



Greater Southdale Area Final Land Use and Transportation Study Report

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Prepared for:

Hennepin County
and
The City of Edina

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This document contains the findings and recommendations of the Greater Southdale Land Use and Transportation Study. The Study is a joint undertaking of the City of Edina and Hennepin County. The Study presents a vision for the future the Greater Southdale Area and describes the ways the City of Edina and Hennepin County can influence making this vision become reality.

The foundation for the Greater Southdale Land Use and Transportation Study comes from the City of Edina’s 20/20 Vision. The City prepared this strategic plan in 2000 to serve as a guide to face the critical issues that will shape Edina’s future. Edina’s 20/20 Vision was designed as an action-oriented guide to planning and resource allocation for a three to five year period. The strategic planning process led to the identification of nine strategic objectives. Each of these objectives addresses important directions and issues for Edina.

One of these objectives (#6) called on the City to “facilitate the evolution of Southdale and environs into a premier shopping, business and health care center.” The Greater Southdale Land Use and Transportation Study represents the strategy for achieving this objective.

Study Area

For the purposes of this study, the “Greater Southdale Area” is defined by Edina’s corporate boundary (east and south), State Trunk Highway 62/Crosstown (north) and the extension of Valley View Road (west). These boundaries capture the core of the Southdale Area (between France Avenue and York Avenue) and the immediately adjacent properties.

While this area serves as the primary focus for the Study, it must be recognized that the implications of future land use and transportation decisions are not confined by this boundary. Development in the Greater Southdale Area affects traffic on streets leading into the Area. Development in the Greater Southdale Area has implications for adjacent residential neighborhoods in Edina and in Richfield. Commercial and office connections extend along the I-494 corridor. These issues and other development implications lie beyond the scope of this study. One objective of the Study is to lay



Figure 1
Boundaries of Study Area

the foundation for further investigation and planning for land use and transportation issues related to future development in the Greater Southdale Area.

Vision for Greater Southdale Area

An old story about two stonecutters describes the need for a vision for the Greater Southdale Area. When asked what they were doing the first stonecutter said, “I’m cutting this stone into blocks.” The second replied, “I’m on a team that is building a cathedral.”

Without a vision, the Area will evolve much like the work of the first stonecutter. Change will be incremental. New pieces are added without a clear picture of what is being built. The vision provides the picture of the “cathedral” that the Greater Southdale Area seeks to become. Each public and private investment helps to build the desired future, not just fit cleanly with the next “stone block”.

The Greater Southdale Land Use and Transportation Study provides a framework for building the future Southdale Area. The cornerstone of that building is the Vision. Articulating the Vision serves several purposes:

- The Vision serves as a tool for evaluating proposals, projects, ideas and new directions. How does this action fit with the Vision?
- Creating a Vision Statement allows a shared understanding of community desires for the future.
- The Vision encourages the community to consider the future, even to a future that is twenty years distant.
- The Vision provides an anchor in times of conflict and change - a way of finding common ground and shared values.
- The Vision encourages imagination, recognizing that the direction it sets will be the reality of the future.
- The Vision creates energy and enthusiasm for maintaining the commitment to the Comprehensive Plan.

The vision for the Greater Southdale Area grew out of the guidance received from meetings and workshops conducted in the planning process. The vision is not a single statement, but a collection of principles that describe the character and qualities of the Greater Southdale Area.

The Greater Southdale Area will maintain and enhance its status as a premier shopping district.

- The Greater Southdale Area offers a combination of businesses and a setting that is unique from any other place in the region.
- The Area provides goods and services that meet regional, community and neighborhood needs.
- The continued evolution and success of the Southdale Shopping Center forms the foundation for the future.

The Greater Southdale Area is place for people to live, work, play and shop.

- The Greater Southdale Area is a vibrant mix of housing, jobs, commerce, health care, and entertainment.
- A strong integration of this mix of land uses creates the opportunity to reduce traffic on Area streets.
- Development in the Greater Southdale Area can meet the housing needs of Edina with options that may not fit in other locations within the city and creates a potential base of customers and employees for local businesses.
- Fairview Southdale Hospital and related health care facilities are essential elements of the Greater Southdale Area.

The Greater Southdale Area is designed to facilitate movement in a variety of ways.

- The street system allows for the safe and efficient movement of vehicles to, within, and through the Greater Southdale Area.
- Careful planning of land use, transit and parking work to mitigate the adverse effects of increased traffic volumes.
- A viable system of internal transit and supporting parking will facilitate non-auto travel in Area.
- An inter-connected system of trails, sidewalks and street crossings allow people to travel to and within the Greater Southdale Area.

Need for Public Action

This vision is not self-creating. Market forces and private investment alone will not create the desired future for the Greater Southdale Area. This Study shows a clear difference between more development and making a great place. The strategic objective for the Greater Southdale Area is not simply to attract private investment. The goal is to create a premier shopping, business, residential and health care center. This action will not be achieved without public guidance, action and investment.

This Study presents a series of public initiatives that shape the future of the Area. These investments are crucial to managing land use and transportation. The Study focuses on the elements of *what*, *why* and *how* for each of these strategic public investments. “What” describes the nature of the investment. “Why” explains the relevance of the investment. This understanding is essential. “Why” brings the commitment to make difficult decisions. The stakeholders understand why the decision is important to Greater Southdale Area. “Why” creates the ability to adapt to changing conditions. Understanding the rationale may lead to other paths with the same outcome. “How” provides the knowledge and the means needed to make the investment. With limited financial resources and other competing needs, it is essential that creative and effective funding strategies are needed to achieve the vision.

The Greater Southdale Area is not a typical redevelopment setting. The focus of redevelopment involves actions to correct physical or economic blight. Neither of these factors are obvious in the Greater Southdale Area. In fact, the opposite is true. The Greater Southdale Area is a vital and dynamic place. A variety of new developments have been proposed or are being considered. Current conditions may create a sense of complacency - if it isn't broken, don't fix it. However, the failure to plan and to act will have consequences:

- Opportunities will be missed. The initiatives described in the Study require the coordination of multiple development projects. One missing piece keeps the whole puzzle from coming together.
- The Greater Southdale Area is not immune to blight. The failure to facilitate the evolution of the Area opens the door to blight. Blight is not a thing, but a cycle of change. Over time, all buildings experience physical deterioration. In a healthy setting, property owners have the incentive and ability to reinvest in the building. Deterioration is stopped before it becomes a problem. The failure to plan and to act could remove this incentive for some properties in the Greater Southdale Area. In a place, like the Southdale Area, blight is the equivalent of a cancer. It begins small and spreads. Blight makes the Area ill both physically and economically.
- The old saying that “an ounce of prevention is worth a pound of cure” applies to community development. The failure to plan and to act may lead to the need for more radical and difficult changes in future. Many things are harder to fix when they are really broken.

Using the Study

This Study articulates the vision for the future of the Greater Southdale Area. This vision cannot be achieved overnight. This master plan presents a framework for guiding actions over the next 20 years. Some actions occur in the near term (1 to 5 years), while other steps may not be taken for years to come.

This Study must not be viewed as a "blueprint." It does not attempt to prescribe the specific location, size and use of structures. Instead, the Plan uses text and illustrations to guide private and public investments in a manner that creates and maintains the Area desired by Edina. The Study shows how the Greater Southdale Area might appear and function by applying the principles and taking the actions described in this Study.

A series of interrelated elements make up the Greater Southdale Area Land Use and Transportation Study:

Forces of Change

The Study explored the critical trends and forces that will shape the future. These forces provide context for the vision and for proposed public initiatives. This section summarizes the findings from the study process. The information in Appendix A contains more detailed information on the forces of change.

Southdale Area Today

This examines current land use and transportation conditions in the Greater Southdale Area. Understanding what exists today is an essential element of planning for the future.

Exploring the Future

The Study describes a future land use pattern for the Greater Southdale Area and implications for transportation systems. The land use pattern is expected to contain more mixed-use developments that integrate housing, shopping and employment opportunities. This mix is expected to create dynamic new development areas and possibilities for new forms of transit. It also seeks to forge a stronger link between the Southdale Shopping Center and the Centennial Lakes area to the south.

Public Initiatives

The Study identified two critical public initiatives: a central “spine” street and an internal transit system. These initiatives are fundamental to shaping future land use and transportation in the Greater Southdale Area. This section describes each of these initiatives, their implications for the future of the Area, and strategies for undertaking the initiative.

Moving Ahead

The Greater Southdale Area Land Use and Transportation Study is not an end, but a step in an ongoing process. The findings of the Study show that the Greater Southdale Area is part of broader land use and transportation systems. This section outlines other important planning that should build on the results of the Study. This section provides additional information on the regulatory and financial tools that can be used to undertake the initiatives and other public actions needed to achieve the vision for the Greater Southdale Area.

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Looking back provides an interesting perspective on planning for the future of the Greater Southdale Area. In 1955, Southdale Shopping Center was beginning to rise out of the ground. France Avenue looked more like a country road than a major urban transportation corridor. Over the past fifty years, the Area has evolved into one of the most unique and identifiable places in the Twin Cities. The key question now becomes “What will the Greater Southdale Area be like in 2025, twenty years from now?”

To help answer this question, the Study used a series of investigations to understand the forces that shape future land use and transportation in the Greater Southdale Area. These investigations collected and analyzed information from a wide range of sources:

- A "futures workshop" assembled a group of visionary thinkers to explore the future of Edina.
- Two public workshops allowed the community to inform and guide the consulting team.
- "Stakeholder" interviews allowed the consulting team to learn about the plans and concerns of key parties in the Area.
- A "community advisory committee" provided critical assessment and feedback throughout the process.
- A "technical advisory committee" contributed information and guidance from key staff.
- City policy makers (Members of the City Council, Planning Commission and Transportation Commission) provided guidance through two workshops.
- The consulting team collected and analyzed an array of data about the Greater Southdale Area.

These investigations produced a series of findings about the Greater Southdale Area. These findings provide the framework for the creation of plans for the future of the Area. This section of the report summarizes the findings. Additional supporting information about the study area, community demographics, market consideration and transportation issues can be found in Appendix A.

More Jobs Than People

In the 1990's, the number of jobs in Edina surpassed its population. This trend is forecast to continue. The most recent forecast by the Metropolitan Council shows Edina's population increasing by 4,075 from 2000 to 2030, a rate of 8.6%. During the same period, Edina is forecast to add more than twice as many new jobs (9,647), growing by 18.3%.

Office development will continue to seek proximity to I-494 and other locations along the regional freeway system. The ability to capture this growth may be constrained by existing regulations. Edina development regulations limit the permissible volume of commercial office development to a .5 floor-to-area (FAR) ratio. This limits the intensity of development use. Under this limitation, many developers of major Class-A projects may find it

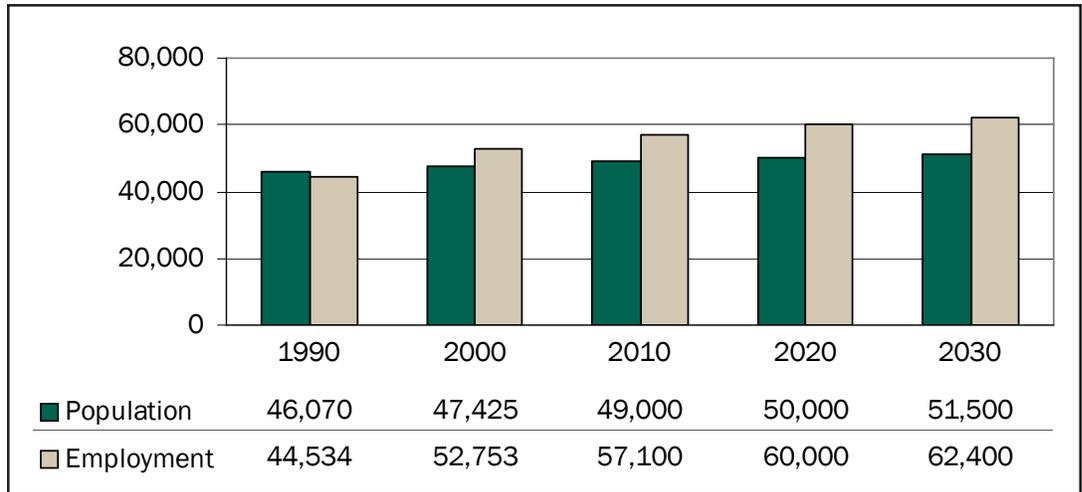


Figure 2
Metropolitan Council Forecasts

difficult to build recognizable - and typically larger and/or taller - buildings (with covered parking), and may seek alternative locations.

Excellent Demographics

Part of the development attraction for Edina comes from excellent demographic characteristics. In 2000, Edina ranked substantially above the County and the region in terms of income, educational attainment and profession occupations. The ability to maintain these distinctions will be a factor in future development.

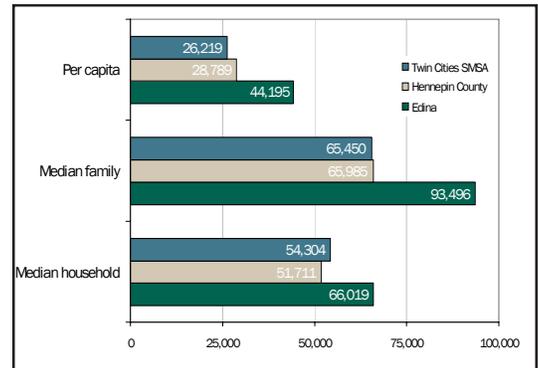


Figure 3
Income (2000 Census)

Older Population

Edina has been a more mature community. The age of the local population is notably higher than the region as a whole. In 2000, almost 23% of Edina residents were age 65 or older. Another quarter of the population (26.5%) fell in the 45 to 64 age bracket.

Age influences many aspects of development, including demand for goods, services and housing. Age shapes expectations for city government. Age is a factor in the number of children seeking enrollment in the public school system.

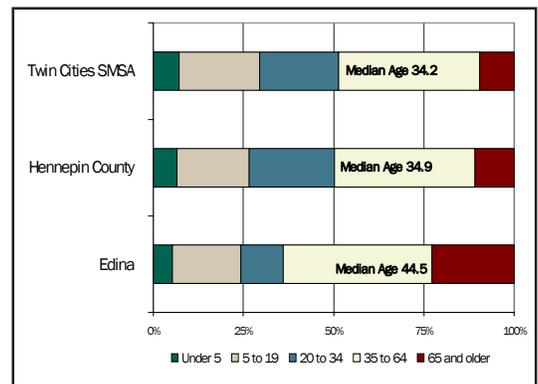


Figure 4
Age of Population (2000 Census)



Figure 5
Regional Context

More Competition

Edina faces additional competition for all forms of development. Suburban growth continues to radiate outward to the west and southwest of Edina. Eden Prairie Center and Ridgedale offer more convenient shopping for these communities than the Southdale Area.

New development planned at the Mall of America is likely to include more than 800,000+ square feet of retail/restaurant space as well as office, hotel and entertainment-related components.

Among its primary competitors, Southdale draws upon the largest market, but median household incomes in the Eden Prairie Center and Ridgedale markets exceed the Southdale market.

Market Pressures

All investigations conducted for the Study reached the same conclusion - the Greater Southdale Area is attractive to new development. Market research and stakeholder interviews are supported by real actions. Consider what occurred during the twelve month course of the Study:

- Southdale Shopping Center was purchased by Mills Corporation. Mills is preparing plans to renovate and enhance this commercial icon.
- Target began planning for the replacement of the existing store with a Super Target.
- Cypress Equities purchased the Centennial Lakes Theater and for redevelopment into housing and retail. A mixed use project is under consideration by the Edina Planning Commission.

Development Opportunities

In addition to redevelopment and renovation, a series of specific development opportunities play a role in shaping the future of the Area:

- Housing will be a part of future development in the Southdale Area. All of the investigations conducted during the study process indicate demand for additional housing. The Greater Southdale Area provides an opportunity for the community to establish additional life-cycle housing. New housing in the area can address the need for entry level housing, empty nester housing, senior housing and affordable housing. Additionally, new housing in the Greater Southdale Area can provide alternatives for Edina residents desiring to move out of existing single-family homes thereby providing opportunities to attract new families to the community.

If highway travel becomes more difficult and expensive, housing near the large Edina employment base may become more attractive. Housing reduces peak period traffic volumes and produces more favorable impacts on traffic than additional office development.

- Office development will continue to seek locations along regional highway corridors and near Fairview Southdale Hospital.
- Edina removed its prohibition on the serving of beer and wine (1996) and hard liquor (2002). These actions have opened new opportunities for eating and drinking establishments in this strong market.
- The Southdale Area offers a strong location for lodging. The area's assets include its direct access to regional highway system with connections to Minneapolis-St. Paul Airport, high-end office space, Fairview Southdale Hospital, and services and amenities such as restaurants, high-end retail shopping and the Centennial Lakes Park.

Health Care Center

Ongoing growth in the health care industry will be fueled by (1) an aging population, (2) the Area's status as a major medical center, and (3) the emergence of new medical technologies and practices (e.g., lasik eye surgery). These factors are likely to drive ongoing growth in demand for medical and other health care-related developments in the Area.

Future plans for Fairview Southdale Hospital influence a variety of land use and transportation issues:

- The Hospital is committed to long-term operations at this location.
- Significant physical expansion of the facility is not expected.
- Future development could come in the form of clinics and other support services.
- The constraints of the Medicare system present economic challenges for serving an elderly population.
- This area is the location of a variety of current traffic problems identified by the public.
- A medical clinic has been considered as a future use for a portion of the Southdale Center.

Density of Development Increases

Recent development proposals demonstrate that future development in the Area is likely to seek greater density. Several factors fuel this trend:

- The value of property requires additional development to make projects financially feasible.
- Market forces encourage the conversion of underutilized property to more intensive uses.
- There is an increasing market acceptance of development containing a mixture of uses.

In planning for future land use and transportation, additional density produces some advantages. Vertical density creates additional opportunity for green space. Development is accommodated by going “up” not “out”. Density increases the potential user base for an internal transit system.

More Traffic

Traffic volumes over the next 15 years are projected to increase regardless of the land use decisions made in the Study. Several factors contribute to future traffic volumes in the Greater Southdale Area:

- France Avenue is a regional transportation corridor. Traffic moves through the Area seeking other destinations.
- Congestion on the regional highway system shifts traffic onto France Avenue and other streets in the Area (York Avenue, 76th Street, 70th Street, and 66th Street).
- Redevelopment will likely seek an intensification of use. As this occurs, future development contributes more potential trips to the transportation system.
- A by-product of successful redevelopment is attracting more people into the Area.

Opportunities to Mitigate Congestion

There is a growing trend in land use and transportation planning to create stronger relationships between live-work-shop land uses in an area. From a regional perspective, these relationships take trips off of the regional highway system. On local streets, mixing land uses and spreading the trips over longer time periods (minimizing peak period volumes) mitigates congestion.

Redevelopment can be used to mitigate the congestion created by traffic volumes by:

- Creating a more diverse and interrelated land use pattern.
- Providing an internal transit system that provides an alternative to automobile trips.
- Providing a functional and connected system of parking and pedestrian improvements that promote non-vehicular movement to and within the Area.
- Coordinating and enhancing the access to property from major transportation corridors.
- Building new streets to take local trips off of France Avenue.

-
- Creating stronger connections with regional transit services to reduce automobile trips.

Pedestrian Improvements Needed

Both public input and study investigations discovered a lack of adequate pedestrian improvements in the Area. Pedestrian movement in the Area is impaired by several factors:

- Lack of adequate sidewalks and trails throughout the Area.
- Sites oriented to automobile use.
- Lack of connection between uses in the Area.
- Lack of connection with adjacent residential neighborhoods.
- Ability to safely cross streets.

Technology

If technology continues to change at the same pace, it is difficult to predict all of the implications for the Greater Southdale Area. The study process raised several important points about technology and future development in the Greater Southdale Area:

- The Greater Southdale Area must be supported by quality and reliable technology infrastructure.
- The Internet provides a competing marketplace for retail goods and services. The Greater Southdale Area can respond by enhancing the experience of shopping in person.
- Technology allows more business to occur in nontraditional settings. Cell phones and WIFI make offices a moveable place. Coffee shops become conference rooms. Office support become retail opportunities.

As the name implies, the Greater Southdale Land Use and Transportation Study is a planning effort focused on the integration of future land use planning and planning for transportation systems. Although the two have been considered simultaneously, they represent somewhat the classic "chicken and the egg" scenario.

Land use and transportation are intertwined. Land use influences the nature of traffic in the Greater Southdale Area. More development adds trips to the local street system. The nature of the development influences the number and timing of these trips. Land use can work to reduce the congestion created by additional traffic through balancing development types and building inter-relationships that reduce the need for travel.

The transportation system also influences land use. Streets determine where development can occur. The nature of the street influences the form and character of adjacent development. Safe and convenient access is one element of sustaining businesses. Transit and parking improvements allow more options in the location and form of development.

The Greater Southdale Land Use and Transportation Study seeks to understand these relationships and how they influence the future of the Area.

Land Use

Planning for the future does not start from a clean slate, but from an existing built environment. The Greater Southdale Area is a dynamic and changing environment. The Study process began by seeking an understanding of existing conditions and their influence on future development.

The map in Figure 6 shows existing land use for property in and around the study area. (Additional information about the characteristics of the study area can be found in Appendix A.)

The Greater Southdale Area has evolved into a mixed use district. The Area began as a place of commerce. Beginning with Southdale Shopping Center, the Area has a strong identity as a retail center. Fairview Southdale Hospital and proximity to the transportation system (highways and airport) served as a catalyst for office development. Over the past 20 years, housing has been added to the Southdale landscape. This evolving mixture of goods, services, jobs and homes creates a framework for future development.

Several important physical characteristics shape the existing development pattern.

Fairview Southdale Hospital is the defining land use on the northern edge of the Greater Southdale Area. The Hospital brings visitors and jobs into the Area. The Hospital provides a catalyst for other development - offices, clinics, services, and parking. Future growth of the Hospital is not expected to result in the outward expansion of buildings. Growth will more likely result in the intensification and specialization of functions within the current building footprint. The success of the Hospital should continue to attract other medical related businesses to the Southdale Area.



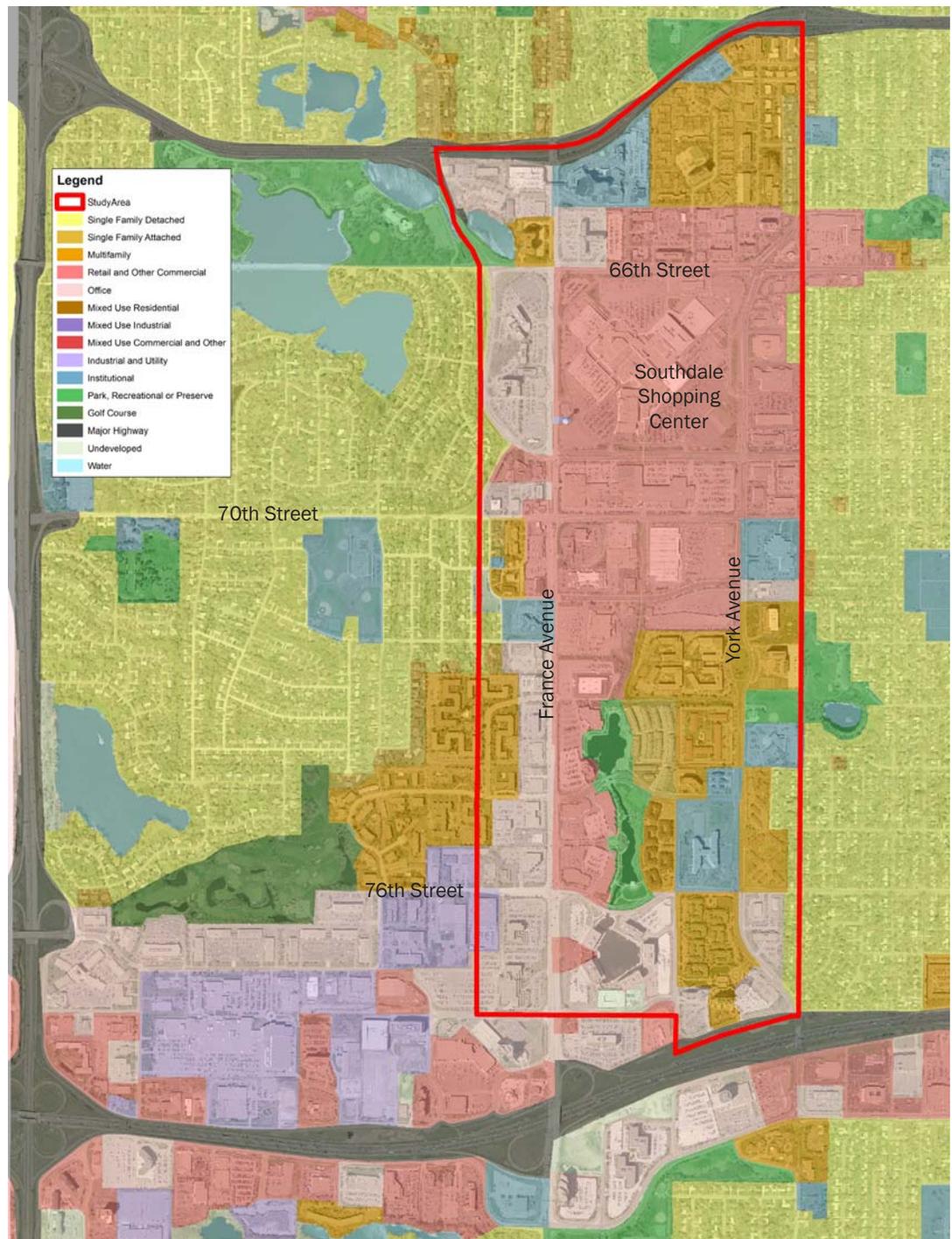


Figure 6
Existing Land Use



No single use defines the Area more than the Southdale Shopping Center. An initial struggle of the Study was to establish an identity for the Area beyond the shopping center. The Southdale Shopping Center anchors the retail environment of the Area. This attraction, combined with the layout of the site, influences traffic patterns. The critical element of the Southdale Shopping Center is not what exists today, but what it will become in the future.

While renovation plans for the Center have not been unveiled, it is unlikely that changes will simply result in a renovated and more functional regional mall. The success of recent restaurant and theater developments show how entertainment expands the character and life of the Center. The possible conversion of a portion of mall space to a medical clinic hints at the variety of potential uses.

The public open space in Centennial Lakes jumps off of the existing land use map. No other comparable place can be found in the Greater Southdale Area. This feature demonstrates how public space can be an anchor and organizing element for development. The combination of open space, recreation and trails make this space an essential part of Centennial Lakes. The preliminary site design for the Centennial Lakes Theater property seeks to incorporate this amenity. The desire to expand this form of public open space into other parts of the Greater Southdale Area has been a consistent theme of input received during the study process.

Edge uses along France and York pose important land use challenges. These uses are commercial properties along the west side of France (south of 69th Street) and the east side of York (between 66th Street and 70th Street). Development in these locations orients to the street for visibility and access. The variety of street access points from these properties creates movement and safety issues along France and York. As traffic volumes increase, the ability to access these properties become more difficult. Development in these locations must also recognize connections with adjacent neighborhoods. These parts of the Greater Southdale Area become transition zones between the neighborhoods and the more intense uses between France and York.



Transportation

The transportation system in the Greater Southdale Area is a network of trails, streets and freeways that not only connects the study area to the larger region providing for the movement of regional travel in, out and through the study area, but also provides connectivity within the study area for local circulation and access. Regional and local travel patterns affect the form and function of the street system in different ways.

The transportation system in the study area is multi-modal - autos, trucks, buses, bicycles and pedestrians all share portions of the system. As with regional and local travel patterns, each mode affects the form and function of the street system differently. The complex interaction of movement and mode with the pattern of development in the Greater Southdale Area results in the transportation conditions that are experienced in the study area today.

To understand how transportation conditions in the Greater Southdale Area will change in the future and how change in the study area can be guided to maintain a viable transportation system, it is important to peel back the layers of the system and look at how each is affected by movement patterns and modes.

Regional movement in the study area is most strongly influenced by two factors - the location of the Greater Southdale Area in relation to other activity centers in the region and the regional attractiveness of the land uses in the study area. The Greater Southdale Area is located between the center and the growing edge of the region, which is important since growth in the region outside the study area will cause growth in the regional traffic that

travels through the study area, independent of any changes in the pattern of land use inside the study area. The regional attractiveness of the land uses in the study area - the heavy concentration of destination retail uses in the study area - makes the Greater Southdale Area a destination for regional trips, which ensures that the level of traffic experienced today will grow into the future.

Local movement in the study area is most strongly influenced by the pattern and intensity of land use in the Greater Southdale area. The current land use pattern (Figure 6) concentrates retail, employment and residential land uses in the study area into a series of districts, each with a dominant type of use. The size and proximity of these districts results in the movement of trips within and among them, in some cases in autos or on transit and in others, by bicycle or walking. The size of the Greater Southdale area is larger than any single walking trip, which results in much of the local movement among districts in the study area being made by driving - on the same streets that are used by regional movements.

The freeway and street system in the study area is organized to support both regional and local movement patterns. Streets in the study area are organized by functional class to differentiate those intended to emphasize regional movement from those intended to emphasize local movement. Figure 7 shows the functional relationship of streets in the Greater Southdale Area.

France, 66th and 76th/78th are regional relievers for the freeways (I-494, TH 62 and TH 100) that frame the study area. These streets are designed to carry higher traffic volumes (they are wider) and, in the case of France Avenue, at higher speeds than on adjacent local streets. Pedestrian crossings, particularly of France Avenue, are complicated by the wider pavements. The higher speeds tend to be less accommodating for bicycle traffic.

York Avenue and 70th Street are also designated as streets that have a higher circulation



Figure 7
Street Functional Classification



Figure 8
Current Traffic Volumes (ADT).

function. While York Avenue is designed to accommodate this function, West 70th Street is not. Rather it functions as a transportation corridor because of its connections between France Avenue and TH 100, but is designed as a residential street. This imbalance is the basis of a local circulation issue along 70th Street. Traffic from the neighborhoods north and south of the street has difficulty accessing 70th Street, particularly during peak commute periods.

The general layout of the regional freeway system, which sandwiches the study area between I-494 and TH 62, focuses regional travel on France Avenue and to a lesser extent on West 70th Street, York Avenue, and West 66th Street. The ability of the freeway system to adequately carry traffic affects the level of traffic on the study area roadways. Congestion on I-494 and TH 100 push regional traffic onto the major streets in the study area.

Traffic conditions in the study area are described by a combination of overall traffic volumes and the level of congestion at intersections. Traffic volumes are reflective of the roles of each street in the network. Freeway access and “frontage” routes carry higher volumes and because of the barrier nature of the freeway system to local movement (not all streets cross the freeways), traffic concentrates at freeway interchanges on France Avenue, Valley View Drive, and Xerxes/York Avenues. The result is that France Avenue and Xerxes/York carry over 20,000 vehicles per day. 66th Street, 70th Street, and 76th/78th Streets each carry between 10,000 and 20,000 vehicles per day. Other streets in the study area carry less than 10,000 vehicles per day. The increased connectivity present in the street network on the eastern side of the study area results in less traffic east of York Avenue on 66th, 69th, and 70th Streets. The generally high levels of traffic on France and Xerxes/York Avenues tend to be an issue with both pedestrians and bicycles.

Traffic congestion at intersections is used as an indicator of how well the transportation system performs. The method of measuring congestion reports a level of service (LOS) based on the delay experienced by vehicles at a particular intersection. Service levels range from A through F with LOS A representing freely flowing conditions and LOS F jammed conditions. LOS D is a generally accepted standard for planning and design of urban transportation facilities.

Available traffic volume count information for major intersections in the study area was obtained from Hennepin County. Data was available for only a subset of the signalized intersections on France and York/Xerxes Avenues for weekday evening peak hour conditions. These locations, shown in Figure 9, were considered representative of conditions over the study area.

Even though the evening peak conditions for

Intersection	LOS
TH 62 Westbound Ramps/France Avenue	C(a)
TH 62 Eastbound Ramps/France Avenue	B(a)
West 66th Street/France Avenue	C(a)
West 69th Street/France Avenue	C(a)
West 70th Street/France Avenue	D(a,b)
Parklawn Avenue /France Avenue	B(a)
West 76th Street/France Avenue	C
Minnesota Drive/West 78th Street/France Avenue	C(a,b)
TH 62 Westbound Ramps/Xerxes Avenue	B
TH 62 Eastbound Ramps/Xerxes Avenue	A
West 76th Street/York Avenue	C

Notes

- (a) LOS E is experienced on one or more approaches at these intersections
- (b) LOS F is experienced on one or more approaches at these intersections

Figure 9
PM Peak Hour Intersection Operations – Existing Conditions

the representative intersections overall are shown to be acceptable, closer review of the conditions shows that individual intersection movements are experiencing worse levels of congestion. Figure 10 shows the locations of the affected movements on France Avenue and illustrates the extent of queue spillback that is the result of the congestion.

