

City of Edina: 54th Street Reconstruction and Arden Park Area Stormwater Plan

Draft Design Stakeholder Feedback

23 October-3 November 2013



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Overview and Executive Summary

Based on stakeholder input and feedback starting in June 2013, beginning with an October 23, 2013 workshop the project design team offered draft designs for the three segments of the project area:

- East, from Minnehaha to France
- Middle, from Brookview to Minnehaha and including the bridge, plus Minnehaha Creek and stormwater issues
- West, from Wooddale to Brookview

The project design team also presented draft designs and updates for the bridge, rapids and grade-control structure, boat landing, lighting, intersection configurations, and bus stops.

On 23 October over 40 people attended an in-person session at City Hall, and 43 contributed via an online survey open through 3 November. Below is a very high-level summary, followed by complete details.

East end: This draft design generated varied responses, with continued concerns about the six-foot increase for a short section just east of Minnehaha Boulevard to accommodate additional parking on Sundays. There was less concern for the two-foot increase for the remaining section. Other feedback included a need for landscaping around the rapids and bridge area.

Middle section, bridge, rapids and grade-control structure, landing: There was consistent support for the draft design, in particular around increased safety for children and creek users by raising the bridge and minimizing the need to cross 54th Street. Stakeholders indicated concerns over whether the new construction would damage the creek or rapids in any way, the overall width of the road, and the potential for undesirable activities underneath the bridge. Some also said they wanted a sidewalk on the south side to watch boat users and other creek activities. Overall, participants strongly supported maintaining the rapids and grade-control structure, and various stakeholders expressed different design ideas for the natural landing.

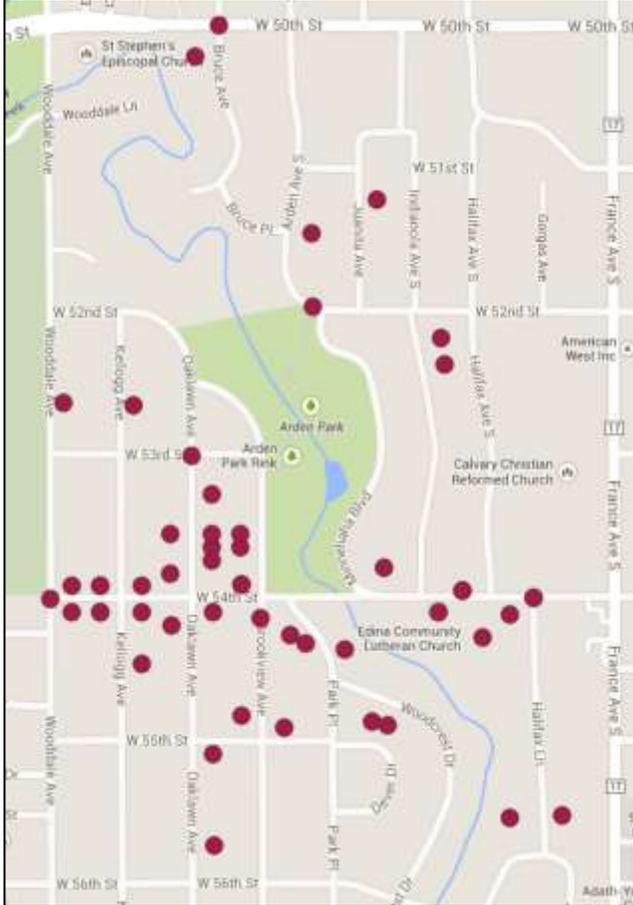
West end: Consistent throughout the engagement process, the draft design for this section generated the most response, especially from residents on 54th Street. The concerns remained focused on the impact of the increased width on properties and vehicle speed. Even though the draft design responded to stakeholder concerns by using shared bike/vehicle lanes (2-foot width increase) and no tree removal, some stakeholders continued to express their objections to bike lanes. Others wrote that a shared vehicle and bicycle lane is appealing.

Lighting, intersection configurations, bus stops: Responses to these topics were limited. Although the in-person and online surveys attempted to clarify that lighting decisions will be made during the final design process in early 2014 and will include stakeholder input, some stakeholders expressed a need for more details and information, and would have preferred being asked sooner.

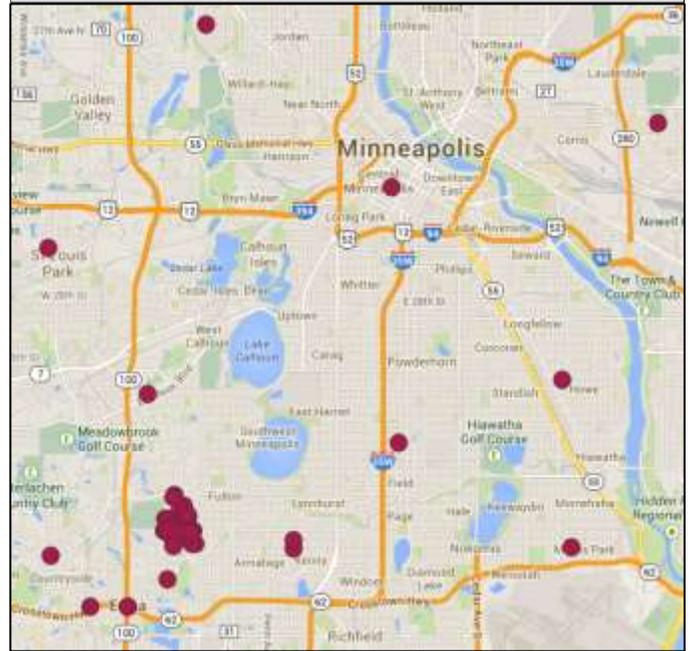
Envision: This innovative new tool to evaluate the sustainability of infrastructure investments was applied to the design and the results displayed at this feedback session. These results will be finalized and included in the Feasibility Study and design recommendations to the City Council in December 2013.

Participants: The maps below show the locations of the in-person and online participants in this round of stakeholder feedback who provided address information.

Map showing participants in project area



Map showing all participants in this round, except 7 people from Princeton, Redwing, Minnetonka, Crystal, Vadnais Heights, Shoreview, and Robbinsdale



Stakeholder Impact on Design: The table below illustrates the impact of stakeholder contributions on the design process.

