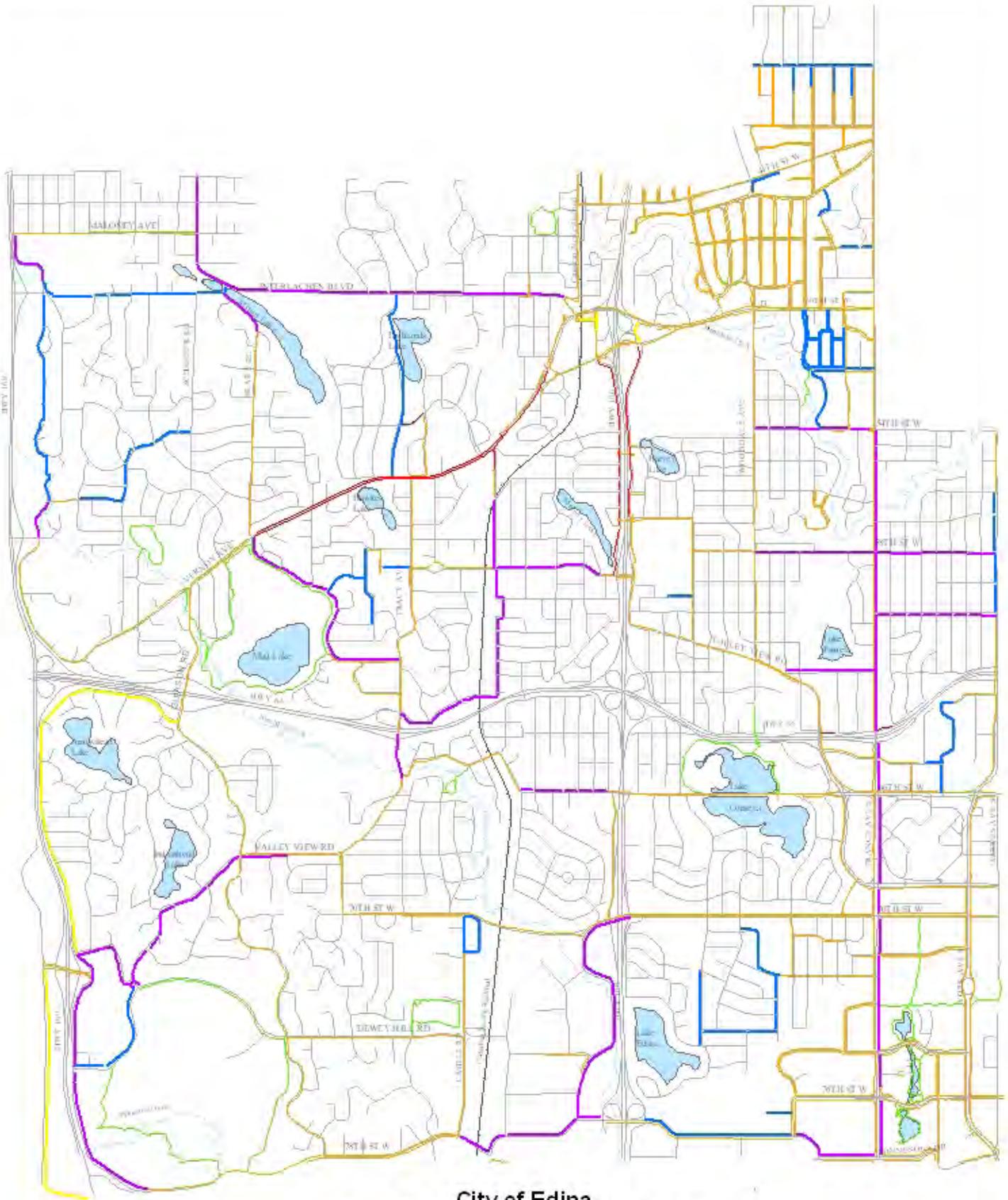
Director Houle said this project has a two year experimental timeframe from the Federal Highway and staff would need their approval as well as Mn/DOTs' to make changes.

ADJOURNMENT

Meeting adjourned.

ATTACHMENT

Attendance spreadsheet.



**City of Edina
Draft Pedestrian Facilities - Fig 8.8**

	Existing Bituminous Sidewalk		Proposed Business/Park/School Sidewalk
	Existing Concrete Sidewalk		Proposed Sidewalk on Roads Classified Collector & Above
	Existing Park Pathway		Proposed State-Aid Sidewalk



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2013 Normandale Traffic Counts