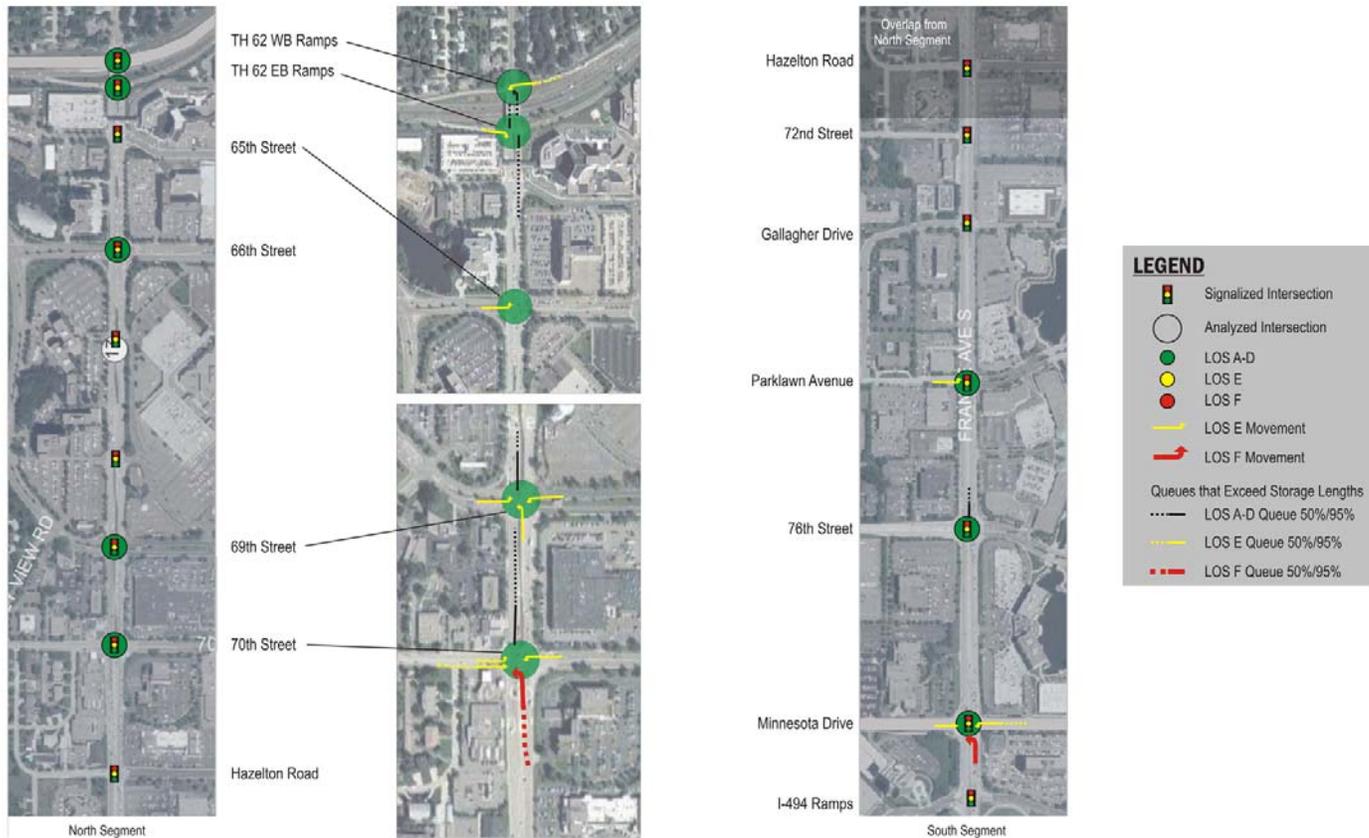


Figure 10
France Avenue Intersection Operations - Existing Weekday PM Peak Hour

The areas of congestion are related primarily to traffic movements entering or leaving the corridor. The location of France Avenue in relation to the freeways and the location of proportionately more high traffic generating land uses on the east side of France Avenue creates a condition where traffic that left turns from France Avenue or enters from the cross street is in conflict with very high volumes of through traffic movement. The high proportion of through movements leaves less green time at intersections for turning and cross traffic and leads to more congested conditions.

The analysis shows that conditions can be highly variable as a result of how turning and through traffic movements combine. At the interchange of TH 62 and France Avenue, the short queuing/storage distance for northbound left turns from France Avenue onto westbound TH 62 causes a queue of waiting vehicles that can interrupt northbound traffic at intersections as far south as 66th Street. Because of the high volumes on France, less green time is available for the TH 62 off ramps and queues on the ramps result, both the eastbound right turn and the westbound left turn ramp movements experience LOS E (capacity) condi-

tions, even though overall the two ramp intersections are at LOS D or better.

Both 69th and 70th Streets provide regional access to the Greater Southdale area (Valley View Drive connects to 69th Street and to TH 169; 70th Street connects to TH 100). Both of these streets serve the southern portion of the concentration of retail uses near Southdale Center. As a result, traffic using 69th and 70th Streets bound for the retail uses crosses similar traffic on France Avenue and proportionately more pressure is placed on these locations. Three of the four left turn movements at 69th Street operate in LOS E. At 70th, the eastbound approach and the westbound left turn is in LOS E, while the northbound left turn is in LOS F. Southbound traffic, while technically operating at LOS D or better at 70th Street tends to generate queues that can extend most of the way to 69th Street. When such queues are present, operations at 69th Street and France Avenue are essentially metered since turning traffic from 69th onto France has difficulty moving onto the congested southbound lanes.

Interaction between the I-494 freeway interchange ramps at the southern end of the study area and the intersection of Minnesota Drive and France Avenue causes similar turning movement issues with eastbound and westbound left turns at LOS E and the northbound left turn at LOS F. Weaving of traffic in the section of France Avenue between Minnesota Drive and the I-494 ramps adds to the variable congestion in this portion of the corridor. Unbalanced lane utilization from southbound motorists attempting to line up for the several on-ramps to I-494 contributes to the congestion in some lanes on France Avenue while other lanes are free flowing.

Although traffic count data was not available for many of the intersections on York Avenue, observations of traffic conditions indicate that several of the intersections on York Avenue in the northern portion of the study area have insufficient storage for turning vehicles. The result is periodic blockage of the through lanes with turn lane demands are high.

Transit service in the study area is focused on the transit center at Southdale Center. Primary connections are provided between the Southdale area and downtown Minneapolis via a combination of express, limited and local routes. The tails of these primary routes extend into Edina and provide local circulator/distributor service. Crosstown service is provided on 66th and 76th Streets and links to transit centers in Richfield and Bloomington. Southwest Metro peak express service from Eden Prairie to downtown Minneapolis using TH 62 stops at Southdale and provides a link to the Golden Triangle area. Figure 11 shows the existing pattern of routes.



Figure 11
Existing Transit Routes

Pedestrian connectivity in the Greater Southdale Area is largely internalized in larger developments. Significant portions of the street system have sidewalks, but some areas do not. The result is a relatively hostile environment for pedestrians that attempt to walk from one portion of the study area to another, particularly along major streets. The wide crossing distances on France Avenue are discouraging to pedestrians and tend to make France Avenue a barrier to pedestrian movement to areas west of the study area. While a similar condition is experienced on parts of York Avenue, there are off-street pedestrian connections along York Avenue that reduce the barrier nature and enhance connectivity with Richfield.

The scale and location of land uses relative to each other in the Greater Southdale Area is large, which tends to reduce the effectiveness of walking as a primary means of travel in the study area. When a five-minute walking circle or pedestrian catchment is used to measure the pedestrian connectivity of the study area, it becomes evident that many of the destination land uses in the study area are not within walking distance of the places people live and work. Figure 12 illustrates the pedestrian catchment areas. The different districts that

make up the study area tend to be of a size that a five-minute walk can get one from the edge to the center. The net result is that trips among land uses within the study area occur, but because of the large size of the study area and limited availability of transit, many of these local trips occur by driving.



Figure 12
Pedestrian Catchment Areas

Alternatives for the Future

In the early stages of the planning process, land use was the starting point. Initial considerations addressed a broader scale issue, understanding the alternatives for controlling future development. Edina faces a choice between three basic alternatives: 1) limit development, 2) incremental change, and 3) managed growth.

Limit Development

Problems related to increased traffic were the most common concern voiced during the initial meeting at the onset of the planning process. A simple response to these concerns would be to limit development. This approach seeks to prevent increased traffic congestion by limiting future development. In reality, the City cannot simply shut the gates. Current land use controls allow for more development at many locations throughout the Area. The City cannot impose a perpetual development moratorium. Even if this were possible, it is not in the long-term interests of either the City or the County. The Southdale Area faces increasing competition for customers and investment. Strict limitations on new development and redevelopment adds to the incentive for businesses to locate in other places. Pursuit of this approach over time places the City in the position of risking a downward spiral of the physical and economic condition of the Area.

Incremental Change

The Greater Southdale Area faces pressure for additional development. Target is proposing to raze its existing facility and build a new Super Target. Cypress Equities proposes to replace the Centennial Lakes Theater with retail and housing. The new owner of Southdale Shopping Center (Mills Corporation) is considering options for improvements. Other development projects are being discussed and explored. In each case, the proposed use adds development to the site. Even in the strong Edina market, additional intensity is needed for development to be financially feasible. No single development project makes a tangible difference in current levels of traffic congestion. The cumulative effect of incremental development will be a transportation system that no longer functions at an acceptable level and potential solutions that have been lost.

Managed Growth

From its inception, the Greater Southdale Area Land Use and Transportation Study has been focused on managed growth. Southdale remains one of the Twin Cities most significant commercial areas. It will continue to attract strong development and redevelopment interest. Accordingly, the land use and transportation planning efforts of this study are focused on crafting a strategy to carefully guide development while supplying supporting public investments that can produce the greatest benefit to the community. The components of the Greater Southdale Area Land Use and Transportation Study are clearly focused on supporting a strategy of managed growth.

Land Use Categories and Typologies

The study process was used to create and evaluate a series of alternative land use concepts for the future of the Greater Southdale Area. These concepts were depicted as a pattern of land uses supported by public redevelopment initiatives. The concept illustrations show broad-scale options for the future. It is important to more clearly understand the nature of development proposed for the Greater Southdale Area.

The section that follows uses text and photographs to define the characteristics of development desired for the Area.

Mixed Use

More property is guided for Mixed Use than any other land use category. This designation reflects the evolving nature of the Greater Southdale Area. Over the past 25 years, the Area has become a mixed use district containing places to shop, to work and to live. In a mixed use district, each land use typically occupied a separate parcel of land. The mixing and integration of uses occurred over a broader area. The concept for the Greater Southdale Area proposes mixed uses at the parcel level.

In simplest terms, the mixed use category shows locations where it is appropriate to allow commercial and residential uses on the same site. Mixed use is not a singular approach to land use and development. The form of development varies from place to place. Adding a residential component to the Southdale Shopping Center site is different than integrating housing with the retail development along York Avenue.

Some mixed use will be horizontal. Each building has a distinct use - retail, office, housing. Buildings with different uses are incorporated into a common site plan. The Cypress Equities proposal for the Centennial Lakes Theater site is an example. The site concept includes housing, retail and restaurant uses. Each use occurs in separate structure.

Mixed use development may also be organized in a vertical manner. In vertical mixed use, a single structure contains different land uses. The Excelsior and Grand development in St. Louis Park and the 5000 France Redevelopment at 50th & France are examples of contemporary vertical mixed use. Retail uses occur at street level with housing occupying the remainder of the building. The mixing of uses may take a variety of forms depending on market and site considerations.

It is important to understand that this land use designation does not mandate mixed use development. Some parcels shown as mixed use in the land use concept will have singular use - retail, office or housing. The objective of this designation is to create opportunity for mixed use. The land use concept recognizes that encouraging mixed use development is an essential element of managing land use and transportation in the Greater Southdale Area.

- Mixed use development helps to mitigate the impacts from traffic produced by additional development.
- Mixed use development creates the potential for shared parking and an overall reduction in parking spaces.



- Mixed use development acknowledges the need for additional density while maintaining a strong retail identity.

Commercial/Retail

The Greater Southdale Area is a place of commerce. It is one of the premier retail districts in the Twin Cities. Edina seeks to maintain and enhance this status in the future. The retail character of the Area must not be lost in the evolution of mixed use development.

Early land use concepts in the Study process had more areas of "red" - parcels guided solely for commercial/retail development. The key question in evaluating land use options became "Would mixed use development be inappropriate on this parcel?" It is easy to be swayed by current use and near term prospects for development. The Galleria and Target parcels are prime examples. These sites are exclusively retail with no immediate pressure for redevelopment with other uses. Yet, there is little that distinguishes these parcels from other mixed use sites in the Greater Southdale Area. Current use does not preclude future opportunity for a mixture of uses.

These considerations guided the designation of Commercial/Retail land use. Only the northeast quadrant of York Avenue/66th Street is designated as solely Commercial/Retail. In other locations, the Commercial/Retail land use shows a retail orientation of a mixed use development. The strips of "red" depict commercial/retail uses with an orientation to the adjacent street.

Office

Mixed use does not fit in every location. Some places should be dedicated solely for office use. These locations are focused on the I-494 corridor and property adjacent to Fairview Southdale Hospital. The Office land use is intended for higher density places of employment. Traffic can be oriented to the regional highway system and potential future transit system improvements.

The land use concept provides alternatives for other higher density uses (housing and hotel development) that might seek these locations.

The Office land use is not the sole place for office development. Some parcels guided for mixed use will retain an exclusive office use. The land use concept also anticipates that office uses will be part of mixed use development project.

High Density Residential

Some parts of the Greater Southdale Area should be exclusively residential in nature. The goal is to create and sustain quality places to live. The largest amount of stand-alone residential land use is High Density. This land use establishes a pattern consisting of multi-family housing types such as apartments and condominiums. Densities within the High Density Residential land use averages between 20 and 45 units per gross acre. Buildings range in height from two to as much as 15 to 20 stories.

The majority of High Density Residential land use in the land use plan depicts existing land use. The only new area of High Density Residential development is along the west side of



Xerxes Avenue north of 66th Street.

High Density Residential land use is also shown within a broader area of Mixed Use development. This approach suggests the orientation of land uses within an area of mixed uses.



Medium Density Residential

This area represents areas of attached housing units (duplexes, triplexes, and townhomes) with a gross density not in excess of 20 units per acre. The areas shown in the land use plan follow existing land uses. No new areas are proposed exclusively for Medium Density Residential.

This type of housing may be part of mixed use developments. Medium density housing styles provide an appropriate transition between commercial or higher density uses and single family neighborhoods outside of the Greater Southdale Area.

Low Density Residential

The land use consists of traditional single-family detached housing units. Only one area of existing low density housing is shown in the land use plan. No new low density housing is proposed within mixed use developments.

Civic/Public Facility

This land use recognizes three key public and civic land uses in the Greater Southdale Area: Fairview Southdale Hospital, Hennepin County Library and Southdale YMCA.

Land Use Alternatives

The process of planning for the Greater Southdale Area included the development of conceptual land use and transportation planning alternatives designed to address the broader objectives for the project area. While each of the alternatives offered different development patterns and uses, the following general goals and objectives were embodied in each:

1. Maintain and enhance the Southdale area as a regional retail destination.
2. Provide a greater range of housing types, densities and costs within the Study Area.
3. Improve the pedestrian environment within the study area and connections to surrounding neighborhoods.
4. Provide a mixed-use, shopping, living and working environment that meets the needs of both residents and visitors.
5. Use redevelopment to mitigate the adverse effects of increasing traffic volumes.
6. Provide an on-site transit circulator to reduce reliance on the automobile and help to relieve traffic congestion on France Avenue.
7. Define and enhance site gateways, precincts and neighborhoods within the study area.
8. Improve the physical appearance of York and France Avenues through streetscape design.
9. Build upon the success of the central open space spine created at Centennial Lakes.

The concepts presented alternative visions for the future of the Greater Southdale Area. They

provided a guide for slightly different views of how private investment might achieve outcomes desired by the public.

Concept One - Southdale as the Retail Hub

Land Use

Concept 1 featured a general land use strategy focused on concentrating retail uses in a relatively tight cluster around Southdale Center, providing a greater mix of uses in existing retail sites, increase housing units and densities within the study area, extending the central open space spine to Southdale and orienting new development along the central spine. Specific planning strategies employed in Concept 1 included:

- Concentrating retail uses near the Southdale Center and Galleria district.
- Developing street-fronted retail along an extended central open space/parkway spine from Centennial Lakes north to the Southdale Center.
- Redeveloping existing strip retail sites along France Avenue and York into mixed-use sites to include ground level (street-fronted) retail with residential and/or office uses in the upper stories.
- Redeveloping the Centennial Lakes retail site into a mixed-use site to include ground level retail with residential and/or office uses in the upper stories.
- Redeveloping portions of the Yorktown Mall retail and York Plaza residential sites to include open space and high density residential uses.
- Redeveloping the existing Target site so that new retail development would include street-fronted retail on the ground level along the central parkway spine.
- Redeveloping the Centennial Lakes movie theater site for a new hotel.
- Modifying Galleria to encourage greater street-fronted retail connections between the Southdale Center and mixed-use redevelopment sites to the south of the Galleria.
- Redeveloping office sites north of 66th Street at the ground level to include retail - to encourage greater land use connections between the Medical District and the Southdale Center.
- Redeveloping the northeast neighborhood to include greater residential density and smaller block patterns.

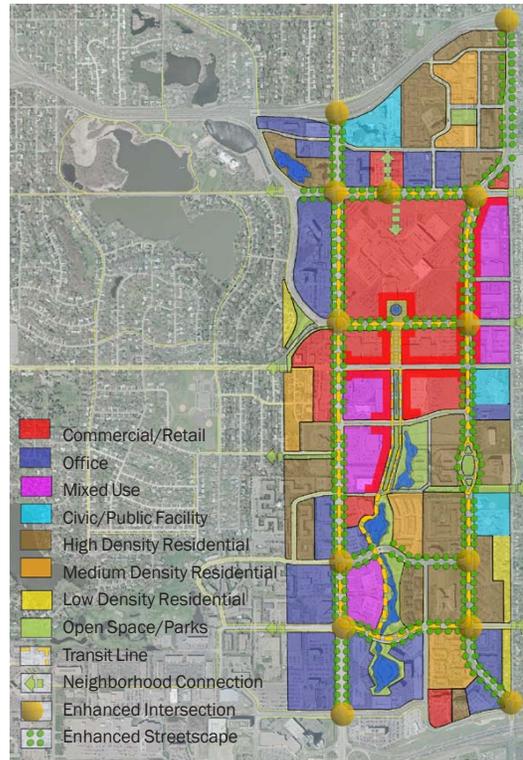


Figure 13
Concept One

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- Redeveloping areas on the west of France Ave. to include more high density residential.

Transportation

The general transportation strategies of this concept were to reduce automobile trips by increasing land use mixes and densities, encouraging a more walkable built environment by decreasing block sizes and fronting streets with architecture and ground level uses that create vitality, and providing a transit circulator to reduce reliance on the automobile. Specific planning strategies employed included the following:

- Realigning 70th Street into 69th Street at the intersection of France Ave. This serves to bring more traffic off 70th Street to the primary retail destinations of the Southdale Center and the Galleria.
- Close 70th Street between France Ave. and the new alignment change. The intent of this action was to reduce traffic between France Ave. and York Ave. along 70th Street, allowing 70th Street to become a pedestrian-friendly, street-fronted retail street.
- Including a new roadway/parkway along the central open space between Centennial Lakes and the Galleria.
- Redeveloping the northeast neighborhood with smaller block sizes, distributing traffic more efficiently.
- Including a new shuttle transit circulator route that runs along France Ave., York Ave., 66th Street, 69th Street, 76th Street and along the central open space spine/parkway from 76th Street north to 69th Street. The primary goal of the circulator is to shuttle people between employment, shopping and residential areas within the study area and to have station links to the regional transit system.
- Redeveloping existing retail sites into more densely developed mixed-use sites that include retail, housing and employment uses to encourage a more walkable neighborhood.

Public Space

The general public space strategies of Concept 1 were to build upon the success of the central open space spine created at Centennial Lakes, enhancing the streetscape character of primary arterials and gateways intersections, identifying potential open space links to the surrounding neighborhoods. Specific planning strategies employed included:

- Extending the central open space spine north to the Southdale Center from Centennial Lakes.
- Creating a new parkway street along the central open space spine.
- Creating a new central park and pond just north of Centennial Lakes.
- Creating a pedestrian street fronted with retail through the Galleria site that culminates at a new public plaza at the south end of Southdale Center.
- Providing a new neighborhood park in the northeast neighborhood.
- Enhancing the physical treatment of France Ave., York Ave., 66th Street and 69th Street with streetscape improvements, including new tree plantings, site furnish-

ings, lighting, sidewalks, and signage. The goal is to create an attractive and distinct identity for the Southdale District as well as provide a more comfortable and pedestrian-friendly streetscape.

- Providing enhanced treatment at key intersections to create district identity and orient visitors. These have been identified on the plan with yellow bubbles.
- Identifying and providing new landscape/trail easements and/or streetscape improvements to provide safer, more pedestrian-friendly connections to surrounding neighborhoods.

Concept Two - Expanded Retail

Land Use

The general land use strategies embodied in Concept 2 were to maintain a concentration of retail uses in the Southdale area but with an expanded area as compared to Concept 1. Additionally, this alternative provided a greater mix of uses in existing retail sites, increased housing units and densities within the study area, and it created a central retail spine from Centennial Lakes to Southdale Center providing a stronger connection between Southdale Center and uses to the south. Specific planning strategies employed included:

- Concentrating retail uses near the Southdale Center, Galleria district and the two development sites to the south of the Galleria.
- Developing street-fronted retail along a new pedestrian-friendly "Main Street" from Centennial Lakes north to the Southdale Center.
- Redeveloping existing strip retail sites along France Ave. into mixed-use sites to include ground level (street-fronted) retail with residential and/or office uses in the upper stories.
- Redeveloping existing strip retail sites along York Ave., between 66th and 69th Streets, for high and medium density residential uses.
- Redeveloping the Centennial Lakes retail site into a mixed-use site to include ground level retail with residential and/or office uses in the upper stories.
- Redeveloping portions of the Yorktown Mall retail and York Plaza residential sites to include mixed-use (with ground level retail along the central "Main Street" and high density residential along York Ave.

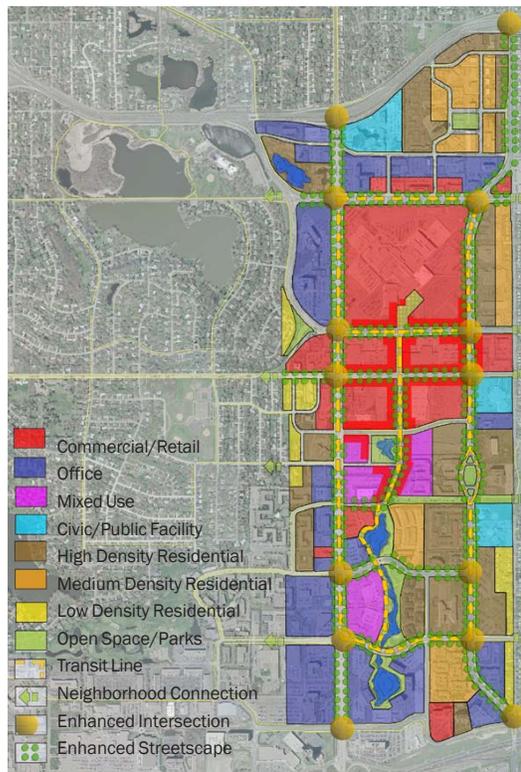


Figure 14
Concept Two

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- Redeveloping the existing Target site so that new retail development would include street-fronted retail on the ground level along the central "Main Street".
 - Redevelop the Centennial Lakes movie theater site for a new hotel.
 - Modifying the Galleria to encourage greater street-fronted retail connections along a pedestrian street between the Southdale Center and mixed-use redevelopment sites to the south of the Galleria.
 - Redeveloping office sites north of 66th Street at the ground level to include retail - to encourage greater land use connections between the Medical District and the Southdale Center.
 - Redeveloping the northeast neighborhood to include greater residential density and smaller block patterns.
 - With the exception of the site between 69th and 70th Streets, very little land use change was planned along the west side of France Ave. in this scheme.

Transportation

The general transportation strategies of this Concept were to reduce automobile trips by increasing land use mixes and densities, encouraging a more walkable built environment by decreasing block sizes and fronting streets with architecture and ground level uses that create vitality, and providing a transit circulator to reduce reliance on the automobile. Specific planning strategies employed included the following:

- Realigning 70th Street into 69th Street at the intersection of France Ave. This serves to bring more traffic off 70th Street to the primary retail destinations of the Southdale Center and the Galleria.
- Reconfiguring lane usage on 70th Street to include on-street parking with one lane of traffic in each direction and encouraging 70th Street to become a pedestrian-friendly, street-fronted retail street.
- Including a new pedestrian-friendly retail "Main Street" along the central core between Centennial Lakes and the Southdale Center. This street would include on-street parking, one lane of traffic in each direction and would become 'pedestrian only' through the Galleria site.
- Redeveloping the sites to the south of the Galleria and the northeast neighborhood with smaller block sizes, distributing traffic more efficiently.
- Including a new shuttle transit circulator route that runs along France Ave., York Ave., 66th Street, 69th Street, 76th Street and along the retail oriented, centrally located "Main Street" and open space from 76th Street north to 69th Street. The primary goal of the circulator is to shuttle people between employment, shopping and residential areas within the study area and to have station links to the regional transit system.
- Redeveloping existing retail sites into more densely developed mixed-use and high density residential sites that include retail, housing and employment uses to encourage a more walkable neighborhood.

Public Space

The general public space strategies of Concept 2 seek to build upon the success of the central open space spine created at Centennial Lakes by extending a retail oriented, pedestrian-friendly "Main Street", providing new parks, enhancing the streetscape character of primary arterials and gateways intersections, and identifying potential open space links to the surrounding neighborhoods. Specific planning strategies employed included the following:

- Creating a new pedestrian-friendly, retail oriented "Main Street" central to the Study Area, connecting the central open space spine from Centennial Lakes to the Southdale Center.
- Creating a new urban village park and pond just north of Centennial Lakes along the new "Main Street".
- Creating a 'pedestrian only' street fronted with retail through the Galleria site that culminates at a new public plaza at the south end of Southdale Center.
- Providing a new neighborhood park in the northeast neighborhood.
- Enhancing the physical treatment of France Ave., York Ave., 66th, 69th and 70th Streets with streetscape improvements, including new tree plantings, site furnishings, lighting, sidewalks, and signage. The goal is to create an attractive and distinct identity for the Southdale District as well as provide a more comfortable and pedestrian-friendly streetscape.
- Providing enhanced treatment at key intersections to create district identity and orient visitors. These have been identified on the plan with yellow bubbles.
- Identifying and provide new landscape/trail easements and/or streetscape improvements to provide safer, more pedestrian-friendly connections to surrounding neighborhoods.

Concept Three - Valley View Road Extended

Land Use

The general land use strategies of Concept 3 include concentrating retail uses along France Avenue, providing a greater mix of uses, increasing housing units and densities within the study area, and orienting new development along the central spine. Realignment of Valley View Drive is a critical component of this concept. Specific planning strategies employed included:

- Concentrating retail uses along France Avenue.
- Developing street-fronted retail along a new central street from Centennial Lakes north to the Galleria.
- Redeveloping existing sites along France Avenue and York Ave. (between 66th and 69th Streets) into mixed-use sites to include ground level (street-fronted) retail with residential and/or office uses in the upper stories.
- Redeveloping portions of the Yorktown Mall retail and York Plaza residential sites to include mixed-use, open space and high density residential uses.
- Redeveloping the existing Target site for mixed-use and high density residential uses.

- Redeveloping the Centennial Lakes movie theater site for a new hotel.
- Redeveloping office sites north of 66th Street for mixed-use.
- Redeveloping the northeast neighborhood to include greater residential density, office along Highway 62 and smaller block patterns.

Transportation

The general transportation strategies of Concept 3 call for reducing automobile trips by increasing land use mixes and densities, encouraging a more walkable built environment by decreasing block sizes and fronting streets with architecture and ground level uses that create vitality, and providing a transit circulator to reduce reliance on the automobile. Re-alignment of Valley View Drive is a key strategy used to alleviate traffic pressures on France Ave. in Concept Three. Specific planning strategies employed included:

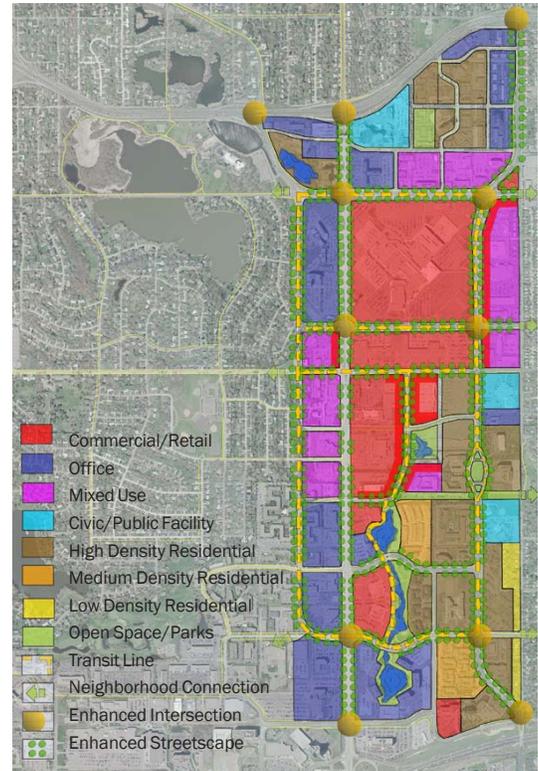


Figure 15
Concept Three

- Realigning Valley View Drive to run parallel to France Ave. and continuous south to 76th Street.
- Including a new pedestrian-friendly retail street along the central core between Centennial Lakes and the Galleria. This street would include on-street parking and one lane of traffic in each direction.
- Redeveloping the sites to the south of the Galleria and the northeast neighborhood with smaller block sizes adjacent to York Ave., distributing traffic more efficiently.
- Include a new shuttle transit circulator route that runs along Valley View Drive, York Ave., 66th Street, 70th Street, 76th Street and along the retail oriented, central street and open space from 76th Street north to 70th Street. The primary goal of the circulator is to shuttle people between employment, shopping and residential areas within the study area and to have station links to the regional transit system.
- Redeveloping existing retail sites into more densely developed mixed-use and high density residential sites that include retail, housing and employment uses to encourage a more walkable neighborhood.

Public Space

The general public space strategies of Concept 3 were focused on building upon the success of the central open space spine created at Centennial Lakes by extending a retail oriented, pedestrian-friendly street, providing new parks, enhance the streetscape character of primary arterials and gateways intersections, and identifying potential open space links to the

surrounding neighborhoods. Specific planning strategies employed included:

- Creating a new pedestrian-friendly, retail oriented street central to the Study Area, connecting the central open space spine from Centennial Lakes to the Galleria.
- Creating a new urban village park just north of Centennial Lakes along the new central street.
- Providing a new neighborhood park in the northeast neighborhood.
- Enhancing the physical treatment of Valley View Drive, France Ave., York Ave., 66th, 69th and 70th Streets with streetscape improvements, including new tree plantings, site furnishings, lighting, sidewalks, and signage. The goal is to create an attractive and distinct identity for the Southdale District as well as provide a more comfortable and pedestrian-friendly streetscape. In the case of Valley View Drive, realignment is proposed.
- Providing enhanced treatment at key intersections to create district identity and orient visitors. These have been identified on the plan with yellow bubbles.
- Identifying and provide new landscape/trail easements and/or streetscape improvements to provide safer, more pedestrian-friendly connections to surrounding neighborhoods. These have been identified on the plan with green arrows.

Future Land Use and Transportation

The Land Use Plan resulted from the review and consideration of each of the initial three alternatives including input collected during the second community meeting. As the alternatives were being analyzed and a preferred concept began to emerge, it became evident that the Southdale Area can be viewed as a series of related districts (see Figure 16). Four districts comprise the core of the Southdale Area: Health Care and Housing on the extreme north edge, Regional Retail including Southdale Center and Galleria, the Promenade, and in the extreme south, Centennial Lakes. Two edge districts flank the core districts on the east and west sides of the study area. These district designations are used as defining elements for specific plan recommendations. Prior to examining each district, a number of general study area comments need to be offered.

The Land Use Plan (Figure 17) speaks to the future of the Southdale Area. It advocates an evolution of the current land use pattern and supporting transportation system in order to position the Southdale Area to remain competitive in the years ahead.

In general, the Land Use Plan advocates a strong component of mixed-use in the Southdale Area. Many of the areas shown on the initial concepts have been earmarked for mixed use rather than pure commercial uses. This approach has been taken in order to flexibly address future market opportunities and to balance traffic generation by mixing land use types and their resulting peak hour traffic characteristics.