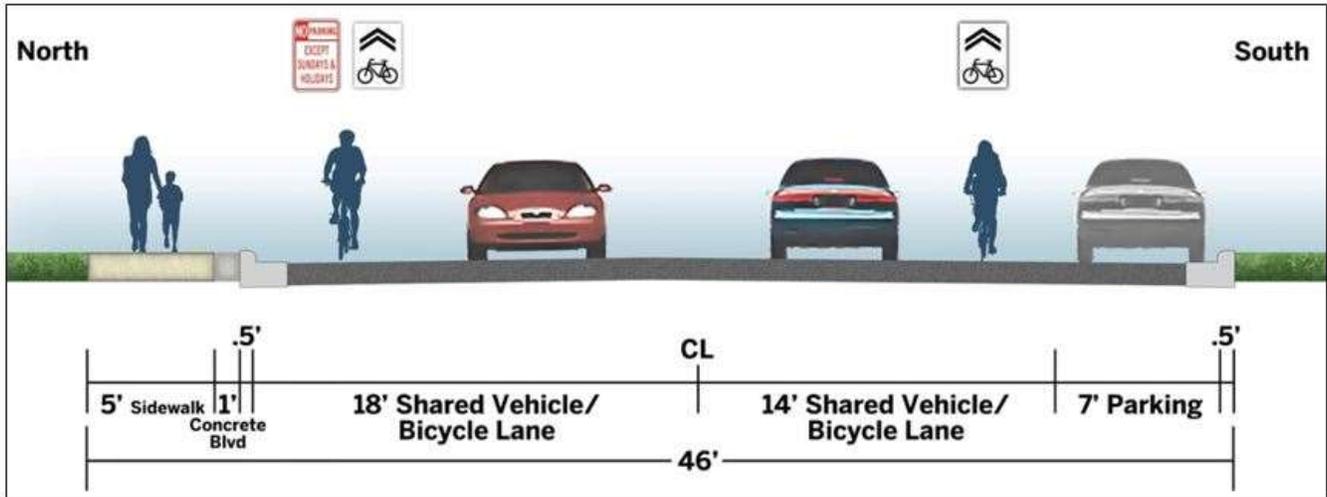
Issues/Components	Summer 2013 Status	October 2013 Status
Boulevard width	5' Turf	1' Concrete
Lane width	17': 11' vehicle, 6' bike	14': Shared vehicle and bike
Sidewalk width	5'	5'
Lighting type	Not specified	Downward facing/dark sky
Bus stop pads	Optional	Not included
Wayfinding signs	Optional	Not included
Tree removal (entire corridor)	8 removed	0 removed
Relocated bus stops	Optional	Recommended; staff work with Metro Transit
Crossing safety at Minnehaha Boulevard/bridge	<ul style="list-style-type: none"> Stop sign: Optional Flashing sign: Optional Zebra crosswalk: Optional 	<ul style="list-style-type: none"> Stop sign: Included Flashing sign: Not included Zebra sign: Not included Shelf under bridge to reduce pedestrian crossings
Bridge	<ul style="list-style-type: none"> Arch or slab: Optional Lighting design: Open Aesthetics: Open 	<ul style="list-style-type: none"> Arch Lighting design: Further feedback required Aesthetics: Further feedback required

East End Draft Design

EAST Description and Cross-Section A

1. East of Minnehaha Boulevard for approximately 350 feet up to Halifax Avenue:

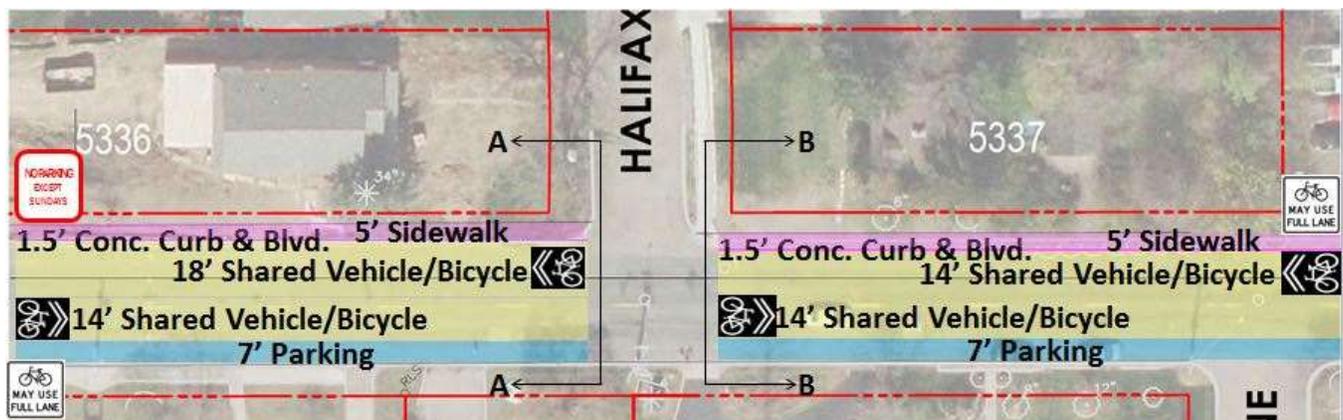
- Increase total width from 40' to 46' *for this short section only*
- One 7' parking lane on the south side
- One 14' shared vehicle/bicycle lane
- One 18' shared vehicle/bicycle lane *and Sunday-only parking* to accommodate parking needs near Edina Community Lutheran Church
- One 5' sidewalk and 1' concrete boulevard for safety, on north for Park access



EAST A Stakeholder Needs/Priorities Addressed

The recommended design addresses the following stakeholder needs, issues, and concerns, and meets minimum State Aid and City design requirements:

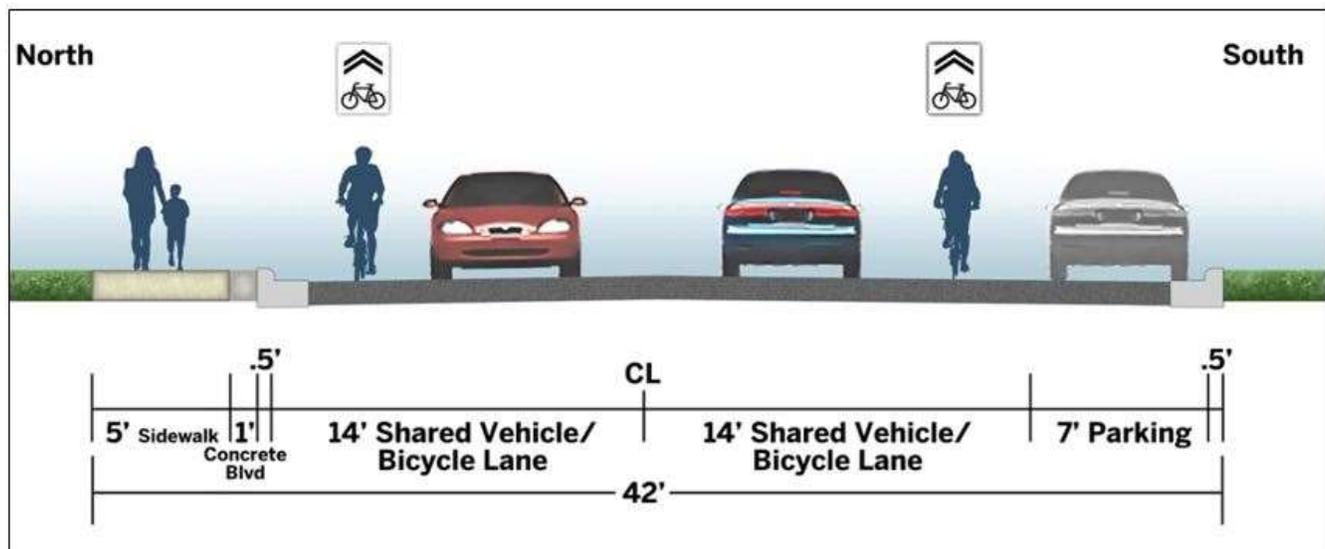
- Minimizes impact to adjacent right-of-way and calms traffic by using MnDOT's minimum width design standards for shared vehicle and bicycle lanes along with a dedicated parking lane
- Removes *no* trees from either side of the street
- Improves safety for bicyclists by creating facilities consistent through the corridor
- Accommodates the need identified in the parking study for additional parking near Edina Community Lutheran Church on Sundays
- Avoids a significant construction schedule delay to relocate 5 overhead power/telephone/cable TV poles and significant impact to Edina Community Lutheran Church landscaping, lighting, and retaining walls by generally matching the proposed south edge of the new concrete curb and gutter with the existing south edge of the existing street
- Minimizes the number of pedestrian street crossings by adding a sidewalk on the North side to support access to Arden Park, the corridor's primary pedestrian destination
- Increases pedestrian sidewalk safety, as recommended by the ADA, by installing a 1-foot detectable warning strip of exposed aggregate concrete between the concrete curb and the sidewalk