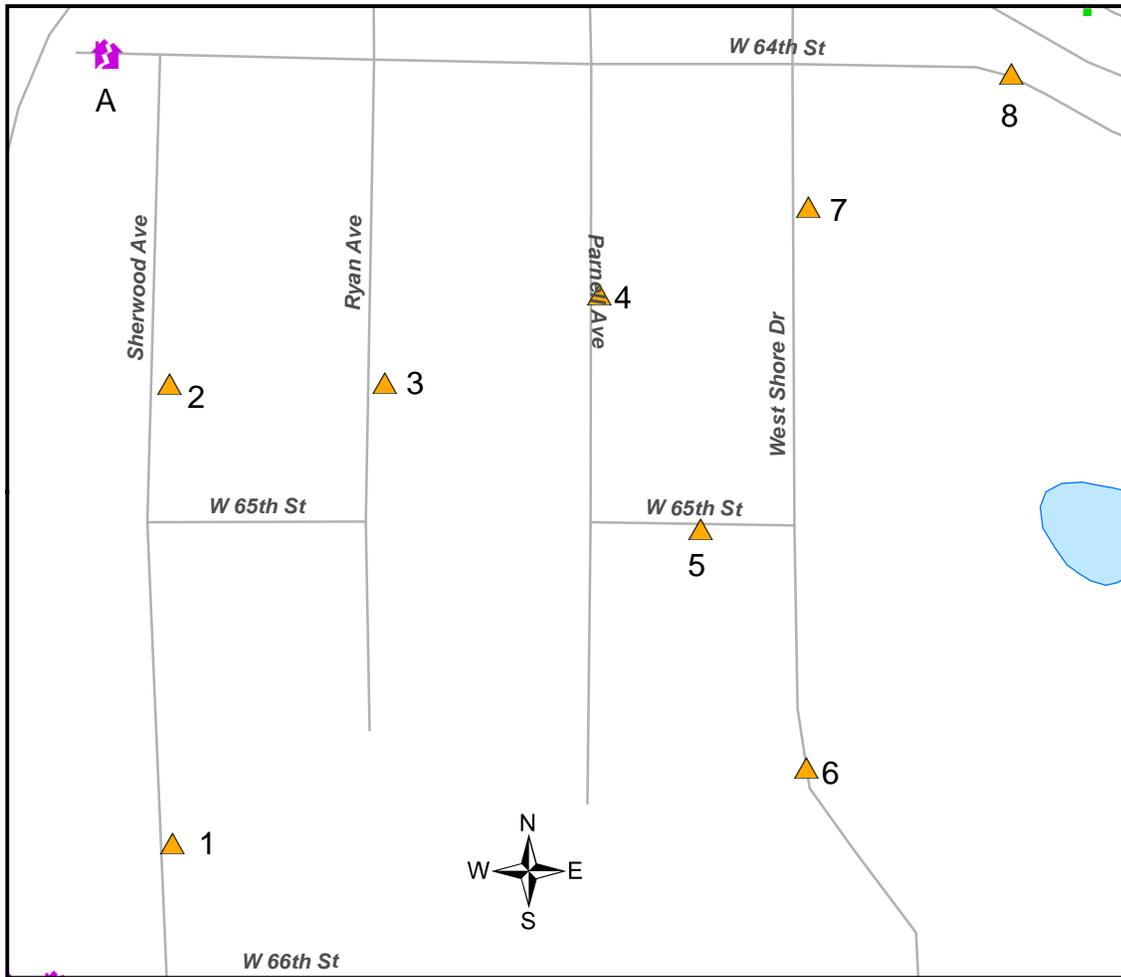
Location	Description	Year	Average Daily Traffic	Average Vehicles Over 25 MPH		85th Percentile Speed*
				NB	SB	
1	Sherwood Avenue north of 65th Street	2012	121	35	20	29.5
2	Sherwood Avenue north of 66th Street	2012	350	57	73	27.9
3	Ryan Avenue south of 64th Street	2012	40	0	1	21.8
4	Parnell Avenue north of 65th Street	2012	37	2	0	22.8
5	65th Street east of Parnell Avenue	2012	118	2	3	22.2
6	West Shore Drive north of 66th Street	2012	413	52	48	26.5
7	West Shore Drive south of 64th Street	2012	261	62	35	27.6
8	64th Street east of West Shore Drive	2012	238	12	17	24.4

* 85th percentile speed is the speed at which 85% of vehicles measured are travelling at or below.



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2013 Normandale Traffic and Crash Data



Traffic Data

Location	Description	Year	Average Daily Traffic	85th Speed, MPH
1	Sherwood Avenue	2012	350	27.9
2	Sherwood Avenue	2012	121	29.5
3	Ryan Avenue	2012	40	21.8
4	Parnell Avenue	2012	37	22.8
5	65th Street West	2012	118	22.2
6	West Shore Drive	2012	413	26.5
7	West Shore Drive	2012	261	27.6
8	64th Street West	2012	238	24.4

Crash Data

Location	Severity	Year	Month	Time
A	Property Damage - No Apparent Injury	2007	Oct.	2200

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Engineering Department • Phone 952-826-0371
Fax 952-826-0392 • www.CityofEdina.com



Date: 10/22/2012

To: Chad Millner

cc:

From: Byron Theis

Re: Traffic Requests in 2013 Neighborhood Reconstruction

Chad,

I have looked at the neighborhoods that are up for reconstruction in 2013 for any recent traffic safety requests. They are listed below.