The plan also advocates strengthening the central circulation spine that runs from the Centennial Lakes office development to the northern reaches of the study area near Crosstown Highway 62. This spine is envisioned as a pedestrian and transit greenway for most of its length as well as being a vehicular roadway from Gallagher Drive to 70th Street. On the edges of the study area, both west of France Avenue and east of York Avenue/Xerxes Avenue, the land use plan calls largely for either mixed-use or residential uses. Future uses in this area have been identified to facilitate ap-

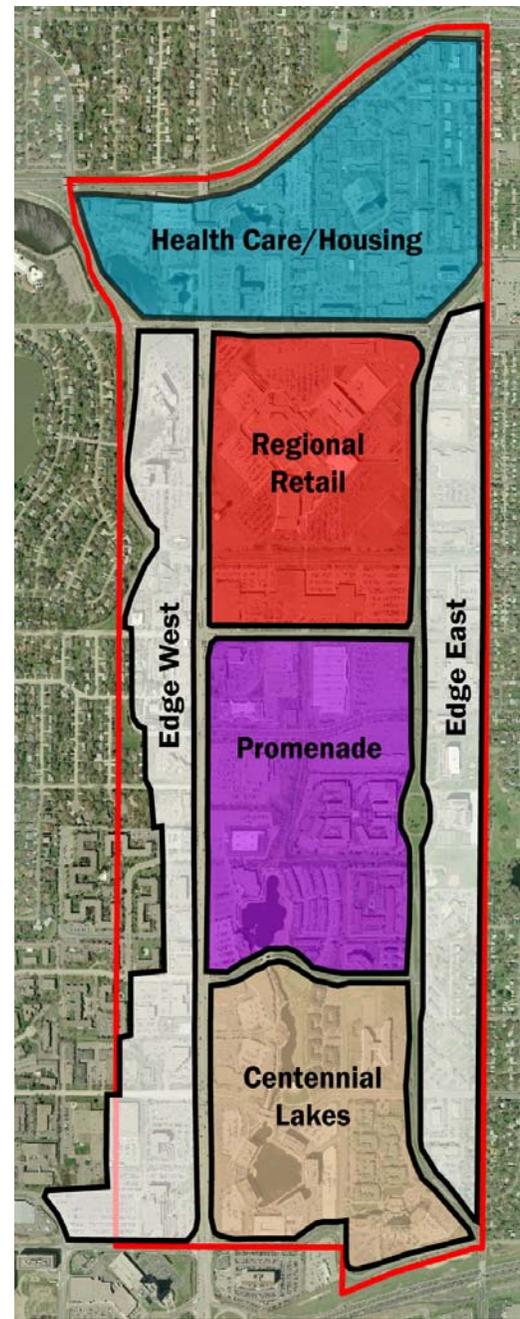


Figure 16
Southdale Area Planning “Districts”.

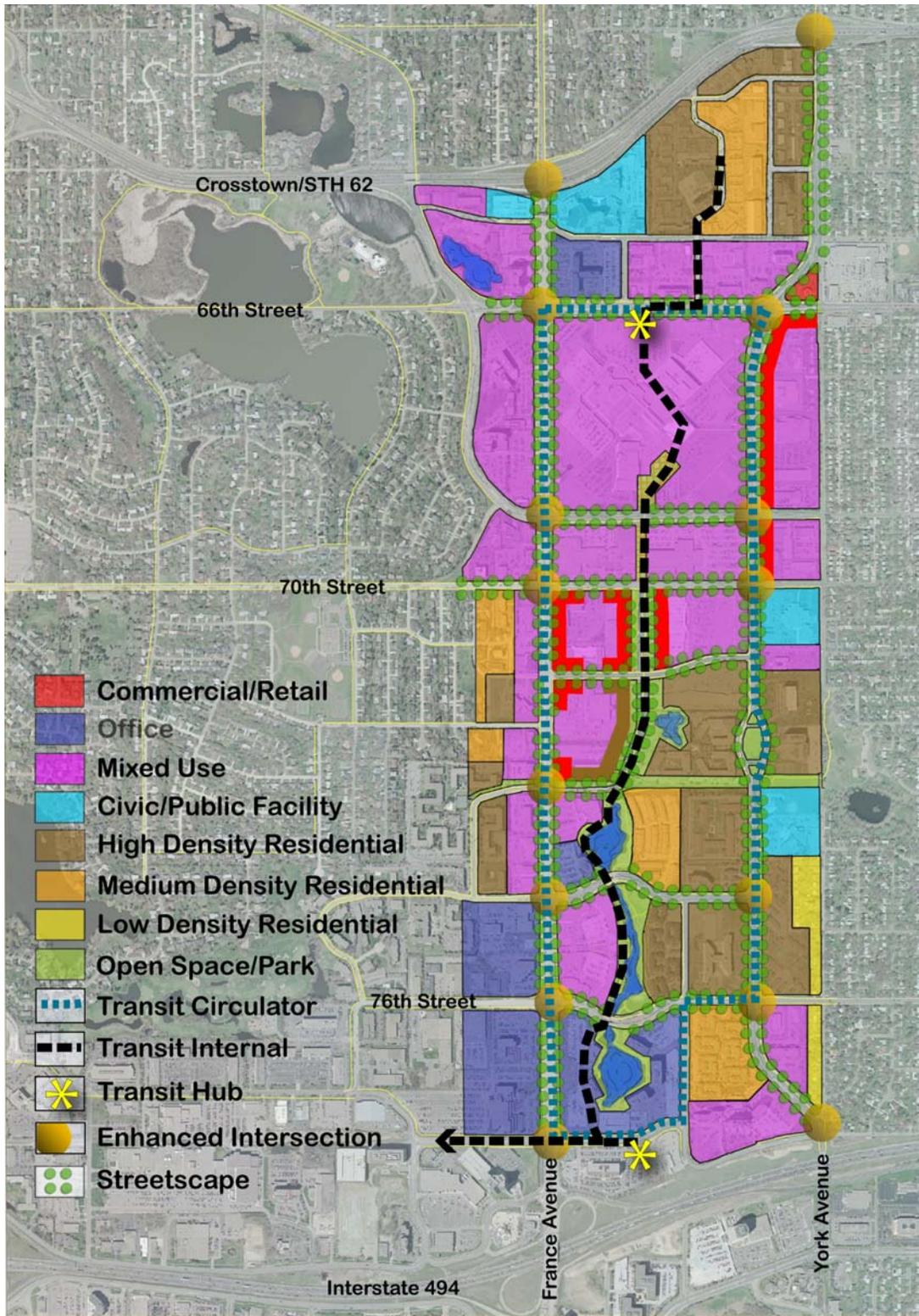


Figure 17
Land Use Plan Concept

appropriate transitions to the lower density neighborhoods in both Edina and Richfield.

Housing is an essential part of the Greater Southdale Area. The housing stock in 2005 includes owner occupied units, market rate rental units, low and moderate income rental units, assisted living and entry level condominium. The vision for the future clearly envisions more people living in the Greater Southdale Area. The proximity to employment, goods and services makes the Area an attractive place for housing. The amenity of the Greater Southdale Area will be enhanced by the transit improvements presented in this Study.

The Area provides unique opportunities to meet the housing needs of Edina. In the 1980s, the City assembled an innovative financing plan to make housing in the Edinborough area available to first-time homebuyers. Future development in the Greater Southdale Area could allow young adults, starting careers, to live in Edina. The Area has historically attracted older residents. Housing for seniors allows older residents to move out of traditional single-family homes while remaining in Edina. This move allows younger families to occupy these homes. A strong relationship between housing, employment and transit can mitigate the traffic implications of new housing.

The Metropolitan Council encourages communities to provide additional affordable and life-cycle housing. Life-cycle housing entails a range of housing options that meet people's preferences and circumstances at all of life's stages. Communities that support life-cycle housing will have housing units, both rental and for purchase that are affordable for low and median-income buyers and for the move-up market. The Livable Communities Act (LCA) authorizes the Metropolitan Council to levy funds to create affordable housing and develop neighborhoods that are pedestrian and transit-friendly. The Land Use Plan provides numerous opportunities for future housing, both as part of mixed use areas and areas designated only for housing. These designations help position the Greater Southdale Area is a potential recipient of regional funds to assist the city of Edina in increasing its future supply of both affordable housing and life-cycle housing.

The following is an overview of the major elements of the plan. In each case, the descriptions are by district.

Health Care and Housing District (north of 66th Street - south of Crosstown Highway 62)

Land Use

In the extreme northwest corner of the study area, Valley View Road provides an entrance to the Greater Southdale Area. The plan calls for mixed-use in the area west of France Avenue with the exception of the parking ramp that serves Fairview Southdale Hospital. To the east of France Avenue, the hospital and medical office complex are expected to remain largely in their current configurations although some future hospital expansion may occur. South of 65th Street, the mixed use land pat-



Figure 18
Health Care and Housing District

tern is shown up to Xerxes Avenue. North of 65th Street, the current mix of high and medium density residential is reflected with anticipation of increased density along Xerxes Avenue. The central spine trail is shown following the general alignment of Barrie Road. In this area, the trail may be a sidewalk connection to the north or in the long-term if properties in the area are redeveloped, it may take a more greenway type of form. Enhanced aesthetic improvements are shown along 66th Street, Xerxes Avenue and France Avenue.

Transportation

The northern gateway to the Greater Southdale area on France Avenue is also the location of significant traffic congestion that will increase in the future as traffic growth occurs. The current design of the interchange at TH 62 (an urban diamond) is the root cause of this congestion. Analysis of the future conditions on France Avenue north of 66th Street indicates that the interchange ramp intersections at France Avenue and TH 62 will operate in unacceptable conditions with the existing lane geometry. The closely spaced diamond interchange design introduces significant queuing to the south on France Avenue that affects intersections south of the interchange area.

An alternate type of interchange design known as a Single Point Urban Interchange (SPUI) would alleviate the poor operations and problems associated with queuing. This type of interchange has most recently been constructed at Penn Avenue/I-494. The basic layout of a SPUI allows for a single intersection where two now exist, which allows for more efficient traffic operations and less queuing. For a SPUI at France Avenue and TH 62, the France Avenue approaches have been assumed to have two through lanes in each direction, dual left turn lanes would be needed on the northbound and westbound approaches and single left turn lanes would be needed on the other approaches. It is likely that Mn/DOT would size the bridge over TH 62 to allow for six lanes of traffic on TH 62. With this configuration, the interchange would be expected to operate in acceptable conditions at LOS D without the level of queuing that is experienced today. While weaving from nearby intersections (65th Street) to the left turn lanes at the interchange would continue, the effect of this activity on France Avenue traffic would be reduced with only one intersection at the ramps rather than two.

Development of this type of interchange would need to be coordinated among Mn/DOT, Hennepin County and the City of Edina. Improvements at this location are not currently programmed in either Hennepin County's or Mn/DOT's improvement programs for the near term. The City would need to work with these other agencies to accelerate the design and construction for a France Avenue SPUI.

Improving the freeway interchange would also improve operations at 65th and 66th Streets by removing the queuing on France Avenue that disrupts these intersections today. The existing geometry at these locations can accommodate future traffic volumes.

The intersections of France and Xerxes/York Avenues with TH 62 and with 66th Street would be enhanced with higher levels of landscaping and pedestrian amenities. Enhanced streetscaping along France and Xerxes/York Avenues and 66th Street is recommended to address pedestrian connectivity in this district. Pedestrian connections to the district to the south would be enhanced to provide for better connectivity with the shuttle system and the Southdale Transit Hub (see discussion in next section).

Regional Retail District (north of 70th Street - south of 66th Street)

Land Use

This segment of the study area contains the highest concentration of existing retail uses since it includes both Southdale Center and Galleria. The Southdale Shopping Center area is identified as mixed use. Open land and the dynamics of Southdale make it a candidate for the addition of future housing, lodging, retail and/or entertainment uses. Intensification of the Southdale area will require additional structured parking. On the north end of Southdale abutting 66th Street, a transit station is shown which will be the confluence of regional bus service, a potential future transit circulator and the transit spine system. Constructing the transit

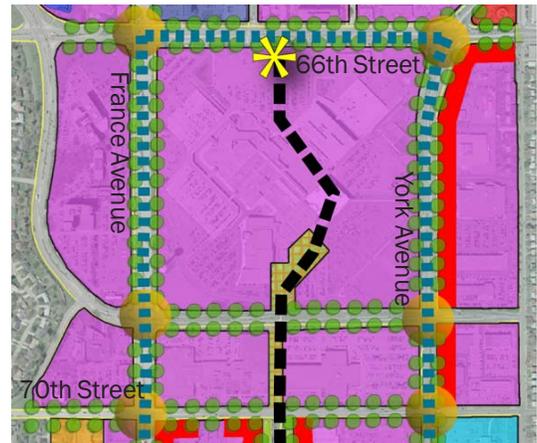


Figure 19
Regional Retail District

spine through Southdale in a meaningful way will be a challenge. Depending on the plans for the shopping center, the preference would be for it to be able to be accommodated as an integral part of the retail buildings or absent the ability to internalize the alignment, the connection could meander around Southdale on an existing easement consistent with the vehicular circulation of the shopping center. South of 69th Street, the transit spine passes under Galleria on an existing easement.

Streetscape improvements are recommended along France Avenue, 69th Street, York Avenue and 70th Street.

Transportation

Both France and York Avenues in this district are dominated by the access system for Southdale Center and the Galleria. These retail centers generate substantial amounts of destination-oriented traffic that contribute to high turning movements on France Avenue. Analysis of the future travel patterns indicates that the intersection of France Avenue and 70th Street is estimated to operate at LOS F during the PM peak in the future with the existing lane geometry. These poor projected conditions could be improved with the addition of the following turn lanes at the intersection:

- Add a northbound left turn lane
- Add a southbound right turn lane; restripe southbound through-right to exclusive through
- Add an eastbound left turn lane
- Add an east bound right turn lane; restripe the eastbound through-right to exclusive through
- Add a west bound right turn lane; restripe the westbound through-right to exclusive through

This will improve the future operating conditions at this intersection to LOS D, but will leave queue conditions similar to existing with delay experienced on the east and westbound approaches and on the northbound left turn lane.

Ultimately, this intersections needs to be addressed in the context of the 70th Street corridor and its relationship to TH 100. If no changes in 70th Street are made, the volume of traffic carried on the portion of 70th Street between TH 100 and France Avenue is likely to grow at a slower rate than has been assumed since the traffic flow on 70th is near saturation during peak periods. In this condition, traffic would be expected to divert around 70th Street west of France and use other routes in the study area. Under this capacity constrained condition, the eastbound movements, the westbound through, the southbound right and the northbound left movements would not be expected to grow, with the traffic that was assigned to these movements in the future conditions analysis absorbed into the other movements at the intersection. Under this capacity restrained condition, the above lane additions would be adequate to retain conditions similar to existing with the future growth under this condition. Under this approach, adding a northbound left turn lane at 69th Street to facilitate turns from France Avenue onto Valley View Drive would be one way of accommodating some of the traffic that wants to use 70th Street. Valley View Drive provides another route to and from the freeway system in the study area and currently has capacity available. Reducing delay in the turn lanes at the intersection of 69th and France would make the Valley View route more attractive.

Alternatively, if 70th Street is widened in the future to carry the projected volumes of traffic, then the overall intersection characteristics would change to the extent that this intersection may be a candidate for grade separation with the through lanes of France Avenue being depressed under 70th Street such that a land bridge (or overpass similar to the design proposed for TH 62) could be used to carry 70th Street across at a grade similar to existing. This would provide space for an improved pedestrian connection at grade on 70th Street. Depending upon the



Figure 20
Example of Land Bridge - Dempster Street at Milwaukee Avenue (Skokie, IL)

design, one or two intersections of 70th Street with ramps to France would be needed. The design could potentially fit into the same right of way as the existing France Avenue as fewer through lanes would be needed on France under the bridge since they would not be under the control of a traffic signal. Figure 20 shows an example from the Chicago area of this type of grade separation. The narrow cross section of the undercrossing is attained by using retaining walls, which are more expensive than side slopes.

When this type of design is applied to France Avenue at 70th Street (Figure 21), the extent of the grade separation, including the points where ramp exits/entrances would occur, would extend from just north of 69th Street to just south of Hazelton Road. In this segment, frontage access would be reduced to one-way access lanes. The extent of the grade separation indicates that it will disrupt the operation of 69th Street and Hazelton Road. Further design would be needed to determine if steeper approach grades that would shorten the area affected could be feasible. Identification of utilities in the roadway shows that a city water main that runs down the west side of France Avenue would be affected and would need to be relocated. Storm sewer would also be affected, but the redesign of the roadway would necessitate a more complex system to replace the gravity feed system currently in place.

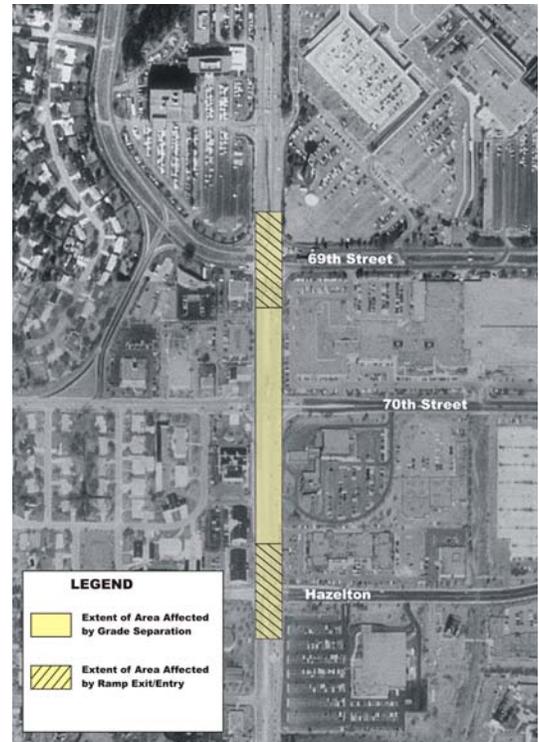


Figure 21
Area Affected by Grade Separation

This type of grade separation offers the most effective means of improving pedestrian crossings of France Avenue. While the crossing distance over France Avenue would not be shorter than today, not having to contend with the through traffic on France Avenue would greatly improve safety and comfort at the crossing and the bridge environment can be enhanced to make the crossing more attractive.

Ultimately, the changes to be made at 70th Street and France Avenue need to be further developed through an analysis of the future of 70th Street as a corridor within the City. Such a corridor study is recommended as a logical follow on step from this study.

Additionally, because of the high amounts of destination traffic in this district, replacing the traffic signal hardware on France Avenue (and to a lesser extent on York Avenue) with a system of traffic adaptive control would allow for traffic signal timing to be adjusted more frequently to match changes in traffic patterns. Extending the traffic adaptive control system throughout the France Avenue corridor from TH 62 to I-494 is recommended for the longer term. In addition to the signal hardware, enhanced traffic surveillance and incident management capabilities would be needed to adequately operate the traffic signal system. These types of activities would require a traffic operation center (TOC). It is suggested that

the TOC be integrated into the City of Edina Public Works Department function to provide a closer proximity and proportionately more attention to system operations than would be the case if the TOC function were integrated with Hennepin County or Mn/DOT's RTMC. Remote feeds/access to the TOC could be provided to other agencies as appropriate. Ultimately, the management of transit priority at intersections would be a TOC function that would need to be accommodated and that argues for a strong linkage between the management of traffic operations and day to day transit operations. Planning for deployment of a TOC for traffic management is recommended as a next step.

While the lane additions mentioned above will improve future traffic operating conditions, the addition of a southbound right turn lane will effectively widen the crossing distance of France Avenue on the north side of 70th Street. Similarly, 70th Street would also have a wider crossing distance if these changes are made. Limited options are available to mitigate the effect on pedestrians that the already wide crossing of France Avenue has. Consideration should be given to developing a grade-separated pedestrian crossing near this intersection that integrates into future mixed use development that may occur near this intersection.

This district is the location of an existing transit hub that could become a future anchor for the shuttle system as well as a potential source of shared parking for the larger study area. Within the concepts of local and regional transit use for the study area, moving the Southdale hub to the southern edge of 66th Street and providing a parking reservoir to both support commute park and ride, but also shuttle park and ride would form one half of a parking/traffic intercept system for the study area. This intercept system would use a larger "park once" strategy in connection with shuttle transit and enhanced pedestrian connections to capture retail/entertainment traffic at the north and south ends of the study area near the freeways. Locating the hub closer to 66th Street would also place the hub in closer proximity to the hospital/medical campus in the district to the north.

The intersections of France and York Avenues with 69th and 70th Streets would be enhanced with higher levels of landscaping and pedestrian amenities. Enhanced streetscaping along France and York Avenues and 69th and 70th Streets is recommended to address pedestrian connectivity in this district. The opportunity exists to increase the amount of space available for pedestrians along 70th Street between France and York Avenues to form a more inviting access to the Promenade area (discussed below) by reclaiming some of the current street and putting that space into sidewalks.

Promenade District (north of Gallagher Drive - south of 70th Street)

Land Use

The area generally lying north of Gallagher Drive and south of 70th Street is looked at as being the most likely locale for future redevelopment efforts, some possibly on a large scale. In the next couple of years, the Target store in the Promenade district is scheduled for reconstruction as a new Super Target format store. Other existing uses along France Avenue may be altered due to business ownership changes, aging buildings and other factors.

When the Target store is reconstructed in this area, it is important that it comply with the focus of the Promenade area that emphasizes pedestrian scale facilities. Where possible, it is important to reduce the scale of retail uses in this area in order to support the pedestrian

friendly vibrancy envisioned along the Promenade. These smaller scale retail uses could be included in future plans as depicted later in this study report.

East of the Edina Promenade, the plan largely calls for high density residential uses with the exception of the existing library to the north. Improved roadway aesthetics are called for along France Avenue and York Avenue. Gallagher Drive is to be improved and connected to the new Promenade street that will accommodate mix of transit, pedestrians and automobiles.



Figure 22
Promenade District

Transportation

The transportation needs in this district are directed toward the establishment of the spine street, referred to in this Study as the “Edina Promenade”. (A detailed description of the Edina Promenade can be found in “Public Initiatives”). The Edina Promenade, including the portion along Gallagher Drive east of France Avenue represents approximately 2,500 feet of new roadway construction and two new intersections (at 70th Street and the internal street proposed between Gallagher and 70th). Basic construction cost for the roadway and intersection elements would be in the \$1.0 to 1.5 million range depending upon the type of pavement surface used.

The intersection of France Avenues with Gallagher Drive would be enhanced with higher levels of landscaping and pedestrian amenities. Enhanced streetscaping along France and York Avenues is recommended to address pedestrian connectivity in this district. A grade separated connection of the existing regional trail that traverses this district, preferably under France Avenue, similar to the crossing of York Avenue, is recommended for this district.

Centennial Lakes District (south of Gallagher Drive - North of Minnesota Drive/78th Street)

Land Use

This area of the study is generally known as Centennial Lakes, taking its name from the successful mixed use and office development that began in the 1980s. East of France Avenue, the land use plan supports the existing Centennial Lakes development. On the north end, the AMC Theater site is designated as mixed use. The Centennial Lakes retail area is also shown as mixed use allowing future intensification possibly including housing. The Centennial Lakes office area is shown with its existing land use types as is the medium density residential development east of Edinborough Way. Parcels along West 78th Street are shown as mixed use.

A transit station is shown at the south end of the Area. In order to maximize the effectiveness of a southern transit station, it should be located as far south as possible and as close to I-494 as possible. I-494 is the location of a planned bus rapid transit line that would make a logical connection at the south transit station.

Improved streetscape is envisioned along France Avenue, York Avenue and 76th Street. The central spine in this area will serve as a connection to the proposed Hennepin County regional trail.

Transportation

At its southern end in the Greater Southdale Area, France Avenue would need to retain its current divided arterial design to address regional traffic needs. Interchange access at I-494 requires the wide cross section south of Gallagher Drive. Land uses in the I-494 area are well developed and set back from the roadway edges in a way that reflects the restricted access from France Avenue in this area. As a result, pedestrian connectivity along France is restricted in this district and should be enhanced.

The intersection of France Avenue and Minnesota Street/78th Street is estimated to operate at LOS F in the future. Proposed change to reach an acceptable LOS would be to add a westbound left turn lane. However, even with the lane addition, queuing would be expected to occur at levels equal to or greater than existing conditions. Because of the proximity of this intersection to the I-494 freeway interchange, further lane additions are unlikely. Review of the future traffic operations shows that the northbound left turn movement, although lower in volume than other movements experiences a high level of congestion, plus it contributes to the weaving that occurs upstream of the intersection. Elimination of the northbound left turn would free sufficient green time at this intersection to reduce queuing to existing levels or better. Elimination of both the northbound and southbound left turns would further improve conditions. Alternate routes are available for both of the movements at nearby intersections.

Further development west of France Avenue toward TH 100 (which is outside of the Greater Southdale Study area) will put pressure on this intersection and on the I-494 freeway interchange. As that development is defined in the future, it is recommended that a study of that corridor be initiated that includes investigation of alternate interchange designs that allow traffic to access that area independent of France Avenue.

This district is also the location of a potential transit hub/station stop that would interface with high frequency transit service envisioned for the I-494 corridor. As such, it would become the southern anchor of both the shuttle system and the parking reservoir for the parking intercept strategy. As with the northern hub, if a parking intercept program is successful, traffic that is traveling through these district would be attracted to the parking



Figure 23
Centennial Lakes District



Figure 24
Edge West District

opportunities in these districts. This would have the effect of changing the distribution of traffic on the streets in the study area and the level of intersection/street modifications would need to be revisited if an intercept strategy is employed.

Edge West District (west of France Avenue to the project boundary)

The Edge West District abuts single-family and multi-family neighborhoods in the City of Edina. Overall, in the Edge West District, the plan responds to two primary factors, 1) creating a definable edge for the more intense uses that abut France Avenue and 2) improving connections from the neighborhood to the Southdale Area and points east.

In the northern portion of the Edge West District, the plan anticipates the long-term retention of the existing office area with the possibility of some infill housing or additional office infill in the future. South of 69th Street, the Land Use Plan calls for medium and high density residential uses as buffers to the existing lower density neighborhoods in Edina to the west. Providing a definable "edge" to the Southdale area is looked at as a factor in helping to ensure the long-term support of these neighborhoods. East of the medium and high density residential uses is mixed use abutting France Avenue. Mixed use in these areas is expected to include housing and office uses that may front on France Avenue. The mix of high density housing on the west and mixed-use abutting France Avenue continues north of Parklawn Avenue. South of Parklawn Avenue and west of France Avenue, office uses are identified consistent with the existing land use pattern.

Edge East District (east of York Avenue to the project boundary)

The Edge East District abuts neighborhoods in the City of Richfield. The concerns in this area echo those on the west, 1) maintaining a definable edge and 2) enhancing connections between the neighborhood areas and the greater Southdale Area.

In the northern end of the Edge East District, the Land Use Plan calls for mixed use to ultimately replace the existing free standing, larger scale commercial uses. In this area which extends south to 79th Street, the plan emphasizes a commercial retail frontage along York Avenue. Having first floor commercial uses will add vitality to the area and emphasize York Avenue as a comfortable pedestrian street. Mixed use in the area that can include residential uses on the east side will support the existing residential neighborhood in Richfield.

South of 70th Street, the Land Use Plan depicts a pattern that is largely consistent with existing uses. The Southdale Library and YMCA remain



Figure 25
Edge East District

as public uses with high density housing generally north of West 76th Street. To the south, the plan identifies office development consistent with the pattern of existing uses

Intersection Operations

Future transportation conditions and improvements have been discussed separately for each of the Plan's Districts. Figure 26 consolidates the conditions for all of the districts and shows projected operating conditions at intersections in the Greater Southdale Area with and without the improvements recommended in the Plan under the future level of development provided by the Plan. (Figure 9 shows this same information for existing conditions.)

Intersection	LOS w/o Improvements	LOS with Improvements
TH 62 WB Ramps/France Avenue	D(a,b)	D
TH 62 EB Ramps/France Avenue	D(a,)	D
West 66th Street/France Avenue	D(a)	
West 69th Street/France Avenue	D(a)	
West 70th Street/France Avenue	F	D(a,b)
Parklawn Avenue /France Avenue	B(a)	
West 76th Street/France Avenue	D(a)	
Minnesota Drive/West 78th Street/France Avenue	F	D
TH 62 WB Ramps/Xerxes Avenue	D(b)	
TH 62 EB Ramps/Xerxes Avenue	B(a)	
West 76th Street/York Avenue	C(a)	

Notes:
 (a) LOS E is experienced on one or more approaches at these intersections
 (b) LOS F is experienced on one or more approaches at these intersections

Figure 26
 Future Level of Service

Development Intensity and Building Height

Future development intensity and building height are important physical considerations that in many cases, will be linked. In order to obtain enough development intensity to justify land values and other economic considerations, the City is likely to see more pressure for taller buildings in the Southdale Area. Accordingly, each of these topics is addressed in a preliminary manner in this study. Definitive projections of future development intensity and appropriate building heights are two topics likely to require more detailed investigations beyond the scope of this study.

Development Intensity

The opening narrative of this report clearly states that the focus of this Study is one of setting a strategy for the future evolution of the greater Southdale Area. This direction stands in stark contrast to a redevelopment plan that calls for specific actions at specific locations.

Despite the general nature of this study, it is essential to quantify at least an example of future

development intensity in order to better assess the relationship between land use and transportation and to identify existing and future transportation issues and opportunities. Looking at future thresholds of development at this time is at best, preliminary in form due to the lack of specific proposals for future change. For example, the new owners of the Southdale Center are formulating future plans, however, none of those plans have evolved to the point of being ready for public review. Similar statements can be made for other properties in the study area.

Absent specific development and redevelopment plans, the consulting team in concert with City staff conducted a review of the project area and defined geographic areas likely to see future intensification and/or change. Factors influencing the identification of potential redevelopment areas included aging buildings, underutilized open areas, and likely market pressures. Those parcels felt to be candidates for possible change are noted in Figure 27.

After defining those areas that may be potential subjects of change, the next step was to assign land use types and future growth values. Land use types were labeled based on the direction of the Land Use Plan. Amounts of future intensification were assigned based on the general influence of the initial market analysis that was completed as part of the study, from trends that seem to be occurring in the Southdale Area, from plans that have received past approval (but never fully built) and/or from plans that have been offered for informal consideration. The future growth assumptions can be found in Appendix B. Again, this information is not intended to be a specific forecast of growth in the greater Southdale Area but rather, an estimate to support a preliminary level of analysis to examine traffic and transit needs and opportunities.

Building Height

When Southdale Center opened in 1956, it defied traditional shopping center design in two significant ways: 1) it enclosed the common interior portion of the mall, and 2) it contained stores on more than one level. Over the past fifty years, numbers of stories and heights of



Figure 27
Areas of Intensification and Change

buildings have evolved over time. Today, the examples of taller buildings exist throughout the Greater Southdale Area. Examples include the Edina Towers at 17 stories, the Durham Apartments at 13 stories, the Yorktown Continental Apartments at 12 stories, Point of France at 13 stories and Edinborough at 19 stories. These structures have been integral parts of the Southdale area landscape for decades.

The current zoning ordinance has provisions that regulate height only through setbacks, not limits on numbers of stories. At the present time, the former AMC theater site is the subject of a proposed 18 story building.

Determining maximum building height in the Southdale Area needs to consider both economic and visual issues. Land values in the Southdale Area and the Southdale Area's heritage as being one of the premier locations in the Twin Cities will continue to result in pressure for taller buildings. In some ways, the overall height of buildings in the area is not as critical as the architectural design of the structure and how scale and mass issues are addressed. Buildings approaching 20 stories now exist in the area and have not proven detrimental to the overall physical environment. Similarly, new structures topping out at 20 stories don't seem inconsistent with the development fabric of the Southdale Area.

Determining appropriate locations for taller buildings should be tied to the overall growth strategy for the Greater Southdale Area. The land use and transportation components of this study place a great deal of emphasis on guiding change in the central portion of the project

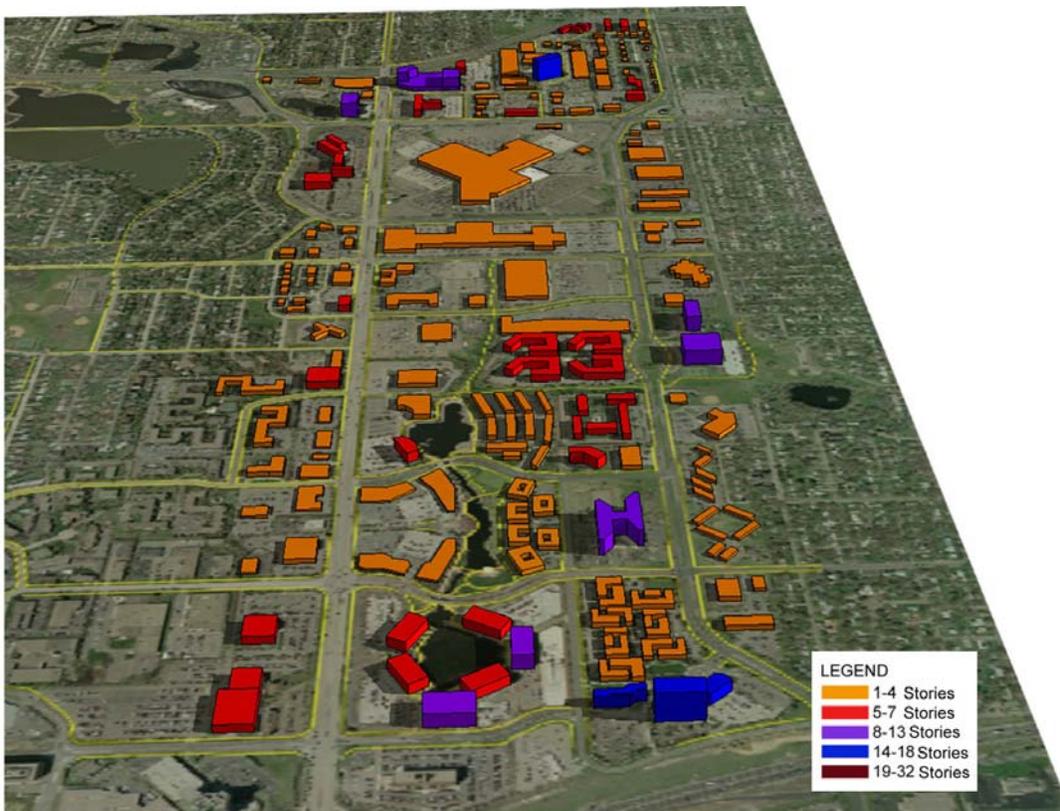


Figure 28
Existing Massing of Greater Southdale Area

area generally extending from the new Edina Promenade retail area on the south to Southdale Center to the north. Buildings along the Promenade from Gallagher Drive to 70th Street need to convey a pedestrian scale. This can be accomplished either by height limitations (3-5 stories) and/or by allowing somewhat taller buildings with increasing front setbacks for the upper stories. Throughout the remainder of this area, taller buildings approaching the heights of existing buildings in the Greater Southdale area would not be out of character. Within this general location, taller structures would not directly impact any of the lower density residential neighborhoods in either Richfield or Edina.