EAST Description and Cross-Section B

2. East of Halifax Avenue to France Avenue:

- Increase total width from 40' to 42' *for this section only*
- Two 14' shared vehicle / bicycle lanes
- One 7' parking lane
- One 5' sidewalk with a 1' concrete boulevard for safety, on north for Park access



EAST B Stakeholder Needs/Priorities Addressed

The recommended design addresses the following stakeholder needs, issues, and concerns, and meets minimum State Aid and City design requirements:

- Minimizes impact to adjacent right-of-way and calms traffic by using MnDOT's minimum width design standards for shared vehicle and bicycle lanes along with a dedicated parking lane
- Results in no tree removals on either side of the street
- Improves safety for bicyclists by creating facilities consistent through the corridor
- Avoids a significant construction schedule delay to relocate 2 overhead power/telephone/cable TV poles and removal of at least 5 trees by generally matching the proposed south edge of the new concrete curb and gutter with the existing south edge of the existing street
- Minimizes the number of pedestrian street crossings by adding a sidewalk on the North side to support access to Arden Park, the corridor's primary pedestrian destination
- Increases pedestrian sidewalk safety, as recommended by the ADA, by installing a 1-foot detectable warning strip of exposed aggregate concrete between the concrete curb and the sidewalk

East: Appealing

- looks good
- More appealing if parking is on both sides 7 days/week.
- Nice, shared use.
- Section B looks to me moving in a more reasonable direction
- sidewalk

East: Concerns

- It's a shame that the current plan does not include exclusive bike lanes. Sharrows seem like a weak attempt to include cycling into the infrastructure, especially on a street frequently used by SVMS students who bike to and from school. We will look back on this as a miss as the biking trend is growing.
- The street currently works fine as is. The parking on the north side while sparsely used provides a visual narrowing of the roadway with just one parked car. As currently configured the road is highly available to bikers for biking along the curbside.
- 12" concrete boulevard still adds unnecessary width to project. ADA may "recommend", but is it required? Is a nursing home/hospital nearby where we have a larger than average wheelchair population?
- way too wide
- Section A effectively moves problem traffic speeds and noise pollution traffic 4' closer to my dining room/family room and will have an exclusive adverse impact on our property value, quality of life, and safety of our property.
- road is way too wide....promotes speeding
- Keep the street configuration as it is. Add a sidewalk and be done. If it isn't broke don't fix it.
- Have curb bump-outs at intersections been explored to help calm traffic speeds?
- don't need parking
- It's concerning the Section B was WITHHELD FROM PRESENTATION at last night's preliminary proposal meeting. Clearly ALL resident tax payers would have far greater support for Section B implementation consistency to the creek.
- don't need the sharrows.
- skip north side parking even if needed on Sundays (they can make other arrangements). 5' bike, 11' travel, 11' travel, 5' bike, 7' parking = 40'
- Narrow streets!
- Keep narrow – sidewalk on south? Where is already exists – put pedestrian sign at bridge
- Why not have 4' sidewalk with 1' concrete curb/rumble strip?
- Keep the current street configuration of the east section of 54th. It currently serves the needs of everyone (residents, bikers, traffic) every day of the week.
- Keep 54th St. roughly same width as now
- This furnishes ample space for the uses
- How about speed bumps? Don't make 54th a thoroughfare! Narrow the street!
- Please don't make 54th a big thoroughfare. Keep the charm
- Falls area is a diamond in the rough. Needs landscaping
- Reduce speed with speed bumps
- Speed bumps?
- Speed bumps are not required on 54th St.
- Do not install speed bumps. Anywhere.
- Considering and blindly instituting are different things
- Overwhelming neighborhood input was narrow not wider. Why ask for input when it is not considered?
- Why can't 18' be 14'?
- 14' too wide. Please show proof of state-aid requirement
- Don't make 54th into a thoroughfare
- Narrower streets would reduce speed
- Narrow streets!
- Speed bumps? Minneapolis has them Douglas
- I'm very opposed to shared vehicle/ bicycle lanes. Especially considering the sloped nature of 54th Street. When wet/icy conditions exist you are inviting trouble.
- Collectively I believe all ADJACENT IMPACTED STAKEHOLDERS would like to see the narrowing

of the shared vehicle/bike lane to 12' by way of variance if/where necessary.

- wider street means faster cars/ awful plan
- More can be done on east side to help calm traffic (curb bump-outs near intersections). Why can't

advisory lanes be used like there is now? Parking is rare and bikers end up using parking lanes when they are empty. My concern is that it will be wide open and enticing for speeders.

- Don't let 54th become a way to bypass 50th

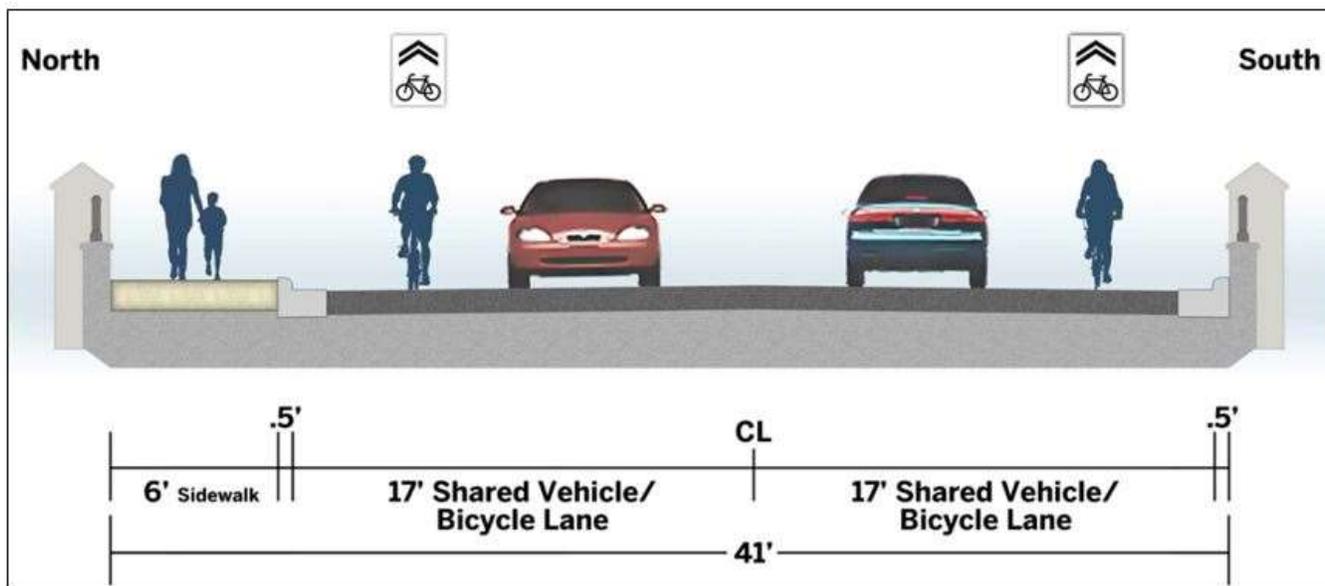
East: Comments

- Parking both sides, shared lanes, sidewalk.
- What exactly is the "significant construction delay"? How long? How bad can it be if we plan on it? It's not like it is unexpected.
- As long as the local residents are good with it!!!
- disappointing. staff needs to push harder for dedicated bike facilities--figure out how to make it work and figure out how to communicate with public.
- Please provide details on the number of residents in this neighborhood who voted for this option.
- Landscaping around bridge and east side of 54th needed