-Mendelssohn: One request for parking restrictions in 2009. Request was asked to be removed by requestor.

-Braemar Hills: No Requests

-St. Patrick's Lane: No Requests

-Gleason Circle: No Requests

-Normandale: One request in 2009 for parking restrictions on West Shore Drive. Request was denied.

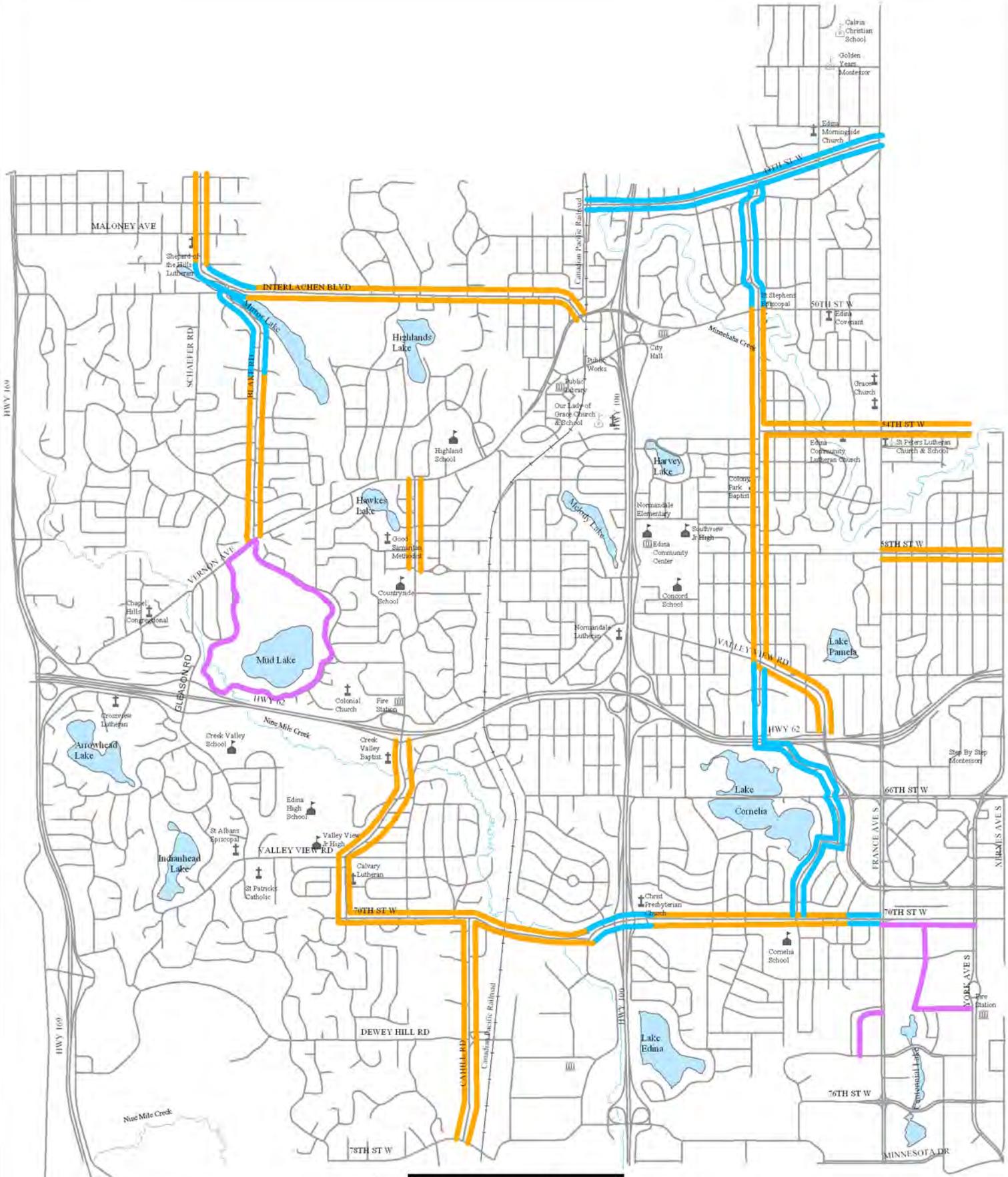
-Lake Edina: Two requests in the same area on Normandale Road between Aspasia Lane and Trillium Lane. Both requests were for speed enforcement in the area in 2009 and 2012. 2009 had no response from requestor. 2012 request was sent to EPD for enforcement after a traffic count was done.

If there are any questions about the requests, please let me know.