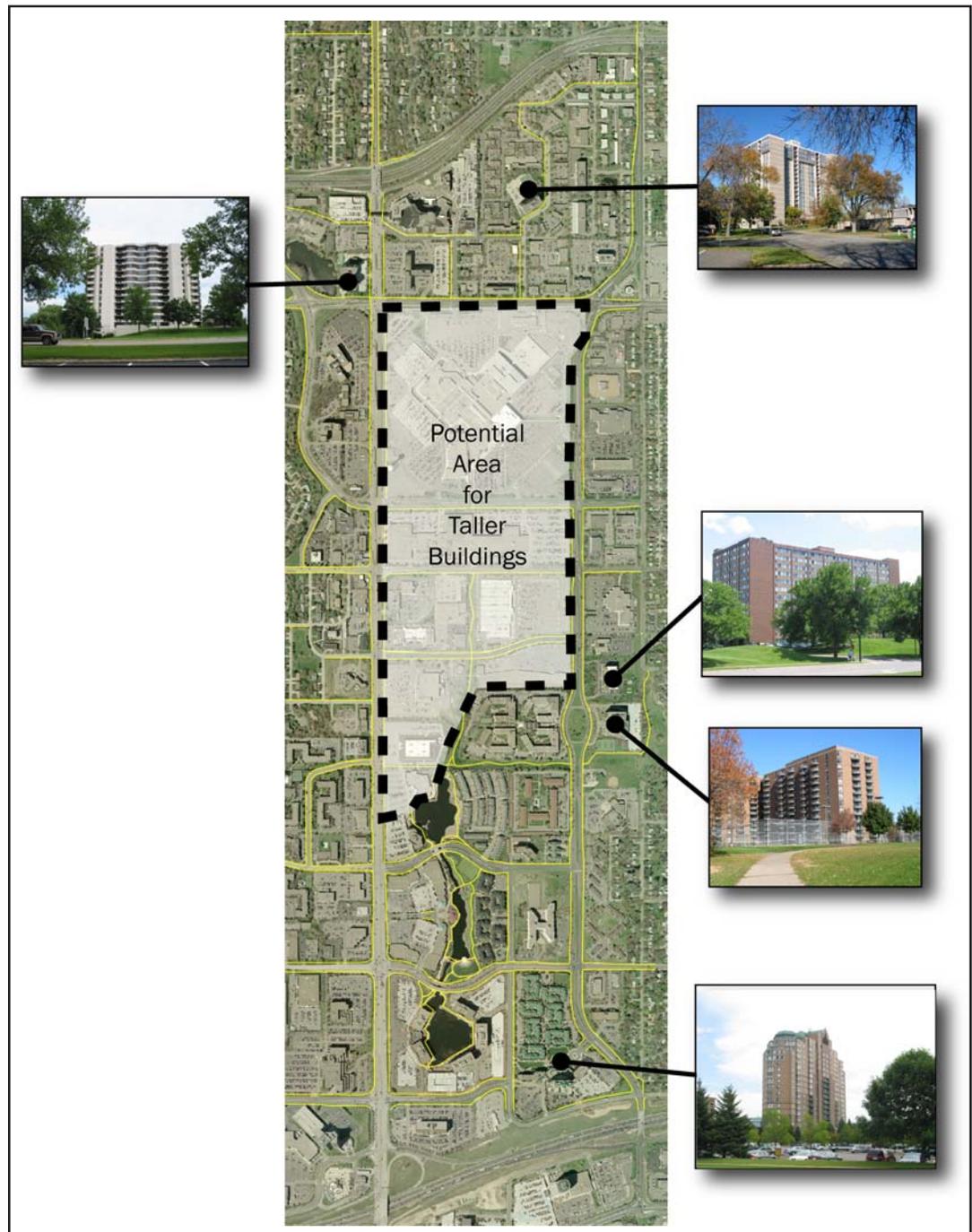


Figure 29
Distribution of Building Height

Pedestrian and Bicycle Circulation

Moving around the Greater Southdale Area on foot or on a bicycle presents a number of challenges. While the area has sidewalks along most of the roadways, many of the walks are located immediately behind the curb presenting an uncomfortable environment for pedestrians. Additionally, there are no sidewalks along the east side of France Avenue from West 78th Street to 66th Street. This situation is particularly ironic since many of the area's newer developments are located along this portion of France Avenue.

From the east, a tunnel under York Avenue connects neighborhoods in Richfield to the Southdale Area. This trail continues northward through the middle of large block areas up to 70th Street where it connects to Galleria to the north. The trail is used by residents from multi-family housing in the area to conveniently access Target and other area businesses.

As the Southdale Area continues to evolve in the future, the needs of pedestrians and bicyclists need to be considered. Particular attention needs to be focused on pedestrian movements across France Avenue and bicycle movements within and through the study area.

France Avenue is a formidable barrier for pedestrians to cross. Because of the width of the roadway, the high traffic volumes that it carries and the number of turn lanes at some intersections, grade separated crossings would be the safest way to accommodate pedestrian movements. Creating grade separated crossings along France Avenue is a costly endeavor and the generally flat topography of the area doesn't create natural crossing points. In spite of these factors, the City and County need to strive to establish safer access and crossing points.

In order to address this issue along France Avenue, a series of initiatives including transit, grade separated crossings and at-grade crossings are suggested. In the southern portion of the study area at West 78th Street, a safer and more convenient means needs to be found to move pedestrians from the business areas lying west of France Avenue to the central spine area that exists within Centennial Lakes. Because of the physical configuration of the roadway in this area, transit is suggested as the most feasible alternative for linking both sides of the roadway. The central spine transit system meanders through the heart of the Southdale Area. This plan advocates the establishment of a transit line that would connect the extensive number of businesses along West 78th Street (and even along West 77th Street) to the Greater Southdale central transit spine.

During the public meetings that were held as part of the project, residents living west of France Avenue stated a strong desire for a safe connection across France Avenue. A connection along West 72nd Street, either as a tunnel under the road or as a pedestrian bridge over the road needs to be established. Such a connection may not realistically be able to occur until redevelopment takes place in the future on either one or both sides of the roadway.

The transportation portion of this report contains information about a possible future land bridge at West 70th Street and France Avenue. Should this occur, it would provide a safe and convenient connection for both pedestrians and bicyclists. The high cost of this improvement, however, realistically makes it extremely difficult to implement in the short term.

West 66th Street contains a sidewalk along the north side of the road that evolves into a



Existing pedestrian tunnel under York Avenue



Existing Conditions - 72nd Street at France Avenue

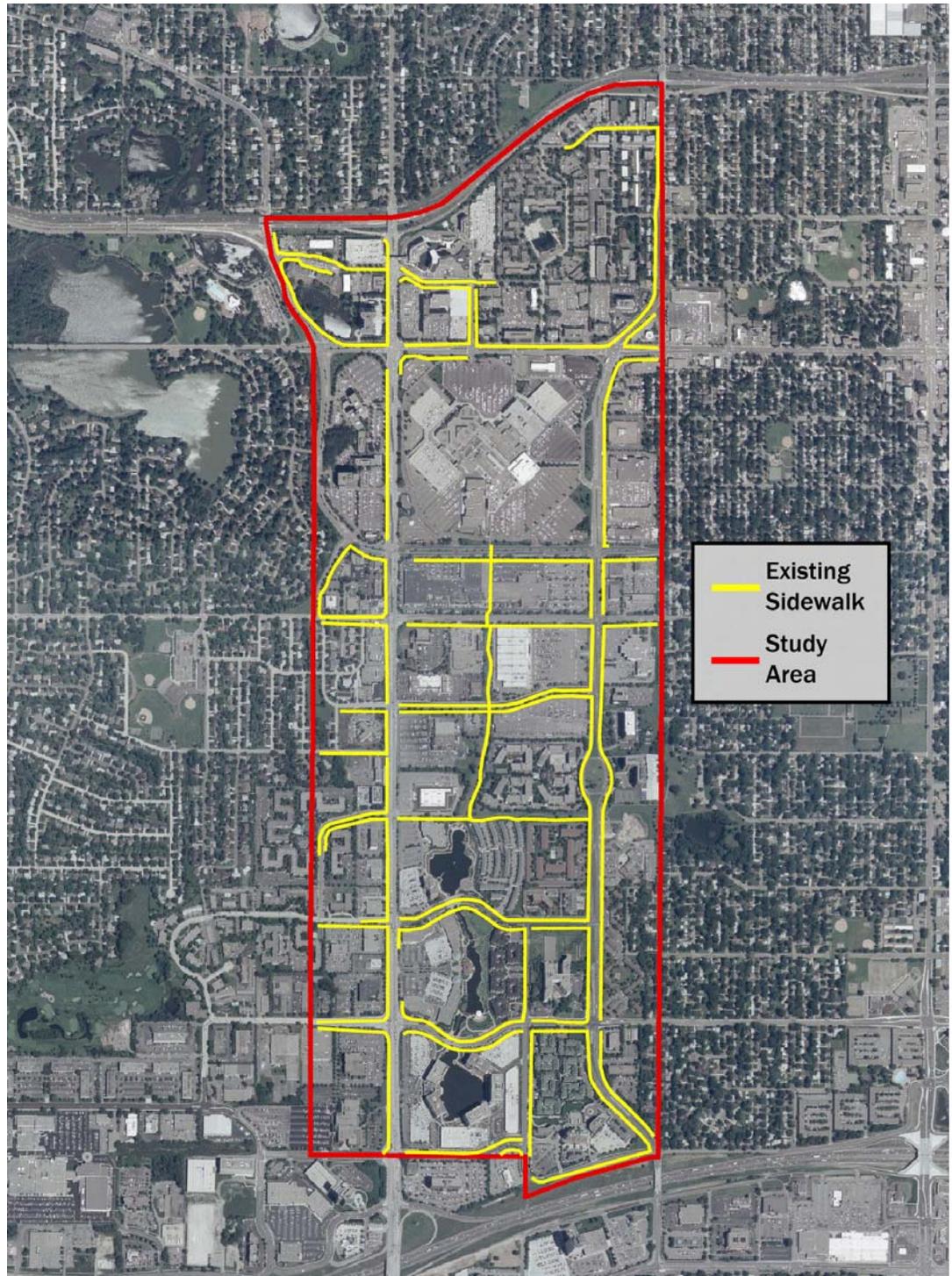


Figure 30
Southdale Area Sidewalks

wider trail west of Valley View Road. At-grade crossings at France Avenue and West 66th Street and at Valley View Road and West 66th Street are expected to continue to provide signalized, crossings for both pedestrians and bicyclists.

At the present time, bicycle travel is not one of the major modes of movement in the Southdale

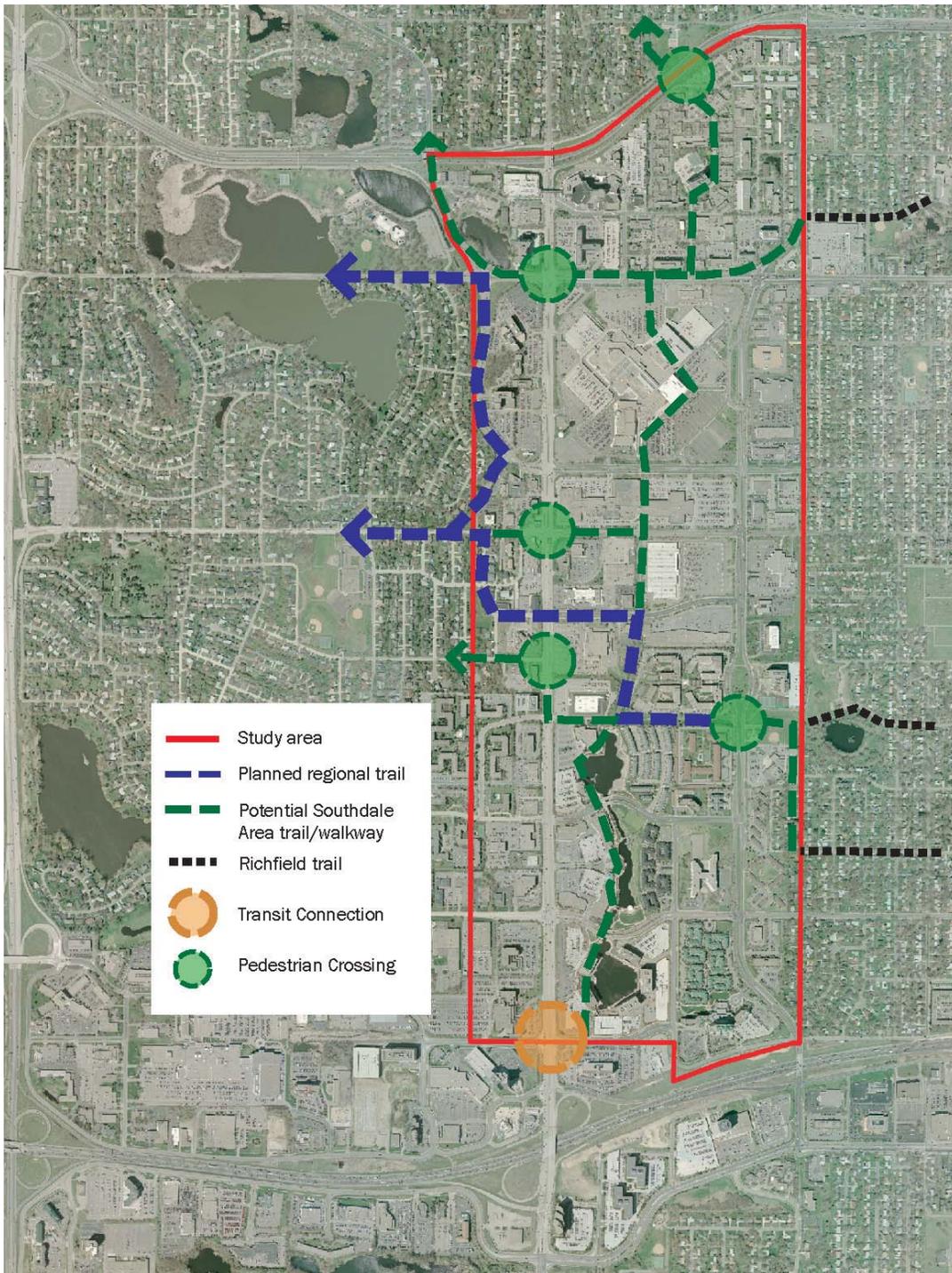


Figure 31
Southdale Area Trail Connections

area, largely due to a lack of safe trails and/or roadways. The Southdale Area Trail Connections map illustrates a system of planned bikeways for the area. The City of Richfield has three bikeway trails that abut the eastern boundary of the study area, one of which continues into the Southdale Area and passes under York Avenue. Three Rivers Park District has plans

for a trail that picks up off of the existing Richfield/Edina trail continuing north along the central spine, west along Parklawn Avenue, then proceeding north to 70th Street whereby it will eventually proceed further west along 70th Street. In addition, the Three Rivers Park Plan shows a trail segment along Valley View road up to 66th Street whereby it then proceeds west along the existing 66th Street trail. The First Tier Trails, Greenway and Parks Master Plan prepared in 2000 by the Suburban Regional Park District identified an Edina/Richfield Corridor. Implementation of the corridor would require the reconstruction of 70th Street and 76th Street.

While these existing and planned trails bisect the Southdale Area and provide reasonable east/west access, they do very little to promote north/south circulation and access. The central spine has the ability to provide north/south access for bicycles on the roadway. Additionally, other north/south routes should be considered. Bicycle lanes or trails along York Avenue should be considered consistent with regional bikeway plans.

Efforts should also be made to connect the neighborhoods north of the Crosstown with the Greater Southdale Area. A grade separated crossing of the Crosstown Highway should be considered extending from Heritage Drive to Beard Place.

The Study identifies two key public initiatives: central spine street and transit systems. These initiatives are critical to achieving the future vision for land use and transportation in the Greater Southdale Area. The initiatives demonstrate the connections between land use and transportation. On the surface, each initiative deals with improvements to the transportation system. Making these improvements also shapes the setting for future land use and development.

Edina Promenade

In the late 70s, early 80s, Edina had a group of visionary planners that conceived the idea of a transit way that would run from the southern end of the Southdale Area near I-494 to Southdale Center on the north. The City was successful in obtaining special legislation to fund and operate transit in the corridor. Although transit service along the route has yet to be established, the transit way corridor today is evident in two principal locations, 1) as a walkway behind the existing Target store on 70th Street and 2) in the greenway section of the Centennial Lakes development.

In examining various concept alternatives as part of the Greater Southdale planning effort, the Edina Promenade became a popular unifying element of the plan. The alternatives depicted the Promenade in a variety of forms ranging from more of a greenway, open space type corridor to an active local street. The Land Use Plan depicts the Edina Promenade as an active local street accommodating pedestrians, automobiles, bicycles and transit vehicles. The design of the street has the potential to address a number of needs and functions.

The Edina Promenade is looked at as a key link between the Centennial Lakes area to the south and the Southdale/Galleria retail area to the north. Key link, in this context, means a number of things. It relates to the physical movement connection between the two areas and it serves as a transition between the green, park amenities in Centennial Lakes and the more pavement intensive retail area to the north.

Between 70th Street and Gallagher Drive, the Edina Promenade has the potential to be a great street. It is envisioned to have an attractive urban form that provides opportunities for a mix of land uses along its frontage including retail shops and offices. The market analysis conducted as part of the study as well as lessons learned from other projects throughout the country strongly indicate that the successful integration retail and office uses in the area will require vehicular access. Automobile usage of the Promenade is not looked at as an unwanted intrusion but rather, as a means of providing needed exposure and access. The Promenade will not be an alternative route to France Avenue or York Avenue. Its character and function will simply not be conducive to large volume or higher speed traffic movement. As such, its frontage will stand in stark contrast to the much more intensively used area roadways such as France Avenue and York Avenue in its civility and in its attractiveness as a place to shop, dine, live, work and walk.

The Promenade has the ability to incorporate green elements into its urban section. It is important that future designs for this corridor extend the green elements of Centennial

Lakes to the doors of Galleria and the Southdale Center. This can be accomplished by ensuring that the roadway section includes generously landscaped boulevard areas, richly textured hard surfaces and attractive, pedestrian scale lighting.

The Promenade impacts four of the six districts identified in the greater Southdale area. The following is an overview of the alignment and character of the spine in each of the districts.

Health Care/Housing District

Initial comments received from the public identified the need for stronger connections between Fairview Southdale Hospital and Southdale Center. People commented that visitors to the hospital frequently have free time and would visit Southdale if a more convenient link existed between the two facilities. In order to respond to this suggestion, the plan advocates investigation of an underground connection between the two facilities in the form of a tunnel. Tunnels are used extensively in the city of Rochester to link Mayo Clinic with downtown and lower level commercial uses. At the present time, Fairview Southdale Hospital is connected by a tunnel to the Southdale Medical Building. Extending the tunnel network to the south across 66th Street with a terminus either in Southdale Center or in the new transit station along 66th Street would provide a convenient, all-weather connection. At this location, the tunnel would be an effective western extension of the Promenade in the Health Care/Housing District to serve the health care facilities.

It is also important to link the housing component of the Health Care/Housing District to points south. North of 66th Street, the Promenade is shown as a pedestrian and potential transit connection utilizing existing streets and right-of-way. In this area, the spine traverses the existing residential neighborhood north of 65th Street.

Regional Retail District

The Regional Retail District includes two of the largest retail attractions in the greater Southdale Area, Southdale Center and Galleria. At the present time, the city of Edina has easement rights to continue the transit way around Southdale Center via the shopping center roadway system and under the Galleria.

If portions of Southdale Center are reconstructed or reconfigured in the future, efforts should be made to internalize the spine connection either as a facility that penetrates the interior of the shopping center or as a system that immediately abuts the exterior of the building. Making a contiguous, convenient connection to the mall will enhance the usage of the transit system and provide enhanced access to Southdale Center.

Promenade District

The Promenade District is the focal point of potential future redevelopment in the greater Southdale Area. Stretching from Gallagher Drive on the south to 70th Street, the area is the home today of a mix of housing, retail and commercial service businesses.

The Promenade District provides an opportunity for the creation of a unique business and living environment along the new spine street. As shown on Figure 32, this area has the potential to accommodate an exciting new mix of uses including vertically mixed commercial and housing. Key components of the Land Use Plan for this area include the following.



Figure 32
Promenade District

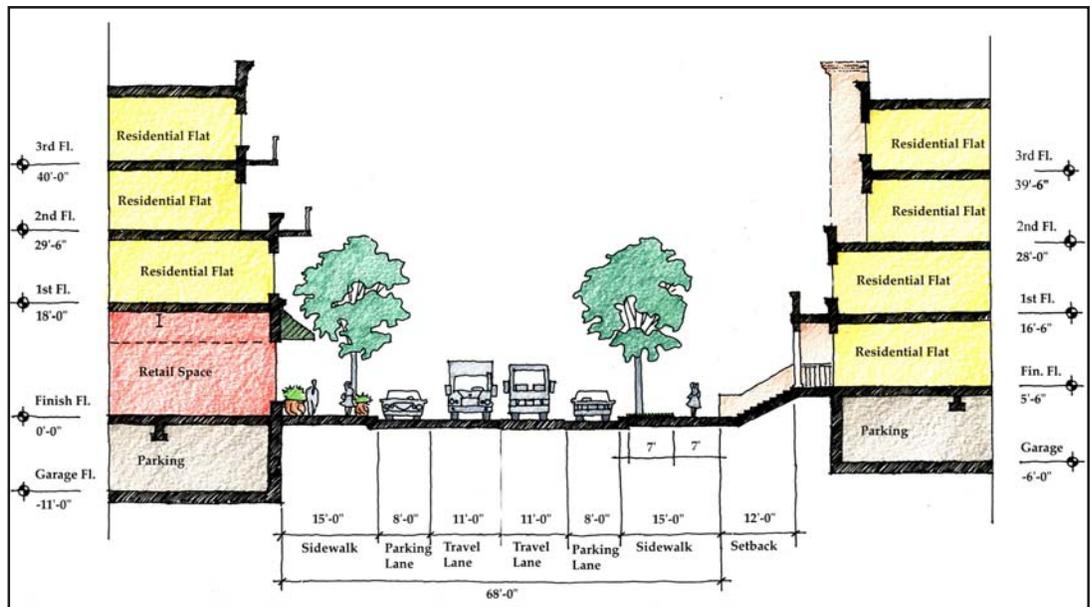


Figure 33
Cross-Section of Edina Promenade Street

The commentary is generally organized by the four quadrants that comprise the Central Spine District.

Creation of a New Pedestrian Oriented Street

The Land Use Plan calls for the creation of a new street that extends south from 70th Street linking back to the west to France Avenue. As shown on Figure 33, this street is expected to accommodate pedestrians, vehicles and transit, most likely a rubber tired vehicle. The form of this street as shown on the plan and section will include significant landscaping, special pavement treatments and decorative lighting. It is intended to create an internal focus within the greater Southdale Area that will have a "Main Street" feel. The street can have an urban greenway feel that will be compatible with the existing corridor that passes through the Centennial Lakes District to the south.

Northeast Quadrant

The northeast quadrant of the Central Spine District is currently the home of the Southdale Target store. Within the next few years, Target expects to reconstruct the store. Target is currently the only big box, retail format store in the District. The new interior street passes along the back side of Target. It is essential that the Target site present an appropriate face along the new street, one which needs to have an appropriate pedestrian scale and business vitality. A blank rear facade of a Target store would be seriously detrimental to the goals for the new street.

Two configurations are offered for the Target store. The first as shown on Figure 34 depicts a traditional big box store layout with one exception. Along the back side of the store, a series of smaller retail uses is shown "attached" to the rear facade. This wrap of smaller retail uses could be independent stores or possibly departments such as the optical area of the Target store. A surface parking lot is shown east of the store along portions of 70th Street and York Avenue.

Figure 34 shows another conceptual layout for the Target store that is more compatible with the mixed-use and pedestrian scale goals of the Central Spine District. This concept includes a more significant wrap of retail and potential mixed-use buildings on the north and west sides of the Target store. Structured parking would be built just east of the store serving both Target and possible smaller scale retail uses abutting York Avenue. Depending on the desired size of the facility, the Target store may require two stories similar to the store in downtown Minneapolis.

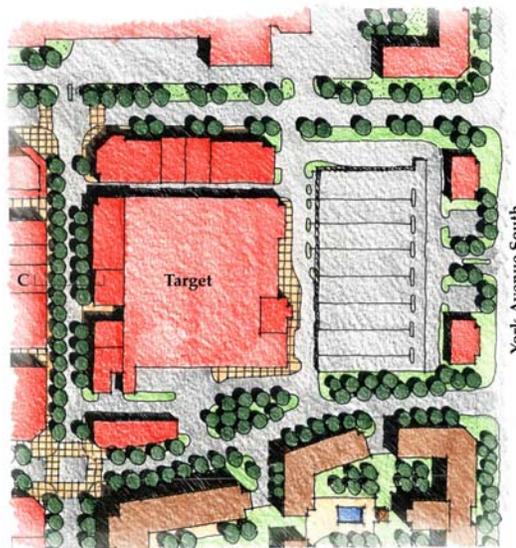


Figure 34
Alternative Target Layout

Northwest Quadrant

The northwest quadrant of the area is the current home of US Bank, Leisure Lane and other commercial uses. The Land Use Plan depicts

this area as a potential future mixed-use and retail center with building faces along the new internal street, 70th Street and France Avenue. Accordingly, the interior portion of the area could contain structured parking for the abutting uses and a landscaped plaza area to serve both the retail users and residents. The block facing the new internal street should have ground floor retail focus.

Southwest Quadrant

Hazelton Road is the northern boundary of the southeast quadrant. The quadrant currently includes a Byerly's store and a Marshall Field's Home Store. In the future, the plan calls for the evolution of this area to include a mixed-use development pattern with an emphasis on housing. As depicted on the concept plan, this area could have underground parking serving adjacent development with central entries off of Hazelton Road and the extension of Gallagher Drive to the east (new internal street). Landscaped courtyards would provide green space and plazas for both residents and retail customers. Uses along the new spine street in this area are expected to be primarily residential with limited ground floor retail and/or office also as a possibility.

Southeast Quadrant

The southeast quadrant is depicted on the Land Use Plan as a high density residential area. This area currently contains the Yorktown Fashion Mall and high density housing.

Overall View

Figures 35-38 show renderings of what the Central Spine District could look like in the future. Buildings on the plan are generally shown as ranging from three to eight stories but could be higher depending on economics and design details.

Centennial Lakes District

The transit way in the Centennial Lakes District is largely in place as a walking trail and promenade. Stretching from the Centennial Lake office area on the south up to Gallagher



Figure 35
Perspective Looking Northeast



Figure 36
Perspective Looking Southeast



Figure 37
Perspective Looking Southwest



Figure 38
Looking South Along Promenade

Drive, the spine has sufficient width to accommodate a future transit vehicle. Connecting the Promenade to the south across West 78th Street to a potential future transit hub location will be more difficult given the existing development pattern and will need to be investigated more thoroughly.

Streetscape Enhancements

The Greater Southdale Area Land Use Plan calls for streetscape enhancements along most of the major roadways. The intent of these enhancements is to create a vibrant and diverse system of streets and sidewalks that provide greater safety, interest and comfort for the pedestrian. These improvements are also oriented toward improving the aesthetics of the Greater Southdale Area.

In the future, as street reconstruction takes place or as major private redevelopment projects occur, the City should implement streetscape enhancements. Since no major reconstruction efforts are imminent at this time, the Greater Southdale Study offers a framework for future streetscape improvements. This framework defines three major street types that may be applicable in the Greater Southdale Area and offers general suggestions for public enhancements.

Promenade Retail Street

The Promenade District, located between the Regional Retail and Centennial Lakes Districts is intended for mixed use with greater levels of multi-family housing and ground level retail. The Promenade provides a north-south connection between the shops at the Galleria and Southdale Center and the Centennial Lakes open space system, homes, shops and offices. It is envisioned to be a vibrant street with active storefronts. A more compact, pedestrian-scaled street space can be achieved by mixing uses and building to the sidewalk edge. Sidewalks should be designed and constructed with quality and craft in mind.

The promenade must be able to handle heavy foot traffic, and provide safety and comfort to attract pedestrian activity. Convenient parking should be allowed on the street to address the needs of pedestrians and retailers. Specific streetscape recommendations include the following:

- Minimum 15' Public Right-of-Way
- High quality, durable and attractive pavement
- Street trees and tree grates or planter cut-outs
- Street lamps
- Benches
- Planter pots
- Window awnings
- Bike parking
- Trash cans
- Banners and signage
- On-street parking

The elements of streetscape for retail streets on the Promenade are illustrated in Figure 39.