Middle Section Draft Design

MIDDLE Description and Cross-Section

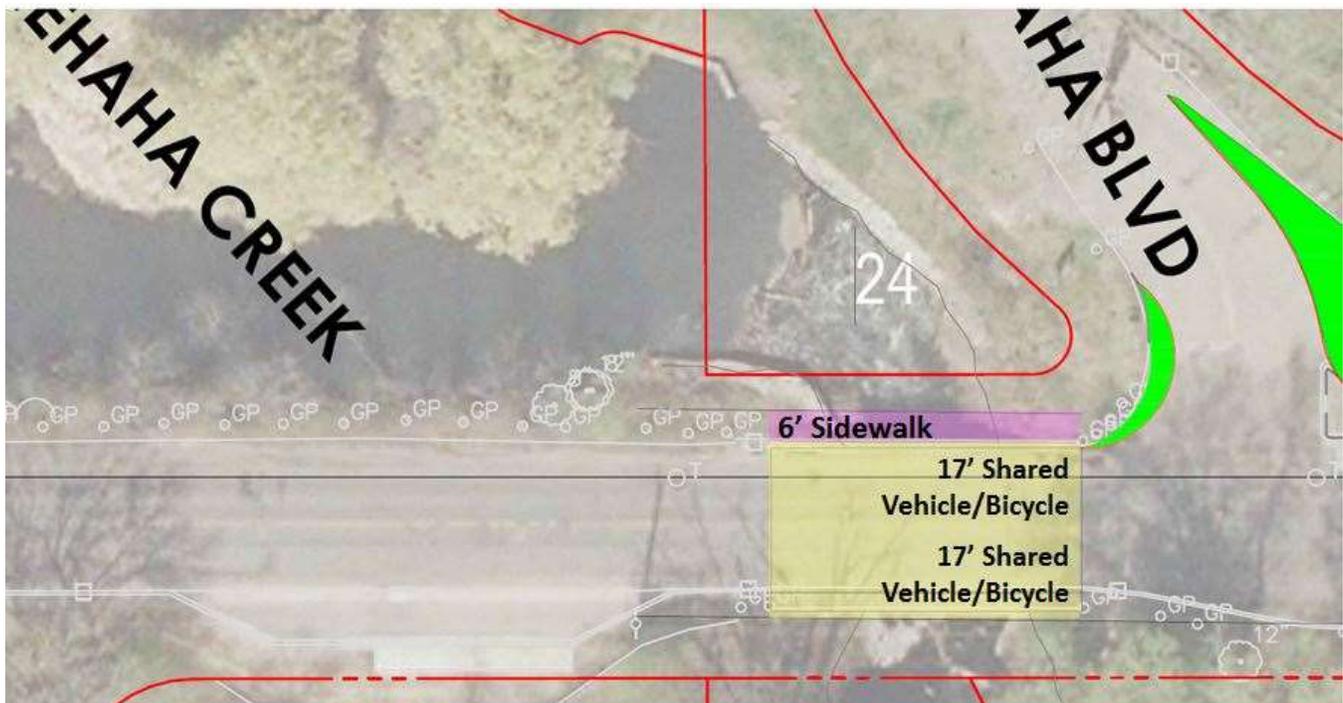
- Increase current width at bridge from 33.5' to 41'
- Two 17-foot shared vehicle / bicycle lanes
- 6-foot sidewalk on the north side to support Park access



MIDDLE Stakeholder Needs/Priorities Addressed

The recommended design addresses the following stakeholder needs, issues, and concerns, and meets minimum State Aid and City design requirements:

- Raises the bridge approximately 3 feet to accommodate the access bench underneath; this improves safety by minimizing the need for creek users to cross 54th Street either to avoid the rapids or run them repeatedly
- Provides sufficient width to accommodate future bridge needs
- Improves safety for bicyclists by creating facilities consistent through the corridor
- Provides decorative knee-wall and railing for vehicle, pedestrian, and bicyclist safety



Middle: Appealing

- very good
- No longer having to cross 54th street to do laps of the whitewater
- raising bridge; access bench; rapids access
- raising the bridge is a great idea to provide better clearance for paddlers.
- Bike lanes, sidewalk
- Repeat creek users could portage back and forth without disrupting traffic.
- I like raising the bridge to be able to run the rapids continually with bench
- Access bench to run rapids repeatedly.
- I love that it is wider to accommodate foot/bike and auto traffic
- The aesthetic of the bridge design were appealing
- Sidewalk
- Raised bridge offering more headroom for boaters going underneath.
- Access bench to portage if desired.
- Widen road.
- This IS a park and should continue to have wild spaces
- I love the height of the bridge for creek users

Middle: Concerns

- It's a shame that the current plan does not include exclusive bike lanes. Sharrows seem like a weak attempt to include cycling into the infrastructure, especially on a street frequently used by SVMS students who bike to and from school. We will look back on this as a miss as the biking trend is growing.
- Why increase from 14' on the east side back up to 17'? That is NOT consistent with the west side. We don't want a wider bridge! What are the future bridge needs you mention? Why on earth does sidewalk increase to 6' when 5' is perfectly fine? What is width of side walls? They add significant amount to the overall width of the bridge. I would keep a railing system so creek can be viewed by motorists AND bicyclists as they are going by. The 41' dimension you use is VERY misleading because it doesn't even take into account the decorative knee wall/railing thickness
- That any re-build would damage the playable feature in the rapids
- How much will the street have to be widened?

- I would put the sidewalk on the south side. Often people enjoy watching boaters on the wave to the south of the bridge. A sidewalk on the south would also make it so pedestrians are not required to cross Minnehaha Boulevard
- The bridge width should - not - affect the creek bed width, as this would affect the water velocity going through the rapids, and potentially negatively affect them. PLEASE - consult with a whitewater flow firm to ensure the rapids are not destroyed, thanks!
- New design will disrupt the kayak surfing wave.
- Too wide. 17' lane is not necessary and should not continue to be forced upon the residents who pay taxes in this community.
- Too wide....can we not cut down on the width of the bridge to keep it quaint and fit into the neighborhood
- Where is the sidewalk on the south side? People like to be able to enjoy BOTH sides of bridge.
- High water might go above bench under bridge and high water times are just when this access is needed.
- Kids and adults alike wanting to observe creek activities on the SOUTH SIDE as they do now, will have nowhere to stand -- which could lead to them simply occupying the bike lane, resulting in an obvious traffic conflict!!!
- It is unfortunate that we are calling these 'shared bike lane' when in fact the road is being widened to an extent that designates the bike lanes.
- there's room for two dedicated bike lanes 6' 11' 11' 6'
- design lacks detail
- Keep stop sign please!
- Why increase shared lane to 17' when it is 14' further down the road? Keep it simple!
- Put sidewalks on both sides of the bridge – no widening required
- 2nd sidewalk No! No!
- 17'? Why so wide?
- Narrow streets!
- I would drive faster if the road was wider...
- Proven fact that wider roads equal faster cars
- The overwhelming input from those most affected was to not widen 54th Street to reduce traffic speed
- Road still too wide
- I'm very opposed to shared vehicle/ bicycle lanes. Especially considering the sloped nature of 54th Street. When wet/icy conditions exist you are inviting trouble.
- Do NOT raise bridge 3"! This is NOT a water park. You are designing for the 1% here...design for the neighborhood first!
- If there is one thing that has been made very clear through this process it is that the local resident tax payer do not support bike lanes that widen W54th st. There are days where we have ZERO road bikers utilizing these lanes. The bike volume/neighborhood support/relevance of the lanes simply don't exist.
- Way too wide! Sidewalk needs to be on both sides of bridge. 17' much too wide for a shared lane!
- Raising the bridge is not well thought out. Kayakers and canoeists just are not a large enough population that occupies this area. Where have they been since the very first neighborhood meeting? That would change the character of the neighborhood for the worst.