Regards,

Byron Theis, Traffic safety Coordinator

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Bicycle Facilities

- Bike Lanes
- Bike Paths
- Share the Road



Engineering Dept
October, 2012

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PROJECT LIMITS



Cobra Drop Lense
Wood Round

Cobra Drop Lense
Wood Round

W 64TH ST
Cobra Drop Lense
Wood Round

W 65TH ST
Cobra Drop Lense
Wood Round

Cobra Drop Lense
Wood Round

W 65TH ST
Cobra Drop Lense
Wood Round

Cobra Drop Lense
Wood Round

W 66TH ST

Cobra Drop Lense
Wood Round

Cobra Drop Lense
Wood Round

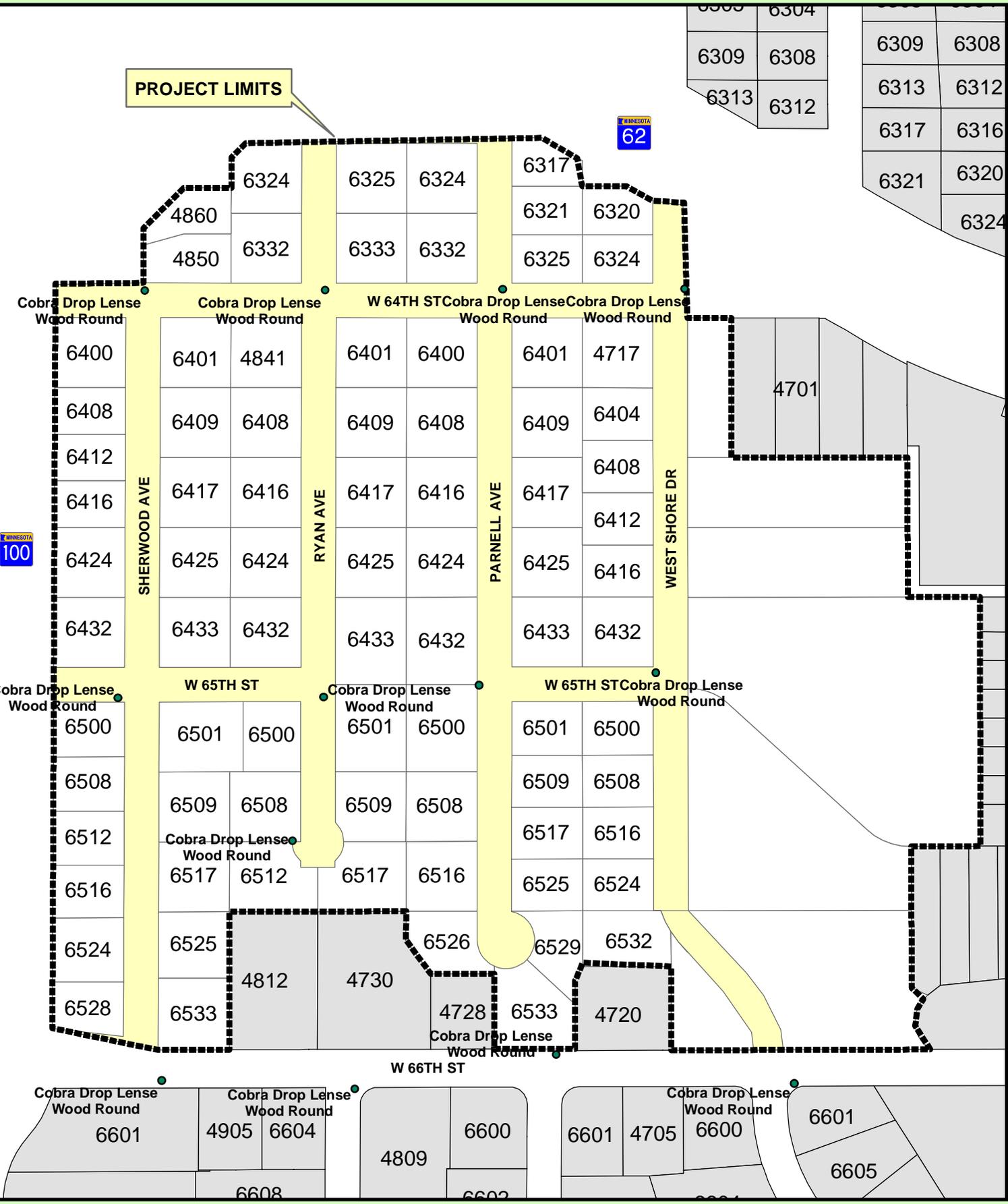
Cobra Drop Lense
Wood Round

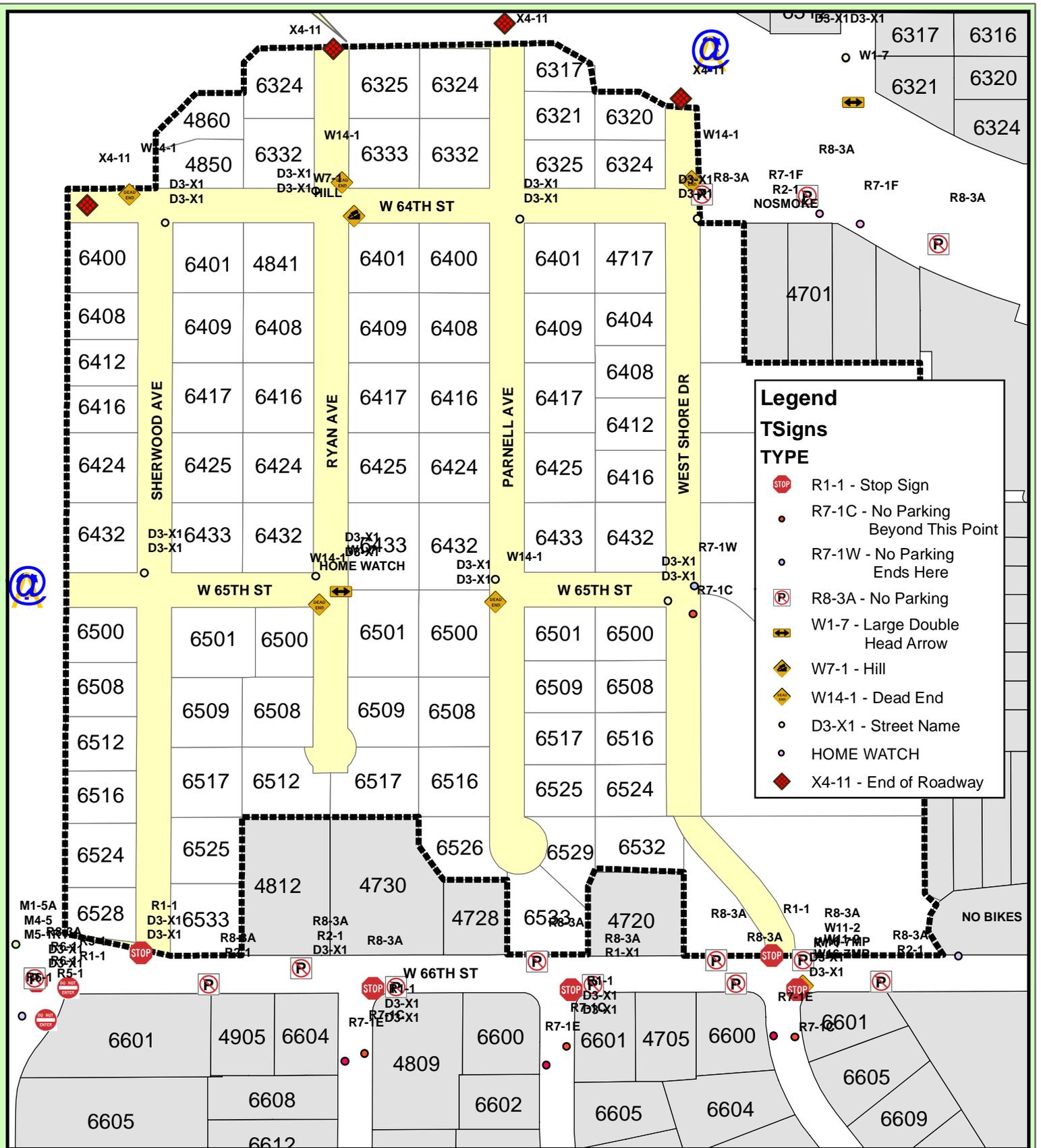


Street Lights Normandale Street Reconstruction Improvement No: BA-394



Engineering Dept
October, 2012





Legend

TSigns

TYPE

- R1-1 - Stop Sign
- R7-1C - No Parking Beyond This Point
- R7-1W - No Parking Ends Here
- R8-3A - No Parking