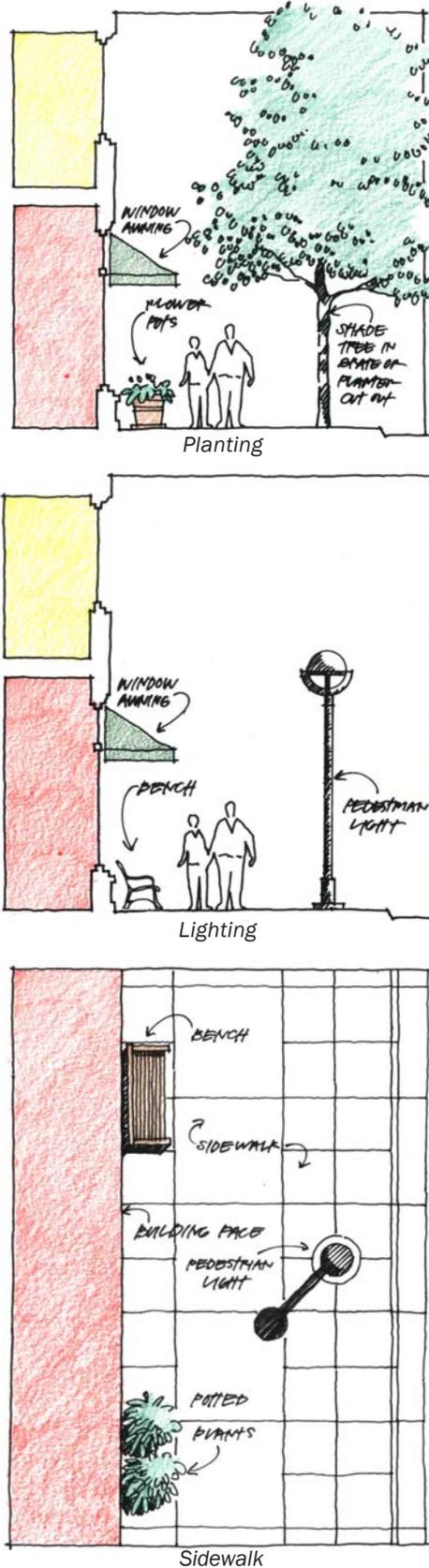


Figure 39
Streetscape - Promenade Retail Street

Promenade Residential Street

Just as the Promenade encourages a more compact and lively retail environment, the residential uses should also strongly reinforce the street. Condominiums and townhomes should provide entries and windows onto the street. Front stoops and porches can help create greater privacy for homes on the first level. A 15' setback is recommended to accommodate these elements. Sidewalks should be detached from the curb and shade trees should be planted in tree lawns to bring more greenery to the street. Specific streetscape recommendations (Figure 40) include the following:

- Minimum 15' Public Right-of-Way
- 15' Setback for stoops and front porches
- Detached concrete sidewalks
- Shade trees planted in tree lawns
- Pedestrian-scaled street lamps
- Benches
- Transit shelters and furnishings
- Bike Parking
- On-street parking

Typical Mixed-Use/Commercial Street with 15' Setback

Future mixed-use and commercial development along 70th Street and York Avenue is encouraged to build closer to the street yet allow enough setback to provide green space and patios between the street and the building front. A 15' setback is recommended to accommodate these elements. Sidewalks should be detached from the curb and shade trees should be planted in tree lawns to bring more greenery to the street. Specific streetscape recommendations (Figure 41) include the following:

- Minimum 15' Public Right-of-Way
- 15' Setback for plantings and patios
- Detached concrete sidewalks
- Shade trees planted in tree lawns
- Street lamps
- Bike Parking

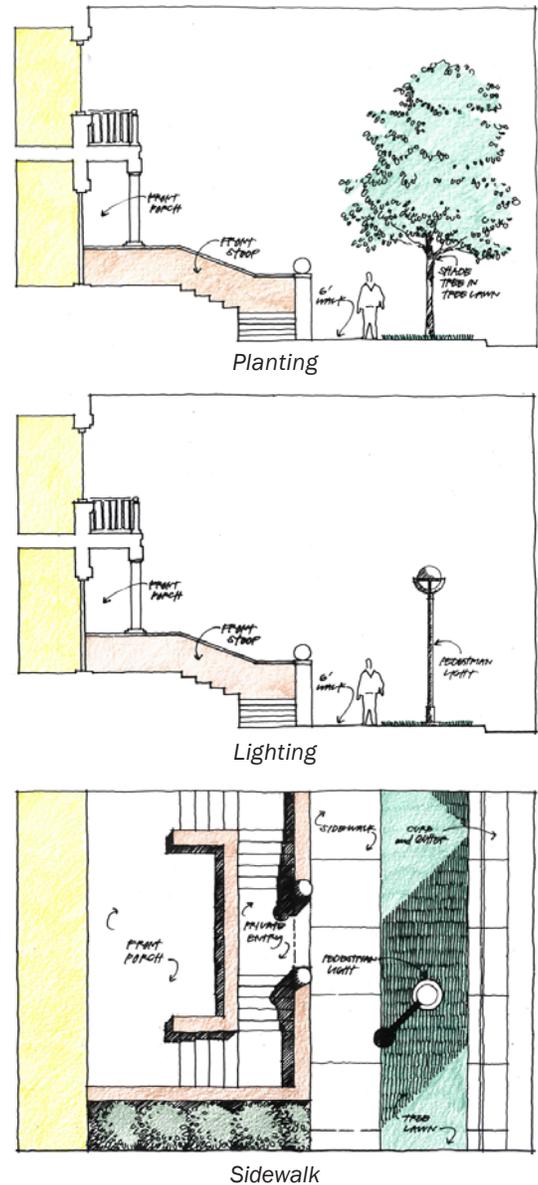


Figure 40
Streetscape - Promenade Residential Street

Typical Residential or Commercial Street with Minimum 30' Setback

Existing residential and commercial development patterns along portions of York and France Avenues indicate buildings set back beyond 30 feet from the public right-of-way. This is a pattern that may continue as the area develops due to heavy traffic on both streets. Sidewalks should be detached from the curb to provide pedestrians with greater distance and safety from moving traffic. Shade trees should be planted in tree lawns to bring more greenery to the street. Street lights should become part of a streetscape kit-of-parts that lends greater identity to the district. Specific streetscape recommendations (Figure 42) include the following:

- Minimum 15' Public Right-of-Way
- 30' Setback for plantings and patios
- Detached concrete sidewalks
- Shade trees planted in tree lawns
- Street lamps
- Transit shelters and furnishings

Typical Streetscape Adjacent to Surface Parking Lot

Existing conditions along France and York Avenues includes a condition where surface parking lots separate buildings from the streetscape. To minimize the negative visual impacts of this condition, it is recommended that a minimum 15' setback is utilized for landscape buffering and screening of parked cars. The public right-of-way should be treated like other parts of either Avenue, with detached walkways and planted tree lawns. Specific streetscape recommendations (Figure 43) include the following:

- Minimum 15' Public Right-of-Way
- Minimum 15' landscape setback for buffering and screening
- Detached concrete sidewalks
- Shade trees planted in tree lawns
- Street lamps
- Transit shelters and furnishings

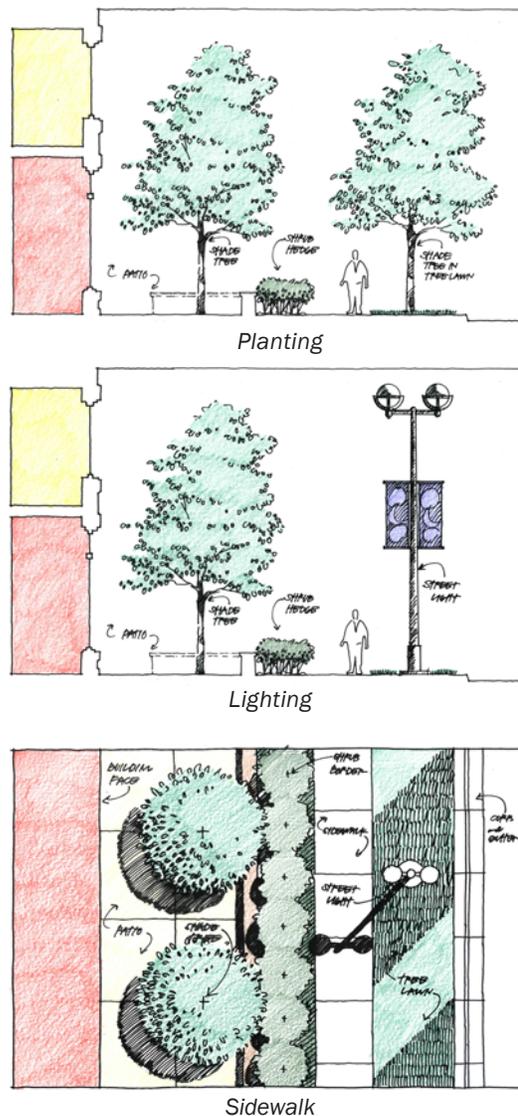
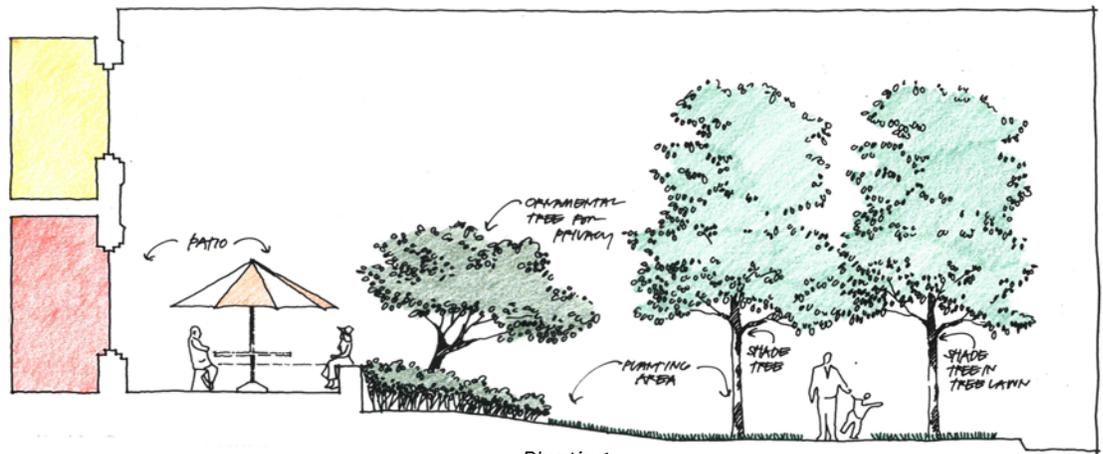
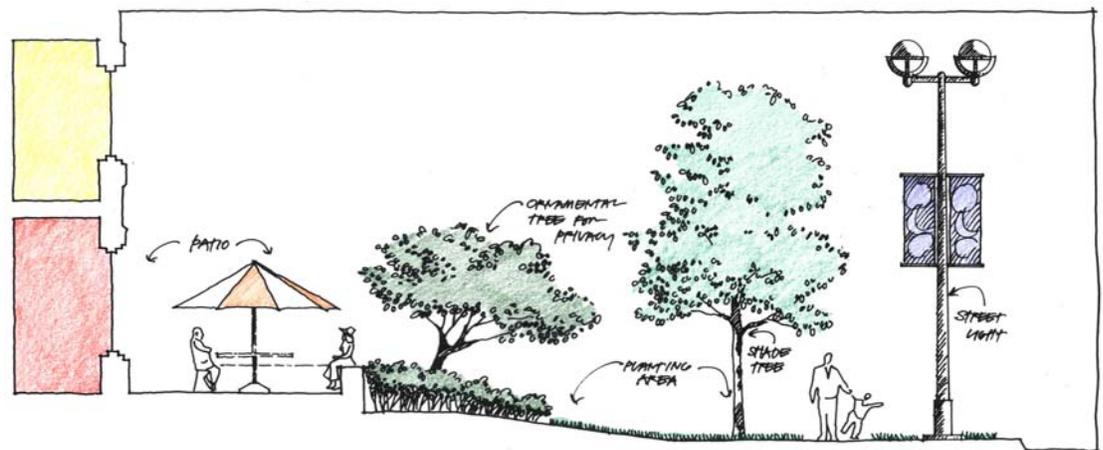


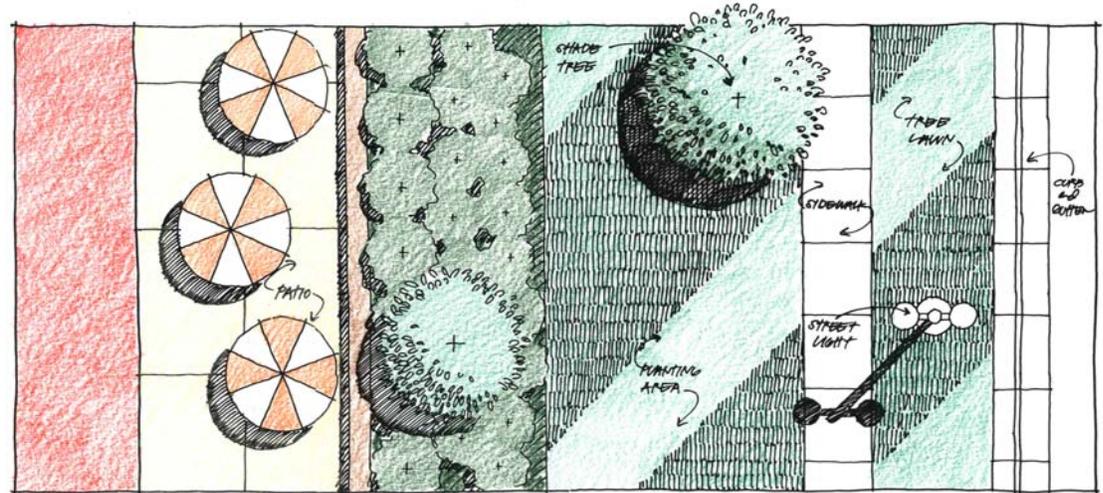
Figure 41
Streetscape - Typical Mixed-Use/Commercial Street with 15' Setback



Planting

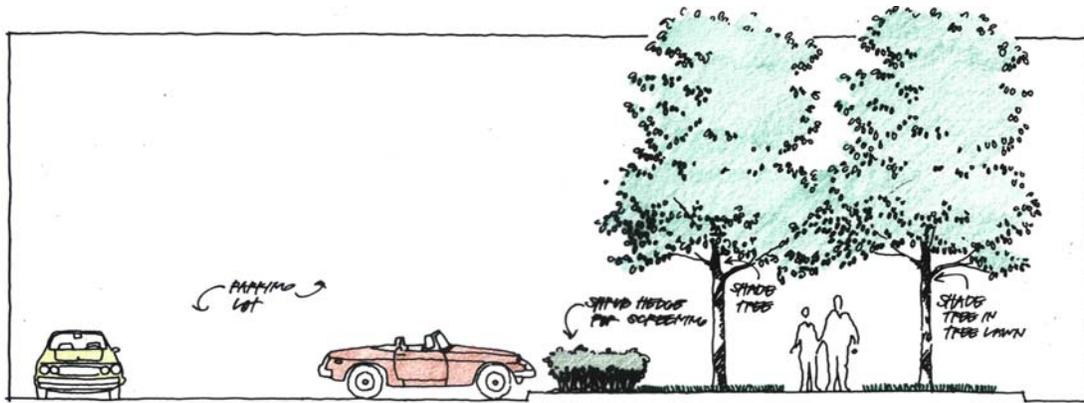


Lighting

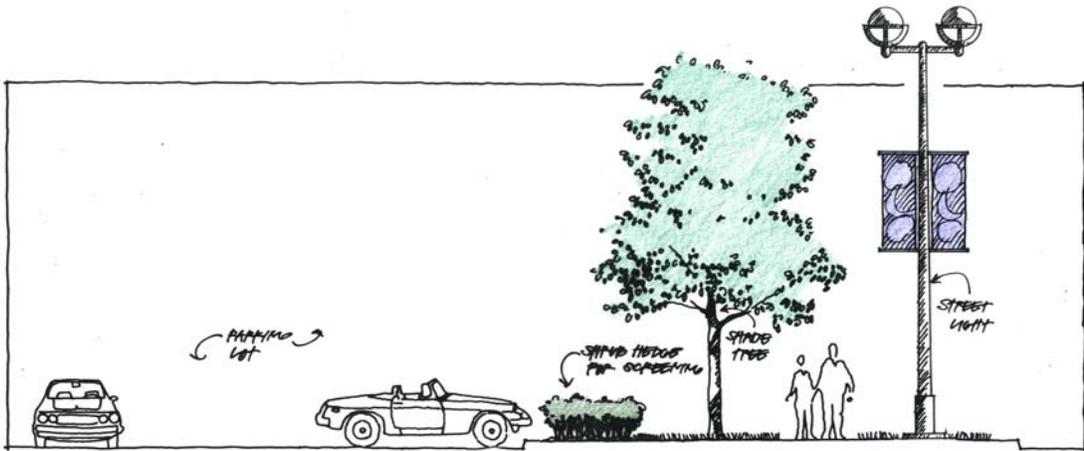


Sidewalk

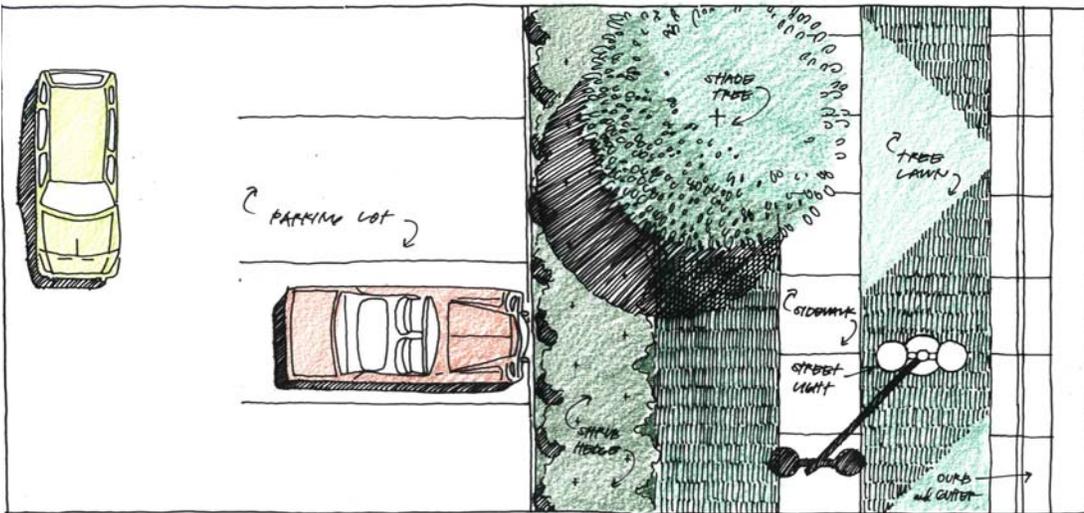
Figure 42
Streetscape - Typical Residential or Commercial Street with Minimum 30' Setback



Planting



Lighting



Sidewalk

Figure 43
Streetscape - Typical Streetscape Adjacent to Surface Parking Lot

Transit System

The transit system proposed to support the Southdale area has three components - regional, circulator and shuttle - each of which is designed to serve different segments of the travel market in the Southdale area and to integrate into a whole. Figure 44 illustrates the general relationship of the three system elements.

The regional element of the system is designed to address the movement of persons to and from the Southdale area. This part of the system incorporates much of the existing transit service that is focused on the transit center at Southdale and along the 78th Street corridor at the southern end of the Greater Southdale area. As BRT or similar higher capacity service develops on I-494, the regional element introduces a transit hub/station stop at the south end of the study area to link to that service. This regional service connects the Southdale area to other regional centers and provides backbone service all day that is augmented with peak period express service.

The shuttle element is the key component of the Southdale transit system and is designed to provide high frequency (five to ten minutes between vehicles) service to move persons within the Southdale area. The shuttle transit service would operate with frequent stops along the central Spine Street. The shuttle would link the regional elements at the north and south ends of the Southdale area. The function of the shuttle is to support development along the Spine Street. As noted previously, almost all of the study area is within a five minute walk of the shuttle alignment. The level of internal tripmaking among future uses in the overall study area is projected to be in excess of 4,000 trips in the peak hour. While not all of these trips would use a shuttle, they do represent a potential ridership market for the service and are indicative of a daily level of internal tripmaking that would support a transit system of the type described. When the projected mix of uses and the relative location of uses is taken into consideration, the shuttle route is proximate to about half of the internal trips that are made within the study area.

The circulator element of the transit system is designed to provide for movement of persons

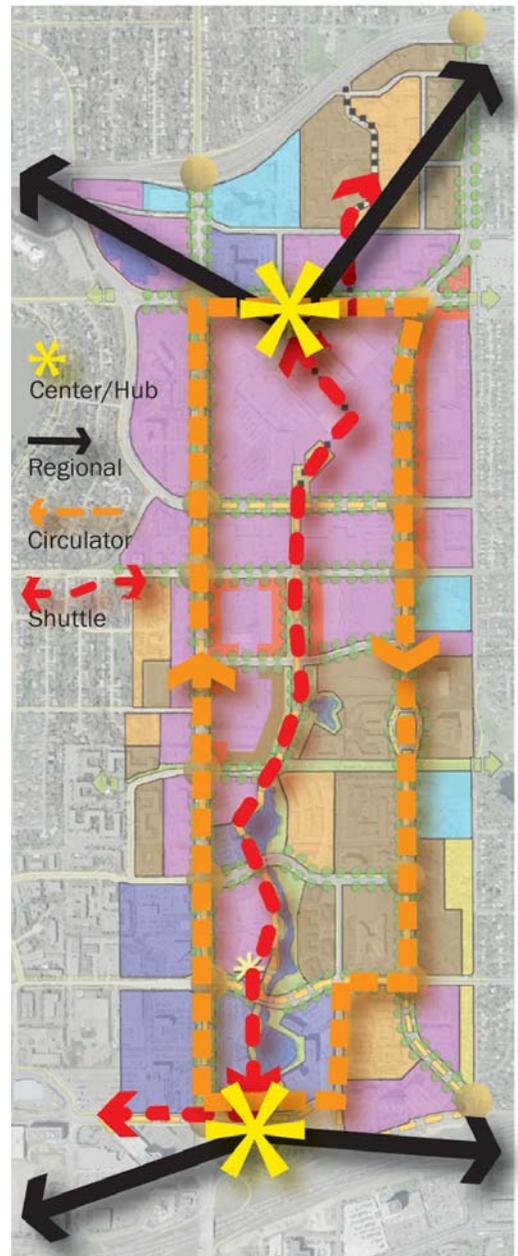


Figure 44
Elements of Transit System

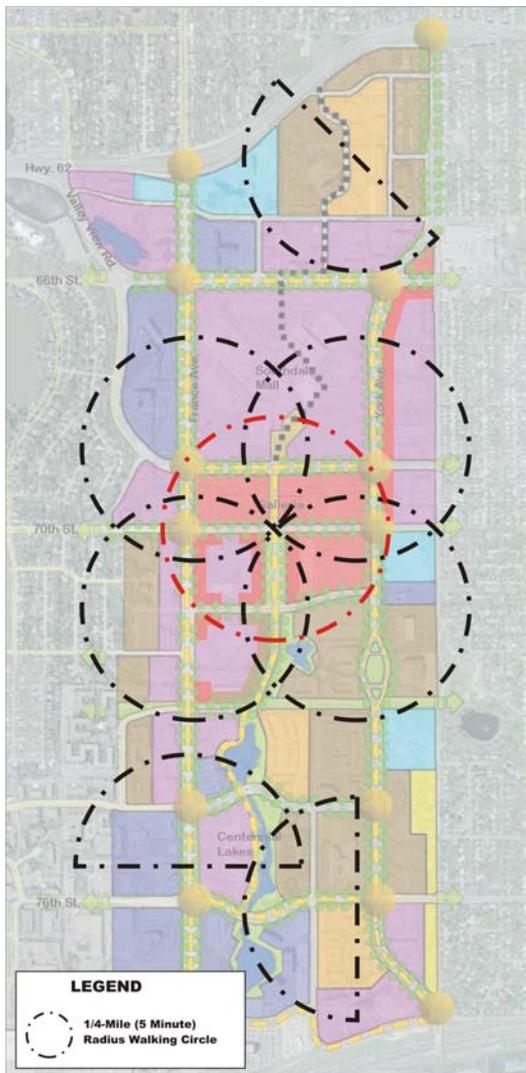


Figure 45
Pedestrian Catchment Areas

within the Southdale area that are not within walking distance of the regional or shuttle elements. Because of the large scale of the study area, not all of the residential and employment areas will be within walking distance of each other or the retail/commercial areas. (Figure 45 illustrates the pedestrian catchment areas for the proposed land use pattern.) Trips between residential and employment areas along the periphery of the Southdale area (i.e., those not adjacent to the central Spine Street) will be beyond walking distance. The circulator element is designed to connect these areas and effectively extend the reach of walking trips at the periphery of the Southdale area. Alternatively, the circulator service could be provided by the “tails” of the regional routes that access the Southdale transit center, similar to the way that service occurs today.

The preferred routes of the transit system elements are generally illustrated in Figure 44 and would consist of three sets of routes. The regional routes would be expected to operate on the freeways and other regional roadways connecting to the Southdale area and would serve transit centers or stops at the north and south ends of the Southdale area. The shuttle route would be a linear north-south service on the central Promenade (essentially along the

existing transit easement). The circulator route would function best along York Avenue, 66th Street, France Avenue, and 78th Street. The circulator may more effectively operate as a combination of fixed route service overlaid by point-to-point service along this general route.

These routes require related improvements to the infrastructure in the Southdale area. The Promenade would be constructed, either as a street for mixed traffic or as a pedestrian/transitway (the width of which would depend upon the transit technology used). For the circulator to operate on the western side of the study area, a system of transit priority would be needed on France Avenue to manage traffic delays and allow transit to function effectively. Transit centers or station stops would be needed for the regional elements at the north and south ends of the Area. The location of the existing transit center at Southdale Center would need to move north to be proximate to 66th Street and a new facility would be needed near 78th Street to serve higher capacity service proposed in the I-494 corridor. These centers/station stops would be areas where parking structures could be beneficial to support park and ride functions.





Boulder



Orlando



Although the regional elements would continue to operate with the big bus technology that is used by the region's transit providers, several transit technology options are available for the shuttle and circulator elements of the transit system. Both the shuttle and circulator could be operated with rubber-tired coaches of varying sizes that would function on a conventional street environment. However, the shuttle element of the system has requirements for more frequent boarding that would dictate multiple boarding doors and a low floor on the vehicle, which may require specialty coaches if a rubber-tired option is chosen. Clearances under the Galleria may limit the size of vehicles on the shuttle route. The circulator would be expected to have wider stop spacing and would be able to use conventional small coaches. Route branding through unique paint and/or vehicle naming schemes would reinforce the presence of the different routes. Both Boulder, CO (Hop/Skip/Jump) and Orlando, FL (Lymmo) have had success with this type of branding.

The shuttle element may be better served by modern streetcar technology of the type used in the Portland, OR and Tacoma, WA systems and scheduled for the Seattle, WA system. The modern streetcar uses a European car that is roughly 60 feet long and 8 feet wide and operates on railroad tracks. In both Portland and Tacoma, the streetcar operates on-street in mixed traffic with boarding at curbside station stops. Portions of the Portland system operate in off-street plazas/pedestrian areas.

The key to the service frequency on the modern streetcar systems is limiting the length of the system to 1 to 3 miles, which is consistent with the length of the shuttle element in the Southdale area. The proposed alignment of greenway/mixed traffic streets in the Southdale area that the shuttle would operate on is also consistent with the technology.

Stop spacing is a maximum of ¼ mile and as short as every block. The streetcar technology provides for frequent service with fast boarding. With two stops per block, the estimated round trip running time is approximately 23 minutes. The round trip time increases to about 27 minutes with stops 300 feet apart.

A detailed costs analysis of the shuttle system was beyond the scope of this study. The following information provides a preliminary indication of the costs associated with this transit system.

- Construction costs for the modern street are about \$50 million per mile inclusive of trackway, station stops, overhead power, signaling and cars. Any technology other than bus would require new maintenance and storage facilities to be sited in the study area.
- With 12 to 15 minute headways, the shuttle system would need two vehicles in operation with one spare. A reasonable estimate of total vehicle expense is \$240,000.
- One possible scenario would operate the system for 18 hours on weekdays and 12 hours on weekends and holidays. With a consistent 15 minute headway, the operating cost of the system would be approximately \$530,000 per year. A increase in weekend headway from 15 to 30 minutes reduces estimated annual expense to \$468,000.

One of the main functions of the modern streetcar in both Portland and Tacoma is to encourage economic development along the route (both are set up to recapture the increase in value

from such development through a tax financing district). This function is compatible and desirable along the shuttle route in the Southdale area, but less so with the circulator route, which serves residential and employment uses on the periphery of the area. This may make the streetcar technology less attractive for the circulator even though it would function well for the circulator.

A third technology that could function for the shuttle and/or circulator is personal rapid transit or PRT. This technology is designed to provide point-to-point service on an elevated trackway. The Southdale area is a sufficiently dense activity center of limited geographic scale that PRT technology could serve the area. PRT technology is still at the prototype stage. The elevated system would require elevated station areas, both for boarding and for PRT car storage, which would introduce substantial vertical elements into the urban design aspects of the street/greenway system in the Southdale area.

Transit improvements are important to the Southdale area to provide for pedestrian connectivity among the land uses in the area. While nodes of activity within the Southdale area can be designed to encourage walking, the distances between nodes is sufficiently large that people will drive rather than walk to reach them. The shuttle and circulator elements of the transit system are designed to extend the reach of an individual walking trip, either by allowing a visitor to drive into the Southdale area, park once and visit multiple destinations or by allowing a Southdale area resident to reach employment or shopping destinations without driving. This concept of extending a walking trip is an important aspect of how the shuttle element supports economic development along its route with frequent service - as people recognize the dependability of the service, they are willing to incorporate it into their activity patterns and ultimately end up spending more time collectively at establishments along the route.

Several steps are necessary to achieve the transit vision articulated above. One involves developing a more detailed technical study to establish the system technologies, routes, operating patterns, costs and benefits. A second step must focus on how to fund the transit service and what actions are needed to implement funding mechanism allowed by the existing enabling legislation. These actions differ between the three system elements.

The regional element relies upon available regional funding for transit, which is dependent upon the actions of Metro Transit, the Opt-out transit providers like Southwest Metro, the Metropolitan Council, the State Legislature, and the Federal Transit Administration. The regional element described above is intended to reinforce existing regional transit planning that is already focusing on the Southdale area and on the regional corridors on TH 62 and I-494. The transit centers included in the regional element are eligible for federal funding, as are park and ride components at the centers. Siting of the centers and capitalizing on air-rights at these locations is something that the City can bring to the regional discussion. Similarly, the City can leverage investment in the shuttle and circulator elements (see below) to reinforce decisions about where to site the regional transit centers and how they link into the development pattern. This level of discussion can influence how regional routes that pass through or adjacent to the Southdale area can be reorganized to best serve the new centers.

The shuttle element lies within the area designated by the State Legislature for the City of Edina to operate a transit system. Within this area, the City has an easement and the ability

to levy for funds for the transit system. The circulator element is only partially within the area already established. The City should initiate an evaluation of a Special Services District in the Southdale area to determine what the boundaries of the district need to be to support the proposed transit elements and what the funding capacity of the district can be. This work will need to integrate with the technical study noted above to determine if the level of funding needed for operating the shuttle and circulator elements is within the capacity of the district or if other sources of capital and/or operating funds will be necessary. It is important to note that Portland and Tacoma assembled funds from multiple non-federal sources to construct and operate their streetcar systems. Both of these systems can serve as examples for funding programs for the Southdale area.

The existing legislation that established the transit area for Edina precludes the use of rail-based transit. If a rail-based technology is selected, the enabling legislation would need to be modified. Although not currently in place, legislation at the federal level is moving toward establishing a “small starts” program that would fund systems like that envisioned for the Southdale area. Should that funding become available, the Southdale area would need to compete for it with other parts of the country. Similarly, depending upon the type of transit technology selected, the system in the Southdale area could be a candidate for a federal demonstration project if the technology or the application of an existing technology is sufficiently innovative.

Travel Demand Management Strategy

Incorporating a strategy for travel demand management (TDM) into the plan for the Greater Southdale area will enhance the ability of the infrastructure in the study area to accommodate future development. TDM programs, whether employer or district based, have been shown to have the potential to reduce vehicle trip making by both employees and residents in the study area. Also, use of TDM as a congestion management measure enhances the market for both regional and local transit services in the study area. The effectiveness of TDM activities will be strongly influenced by the approach used to implement them and the ability to implement additional transit service in the study area. The effectiveness of the TDM elements is also affected by how early in the development process interaction occurs between the City of Edina, other agencies and project sponsors.

The recommended strategy for the Greater Southdale Area is to implement TDM on a district basis and link the TDM components to the benefit district that the City is empowered to create for the transit system. Elements to be included would be development of a district TDM coordinator position, either via the benefit district, as city staff, or via use of the existing 494 Commuter Services organization. The function of the District/Coordinator would be to facilitate TDM strategies in the Greater Southdale Area, which would include:

- Increasing ridesharing activities via carpooling and vanpooling
- Increasing the use of public transportation and alternate modes/choices (walking, bicycling, telecommuting, staggered/flex work hours, compressed work weeks)

Establishing a district would provide for a focused return on investments related to TDM. The coordinator position provides a central clearinghouse function for TDM that provides for

better outreach and accordingly, better implementation of TDM by employers. While a TDM strategy for the study area could be entirely voluntary, the City could facilitate the strategy by requiring TDM activities for new development, either as a policy or through implementation of an ordinance that requires TDM for projects above a threshold size.

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Achieving the vision for the Greater Southdale Area will be a balancing act between land use and transportation objectives. In perfect world, the necessary transportation system is in place before development occurs. This approach does not fit the reality of the Greater Southdale Area.

Private investment is attracted to the Greater Southdale Area and seeks to use land with greater intensity. This new development adds trips to the local street system and increases existing congestion. Some enhancements to the street system have been identified (i.e. – France/Crosstown interchange), while others (West 70th Street) require further investigation. The combined requirements for planning, design and funding place these enhancements years into the future. If transportation issues become an unduly restrictive limit on development, Edina faces the risks of missed opportunities and discouraging reinvestment in the Greater Southdale Area. These factors make land use and transportation inextricably linked for the future of the Greater Southdale Area.

Actions that should be taken to address the issues raised in this Study and to achieve the vision include:

- Development agreements for all projects should include provisions for supporting the establishment of a special service district to build and operate the transit system.
- Use development projects to achieve the long-term transportation objectives for the Area, including sidewalks, trails, and pedestrian street crossings.
- Incorporate streetscape improvements into all new development projects.
- Determine if the City is willing to use tax abatement as a tool to finance public initiatives in the Greater Southdale Area. If so, then determine if any actions are needed to preserve capacity until needed. For example, an abatement levy could be collected from Cypress Equities project, but details of the application are not yet known.
- Determine if Hennepin County is willing to use tax abatement as a tool to support transit system or any other initiatives identified in the Study.
- Use development projects to establish right-of-way needed to construct the Edina Promenade and internal transit corridor.
- Work with Target site should ensure that development on this site promotes the objectives of this Study.
- Work with Mills Corporation to preserve a site for the transit hub on the north portion of the Shopping Center campus.
- Work with Mills Corporation to define the appropriate corridor and relationship for the internal transit system on Shopping Center campus.
- Determine if "land bridge" concept for France Avenue/70th Street merits consideration. If so, immediately undertake conceptual design to provide information needed to coordinate roadway with adjacent development plans. Work probably requires corridor study of 70th Street.

In addition to these actions, there are a series of other steps for moving ahead in addressing the land use and transportation needs of the Greater Southdale Area.

Financing the Promenade

One of the most important public initiatives described in this Study is the Edina Promenade. Preliminary estimates suggest that the cost of street and streetscape improvements would be \$1,630 per linear foot (see Figure 46). Assuming that the Promenade spans an 1,800 foot distance, the cost of construction totals \$2,934,000. These improvements would require a bond issue of approximately \$3,200,000 (including finance expense and capitalized interest). Annual debt service could range from \$235,000 to \$400,000, depending on term and interest rates. These figures provide the City with an indication of the revenues from tax abatement, special service district levy, and/or special assessment that would be needed to build the Promenade.

Pavement Cost							
	Length	Width	Depth(ft)	Q(cf)	Q(cm)	Unit Cost	COST/LF
AC(B)	5,280	19	0.3	30,000	*1,425/tn	90/tn	\$ 24
AB	5,280	19	1	100,000	2,857	55	30
AS	5,280	19	0.67	67,000	1,914	35	
Total Pavement Cost							
Lanes	2						
Concrete Curb/Gutter							
B-2	5,280	2			10,560	20	
(Bothsides)							
Behind Curb Streetscape							
Landscaping, sidewalk, street lighting, furnishings and irrigation (Both Sides)							900
Signs and Striping.							
LS					Varies from 15,000 to 25,000		4
Drainage							
Using 30% of roadway items.							323
25% Contingency/escalator							
							350
TOTAL							\$ 1,630

Figure 46
Preliminary Cost Estimates for Promenade

Sewer Infrastructure

Moving ahead with the land use strategies identified in this study will result in increased densities and accordingly, the need for additional sanitary sewer capacity. This issue impacts the City of Edina as well as the neighboring communities of Richfield and Bloomington. To date, short-term solutions to sewer capacity issues such as retention of effluent during peak hours have accompanied large redevelopment proposals such as the Best Buy corporate headquarters in Richfield. Continued redevelopment in the Southdale Area as well as redevelopment in each of these adjacent communities will require a permanent, long-term solution to

sewer capacity issues involving both regional and local authorities.