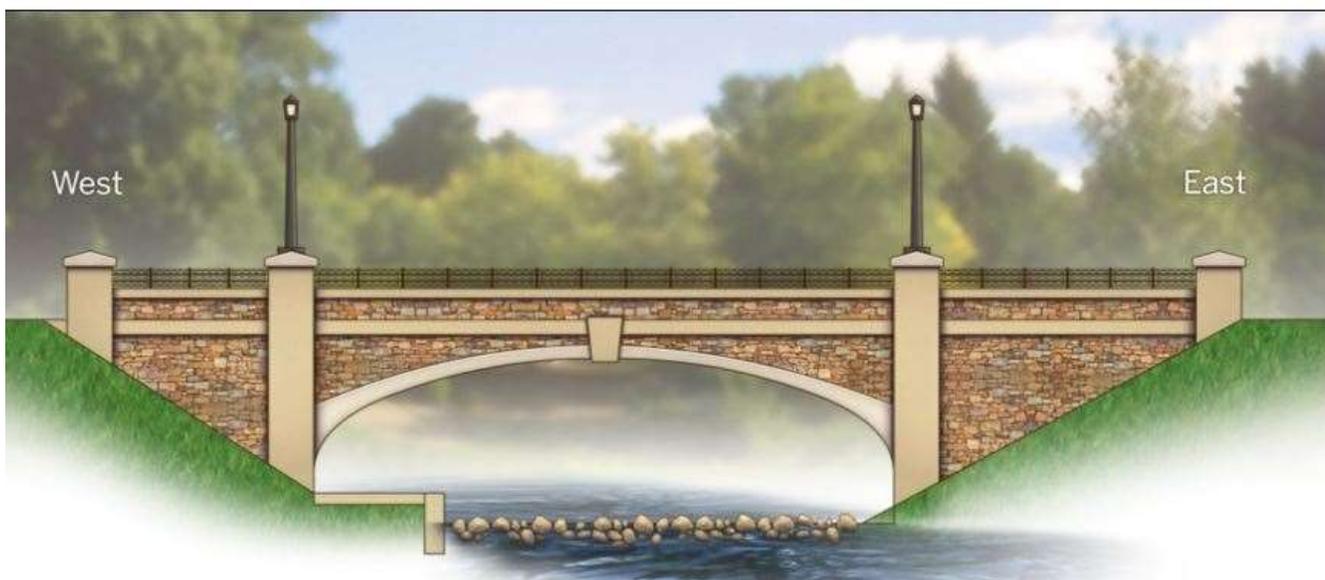
Middle: Comments

- Go whitewater!!!!!! :)
- Again, from empirical observation this past Summer, most of the creek "audience" congregated on the SOUTH SIDE -- where a bike lane would now be. This makes no sense.
- Would like to see wave improved with possible fish ladder. Runoff when it rains drops a lot of oily water just below bridge.
- better judgment needed--can't expect to approach projects the same way (even with increased opportunities for public input) and expect better results
- Be sure to allow for future path on Minnehaha Boulevard

Middle Draft Design: Bridge

MIDDLE Bridge Design

- A natural bridge design is recommended based on stakeholder feedback to fit better into the neighborhood and complement the creek
- The actual type of bridge has not been determined; the City will gather stakeholder feedback on bridge aesthetics, lighting, and railing during final design
- The *example* bridge shown accommodates water flow and recreational use
- 42-feet of clear width provides for a 10-foot wide access on the west within the arch
- Stone texture and coloring could match local limestone



Bridge: Appealing

- Stone looks nice, appealing to the neighborhood; lights provide safe, yet intimate illumination
- Limestone fascia
- Looks good
- 10 foot wide access within arch; access to rapids for recreational use
- I love paddling and surfing on these rapids and keeping them is the most important part of the project for me. Please keep them. A mini whitewater park would be amazing even if it's just one wave.
- Good looking bridge design
- Shelf to allow portaging is a great idea for safety. BUT, only necessary if the gradient and recreational feature (wave) is maintained
- [Shelf, raised bridge] Best solution for safety of creek users – so they can stay off the road
- Natural look. Local materials
- Nice aesthetic.
- Access bench
- I like the ability to see the creek thru the metal fencing
- Stone and general design look appealing and of quality

Bridge: Concerns

- I have not seen accurate contextual elevations. Isolated bridge image does not provide enough information to understand how design relates to surrounding area

- in drawing, stone looks too golden--consider matching color of stone on 50th
- Ledge under bridge will attract unwanted activity; not a safe feeling when walking at night
- Need to show people so we have an idea just how large this is
- What will the path as a feature be like if it is under water in the spring?
- shelf
- How is the path under the bridge sitting decided. Will it be on summery water levels and then under water in the spring when water is released?
- If the concrete has to be removed please rebuild the wave the same or better.
- Walkway under bridge will attract graffiti
- Where does the south end of the walkway go? Walkway not required. Minimize change to current grade.
- No walkway below bridge. Crosswalk is good enough.
- Walkway under bridge will draw unsavory behavior
- Do not raise bridge!
- Minnesotans know how to portage – no walkway under bridge
- No walkway under the bridge, please
- No walkway needed under bridge. Portaging helps slow down traffic
- Walkway under bridge is unnecessary. Bad spot to portage from north side
- Boaters should portage above bridge – this will be a congregating point for teenagers
- Good spot for drinking alcohol if you're a minor
- This encourages crime, vandalism, teen congregation for better part of the year
- Headroom on passage limits its functional
- While a nice, aesthetically appealing design, the curvature cuts down the headroom on the creek left side.

Bridge: Comments

- Keep bridge the same height it currently is
- How much will the grade of the bridge have to be raised to accommodate the path underneath?
- Copy design of other bridges over Minnehaha. Example: 50th and Wooddale by St. Stephens
- Is current bridge structure being removed?
- What is grade-control structure?
- Make bridge look old and charming – not modern or cheap
- Design lacked detail--how will raising the bridge affect elevation of street, especially at Minnehaha intersection?

Middle Draft Design: Rapids and Grade-control Structure

MIDDLE Rapids and Grade-control Structure

- Most stakeholders who provided feedback preferred maintaining the rapids for recreational use
- A number had questions about the purpose and need for the above-water portion of the grade control structure
- As of mid-October, the Minnehaha Creek Watershed District was exploring the feasibility of removing the structure while maintaining a form of rapids for recreational use

Middle rapids/grade-control: Appealing

- That the community is overwhelmingly for maintaining the rapids and not "dumbing them down" to a meager water slide.
- YES...keep the rapids AS IS!
- maintain the rapids
- Keep the rapids playful.
- Keeping rapids
- Maintaining the rapids as they now are would be fantastic.
- improving wave
- Maintain the rapids!
- the rapids look like fun for a number of kayakers and it would be great if we can accommodate them
- Keep as natural as possible
- Maintain rapids
- I like rapids ☐
- I have personally witnessed fish navigating up and downstream through the rapids so a fish ladder that risks destroying the rapids is unnecessary. At most, a minimal width ladder would suffice.
- Keep rapids.
- Maintain grade control structure (rapids)
- Please have the watershed district consult with a knowledgeable whitewater park firm to preserve rapids for the wide spectrum of activities – kayaking, canoeing, tubing, shore fishing – they currently provide
- No messing with the rapids! Keep 'as is'
- Don't change the bridge footings or alter the current grade. This is a one-of-a-kind spot in the area for recreational use
- Structures to allow fish to move upstream should not change the character of the creek
- Minimize tree removal. Allow current gradient and recreational feature to remain
- I kayak the rapids on 54th St bridge, and don't want to see any changes to the rapids
- The 54th St bridge area is a favorite spot for many paddlers including myself. Other paddlers I have spoken with, do NOT want to see any changes to the rapids. To have a spot like this to paddle within the metro is a great thing. I don't see any reason to make changes to the rapids itself. Paddlers who go here also frequent local business' and spend money. I know I do. Changes to the rapids will result in lost revenue for the area.
- if a safe, fun water recreation facility can be created/enhanced as part of this project, that's a good thing