- W1-7 - Large Double Head Arrow
- W7-1 - Hill
- W14-1 - Dead End
- D3-X1 - Street Name
- HOME WATCH
- X4-11 - End of Roadway



Signs

Normandale Neighborhood Roadway Reconstruction Improvement No: BA-394





To:
Cc:
Bcc:
Subject:

Toby Muse

FYI - BETF comments are below. Toby Muse, P...

11/08/2012 08:52:11 AM

From: Toby Muse/seh
To: Paul Pasko/seh@seh,
Date: 11/08/2012 08:52 AM
Subject: Fw: Meeting

FYI - BETF comments are below.

Toby Muse, PE | Project Manager
SEH | 10901 Red Circle Drive, Suite 300 | Minnetonka, MN 55343
952.912.2616 direct | 888.908.8166 fax
www.sehinc.com
SEH—Building a Better World for All of Us™

----- Forwarded by Toby Muse/seh on 11/08/2012 08:51 AM -----

From: Wayne Houle <WHoule@EdinaMN.gov>
To: Chad Millner <cmillner@EdinaMN.gov>, "Toby Muse (tmuse@sehinc.com)" <tmuse@sehinc.com>,
Date: 11/08/2012 08:19 AM
Subject: FW: Meeting

Please include these comments with the feasibility study.