The future land use information identified in Appendix B can be used as one example of future growth for the purposes of conducting additional sewer capacity analyses. This scenario, however, does not represent full build-out for the Southdale Area nor does it necessarily represent the most realistically intensive scenario possible.

The request for additional sanitary sewer capacity is formally addressed through the comprehensive planning review process with the Metropolitan Council. The sewer capacity issue will need to be addressed either as a comprehensive plan amendment for the Southdale Area is prepared or as part of the complete comprehensive plan update that is mandated by the 2008 deadline. City staff from Edina and the surrounding communities should meet with representatives of the Metropolitan Council's Environmental Services staff to further investigate both short-term and long-term solutions to sewer capacity issues.

Comprehensive Plan and Zoning

The evolution and change of the Greater Southdale Area over the next 10 to 20 years will be guided and regulated by the Edina's Comprehensive Plan and Zoning Ordinance. Both of these documents need to be modified to reflect the strategies and directions offered in this report. The following is an overview of the planning and zoning implications of the Study.

Comprehensive Plan

By 2008, the city of Edina like all Twin Cities metro area communities will need to complete an update of its Comprehensive Plan. The land use component of the Greater Southdale Area Land Use and Transportation Study doesn't represent a radical departure from the current land use designations, however, it does differ in a number of areas. The Greater Southdale Area Land use and Transportation Study is expected to define future land use for the Southdale Area. Accordingly, the land use component of the upcoming Comprehensive Plan update should include the Future Land Use Plan contained within this document.

Zoning

Zoning regulations found in City Code Section 850 control land use. The Current Zoning map (Figure 47) shows the current zoning designation for parcels in the Study Area. The following list provides a description of the principal uses allowed in each zoning district found in the Study Area.

- Single Dwelling Unit District (R-1). Buildings containing not more than one dwelling unit. Publicly owned parks, playgrounds and athletic facilities. Publicly and privately owned golf courses, but not including driving ranges or miniature golf courses as a principal use.
- Double Dwelling Unit District (R-2). Buildings containing two dwelling units.
- Planned Residential District - 3 (PRD-3). All residential buildings. Also day care facilities licensed by the State.
- Planned Residential District - 4 (PRD-4). All residential buildings. Also day care facilities licensed by the State.

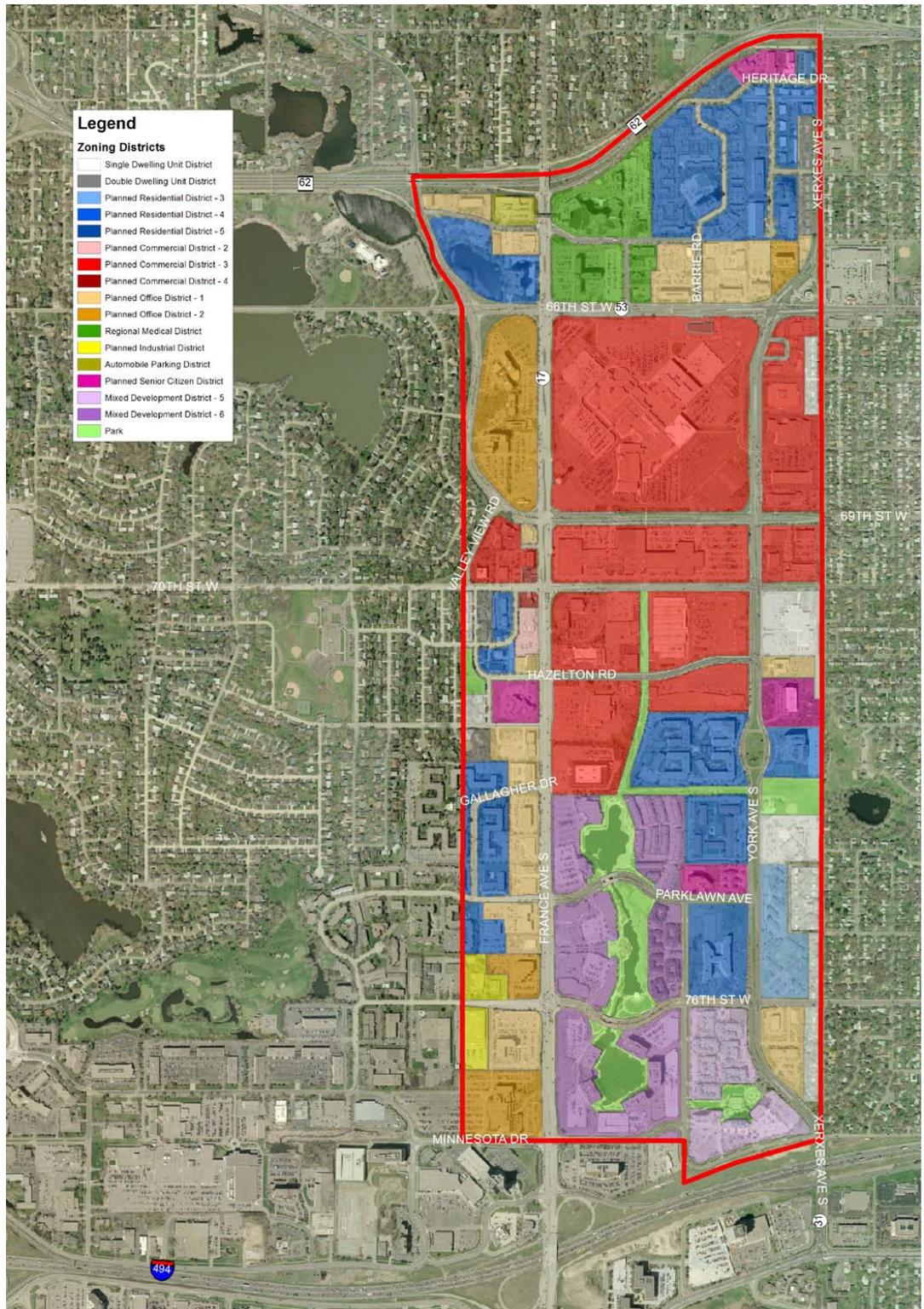


Figure 47
Current Zoning

-
- Planned Residential District - 5 (PRD-5). Rest homes, convalescent homes and nursing homes.
 - Planned Senior Residential District - 4 (PDR-4). Buildings containing 4 or more dwelling units, all but one of which are senior citizen dwelling units.
 - Planned Commercial District - 2 (PCD-2). Commercial uses permitted in the PCD-1 district which include smaller, neighborhood convenience type businesses and offices generally less than 2 stories and with a floor area ratio under 1.0. Permitted uses in the PCD-2 district are allowed to be larger with a maximum building height the lesser of 4 stories or 50 feet and a floor area ration under 1.5. Some example uses include recreation establishments, animal hospitals and kennels, department stores under 40,000 square feet, furniture stores, hotels and motels, laboratories, post offices, theaters and funeral homes.
 - Planned Commercial District - 3 (PCD-3). General commercial uses permitted in the PCD-1 and PCD-2 districts as well as automobile agencies selling or displaying new, unused vehicles; boat or marine stores selling or displaying new, unused boats; department stores or shopping centers exceeding 40,000 square feet of gross floor area; and sexually oriented businesses.
 - Planned Commercial District - 4 (PCD-4). Automobile service centers. Car washes. Gas stations.
 - Planned Office District - 1 (POD-1). Business and professional offices. Financial institutions including drive-through facilities. Post offices. Clubs, lodge halls and non-profit organizations, excluding those providing beverage services in the building or lot. Medical and dental offices and clinics. Employment agencies. Travel bureaus. Day care. Public or private colleges, universities or schools. Facilities for athletic, health or weight control purposes.
 - Planned Office District - 2 (POD-2). Same as Planned Office District - 1. Difference between districts is that POD-1 is limited in height to the lesser of 4 stories or 50 feet, whereas POD-2 has no height maximum except as determined by setbacks.
 - Regional Medical District. Hospitals. Medical and dental offices and clinics. Laboratories performing medical or dental research, diagnostic testing, analytical or clinical work, having a direct relationship to the providing of health services. Laboratories engaged in the production or manufacturing of goods or products for commercial sale or distribution are not included.
 - Planned Industrial District. Principal uses allowed in the Planned Office District as well as manufacturing, processing, assembly, packaging and warehousing. Scientific research, investigation, testing or experimentation. Office equipment showrooms or repair services. Printing shops. Businesses that provide a service on the customer's property such as a contractor.
 - Automobile Parking District. Parking lots. Drive-through banking facilities.
 - Mixed Development District - 5 (MDD-5). Buildings containing not fewer than 10 dwelling units or senior citizen dwelling units. Publicly owned or operated civic, parking, cultural institutions, park or recreational facilities. Offices. Financial institu-

tions. Suite Hotels. Day Care.

- Mixed Development District - 6 (MDD-6). All principal uses in MDD-5 plus health and athletic clubs; department stores or shopping centers greater than 40,000 square feet; and uses in PCD-1 and PCD-2 subdistricts except animal hospitals and kennels, automotive accessory shops, clubs and lodge halls, exterminating offices, undertaking and funeral home establishments.

A substantial portion of the Study Area is either zoned in the Planned Commercial category or the Mixed Development category. Both of these categories include a wide range of permitted, accessory and conditional uses, most of which fit the long term direction of the plan and some that are inconsistent.

After approval of the Greater Southdale Area Land Use and Transportation Study, a detailed comparison of the current Zoning Code and the land use direction provided in the Study needs to be accomplished. Two specific areas need to be addressed. First, a careful review of allowable uses in each zone needs to be completed. In some cases, the existing list of uses is either too broad or doesn't adequately address high density residential uses as a permitted use. For example, in the Planned Commercial District - 3 (PCD-3), drive-through facilities are allowed as accessory uses. Drive-through uses would be inconsistent with the intent of the Central Spine District although they could continue in other geographic districts of the study area.

In a similar way, the southeast quadrant of the Central Spine District is slated for future high density housing. The site containing the existing Yorktown Fashion Mall which is located in this area could only contain housing as a conditional use over a principal commercial use. In order to allow only housing in this area, the subject land would either need to be rezoned or housing would need to be added as a permitted use in the zone. The former approach in this case would likely be the most effective since it would not broadly open the PCD-3 district to housing as a principal use.

The second area of the code that should be reviewed concerns height limitations. The Southdale Area will continue to see proposals for added building height because of high property values. At the present time, the Zoning Code limits height in the PCD-3 district through building setbacks. Setbacks are generally required to be either 50 feet or the height of the building, whichever is greater. These setback limitations are certain to result in variance requests for taller buildings. An examination of the relationship between allowable building height and stipulated setbacks needs to be conducted to reduce or eliminate standards that create variances in areas where added building height is deemed appropriate.

Maximum floor area ratios (FAR) for various zoning classifications should also be reviewed. FARs for uses in the Planned Commercial District, Planned Office District, Planned Residential District, Planned Senior Residential District and Planned Medical District generally range from .3 to 1.5. It is likely that these FARs will need to be adjusted upward in some cases to accommodate a scale of development appropriate for the Southdale Area.

Financial Tool Box

Previous public development initiatives in the Greater Southdale Area were funded with tax increment financing (TIF). This tool allowed the City to capture the property tax revenues from new development and use these monies to pay for public improvements and development activities. Tax increment financing is not likely to play a role in facilitating public objectives in the Area. At this point, the elements needed to establish a TIF district (i.e. - structurally substandard buildings) do not appear to be present at key development locations.

The inability to use TIF does not mean that Edina lacks the financial resources. Special service districts and tax abatement will be keys to making the public investments identified in this study. This section discusses the applications and issues for the use of the finance tools in the Greater Southdale Area

Special Service District

A special service district has the capacity to finance any of the public improvements planned for the Greater Southdale Area. In simplest terms, a special service district is a special taxing district. It allows a city to collect money to support services and improvements in commercial areas.

Statutory Authority

Edina has the unique ability to draw on two forms of special service district authority. The City can use the general statutory authority in Minnesota Statutes, Chapter 428A. Edina has also received special legislation for a special service district related to a transit system.

The special legislation for a transit system (Minnesota Law 1989, Chapter 241) special service district in Edina contains several important stipulations:

- The transit system cannot operate on fixed rails.
- The system can operate in an area bound by the Crosstown/CSAH 62 (north), Highway 100 (west), and the city limits (south and east).
- In addition to the acquisition, construction, maintenance and operation of a public transit system, this special service district can be used to provide "any other service provided by the city authorized by any law."
- The legislation can be used to establish only one district. This authority does not preclude the creation of other special service districts under general state law.
- The service charge may be imposed on the property and improvements of Fairview Southdale Hospital.

All other aspects of the transit system special service district are governed by Chapter 428A.

Potential Uses

The general statutory authority contains few limits on the potential uses of special service districts. The nature of the improvements that can be funded with a special service district are not defined (or limited) by statute. The special service district cannot be used to finance services that the City provides through the general fund throughout Edina, unless the services are provided at a higher level. The statute does not impose any other limitation on the

nature of services.

Under the combined special service district authority, the potential applications in the Greater Southdale Area include:

- Construction and operation of the internal transit system.
- Construction of the spine street.
- Construction and maintenance of public amenities, including streetscape and public open space.
- Construction of sidewalks, trails, bridges and other improvements to enhance pedestrian movement.
- Construction and maintenance of public parking facilities.

Other services and improvements can be undertaken and financed by a special service district if authorized by the enabling ordinance.

Process

The process of creating special service district and authorizing the collection of service charges is different from other public finance tools. The use of a special service district requires a collaboration of property owners and the city. The process to create a district and to levy taxes must be initiated by petition of property owners and is subject to owner veto.

There are two separate steps in the process: (1) adoption of an ordinance establishing the service district and (2) adoption of a resolution imposing the service charges. Neither step can be initiated by the City.

The City must receive a petition to undertake the processes to create the special service district and to impose service charges. At a minimum, the petition must be signed by owners representing 25% of the area that would be included in the district and 25% of the tax capacity subject to the service charge.

The actions of the City Council to adopt the ordinance and the resolution are subject to veto by property owners. To veto the ordinance or the resolution, objections must be filed with the City Clerk within 45 days of initial City Council action to approve. The objections must exceed 35% of area, tax capacity, or individual/business organizations in the proposed district. The specific veto requirements depend on the nature of the service charge.

Service Charge

Although the special service district functions like a taxing district, the revenue source is called a “service charge”. The service charge may be imposed on any basis that is acceptable to the City and the property owners. A service charge can function like a property tax, based on tax capacity value of the parcel. Other common means of allocating costs include lot frontage and lot area. This flexibility is an asset in creating funding plans that spread the costs of improvements and services over a broad area.

The service charge applies solely to non-residential property. State law limits the application of a service charge only to property that is classified for property taxation and used for commercial, industrial, or public utility purposes, or is vacant land zoned or designated on a

land use plan for commercial or industrial use. Other types of property may be part of the service district, but may not be subject to the service charge.

Bonds

The special service district statute also includes the ability to issue bonds to finance improvement costs. The bonds must be supported "primarily" from revenues produced by the service charge. They may also use special assessments and any other legally available nontax revenue to pay debt service. This provision suggests that if nontax revenues are the "primary" source of revenue to pay debt service, then a general property tax may be applied. This approach is similar to the application of property taxes to 429 improvement bonds when less than 100% of the costs are assessed. The bonds may be general obligations. The bonds may be issued without an election and are not subject to any debt limit.

Challenge of Mixed Use Development

The inability to create mixed use special service district poses a challenge for creating an equitable funding plan. Some existing housing in the Greater Southdale Area will benefit from the ability to use an internal transit system. As evidenced by the plans for redevelopment of the Centennial Lakes Theater site, future development is likely to be a mixture of housing and commercial uses. Neither the special nor general forms of special service district allow service charges to be levied against residential property.

There have been some discussions about revising the special service district statutes to allow for residential property to petition to be included in the district and subject to the service charge. This change would enhance the use of this tool in the Greater Southdale Area. To date, one group has been willing to carry this legislation.

A hybrid option is to use both a special service district and a housing improvement area. The housing improvement area is a similar form of special taxing district. Housing improvement areas apply only to owned housing. None of the existing special taxing district alternatives include rental housing.

The housing improvement area statute contains broad language about the nature of eligible improvements. Housing improvements are defined by the city's enabling ordinance. This definition does not preclude any of the improvements called for in this study.

The application of the housing improvement area for services is less clear. Unlike the special service district, the housing improvement area statutes do not specifically authorize an area to pay for services rendered to housing units.

Using the Special Service District

Additional planning and analysis for the use of special service districts in the Greater Southdale Area should be performed in conjunction with work on the related public improvements. Specific steps for the use of special service districts include:

-
1. Determine the boundaries of the special service district for the transit system.
 2. Evaluate the ability to obtain special or general legislative authority for a mixed use special service district.
 3. Explore the need to use the special service district to finance other improvements and services in the Greater Southdale Area.
 4. Evaluate options for allocating the costs of improvements and services.
 5. Determine if the needs of the Greater Southdale Area can best be met through a single or multiple districts.

Tax Abatement

Although tax increment financing is not likely to play a role in funding improvements in the Greater Southdale Area, tax abatement offers a valuable replacement. The name "tax abatement" is misleading. No taxes are abated using this tool. In reality, tax abatement functions similar to TIF. Each taxing jurisdiction (city, county, school district) has the ability to levy a property tax equivalent to taxes paid by a parcel of property. The proceeds of this levy can be used to finance the public improvements proposed for the Greater Southdale Area. This section highlights the use of tax abatement.

Funding Capacity

Several factors influence the capacity of tax abatement to provide funding to undertake investments in the Greater Southdale Area.

Amount of Abatement

The use of tax abatement involves decisions to capture tax revenues from a series of redevelopment projects. The amount of the abatement depends on both the nature of the development and the approach to abatement. The options for abatement include:

- The abatement can be based on all or part of the valuation of a parcel.
- The abatement can apply solely to the new value produced by improvements. In this approach, tax abatement functions similar to tax increment financing.
- An abatement levy can account for existing property values. In tax increment financing, this value forms the base value of the TIF district and is not captured. Since no new value is created, this approach acts like a new property tax for any other purpose.
- An abatement levy could be applied to property valuation as it comes out of an existing TIF district. In practical terms, this approach extends the use of this valuation for community development purposes.

The abatement may last for a maximum of 20 years if not all taxing jurisdictions agree to grant an abatement. Property subject to tax abatement cannot be located in a TIF district.

City/County Collaboration

The use of tax abatement provides an opportunity for continued city and county collaboration in Greater Southdale Area. Each jurisdiction makes an independent decision on the use of tax abatement.

Maximum Abatement

The total tax abatement in any year may not exceed 10% of the current levy. Based on Edina's proposed levy for taxes payable 2006, the annual cap is \$2,082,000. This annual revenue equates to a funding capacity of approximately \$26,000,000 (20 years of debt payment at an average interest rate of 5.00%).

Bonds

It is likely that tax abatement revenues would be used to support bonds issued to finance public initiatives, such as the central spine street. Careful planning will be needed the type and structure of debt needed to make these investments.

Abatement Bonds

The statutes governing tax abatement authorize the issuance of tax abatement bonds. Two parts of this statute are particularly important in evaluating the use of these bonds:

- The maximum principal amount of these bonds may not exceed the estimated sum of the abatements for the property for the years authorized. This limitation may not fit the reality of use in the Greater Southdale Area. All abatements may not be known at the time of issuance. The result would be less debt capacity than needed.
- The statute limits the use of these bonds. The proceeds may be used to (1) pay for public improvements that benefit the property, (2) to acquire and convey land or other property, as provided under this section, (3) to reimburse the property owner for the cost of improvements made to the property, or (4) to pay the costs of issuance of the bonds.

Other Debt Options

Given the constraints of abatement bonds, the city may look for other options to finance needed investments. Improvement bonds and special service district bonds provide alternative tools for financing public improvements. Revenue from abatement levies would be used to pay debt service on these bonds.

Additional Investigations

From the beginning, the scope of the Greater Southdale Area Land Use and Transportation Study was limited to defining a general growth strategy for the project area. The growth strategy is intended to guide private investment and focus public investment in manner this is supportive of private initiatives. Accordingly, it was anticipated from its inception that this planning effort would result in a number of subsequent investigations and actions that would be necessary to position the City to direct and react to change in the Southdale Area.

The following additional investigations are recommended based on information developed during the completion of this study.

West 70th Street

From the west, West 70th Street is a primary entrance into the Southdale Area. Because of the interchange at TH 100, this route will continue to be a primary means of access. For most of its length, West 70th Street between TH 100 and France Avenue is a two lane road flanked by single-family land uses with numerous driveway cuts along the route. In its current configu-

ration, it poses significant limitations on traffic movements and it directly impacts the quality of life for residents along the corridor.

A follow-up effort is needed to examine West 70th Street and to define its future form and function. The follow-up effort should generally include an examination of alternative accesses to the Southdale Area from the west, reconfiguration of West 70th Street and substantial land use changes along the corridor.

France Avenue, North of the Crosstown

France Avenue, north of the Crosstown presents a similar situation to West 70th Street. From the north, this segment of France Avenue links the 50th and France commercial area with the Southdale Area. The width of the existing roadway is inadequate to comfortably accommodate four lanes of traffic. The adjacent land use pattern consists of single-family and two family residences with numerous driveways along the corridor.

A similar follow-up effort to that described above for West 70th Street needs to be completed for France Avenue. Traffic volumes and noise impacts have had an influence on property owner's willingness to maintain and improve their properties. Maintaining the northern portion of France Avenue as a functional gateway to the Southdale Area as well as maintaining/enhancing the appearance of the corridor is important to the greater Edina community.

Redevelopment of Pentagon Park – Connection along West 77th Street

Pentagon Park which is a predominately office development along TH 100 between 77th Street and I-494 has been discussed as a redevelopment site. Between Pentagon Park and the greater Southdale Area along 77th Street, a significant employment base exists with the potential for additional business infill and redevelopment.

Follow-up efforts need to consider the current future potential of this area for development and redevelopment in order to define a stronger link between the area and the greater Southdale area. Extension of the Southdale transit system to serve this area appears to be a logical connecting link but stronger visual and functional links may be possible through subsequent land use planning and design efforts.

Crosstown 62/France Avenue Interchange

The transportation sections of this report discuss the capacity and design restrictions posed by the existing Crosstown 62/France Avenue interchange. Follow-up efforts involving MN/DOT and Hennepin County need to be initiated to define a strategy for the design and funding of the reconstruction of the interchange.

Design Guidelines for the Central Spine District

The Initiative section of this Study provides an overview of the Central Spine District with general recommendations on streetscape, built form, parking and pedestrian circulation. A more detailed level of information will be needed for this area. It is suggested that the City initiate the assembly of design guidelines for the area addressing the public realm and private improvements. Regarding private improvements, the standards should address required setbacks, acceptable materials, appropriate building forms and mass and building height.

This appendix containing supporting information created during the study process. This information provides additional background on the current characteristics and future development of the Greater Southdale Area. The appendix is divided into four topic areas:

- Study Area Characteristics
- Community Demographics
- Market Conditions and Issues
- Transportation

Study Area Characteristics

School Districts

School districts receive the student population and tax base. The Study Area falls into both the Edina and Richfield School Districts. Figure A1 illustrates the boundaries of the school districts in the Area.

Year Structure Built

The City of Edina and Hennepin County have built geographic information systems (GIS). GIS contains information connected to property at the parcel level. Maps using GIS parcel data may create some odd looking locations where the site consists of a series of property parcels. For example, the block containing Southdale Shopping Center contains nine parcels.

One element of GIS is the year built for the current structure on each parcel. The map in Figure A2 illustrates the relative age of development in and around the Study Area.

Property Valuation

Estimated market value (EMV) is the amount set by the City Assessor's Department as the basis for property taxation. The maps in Figures A3-A5 show the EMV (taxes payable 2004) for commercial, single family residential and multiple family residential property. Grouping the parcels with similar classifications makes it easier to look for development patterns. Figure A6 compares property values in 1994 and 2004 for all parcels in the Study Area.



Figure A1
School District Boundaries

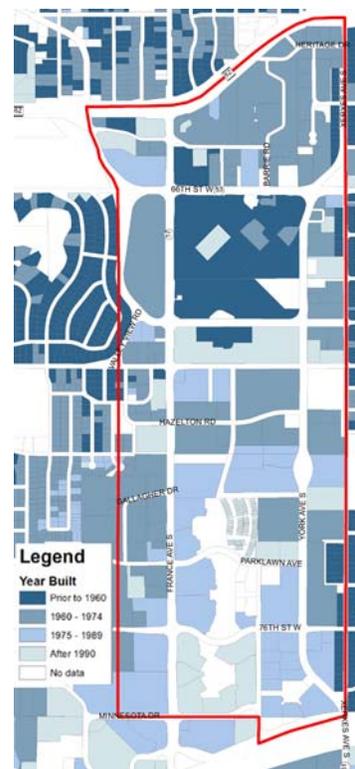


Figure A2
Year Structure Built



Figure A3
2004 Estimated Market Value - Commercial/
Industrial Property



Figure A4
2004 Estimated Market Value - Single Family



Figure A5
2004 Estimated Market Value - Multiple Family
Residential Property



Figure A6
Change in Estimated Market Value - 1994 to 2004



Figure A7
Population in Census Blocks

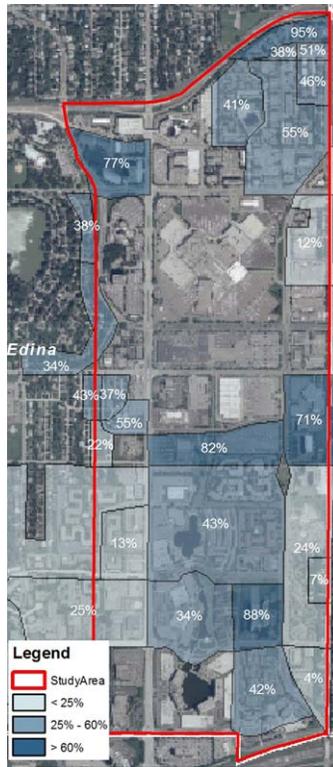


Figure A8
Percent of Population Age 65 and Older



Figure A9
Percent of Population Under Age 20

Census Block Data

A portion of the data from the 2000 Census is reported at the “block” level. The area included in each block is determined by the Census Bureau. The maps in this memorandum use data from each Census block that falls completely or partially within the Study Area. The following data sets are depicted in this memorandum.

- Population (Figure A7)
- Percent of population age 65 and older (Figure A7)
- Percent of population under age 20 (Figure A9)
- Housing units (Figure A10)
- Percent of housing units owner occupied (Figure A11)



Figure A10
Housing Units in Census Blocks



Figure A11
Percent of Housing Units Owner Occupied

Community Demographics

The nature of Edina influences land use and transportation in the Greater Southdale Area.

- Edina residents create demand for goods and services in the Area.
- Edina residents contribute to the employment pool for Area business.
- Edina residents influence the demand for housing.

This memorandum describes demographic characteristics of Edina using information from the U.S. Census. (Unless otherwise indicated, the information in this memorandum refers to the 2000 Census). In preparation for the Futures Workshop we would like you to think about how these characteristics will appear in the 2030 Census. How will Edina be different and the same? What do the changes suggest for the Southdale Area?

Age

Edina is an older community. The median age of the Edina residents is 44.5 years. This age is substantially higher than medians for Hennepin County (34.9), the Twin Cities region (34.4) and the entire state (35.4). The charts in Figures A12 and A13 compare the age distribution of Edina's population with the County and the region.

Edina's population grew by almost 3% from 1990 (46,070) to 2000 (47,425). The population age 45 and older increased by 2,306 while the 19/20 to 44 group lost 2,035 people (see Figure A14).

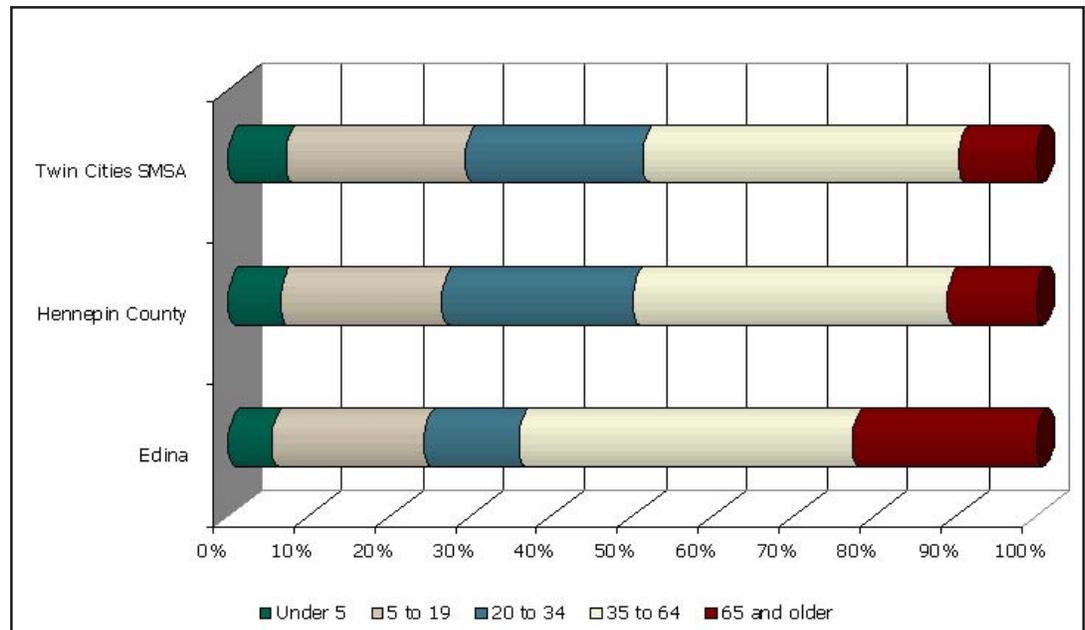
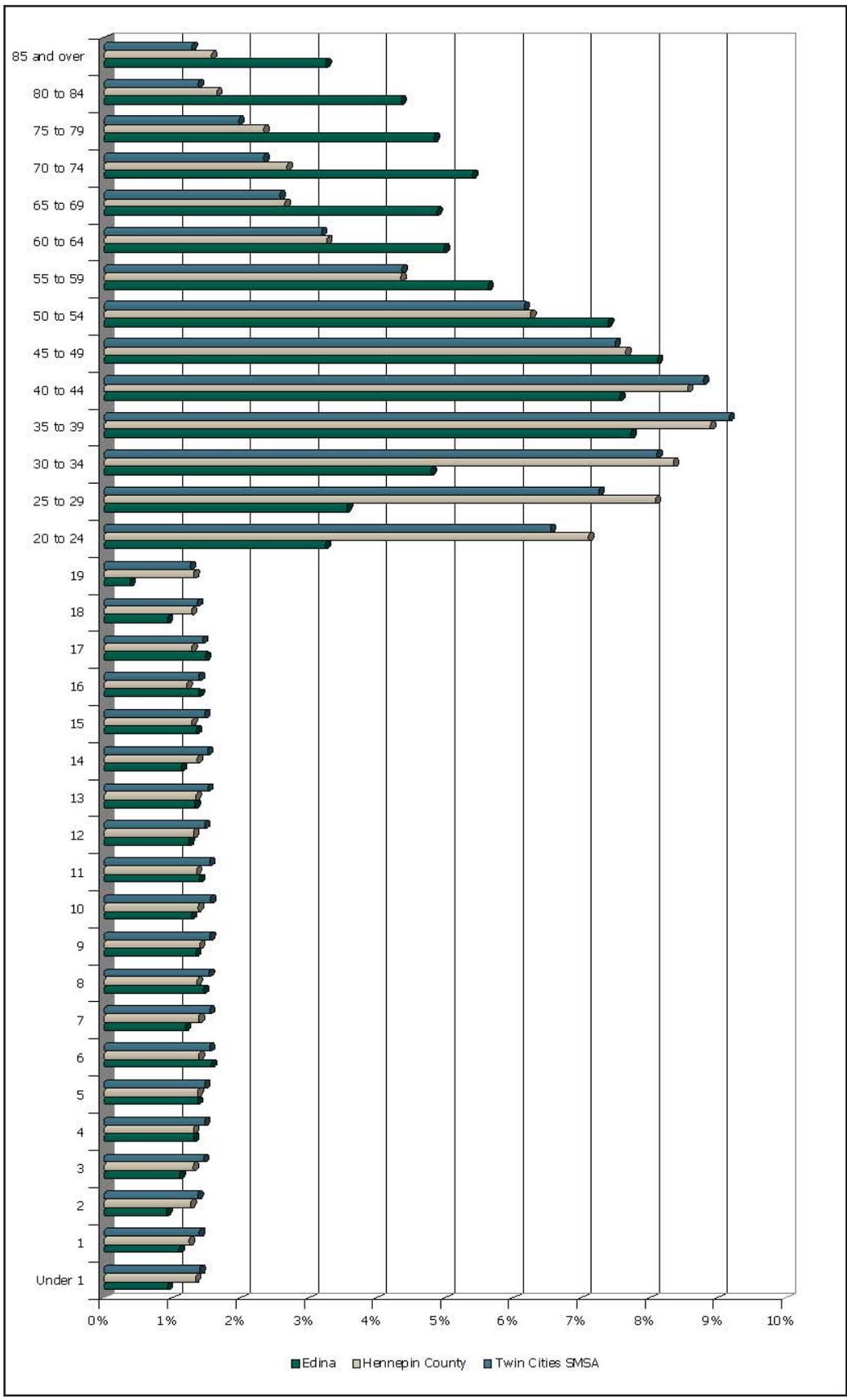


Figure A12
Age Distribution of 2000 Population



More population in all groups age 45 and older

Fewer "younger adults" (ages 20-34)

Figure A13
Age Distribution of 2000 Population

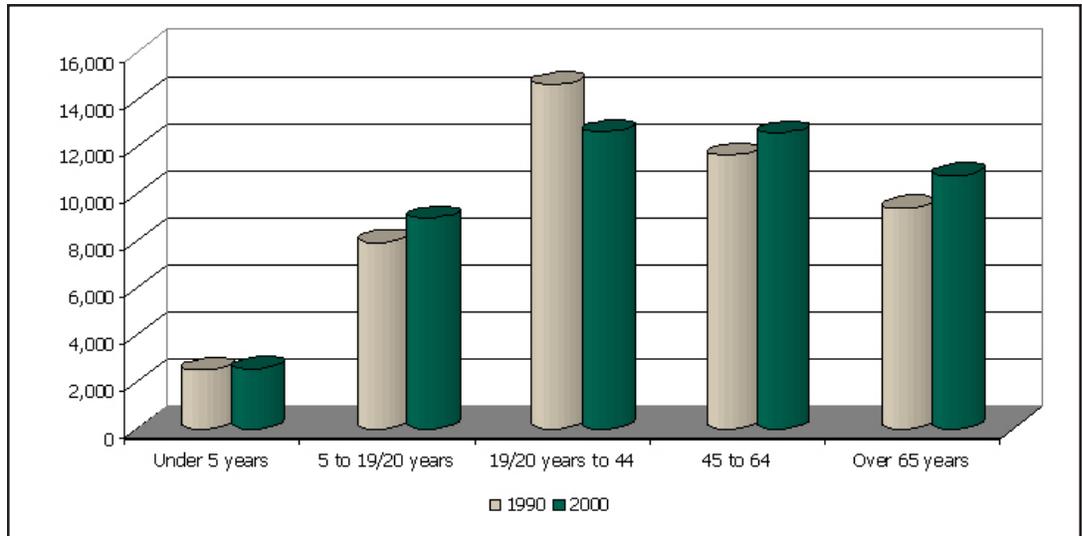


Figure A14
Population Change 1990 to 2000

Length of Residency

The Census does not measure the length of time lived in a particular municipality. The closest statistic is the year moved into the housing unit occupied at the time of the Census. Almost 60% of householder lived in the same unit for less than 10 years. This movement is slightly below levels for the County and the region (see Figure A15).