Middle rapids/grade-control: Concerns

- changing the current rapids area
- Removing the grade change would mean less oxygen for fish and less fun for the community
- HOWEVER -- the creek bed is made of deteriorating asphalt. So while this project is being constructed, it would make sense to replace that creek bed with a new facing.
- disrupting the kayak and canoe play wave. Would also like to see all the jagged rock on the slide to the wave removed. I have seen enough people capsized

and swim prior to the wave get cut up on the very jagged rock

- Change to grade control structure by watershed district
- The less “engineered” the creek is the better. Fish can and do pass under the current bridge
- The recreational value of the wave of current creek gradient must not be minimized. The flow and creek should not change.
- Creek is not intended for rapids. This is a neighborhood not a park
- Again, "messing with" creek beds in the slightest can lead to wholly unexpected results, sometime destroying the "good" in search of the "ideal." It's best to bring in a whitewater consulting company well-versed in these designs to ensure this doesn't happen.

Middle rapids/grade-control: Comments

- Keep it simple. Don't raise the grade. Keep street narrower to keep speeds down
- Take these things out [Above-water grade control structures]
- Rapids one year, drought the next. How does a “grade-control structure” improve this and what is that?
- Would like to see wave improved and a fish ladder installed.
- One need only look to the Vermilion River in Hastings as a natural model for constructing a smooth surfaced, yet harmonious, pool / drop type rapids.

Middle Draft Design: Landing

MIDDLE Landing

- A natural landing is recommended based on stakeholder feedback
- Landings are proposed on the west side of the Creek on both north and south sides of 54th Street
- Exact types, locations, and configurations will be determined once the Minnehaha Creek Watershed District completes their evaluation of the grade control structure



Middle landing: Appealing

- good
- Natural landing makes more sense
- Like the approach
- yes, landings
- I real landing would be great and it would be awesome to have one that prevents erosion.
- Natural look
- This nicely melds with the environment while still providing access.
- Looks nice
- The canoe landings shelf are all based on maintaining the recreational nature of the creek is essential
- This would make it much easier to get in and out of the creek

Middle landing: Concerns

- How close will landing be to the actual rapids?
Preliminary design looked very close and dangerous
- Do not want to see any changes here
- Frost / heave could dislodge such a design?
- Nothing. If redevelopment of this landing only stands to draw external traffic into our Edina Tax Payer community, having an adverse impact on the home owners
- why do we need a landing if you will be able to pass under the bridge. This is not a state park area, this will promote loitering.
- It might make more sense to make the shore more on-level with the creek so (using that green canoe as an example) boats could be easily pulled up and not go floating off???

- Current landings make little sense for portaging. Move them to improve the route and make sure the path under the bridge corresponds. [Make sure landings are on the same side as the under-bridge shelf]
- Loose aggregate is slippery
- All of the overwhelming creek landing support seemed from a group of Mpls White Water Kayaker that chose to attend an Edina meeting. This was a very concerning part of the robust design process.

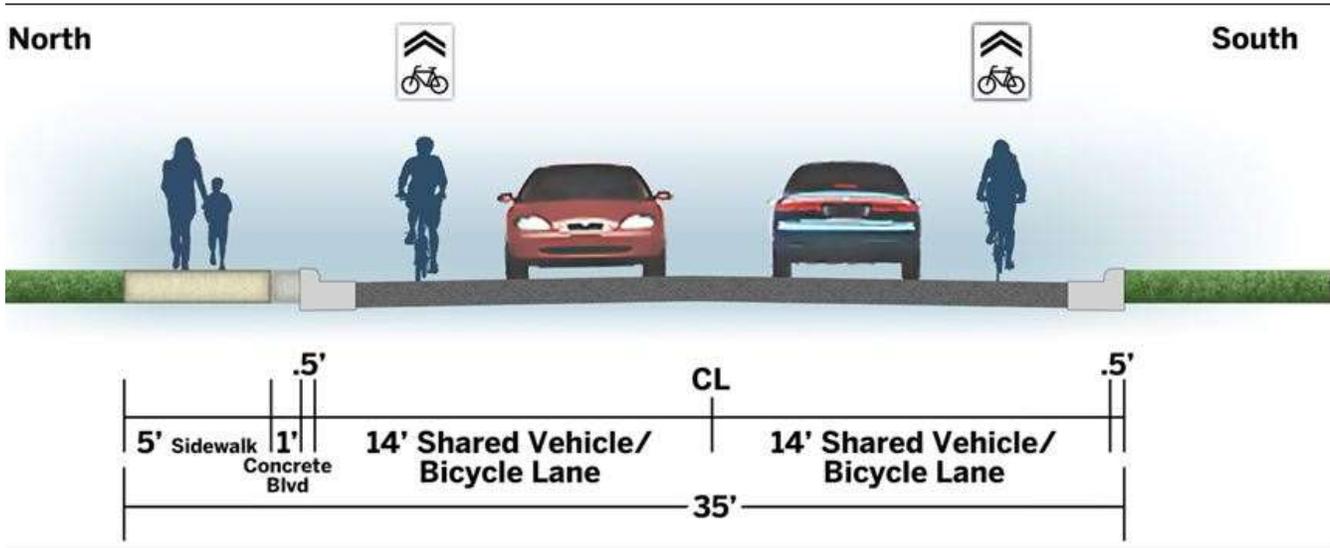
Middle landing: Comments

- Canoes always land on the east side – due to current
- Would changes remove wildflowers/general flora on west bank?

West End Draft Design

WEST Description and Cross-Section

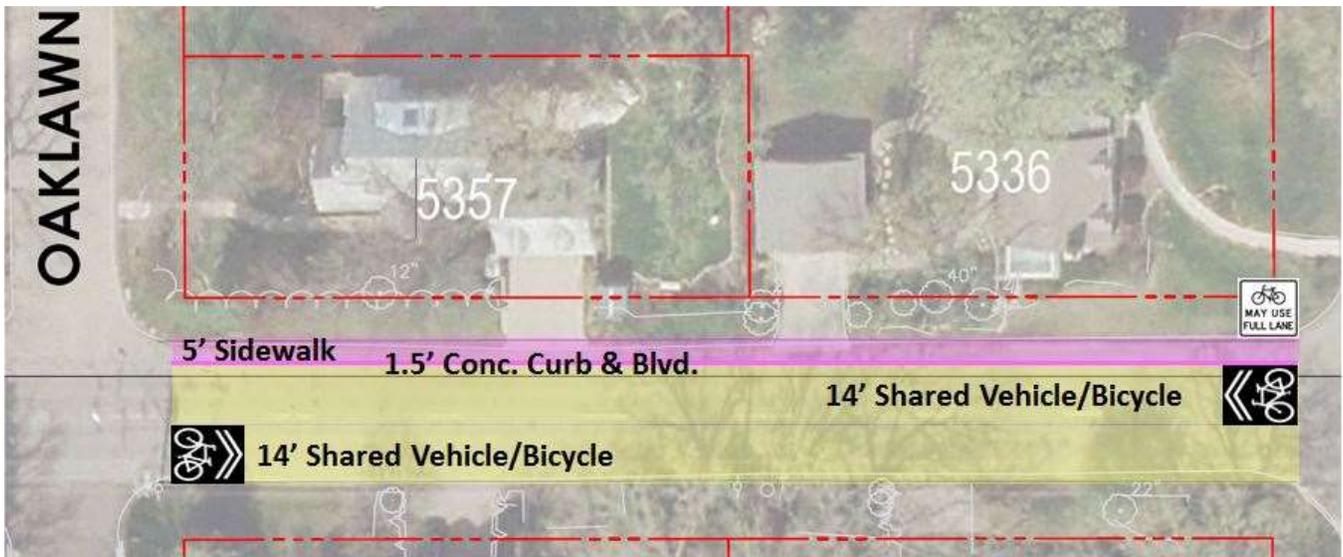
- Increase current average width of 32' to 35'
- Two 14' shared vehicle / bicycle lanes
- One 5' sidewalk and 1' concrete boulevard for safety, on north for Park access