Wayne Houle, Director of Engineering

952-826-0443 | Fax 952-826-0392
WHoule@EdinaMN.gov | www.EdinaMN.gov

**...For Living, Learning, Raising Families & Doing
Business**

From: Peter Kelley [mailto:katie1_lizzie2@yahoo.com]
Sent: Thursday, November 08, 2012 8:17 AM
To: Wayne Houle
Subject: Re: Meeting

Thanks Wayne,

I thought I'd gotten them comments out. Sorry, must have slipped through the cracks. Here's are general thoughts on the two areas that have bike routes:

a. Lake Edina area: Hibiscus and Kellogg are designated as primary bike routes in the comp plan. Group discussed access via West Shore Drive and possible future connections to 9 mile creek trail. As this is completely a residential area near few obvious bike destinations at the moment, it was felt that no special treatment was called for apart from possible signage indicated a bike route.

b. Normandale Neighborhood: Westshore Drive is a primary bike route. Close proximity to the Edina Water park and the ped bridge over Hwy 62 make Westshore drive an important bike route. The group recommends the redevelopment project consider a separated bike lane, or at minimum road markings and signage. Discussion included questions related to the prohibition of bikes on the paths around Lake Cornelia and if there was any chance of that being lifted.

Author of Paraglide, a young-adult adventure novel
Sample or purchase Paraglide at Amazon.com

From: Wayne Houle <WHoule@EdinaMN.gov>
To: 'Peter Kelley' <katie1_lizzie2@yahoo.com>
Cc: Chad Millner <cmillner@EdinaMN.gov>
Sent: Wednesday, November 7, 2012 9:46 AM
Subject: RE: Meeting

Peter:

Sounds good to me. Can you forward the BETF's comments on the Neighborhood Reconstruction Feasibility Studies? I don't believe that I have seen them yet.



Wayne Houle, Director of Engineering

952-826-0443 | Fax 952-826-0392

WHoule@EdinaMN.gov | www.EdinaMN.gov

**...For Living, Learning, Raising Families & Doing
Business**

From: Peter Kelley [mailto:katie1_lizzie2@yahoo.com]
Sent: Wednesday, November 07, 2012 9:44 AM
To: Wayne Houle
Subject: Meeting

Hi Wayne,

I've got a meeting on the calendar for today at 1:00, but something's come up and I need to cancel. Sorry for the late notice. I don't have too much to discuss, so I think we can just wait until December if that's okay with you?

Hope things are going a bit more smoothly for you.

Peter

Author of Paraglide, a young-adult adventure novel
Sample or purchase Paraglide at [Amazon.com](https://www.amazon.com)

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Appendix L

November 7, 2012 Normandale Neighborhood Informational Meeting

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Normandale Neighborhood Roadway Reconstruction

October 29, 2012

Dear Resident:

Please plan to attend an informational meeting about a bike lane and the sanitary sewer service replacement program from 5:30 to 7 p.m. Wednesday, Nov. 7 at the Public Works and Park Maintenance Facility located at 7450 Metro Boulevard.

The Edina Transportation Commission (ETC) recently reviewed the draft feasibility study for the Normandale Neighborhood Roadway Reconstruction Project and recommended a bike lane along the east side of West Shore Drive from 64th Street to 66th Street. Please recall that pedestrian facilities are no longer special assessed under the revised special assessment policy.

We will also continue the discussion about the sanitary sewer service replacement program. Recall that the sanitary sewer service replacement is assessable.

Please save the date of Dec. 11 for a project hearing before the City Council.

If you have any questions, please contact Toby Muse at 952-912-2616 or tmuse@sehinc.com or me at 952-826-0318 or cmillner@EdinaMN.gov.

Sincerely,

A handwritten signature in black ink that reads "Chad A. Millner".

Chad Millner, PE
Assistant City Engineer

ENGINEERING DEPARTMENT

7450 Metro Boulevard • Edina, Minnesota 55439
www.EdinaMN.gov • 952-826-0371 • Fax 952-826-0392

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2013 Normandale Neighborhood Roadway Improvements BA-394 Informational Meeting

November 7, 2012

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Agenda

- Introductions
- Project Location
- Process & Timeline
- Project Details
 - West Shore Drive Primary Bike Route
 - Assessment for Sanitary Sewer Service Pipe Reconstruction
- Communications
- Q&A

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Introductions

SEH Staff

<p>Project Manager Toby Muse</p> 	<p>Sr. Project Manager Paul Pasko</p> 
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City Staff

<p>Asst. City Engineer Chad Millner</p> 	<p>Engr. Coordinator Sharon Allison</p> 	<p>Environ. Engr. Ross Bintner</p> 	<p>Director of Engineering Wayne Houle</p> 
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Process



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Typical Timeline

<p>August/September</p> <p>October 25, 2012</p> <p>December 11, 2012</p> <p>January-March</p> <p>April/May</p> <p>October/November</p> <p>Spring 2014</p> <p>Summer/Fall 2014</p>	<p>Evaluate Infrastructure / Informational Meeting</p> <p>Edina Transportation Commission (ETC) reviews draft feasibility report</p> <p>Public hearing</p> <p>Plan preparation and bidding</p> <p>Construction begins</p> <p>Construction concludes</p> <p>Warranty work</p> <p>Final assessment hearing</p>
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Project Location