The Census also asks where people lived five years ago (1995). Over 62% of Edina residents lived in the same house (see Figure A16). People moving into Edina tended to come from Hennepin County (the Census does not distinguish movement within Edina). Less than 10% of the population lived outside of Minnesota in 1995.

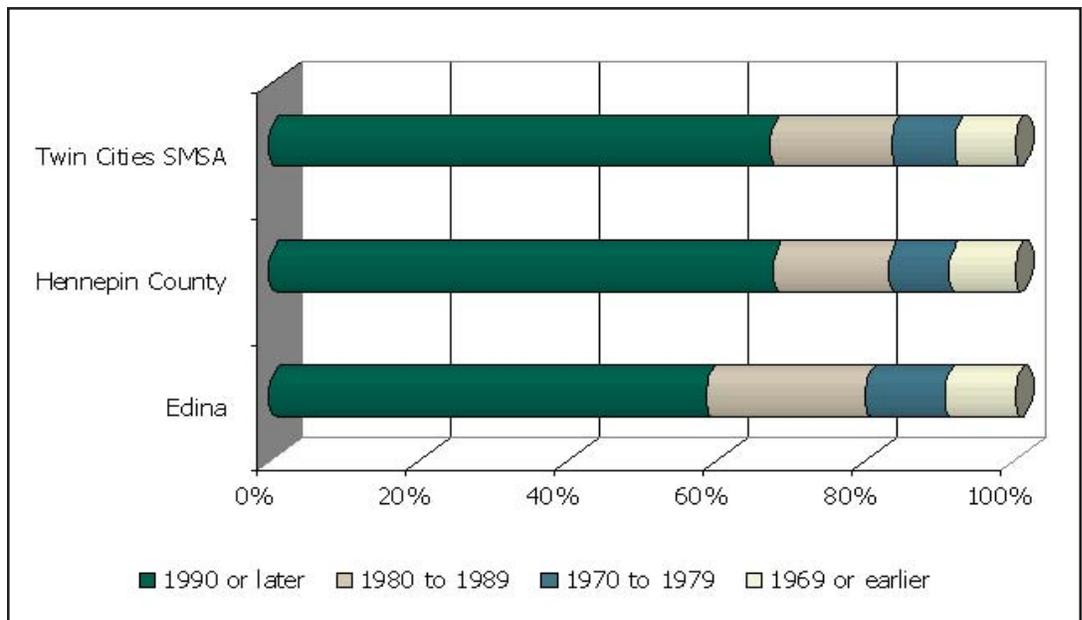


Figure A15
Year Move Into Housing Unit (all occupied housing)

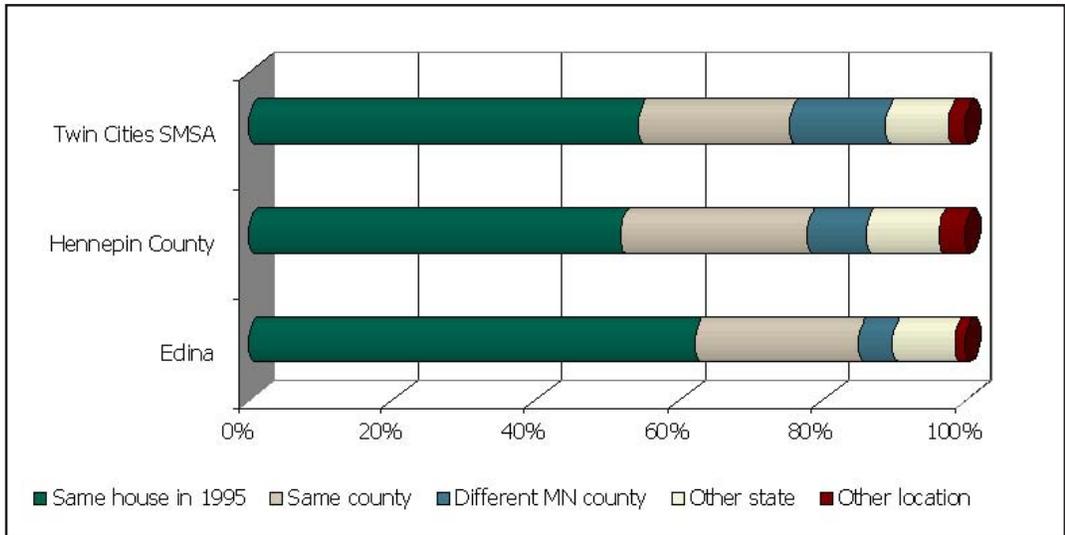


Figure A16
Place of Residence in 1995

Education

Edina’s population is well educated. Over 58% of the population age 25 and older have earned bachelor’s degree. Only one-third of the region’s population (age 25+) has a bachelor’s degree.

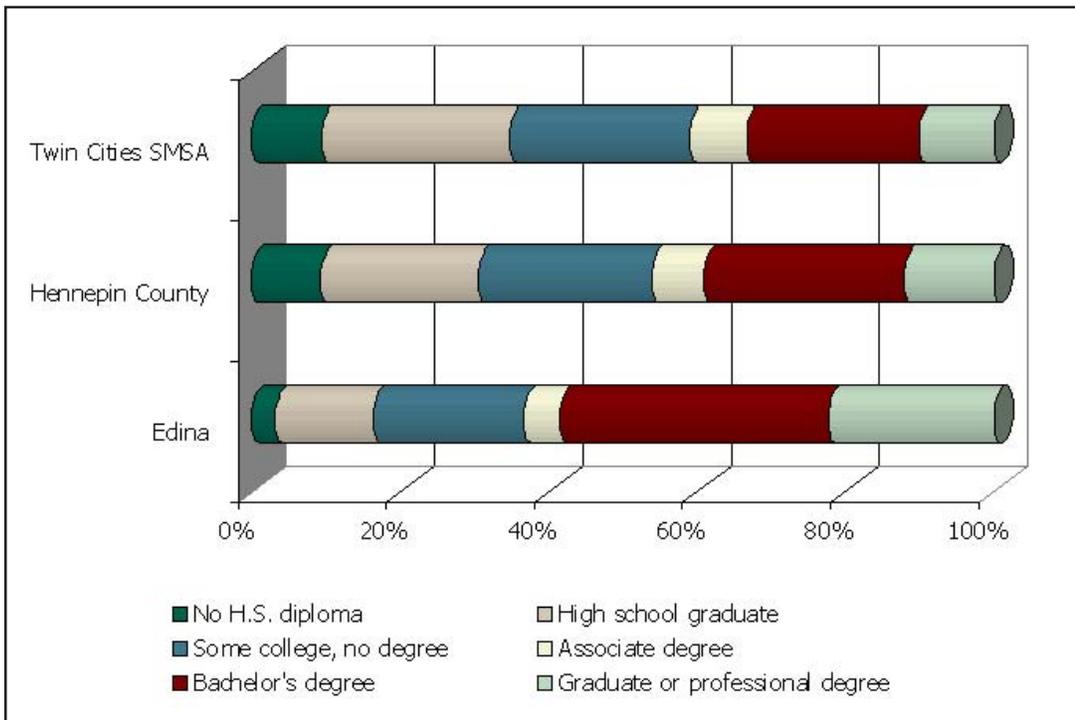


Figure A17
Educational Attainment (population age 25 and older)

Income

Edina is a wealthy community. In the three reported measures of income (per capita, family and household), Edina exceeds County and regional levels.

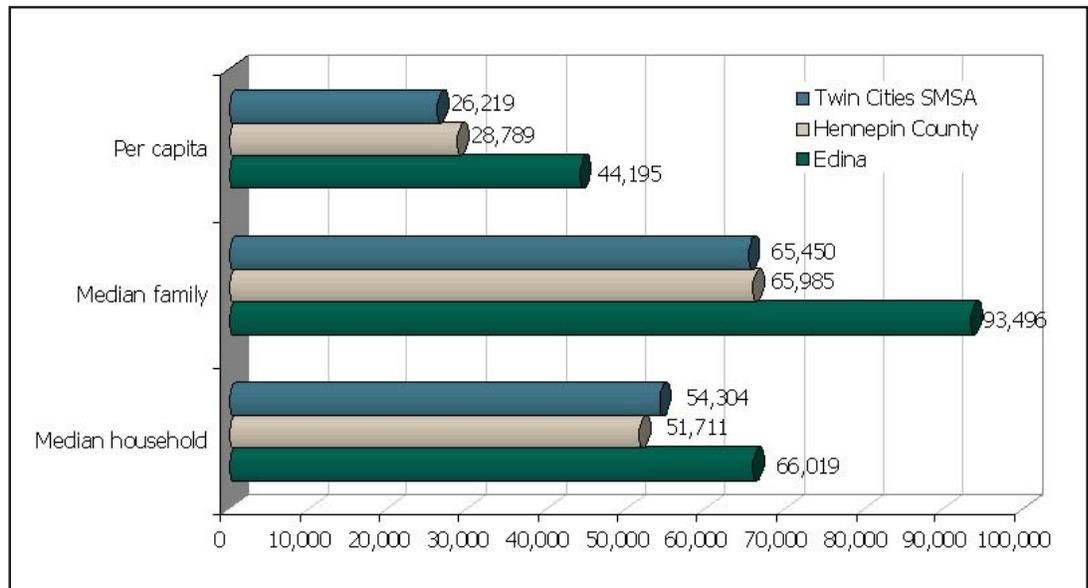


Figure A18
Income (in 1999)

Employment

Over 93% of Edina's work force (employed persons 16 years of age and older) list occupations in managerial/professional, service, or sales/office.

Only 27% had a place of employment in Edina. Minneapolis is a close second, serving as the place of employment for 24% of Edina workers. In total, three-quarters of these workers had

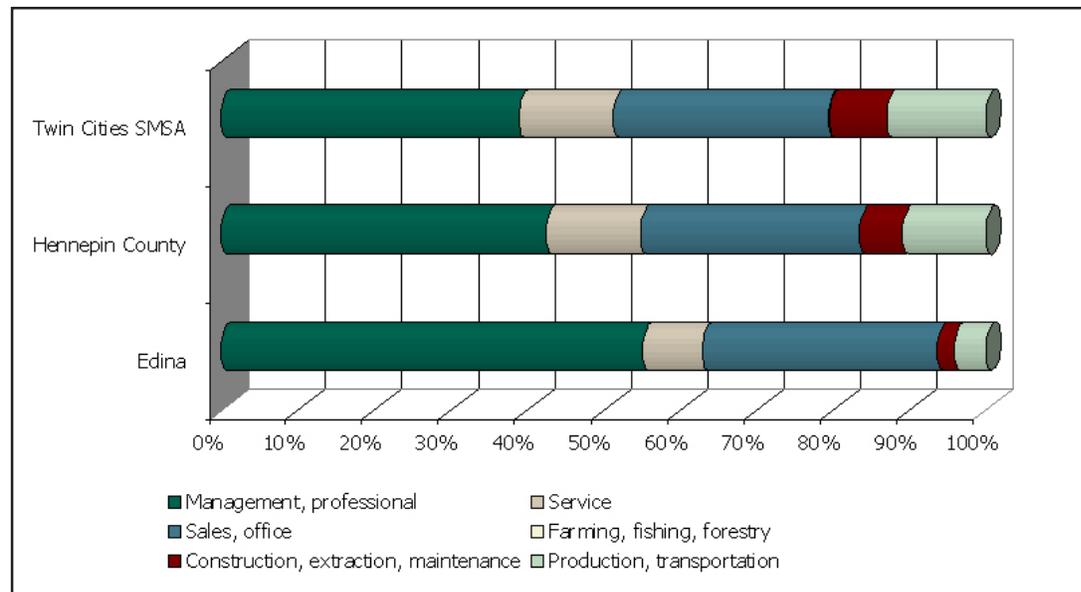


Figure A19
Occupation (employed population 16 and older)

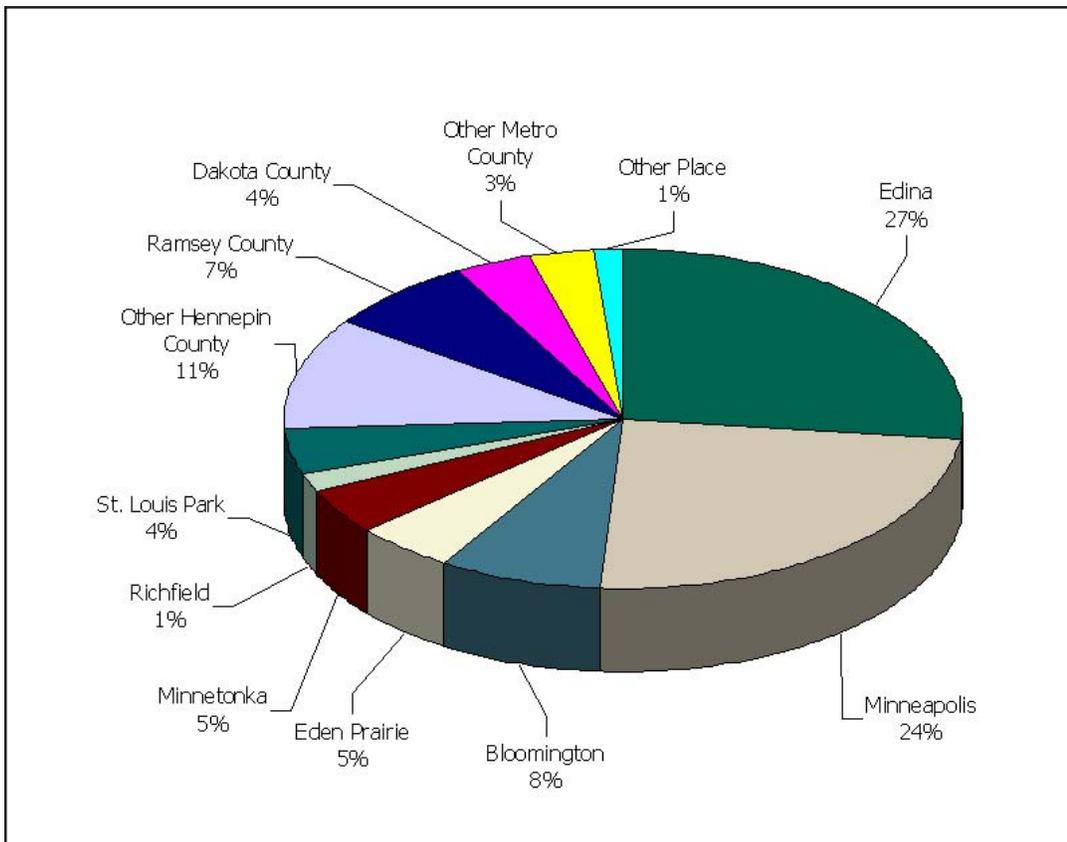


Figure A20
Place of Employment

employment in Edina or in an adjacent city.

The place of employment data suggests that residents tend to work close to Edina. Travel time to work reinforces this observation. Seventy-nine percent of local workers reported 30 minutes or less in average travel time to work. Edina residents spent less time commuting in

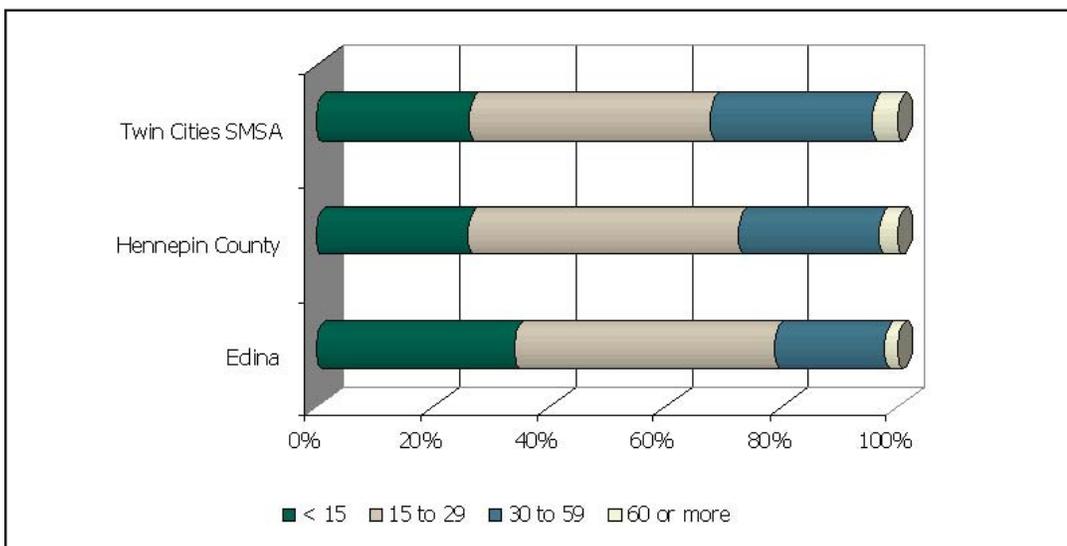


Figure A21
Travel Time to Work

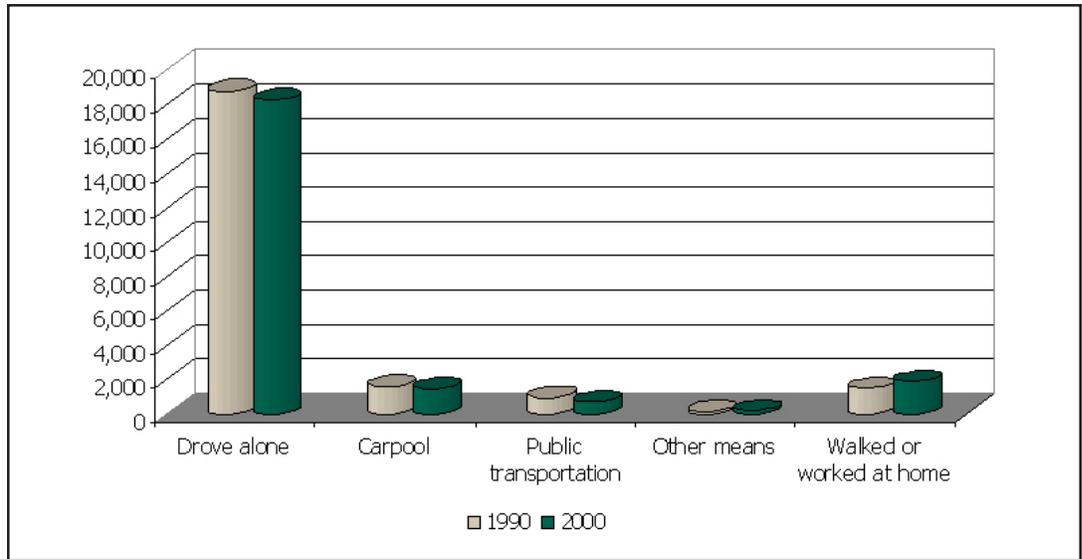


Figure A22
Means of Commuting to Work

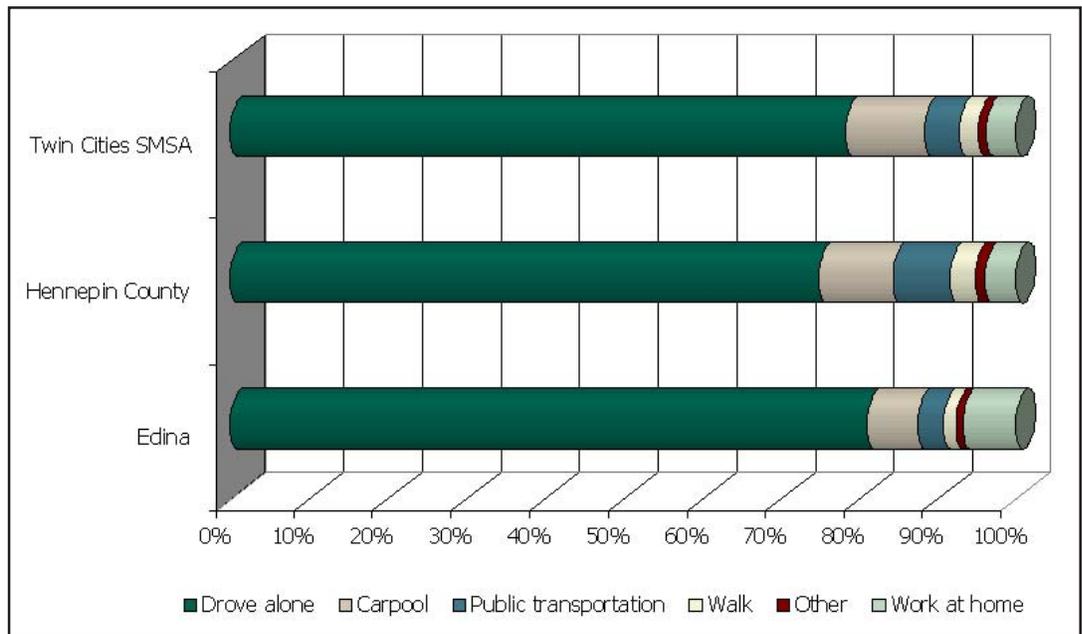


Figure A23
Means of Commuting to Work

comparison with the County and the region.

The Census asks people about the method of travelling to work. Over 81% of Edina workers drove alone to work. Only 4% reported using some means of public transportation. Seven percent of employed residents worked at home, a slightly larger share than shown by the County and the region.

Another aspect of employment is the number of people working in Edina. The Census tracks this information through the place of employment. A total of 45,516 people reported Edina their place of employment. At this level, Edina is home to almost as many workers as

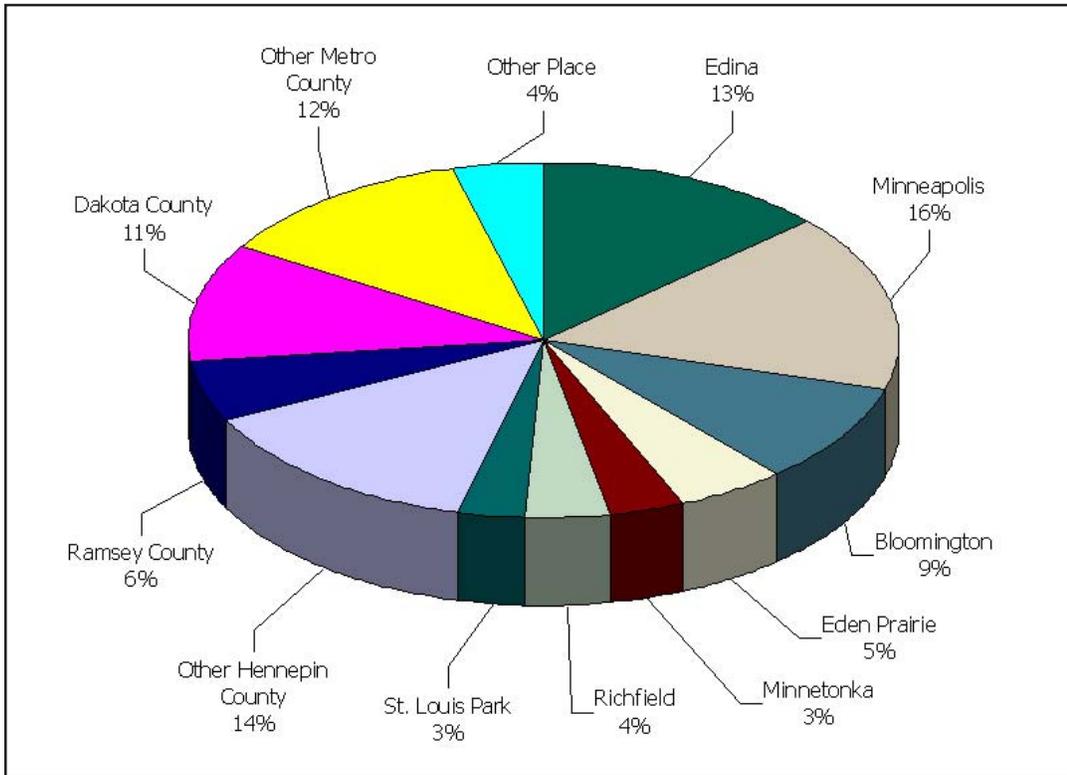


Figure A24
Edina as Place of Employment

residents. Thirteen percent (13%) of these workers live in Edina. Over 40% of people working in Edina lived in adjacent cities. Only 4% lived in a county outside of the seven-county metropolitan area.

Households

The number of households in Edina increased from 1990 (19,860) to 2000 (20,996). This growth occurred in the number of “nominally” households, primarily people living alone.

Households in Edina tend to be smaller when compared with the County and region. The average household in Edina contained 2.24 people. This compares with 2.39 for Hennepin County and 2.56 for the Twin Cities region. Households living in owned housing tend to be larger (2.42 persons) than in rental housing (1.66 persons).

Housing

Single-family detached dwellings are the primary housing type in Edina (over 57% all housing). Edina also contains a large supply (33% of total) of multiple family housing with structures containing ten or more dwelling units. The total number of housing units increased by 3% from 1990 (20,983) to 2000 (21,655).

The amount of owner occupied single family housing is comparable with the County and the region (Figure A28). Edina has a larger stock of larger multi-family structures. The chart in Figure A29 shows the distribution of population among housing types. Over 88% of Edina’s population lives in either single family detached or larger multi-family housing.

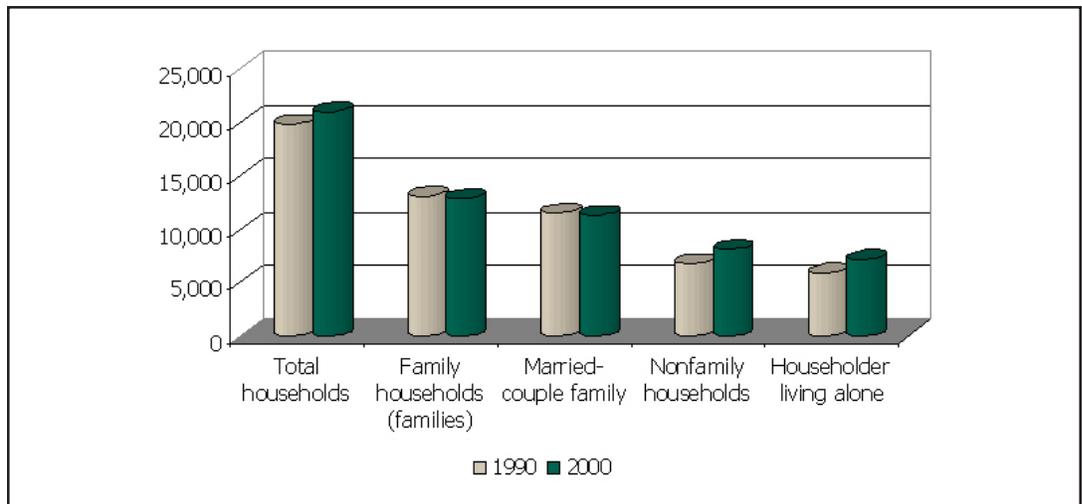


Figure A25
Households 1990 and 2000

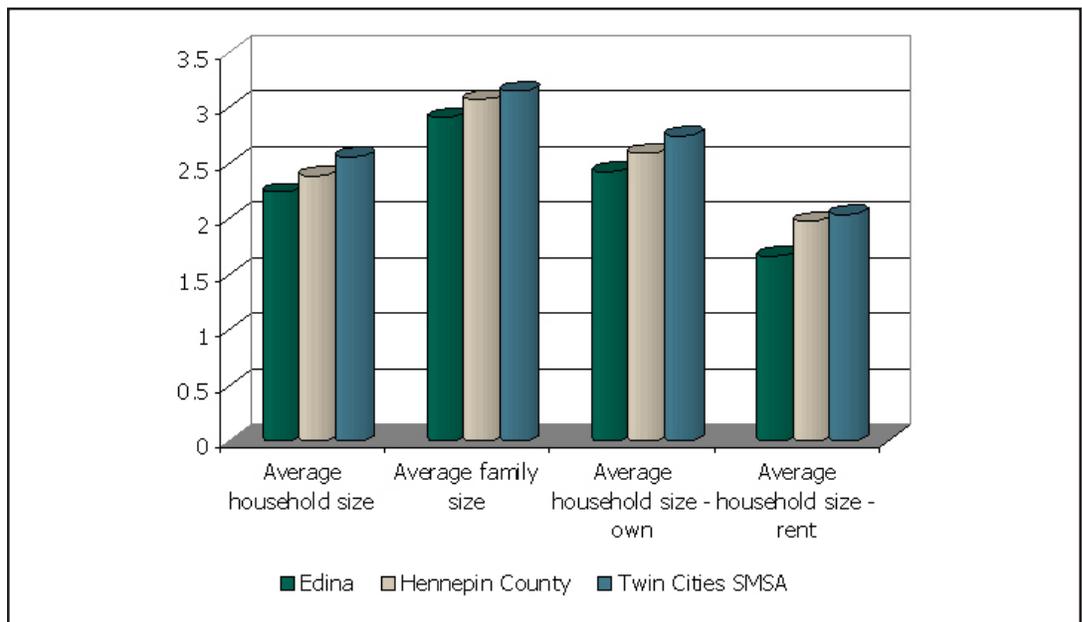


Figure A26
Household Size (persons per household)

The Census looks at relationships between housing and the characteristics of the population.

- Figure A30 compares unit type and tenure for different age of householders.
- Figure A31 compares the age of the housing (year built) and tenure according to the age of the householder.
- Figure A32 shows the household size (persons per household) for different housing types.

These charts illustrate what exists. The Census data does not explain the factors that create these differences. Influencing factors may include availability, affordability and preference.