WEST Stakeholder Needs/Priorities Addressed

The recommended design addresses the following stakeholder needs, issues, and concerns, and meets minimum State Aid and City design requirements:

- Minimizes impact to adjacent right-of-way and calms traffic by using MnDOT's minimum width design standards for shared vehicle and bicycle lanes
- Removes *no* trees from either side of the street
- Improves safety for bicyclists by creating facilities consistent through the corridor
- Avoids a significant construction schedule delay to relocate 8 overhead power/telephone/cable TV poles and remove at least 5 trees by
- Minimizes the number of pedestrian street crossings by adding a sidewalk on the north side to support access to Arden Park – the corridor's primary pedestrian destination
- Increases pedestrian sidewalk safety, as recommended by the American with Disabilities Act (ADA), by installing a 1-foot detectable warning strip of exposed aggregate concrete between the concrete curb and the sidewalk



West: Appealing

- fine
- Shared bike/vehicle lanes
- I like that the trees stay yet we have sidewalk and shared road program
- sidewalk
- Shared vehicle/bicycle
- Sidewalk
- Feedback is beginning to be heard, but not there yet.

West: Concerns

- Would be very happy if we could get west design to 34'. Thank you
- Width
- Still could be narrower overall; 1 foot concrete rumble boulevard does not need to make the overall sidewalk 6' wide...the 1 foot boulevard should be included in the overall 5' sidewalk...still appropriate for City snow removal equipment for City maintained sidewalks
- It's a shame that the current plan does not include exclusive bike lanes. Sharrows seem like a weak attempt to include cycling into the infrastructure, especially on a street frequently used by SVMS students who bike to and from school. We will look back on this as a miss as the biking trend is growing.
- Concrete boulevard/"glorified sidewalk extension" adds unnecessary width to the project; keep 5' sidewalk and just incorporate a 6"-12" rumble strip within the 5' overall width; still meets ADA requirements; a wheelchair only needs 5' to turn around; 35' is still too wide! We want to be as close to the existing 32'+/- width we currently have...adding an additional 12" of concrete is just not a luxury we can afford here. France/Wooddale Avenues' sidewalks are currently 5'.
- 14' is still too wide for the road. Need to control speeding by having narrow lanes
- 6' bike, 10', 10' + 2' = 28
- Where will the signage go and we need to keep that to a minimum
- Still too wide. Interesting that the 1' detectable warning strip is ADA recommended NOT required, which is how it was represented at meeting.
- The North side is required to take the lion's share of the overall change in width...there has to be a more equitable way to share this with the South side
- design lacks detail; 14' lanes
- Is the overall 35' width centered along the 60' right-of-way? It isn't, therefore the north side bears the brunt of excess
- This ENTIRE community wants to maintain more narrow driving lanes which support the high demand for walk-ability/safety that ALL resident/taxpayers support.

- Road is too wide – retain current width for safety and ambience
- Where is the proof that 14' is required vs. suggested?
- 34.5' still too wide – does not appear to be centered on right of way
- 14' still too wide. Our understanding that variance allows 11'
- Street too wide
- Narrow streets!
- Can this be done at 32' no wider
- These are larger dimensions than minimum
- 32' is fine!
- Street still too wide – need to keep speeding down
- Center the project
- Sidewalk too big!
- No bike/vehicle road. Vehicle only please
- Too wide!
- Which way will snow be plowed? North into yards? South into street?
- Have checked with MNDOT. Not the minimum!
- Speeding still occurs when widened
- Don't let 54th become a way to avoid 50th
- Don't need 1' concrete boulevard
- 4' sidewalk
- 32' total width
- Street still too wide – keep current width
- Would like to see proof of state-aid regulation for MN street width of 14'. 11' is plenty of space!
- Boulevard not required – and not worth widening the street, losing green space, diminishing property values
- 1' boulevard not necessary with 5' sidewalk
- 1.5' concrete boulevard not necessary. Respect wheelchair etc. needs, but sidewalk along accommodates them
- Still too wide!
- Don't take people's property for a bike lane
- No need for bike lanes
- Would prefer narrower travel lanes – at the expense of any bike lanes
- 4' sidewalk
- Too wide! Please keep the charm of the neighborhood
- Bike lanes not necessary on 54th
- Street still too wide – keep at 32' – concerned about speeding!
- 7:10 7:11 54th is secondary so why do we need the bike lane
- This would create a busier boulevard and decrease property value and eliminate green space. Do not widen
- Keep street narrow. Take out marked bike lanes – confuses drivers now – keep at 32'
- We do not need bike lanes at all. We haven't needed them – not that much bike traffic
- 35' is still too wide! Explain where ADA "recommends" 12' warning strip
- Remove mud delta
- I'm very opposed to shared vehicle/ bicycle lanes. Especially considering the sloped nature of 54th Street. When wet/icy conditions exist you are inviting trouble.
- do we really need sharrows??? People get it to slow down for the bikers, they don't need to have this pointed out...let common sense prevail!!!!
- Concerned for trees on north side; encroachment will eventually kill them due to root disturbance during digging; what trees would be removed if poles are moved further south? how far south are you making your assumptions? How many poles are you talking about? None of this was clear. What 5 trees are the ones to be removed? We've waited decades to get a new road, a schedule delay is not an issue here. You may as well do it right!
- Please reduce sidewalk to 4' with 1' strip which will work best with City equipment
- why do we need the 1' concert boulevard, incorporate that into the sidewalk width so the Cities snow plowing equipment will still work to maintain the sidewalk

West: Comments

- Narrowing the entire lanes to 13', removing the 1' warning strip, allowing a 5' sidewalk, might be a solution.
- Accommodate pop up drain pipe
- 5401 Oaklawn. Pop-up drain pipe from our gutter system – buried under pavers to northwest corner. Power line buried underground in driveway next to street. Low spot in grade by driveway on northwest corner.
- Have found exposed aggregate is slippery. Dimples better! [For the 1' boulevard]
- Recommended? Not required?
- West side: if design is not flexible and needs to have an unnecessary 1' concrete boulevard, then explore the idea of taking down utility poles. That will allow project to be centered along the right-of-way (back edge of south curb to back edge of north sidewalk). I may be more inclined to go with a 34' wide scheme with poles staying intact. It was stated that the required distance a new curb must be from a utility pole is 2'-0" (is this truly measured from face of curb- not backside- to face of utility pole?). 35' total width is still much wider than the current 32'+/- along most of the west side.
- Disappointing all around. Please evaluate public input process--generated a lot of comments, but not better results.

Lighting

Lighting

- Based on stakeholder feedback, the design recommends low-impact, downward-directed (sky-friendly) lights
- These are consistent with established City lighting standards
- The City will gather additional stakeholder feedback on preferred light styles during final design

Lighting: Appealing

- More light is good...gets very dark along the stretch at night.
- Subtle lighting.
- a large street light at the 54th/park place intersection
- How can I answer this without an image of the lights?