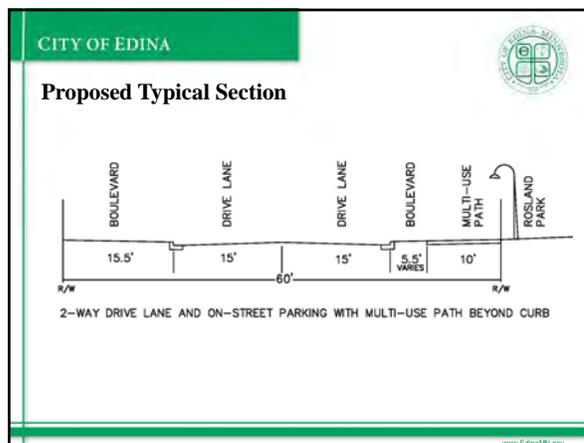
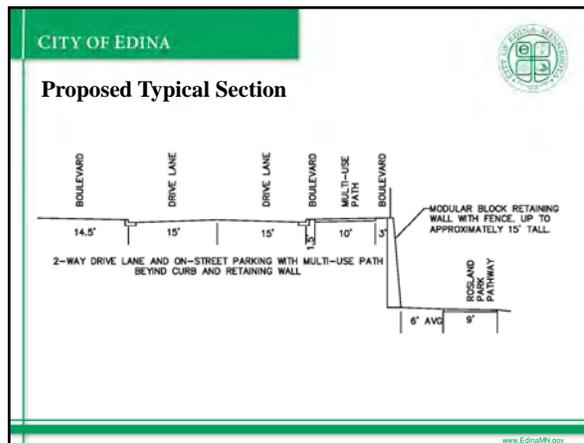
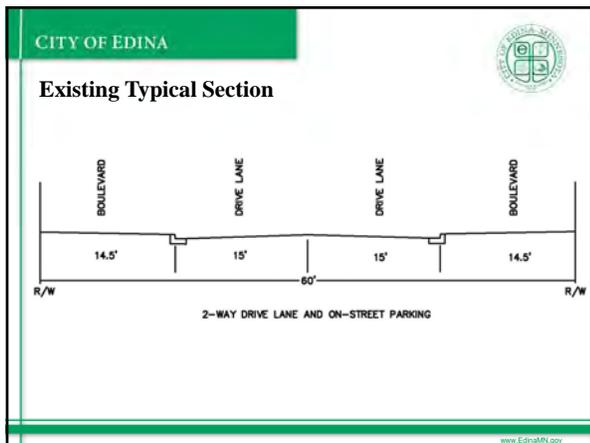
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Questionnaire Results

Neighborhood	% of Questionnaires Returned	Has a Sump Pump		Change Existing Lighting		Prefer New Sidewalk	
		Yes	No	Yes	No	Yes	No
Normandale	63% (52 / 83)	7 (13%)	43 (83%)	21 (40%)	27 (52%)	6 (12%)	45 (87%)

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Proposed Lights

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NORMANDALE NEIGHBORHOOD ROADWAY IMPROVEMENTS

INFORMATIONAL MEETING

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Typical Section Showing Sanitary Sewer Service Ownership

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Proposed Sanitary Sewer Service Improvements

- Homeowner can reconstruct segment C using their own contractor and
 - You have the option to add the cost to your street assessment,
 - or
- The City will reconstruct segment C using its contractor and the cost (\$2,500) is automatically added to the cost of your street assessment.

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Preliminary Assessments

Neighborhood	Estimated Assessment per REU	# of REU's
Normandale – Street	\$12,300	89
Normandale – Sanitary Sewer Service	\$2,800 \$2,500 (1)	70
Total Preliminary Assessment	\$12,300 - \$14,800	\$14,800

(1) This is not an estimate.

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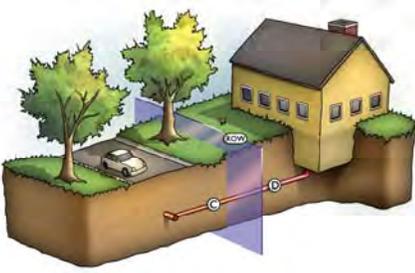
Preliminary Assessments

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Typical Section Showing Sanitary Sewer Service Ownership



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What You Can Expect

- We will keep you informed.
- You will have opportunities to provide input.
- Private utility work is to be completed before City work.
- We will do our best to minimize inconveniences, but construction does not come without pain points.

Providing Input

- Informational meetings, comment cards, public hearings and questionnaires

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Communication Tools

- Become a neighborhood captain to help facilitate project communication.
 - Let us know of someone in your neighborhood who might fit this role.
- You will be notified of all meetings, hearings, schedules and questionnaires via regular mail.
- Public hearing notices are also published in *Edina Sun-Current*.
- Door hangers are hung when there time-sensitive information.
- Final assessment notices are mailed one year after construction.