Lighting: Concerns

- feedback on lighting should have been gathered earlier
- Otherwise this is a waste of time.
- No need to light the place up like a Shopping Center. As is, the natural light in the Summer months was more than enough.
- it simply, urgently needs to be lighted - far too dangerous as it is now
- what are "established city lighting standards"?
- Show us pictures of the lighting options.
- is this cost necessary?

Lighting: Comments

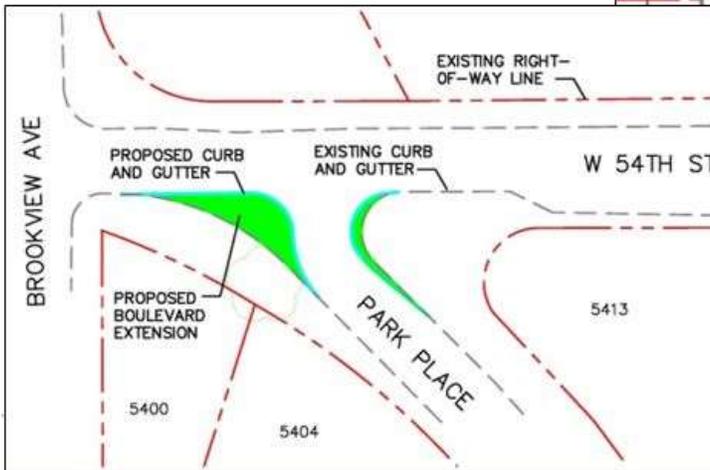
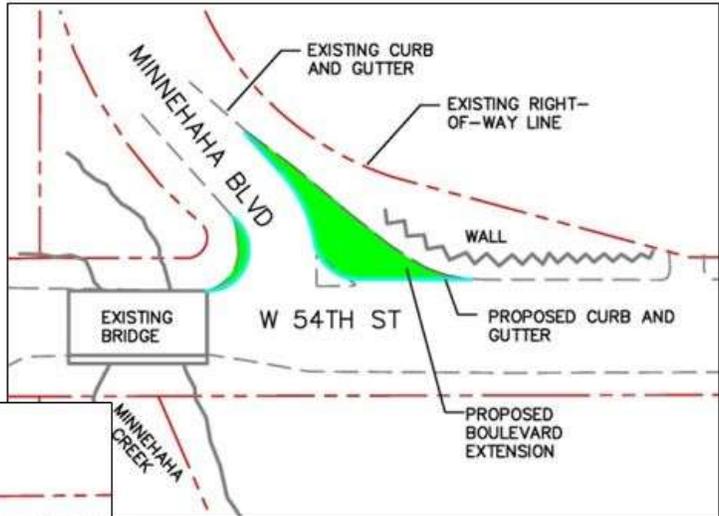
- lighting should not be an option, but a requirement wherever there is not adequate lighting for pedestrians

Intersection Configurations

Intersection Configurations

The recommended design reduces the curb radii at Park Place and Minnehaha Boulevard intersections.

- Reduces turning vehicular speeds
- Makes vehicle turns more deliberate for drivers and predictable for pedestrians
- Decreases maintenance



Intersections: Appealing

- looks good
- I do like the boulevard extension
- less pavement
- It's interesting, and will slow traffic.

Intersections: Concerns

- Right now, the parking to the right and left of the wording "Minnehaha Boulevard" though minimal -- means people turning right from 54th St. are met with a one-lane constriction.
- It might be best to keep cars (using a "No Parking Within 30ft." or what have you) away from 54th St.
- As is, cars parking on both sides led to congestion this past Summer.
- some negative feedback from past projects where curb radii reduced
- design lacks detail
- You have provided drawings with no rendering of sidewalks.

Bus Stops

Bus Stops

- The City will contact Metro Transit about relocating the existing bus stop on the south side of 54th Street at Minnehaha Boulevard to the existing sidewalk near the Edina Community Lutheran Church
- The City will contact Metro Transit about ridership along 54th Street and determine if bus stops can be consolidated along the corridor
- Small concrete bus stop pads may be placed at proposed bus stops where no sidewalk exists

Bus stops: Appealing

- fine
- Good to consult MTC; not really sure how many passengers are picked up/dropped off at locations in question
- The Bus Stop is MOVED AWAY from the creek, thank you!

Bus stops: Concerns

- It goes without saying that our family is absolutely opposed to the relocation of a bus stop to directly in front of our house. Our home is set very close to this street and this proposal is unacceptable to us.
- We are opposed to concrete bus stop pads. The non-peak bus volume is incredibly low. I am in support of any measure that reduces the number of buses that drive down W54th.

Bus stops: Comments

- This should already have been done

Final comments (online only)

- Obviously, take to heart the input from nearby residents but remember that streets are for public travel. Don't design a street for the people who live on it. It isn't their street. It's our street. "Our" meaning the pedestrians, cyclists, transit users and motorists who make up the general traveling public. If W. 54th were a private access road, the residents along it could make it as narrow as they want and dictate both its function and aesthetics. But it is not a private access road--it's a public street. Safety and ease of travel for all modes should drive the design. I don't think that is what has happened here.
- There has been improvement, but still more needs to be done for this to be considered a successful project. Much more detail needs to be shown (where are poles in relation to property lines? what trees would come down if poles were moved? are these even desirable trees? cross sections are not showing distances to adjacent property lines, etc.). Designers still don't seem to realize that 75% of the homes along this stretch of road have side yard setback conditions, with several homes having non-conforming conditions- **THESE HOMES ARE ALREADY CLOSE TO THE ROAD!** We are NOT too excited about having road/sidewalk creeping any closer to our homes. Get creative and find a way to get this done with a narrower footprint! Refer to the recent 44th Street Reconstruction project...how can a State-Aid road that is designated as a primary bike route (54th Street is a secondary route) be 33'-5" total width (at 44th & Coolidge) and not striped, with no intention to stripe? It baffles me that we are not able to get to that dimension. As a side note, I am convinced that whoever looked at this stretch of 54th Street as a secondary bicycle route had no idea that 75% of homes had side-yard setback and didn't even take that into account when making that determination.
- Halifax has had a very large incident of increased traffic -no left on 50th;waste collection from several competing services-(multiple trips to accommodate); lots of building activity- calling for large, heavy equipment which frequently blocks local drivers vision -both from driveways and passing ease as well as wear and damage to roads. - --Solutions or help might come from a small damage tax on heavy vehicles/schedule a short time frame to drop delivery of building materials/developing a plan for driveway building prior to main residence (which in later project development might accommodate in- driveway truck worker parking) city and resident contribution to single service supplier -once a week- which would accommodate fewer trips to damage roadways-and likely save overall costs.(such as garbage collection, or other neighborhood routine activities such as yard care, and so on.) Possibly speed bumps -and/or traffic turn circles at end street busy corners might help to reduce speed -especially on our busy child- active streets.
- thank you for keeping us all in the loop on this project!
- I would like to mention that by reducing the traffic on 50th Street to one lane, it has caused such a back up to 50th and Wooddale, that the cars turn there and then cut over on 54th. If you ask me, 50th Street should still be two lanes. You have moved a large portion of traffic onto a much more residential street (54th). I am not at all encouraged that any of the residents' comments will be addressed, since the City council seems to do whatever it decides is in its best interest and not ours. That is why I don't even bother to attend meetings that appear show concern for what the residents care about.
- It's important to keep the integrity of the neighborhood and don't promote this, any more than it is, as a cut through to avoid traffic jams on 50th. The road needs to be as narrow as possible to keep the speeding down, as so many studies prove this!!! Also important to not "cut into" the north side of 50th street. Work with in the present footprint of the street!!!