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City Extra

“City Extra” emails are the best way to receive regular updates once construction begins. These are free weekly email updates about your project.

- Sign up on City of Edina website, www.EdinaMN.gov.
 - Check the box next to your project name.
- If you cannot receive email, we will mail you City Extra updates upon request.
- It’s the best way to stay informed.
- Don’t delay; sign up today!

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Contact Us

Toby Muse, SEH Project Manager
 Phone: 952.912.2616
 Email: tmuse@sehinc.com

Chad Millner, City of Edina Assistant City Engineer
 Phone: 952.826.0318
 Email: cmillner@edinamn.gov

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Thank you!

Questions?

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**Normandale Neighborhood Roadway Improvements
Sidewalk Informational Meeting
November 7, 2012**

	NAME	ADDRESS
1	CITRICK S. SPENCER	6517 RYAN AVE
2	Barb Vogt	6333 Ryan Ave.
3	Beverly Johnson	6432 Ryan Ave S
4	Delores Ghl	6325 Ryan Av, S
5	Barb Spartz	6409 Farneff Ave
6	Pat Kellenner	6416 West Shore Dr
7	Ally C. Johnson	6525 Sherwood Ave
8	SCOTT FORBES	6516 WEST SHORE DR.
9	KEN CHAVIS	6509 RYAN AVE.
10	SKIP THOMAS	
11	Kara Carpenter	6801 Sherwood Ave
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From: Irene Olson
To: imuse@sehinc.com
Subject: bike land along West Shore Dr.
Date: 11/05/2012 11:52 AM

I have asked to have my email address added to your information file, but do not get it. I only hear about what is going on by my neighbor. Please add my email address irene6404@gmail.com. Thank you.

We am very much against adding a bike lane on West Shore Dr. First there is a walking lane around the lake, but the people with their children and dogs walk on the rode with the bikes. This is a bad accident waiting to happen. They do not ever think about the cars driving on the road that was indeed made for cars. If you add a bike lane, they will NOT stay in the bike lane. I know that there will be a serious accident if this happens. Think about this before you add another bike lane which we do not need.
Roy & Irene Olson.

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2013 Normandale Neighborhood Roadway Improvement Project
 Summary of November 7, 2012 Informational Meeting Question and Answer Session
 Meeting Location: Public Works and Parks Maintenance Facility
 Meeting Time: 5:30 pm to 7 pm

Number	Question	Answer
1	Why can't more City lots be added to the City's portion of the assessment?	City owned property was divided into 7 REUs on buildable land. The lot sizes are in proportion to the size of other REUs in the neighborhood already developed.
2	Are there other dedicated bicycle route options for this project, like on-street lanes?	Staff considered on-street bicycle lanes but determined they were not feasible because 1) there is a long review process to gain approval from the federal government and 2) the addition of bicycle lanes on both sides of the street would cause a parking hardship for properties located mid-block between 64th Street and 65th Street and between 65th Street and 66th Street.
3	Why aren't bicycles allowed on the Rosland Park pathway?	Multi-use travel around the trail would present a safety hazard due to the existing tight curves on the trail. There is not enough adequate sight distances to avoid bicycle/pedestrian collisions.
4	Why not bury the multi-use trail closer to the walking path and hide it from the street?	Staff will consider this option and present it in the feasibility study for Council review.
5	Will landscaping near 66th Street be incorporated if the multi-use trail is included as part of the project?	Yes, the City will review the Comprehensive Water Resource Management Plan to take a look at how incorporating proposed landscaping features will contribute to water quality contributions to the nearby pond.
6	How many bikes use this area?	The City does not count bicycle traffic, but expects that if a multi-use path is incorporated, bicyclists from the south and west of the neighborhood will use the route to access the pedestrian/bike bridge over Highway 62 and the water park.
7	What is driving this project, the street reconstruction portion or the sanitary sewer portion?	Ultimately, the street reconstruction portion. Since the sanitary sewer system is in poor condition, now is a good time to rehabilitate/reconstruct it as well.
8	My sanitary sewer service is 26 years old, do I have to replace it?	Check in with the City's building department to see if there are records of what upgrades have been done to your sanitary sewer service. If you have upgraded your service from at least the property line to the main, you will be exempt from the sanitary sewer service reconstruction assessment portion of the project.
9	Does Segment D need to be upgraded as part of the project?	No. Only Segment C will be required to be reconstructed as part of this project. However, staff recommends you at least get quotes from private contractors to reconstruct Segment D along with Segment C to avoid possible future repair costs.
10	Can we upgrade Segment D at a later date?	Yes. You can upgrade Segment D at any time, but if you choose to do it as part of this project, there will be defined timeframes when the work will need to occur in order to continue project progress.
11	How long will a liner last?	Manufacturers state 50 years.