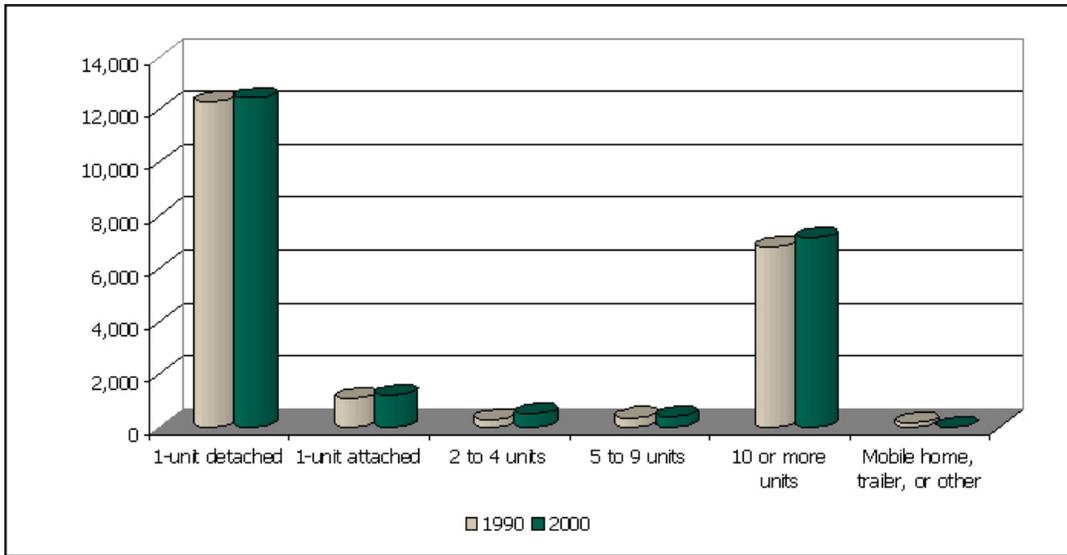


Figure A27
Housing Type (units in structure)

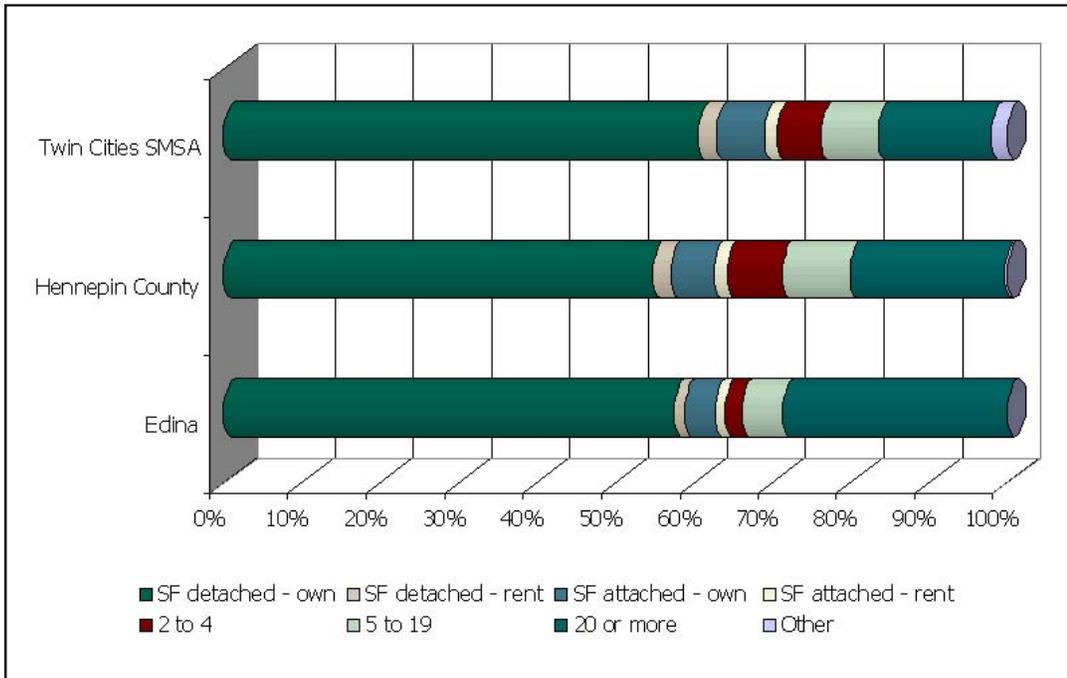


Figure A28
Housing Units in Structure

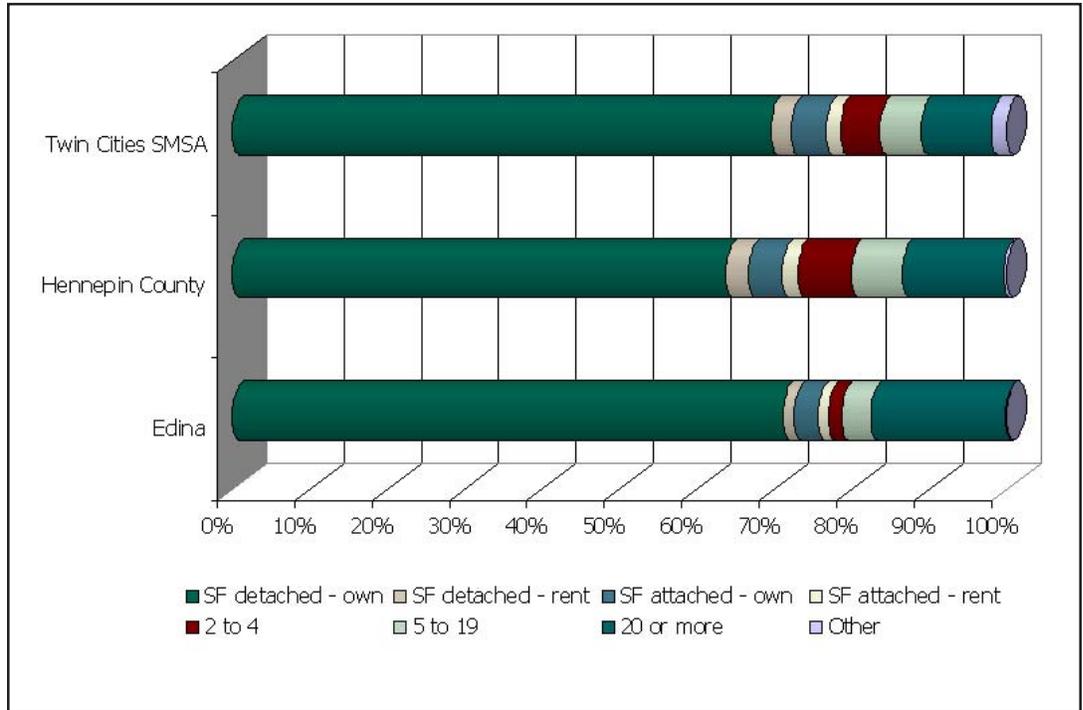


Figure A29
Population in Housing

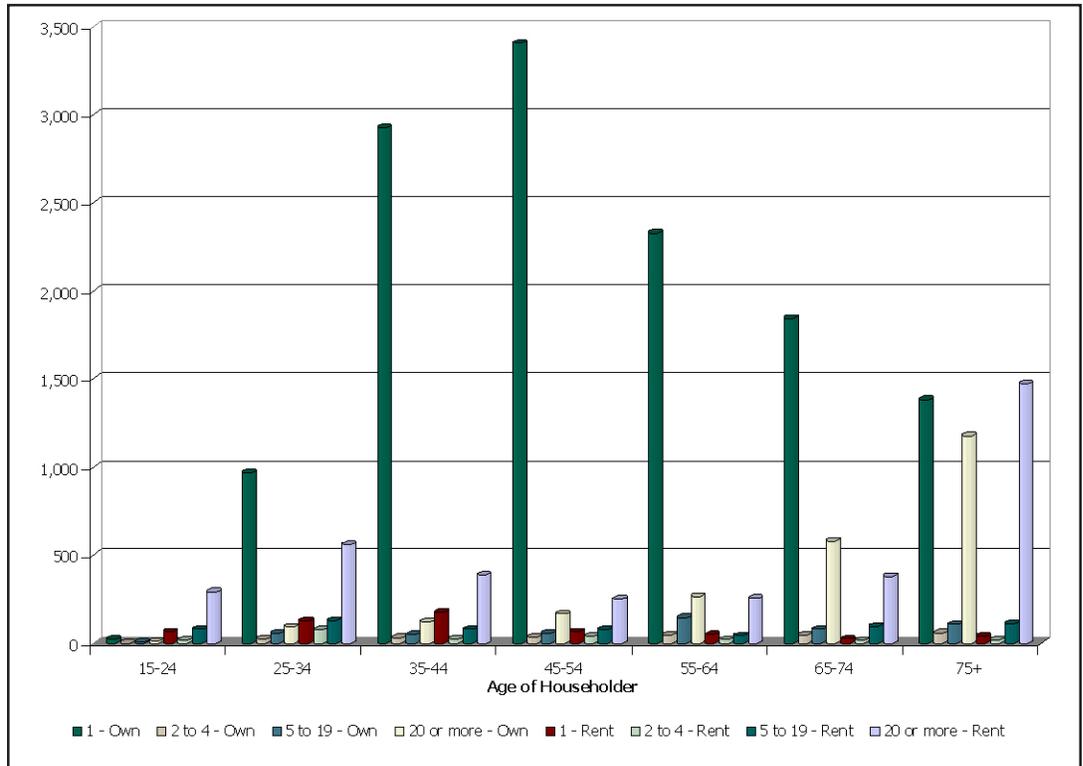


Figure A30
Age of Householder/Housing Type

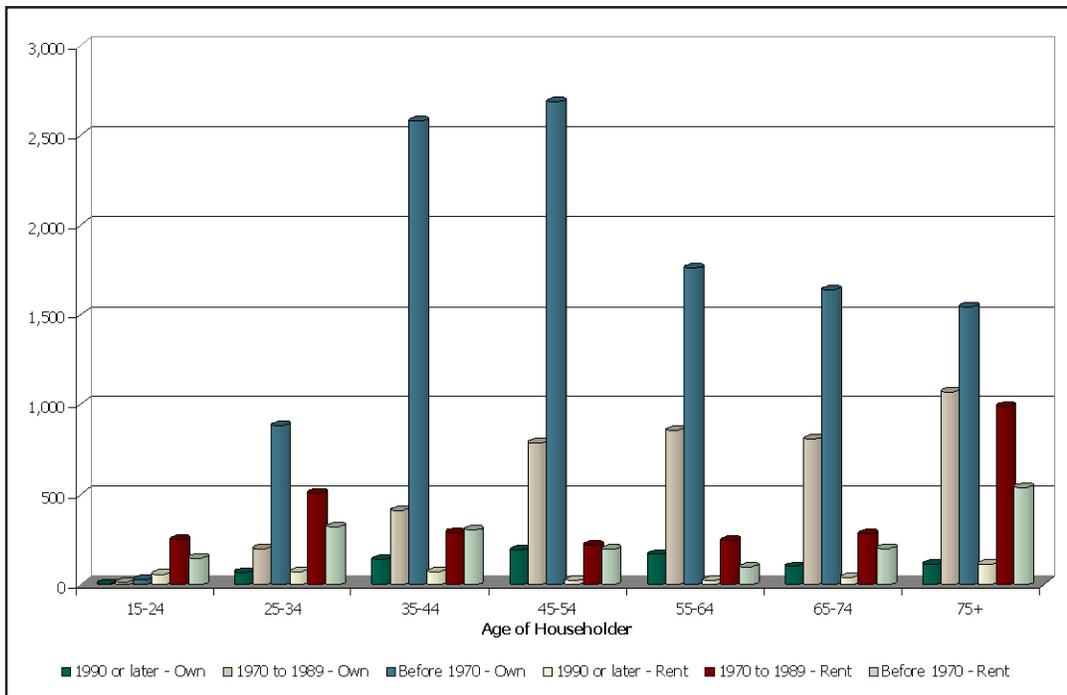


Figure A31
Age of Householder/Year Unit Built

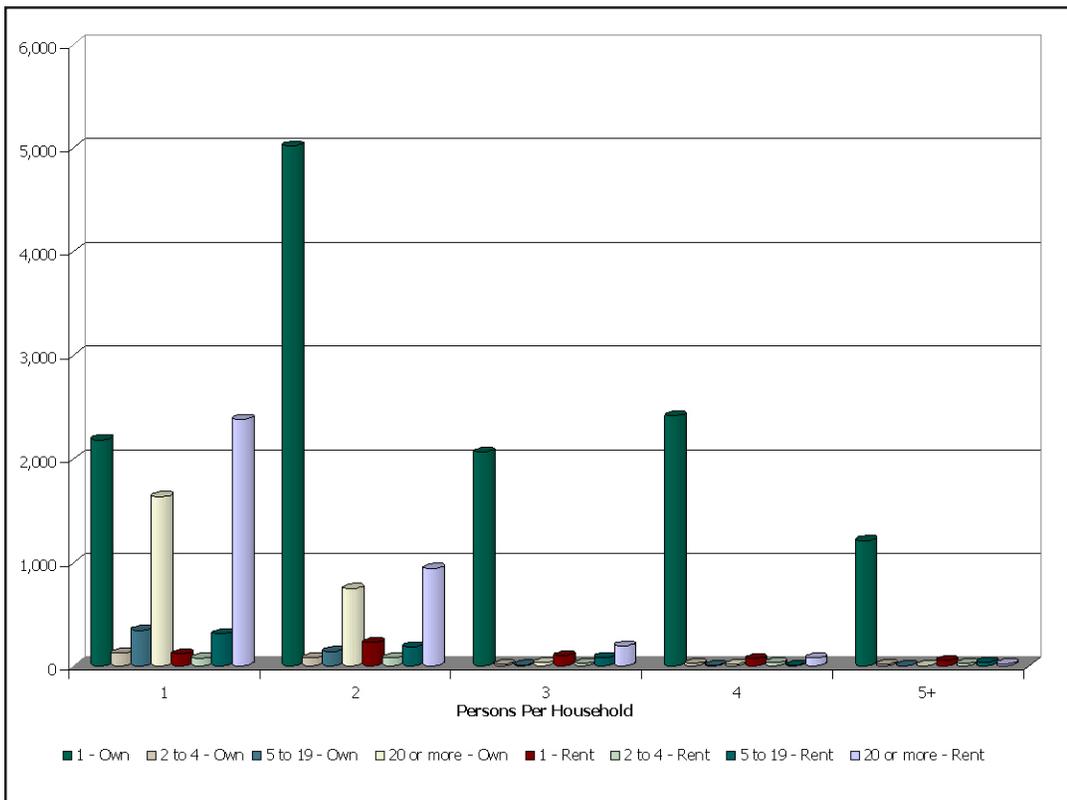


Figure A32
Age of Householder/Size of Household

Market Conditions and Issues

This memorandum presents preliminary overviews of market conditions and emerging factors and issues in the Southdale Corridor. These overviews are intended to provide a market-based perspective on the planning process and elicit discussion regarding future planning and development concepts for the Corridor. The overviews discuss general conditions and emerging trends/issues for retail, office, lodging and residential markets.

Retail Market Overview

General Conditions

The Southdale Center mall and other retail properties have contributed significantly to Edina's reputation as one of the Twin Cities' prestigious locations. At this time, different sources hold varying opinions regarding Southdale Center's position among the metropolitan area's regional malls. Most local experts agree, however, that the general Southdale corridor continues to maintain its position as the metro area's premier retail destination. The area derives this status from:

1. The local demographic profile;
2. General reputation; and
3. A recognized concentration of high-end stores, particularly in the furnishings category.

Major retail centers in the corridor include Southdale Center, the Galleria, Yorktown Mall, Yorkdale Shoppes, Centennial Lakes Plaza and freestanding stores such as Best Buy, Target and Marshall Field's Home Store. The Southdale Center and Galleria represent the largest and most prestigious of these.

Southdale Center currently reports vacancy rates of 18 percent. These are concentrated primarily in its basement-level and its 3rd-floor; management is planning a fitness/wellness center for parts of this vacant space.

At this time, Southdale Center's primary competitors include the Mall of America, Ridgedale Center, and Eden Prairie Center. These properties all contain more than 1 million square feet of leasable space, and command net lease rates typically ranging from \$30 to \$50 per square foot and up to \$100 per square foot at the high end (as high as \$150 at the Mall of America).

Many sources maintain that the Mall of America derives much of its business from visitors and does not compete for Southdale's core markets in Edina, southwest Minneapolis and the western parts of Richfield and Bloomington. Despite this, recent surveys show that, among Southdale shoppers, the largest percentage (71 percent) had also shopped at the Mall

<u>Center</u>	<u>GLA</u>
Southdale	1,821,000
Eden Prairie Center	1,183,000
Mall of America	2,468,000
Ridgedale Center	1,157,764

Source: United Properties

Figure A33
Southdale Center and Primary Competitors

of America within the prior 90-day period; the next largest "cross-shopping" location was Eden Prairie Center, at 35 percent.

The Galleria occupies a slightly different niche, with greater emphases on local retailers, high-end stores, and categories such as arts and furnishings. The Galleria maintains occupancy rates in the mid-90 percent range, with lease rates ranging from \$30 to \$45 per square foot on a net basis.

Emerging Factors and Issues

Recent Events: A number of events have changed retail outlooks for the Corridor. These include:

1. Edina's removals of prohibitions on the serving of beer and wine (1996) and hard liquor (2002). These actions have opened new opportunities for eating and drinking establishments in this strong market.
2. The development in the 1990s of the various residential and office uses in the corridor, most notably the Centennial Lakes office park, which provides a critical mass of daytime traffic within walking distance of the corridor's shopping centers.
3. Recent sale by the Target Corporation of its Marshall Field's brand and Twin Cities-based Mervyn's stores will change the management and profile of at least some stores at Southdale as well as at competitive regional malls.

Demographic Profiles: Among its primary competitors, the following table shows that while Southdale still draws upon the largest market, median household incomes in the Eden Prairie Center market in particular as well as in the Ridgedale market exceed those in the Southdale market.

<u>Market</u>	<u>2000</u>	<u>2004</u>	<u>2009</u>	<u>Avg. Ann. Change</u>
<u>Southdale</u>				
Households	47,326	46,770	45,998	-0.3%
Median Household Income	\$58,782	\$63,216	\$68,909	1.8%
<u>Eden Prairie Center</u>				
Households	18,973	19,563	20,233	0.7%
Median Household Income	\$74,845	\$83,174	\$92,663	2.4%
<u>Mall of America</u>				
Households	23,391	23,347	23,281	-0.1%
Median Household Income	\$46,712	\$50,529	\$55,714	2.0%
<u>Ridgedale</u>				
Households	20,595	20,877	21,221	0.3%
Median Household Income	\$63,861	\$68,900	\$74,833	1.8%
<i>Source: U.S. Census; Claritas, Inc.</i>				

Figure A34
Households and Median Household Income Comparisons (3-Mile Radii)

Shifting Suburban Growth Patterns: In addition to the existing markets around Ridgedale and Eden Prairie Center, as suburban growth continues to radiate outward, the outlying communities to the west and southwest feature higher socio-economic profiles than those in the south. These communities to the west and southwest are more convenient to the Eden Prairie Center and Ridgedale than to Southdale or the Mall of America. Among these 'second-ring' suburban communities, the Metropolitan Council forecasts the fastest growth in the southwestern communities of Savage, Shakopee, Prior Lake, Chaska and Chanhassen.

City	Median Household Income	Median Home Value
Apple Valley	\$69,752	\$154,300
Burnsville	\$57,965	\$155,900
Savage	\$75,097	\$168,400
Shakopee	\$59,137	\$141,500
Prior Lake	\$75,363	\$175,100
Chaska	\$60,325	\$161,000
Chanhassen	\$84,215	\$210,700
Shorewood	\$96,589	\$254,300
Orono	\$88,314	\$324,400

Source: U.S. Census

Figure A35
2000 Wealth Indicator Comparisons, Selected Outlying Cities

New development planned at the Mall of America is likely to include more than 800,000 square feet of retail/restaurant space as well as office, hotel and entertainment-related components. While specific configurations and tenant profiles are unknown, this development may offer additional competition with businesses in the Southdale corridor.

Edina still offers potential opportunities for additional retailers and development formats, which may include additional "big box" retailers, eating and drinking businesses, entertainment venues and others.

Office Market Overview

General Conditions

The Southdale Corridor is set within the southwest suburban office market. This comprises the largest suburban office market in the Twin Cities, with the metropolitan area's primary concentrations of high-end suburban office space.

Trends in the southwest office market conform to those of the overall Twin Cities office market. In the overall market, vacancy rates have climbed from a range of 6 to 7 percent in the late 1990s to approximately 15 to 20 percent today. As vacancies increased, lease rates fell, from late 1990s levels in excess of \$20 per square foot at some properties to a general range of \$12 to \$16 for typical Class-A space. In recent months, however, the market has achieved positive absorption, and vacancy rates have stabilized.

Market	Total Sq. Ft.	Percent Class A	Avg. Class A Net Lease
Metro Area	26,078,550	46%	\$12.96
Southwest	13,221,356	52%	\$14.36
West	8,355,502	52%	\$15.53
Minneapolis CBD	26,078,550	55%	\$13.60

Source: United Properties

Figure A36
Major Office Submarkets

The Twin Cities office market has been driven by growth in professional employment, which has been increasingly concentrated in service industries such as information, financial activities, professional/business services and health care. As shown below, these have accounted

for the much of Hennepin County's employment growth over the last three, ten, and twenty-year periods.

<u>Year</u>	<u>Metro Area</u>	<u>Southwest</u>	<u>West</u>	<u>CBD</u>
1995	861,572	261,062	126,803	(72,927)
1996	1,240,369	258,528	111,403	298,342
1997	1,468,600	361,494	45,264	428,029
1998	1,772,356	179,895	277,053	178,024
1999	1,503,769	379,780	150,636	(56,964)
2000	2,968,102	982,484	842,358	325,214
2001	330,112	60,579	(8,963)	432,984
2002	(1,826,230)	(252,152)	(240,470)	(1,099,991)
2003	(849,024)	(87,492)	49,418	(371,877)
<u>2004*</u>	<u>344,864</u>	<u>47,375</u>	<u>224,136</u>	<u>605</u>
Subtotal	7,814,490	2,191,553	1,577,638	61,439
* 1st half				

Source: United Properties

Figure A37
Office Absorption Trends, Selected Markets, 1995-2004

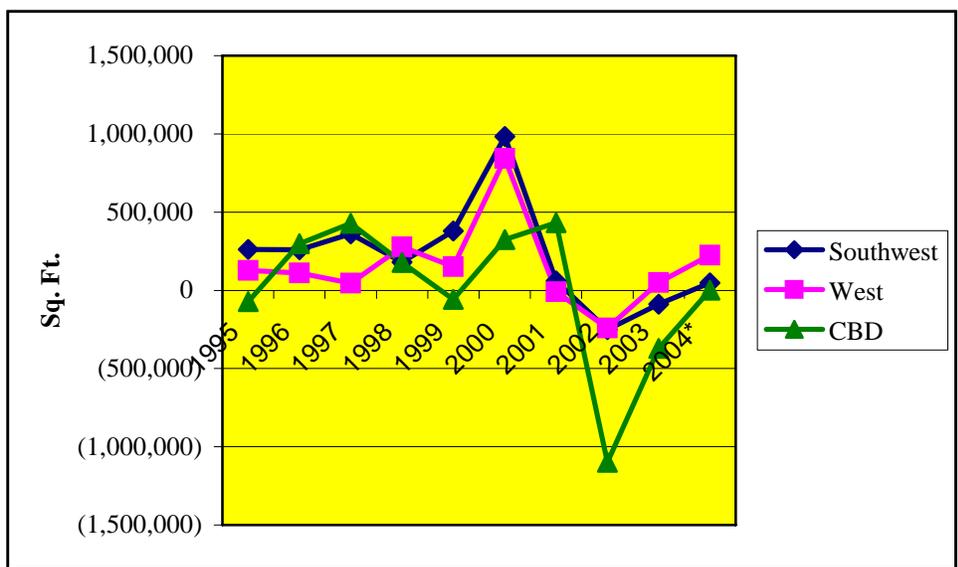


Figure A38
Office Absorption Trends

The Southdale Corridor itself contains an estimated 2.5 million square feet of office space (Welsh Companies). This inventory includes:

1. The Centennial Lakes office park, containing a total of approximately 820,000 square feet in five buildings. Built in the late 1990s, this is one of the high-end suburban office parks in the metropolitan area, with net (asking) lease rates ranging from \$15 to \$19 per square foot. While one of these buildings sustains a 32 percent vacancy rate, the other four buildings maintain occupancy rates in the mid- to high 90 percent range.

	1983	1993	2000	2003	Compounded Ann. Growth Rate			2013	10-Yr. Growth Rate
					20-Yr.	10-Yr.	3-Yr.		
Total Payroll Employment	617.68	787.13	877.05	831.78	1.5%	0.6%	-1.8%	933.96	1.2%
Total Nonfarm	605.20	773.58	861.81	815.98	1.5%	0.5%	-1.8%	918.51	1.2%
Natural Resources & Mining	0.06	0.12	0.09	0.08	1.0%	-3.9%	-3.5%	0.06	-2.0%
Construction	17.63	21.63	29.98	29.39	2.6%	3.1%	-0.7%	32.09	0.9%
Manufacturing	96.70	102.10	101.18	82.09	-0.8%	-2.2%	-6.7%	81.70	0.0%
Food Manufacturing	7.96	6.39	4.59	4.65	-2.7%	-3.1%	0.5%	5.27	1.3%
Fabricated Metal Product Manufacturing	11.47	11.70	12.22	9.44	-1.0%	-2.1%	-8.2%	8.73	-0.8%
Machinery Manufacturing	15.98	14.95	15.35	9.42	-2.6%	-4.5%	-15.0%	8.75	-0.7%
Computer and Electronic Products	27.60	24.72	24.27	19.85	-1.6%	-2.2%	-6.5%	20.40	0.3%
Wholesale Trade	42.37	44.91	49.97	46.72	0.5%	0.4%	-2.2%	54.34	1.5%
Retail Trade	65.13	75.83	81.50	78.35	0.9%	0.3%	-1.3%	80.45	0.3%
Transportation, Warehousing, & Utilities	27.12	39.30	42.45	36.72	1.5%	-0.7%	-4.7%	40.92	1.1%
Information	17.51	22.57	27.16	22.89	1.3%	0.1%	-5.5%	25.93	1.3%
Publishing Industries (except Internet)	6.17	10.00	8.23	8.24	1.5%	-1.9%	0.0%	8.76	0.6%
Telecommunications	7.02	5.86	9.07	6.80	-0.2%	1.5%	-9.1%	8.41	2.1%
Internet Service Providers, Web Search Portals, & Data Processing Service	1.89	3.32	6.14	4.24	4.1%	2.5%	-11.6%	4.70	1.0%
Financial Activities	50.02	70.96	75.33	79.01	2.3%	1.1%	1.6%	85.56	0.8%
Finance and Insurance	37.72	53.23	58.07	62.07	2.5%	1.5%	2.2%	67.67	0.9%
Real Estate and Rental and Leasing	12.30	17.73	17.26	16.94	1.6%	-0.5%	-0.6%	17.89	0.5%
Professional & Business Services	78.02	125.97	164.74	146.09	3.2%	1.5%	-3.9%	175.02	1.8%
Professional, Scientific, and Technical Services	30.26	51.37	70.23	64.19	3.8%	2.3%	-3.0%	77.79	1.9%
Management of Companies and Enterprises	21.94	29.90	35.74	30.75	1.7%	0.3%	-4.9%	34.57	1.2%
Administrative, Support, Waste Management, & Remediation Services	25.82	44.70	58.77	51.15	3.5%	1.4%	-4.5%	62.66	2.1%
Education & Health Services	57.98	78.19	88.65	99.20	2.7%	2.4%	3.8%	128.04	2.6%
Educational Services	5.75	8.74	10.73	12.82	4.1%	3.9%	6.1%	17.95	3.4%
Health Care and Social Assistance	52.23	69.45	77.92	86.38	2.5%	2.2%	3.5%	110.09	2.5%
Leisure & Hospitality	47.10	60.92	65.06	66.39	1.7%	0.9%	0.7%	83.67	2.3%
Arts, Entertainment, and Recreation	8.32	10.39	9.89	10.82	1.3%	0.4%	3.0%	11.92	1.0%
Accommodation and Food Services	38.78	50.53	55.17	55.57	1.8%	1.0%	0.2%	71.75	2.6%
Other Services (except Public Administration)	26.24	31.50	33.29	32.74	1.1%	0.4%	-0.6%	38.43	1.6%
Government	79.31	99.59	102.42	96.31	1.0%	-0.3%	-2.0%	92.29	-0.4%

Source: Bureau of Labor Statistics; economy.com

Figure A39
Employment by Industry Sector (000s of Jobs): Hennepin County 1983-2013

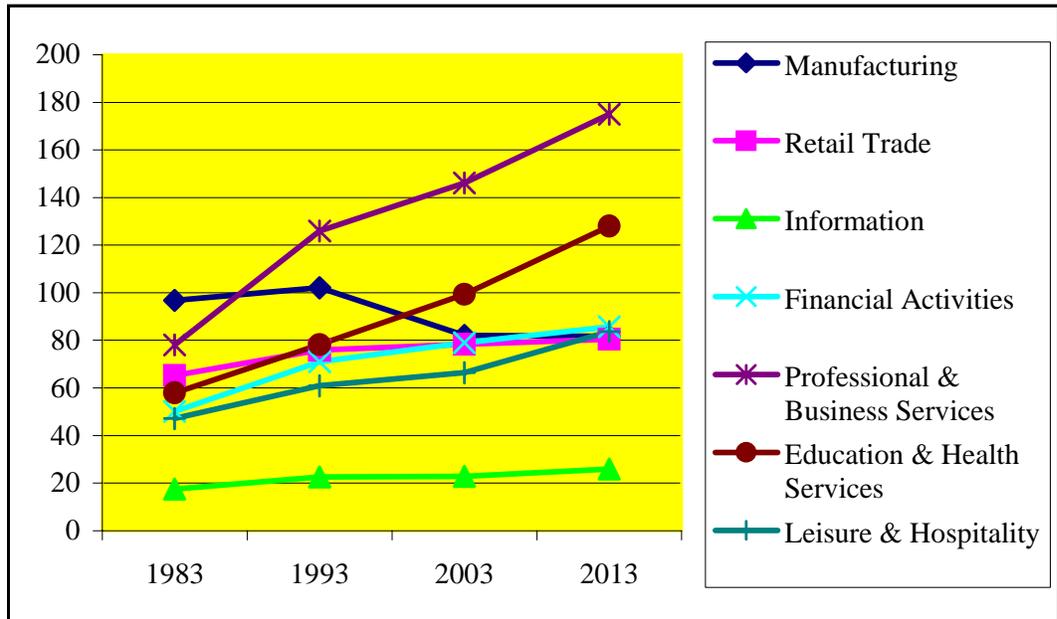


Figure A40
Selected Major Industry Sectors (000s of jobs)

2. Medical office space clustered around Fairview Hospital (830,000 square feet).
3. Buildings along the west side of France Avenue (540,000 sq. ft.) and
4. Additional buildings containing approximately 340,000 square feet.

Tenants in general office (excluding medical) buildings professional service providers involved in fields such as financial services, real estate, insurance, advertising, software development, consulting, and other such industries. Brokers and property managers report that the Southdale and southwest suburban market tenancies are driven by growing businesses and new businesses from within the existing market, with very little interaction with the downtown Minneapolis market.

Emerging Factors and Issues

Suburban office submarkets will continue to seek proximity to I-494 and other locations along the regional freeway system. Potential areas for new Class-A office development include locations to the east of the Mall of America and along the Highway 610 Corridor in the northern part of the metropolitan area.

Health Care Industry Growth: Health care represents the largest, fastest-growing, and most consistently-growing industry sector in Hennepin County. This growth has driven new medical office development in the Corridor and directly south of the Corridor. Ongoing growth in this industry will be fueled by (1) an aging population, (2) the Corridor's status as a major medical center, and (3) the emergence of new medical technologies and practices (e.g., bariatrics, lasik eye surgery). These factors are likely to drive ongoing growth in demand for medical and other health care-related developments in the Corridor.

Office Building Intensity: Edina development regulations limit the permissible volume of commercial office development to a .5 floor-to-area (FAR) ratio. This limits the intensity of development use. Under this limitation, many developers of major Class-A projects may find it difficult to build recognizable - and typically larger and/or taller - buildings (with covered parking), and may seek alternative locations.

Lodging Market Overview

General Conditions

Through 2003, the Twin Cities lodging market, like that in most areas, has encountered declining occupancy rates, average daily rates (ADR) and revenues-per available room (RevPAR) since reaching peak levels in 2000. As of mid-year 2004, however, performances have improved over mid-year 2003 levels, and industry experts foresee improved market conditions in the short-term future.

Year	Occupancy	ADR	RevPar
1998	68.9%	\$79.06	\$54.47
1999	67.6%	\$80.99	\$54.75
2000	68.5%	\$84.56	\$57.92
2001	62.6%	\$85.65	\$53.62
2002	60.5%	\$83.26	\$50.37
2003	60.1%	\$79.72	\$47.92
Avg. Ann. %	-2.7%	0.2%	-2.5%
<i>Source: Smith Travel Research.</i>			

Figure A41
Metropolitan Area Lodging Indicators

Within the metropolitan area, the I-494 corridor features a major concentration of lodging facilities.

The Southdale Corridor offers a strong location for lodging. The area's assets include its

direct access to:

1. I-494, which connects the area to the Minneapolis-St. Paul Airport, as well as Highway 62;
2. High-end office space;
3. The Fairview Southdale hospital;
4. Services and amenities such as restaurants, high-end retail shopping and the Centennial Lakes Park.

Despite this array of assets, the entire City of Edina contains just one lodging facility. A number of historical factors account for this:

1. The Corridor is not directly contiguous to I-494; sites located within the Bloomington city limits offer the most immediate access and visibility to the highway.
2. Prior to 1996, Edina did not allow liquor service in restaurants; prior to 2002, permissible liquor service allowed only beer and wine. Consequently, no full-service lodging facility would find Edina suitable.
3. Prior to 1997, Edina did not contain high-end office properties, placing the Corridor at a relative disadvantage relative to locations closer to business concentrations such as the Normandale Lakes or Opus office parks.

<u>Year</u>	<u>Occupancy</u>	<u>ADR</u>	<u>RevPar</u>	<u>Rm Supply</u>	<u>Rm Demand</u>	<u>Revenue</u>
1998	74.3%	\$71.50	\$53.14	481,070	357,558	\$25,565,464
1999	66.0%	\$74.02	\$48.85	481,070	317,494	\$23,501,675
2000	66.2%	\$74.97	\$49.63	481,070	318,448	\$23,873,170
2001	57.2%	\$79.46	\$45.43	511,331	292,308	\$23,227,558
2002	56.0%	\$78.24	\$43.84	545,427	305,644	\$23,914,111
2003	61.6%	\$73.50	\$45.25	553,737	340,911	\$25,057,346
Avg. Ann. %	-3.7%	0.6%	-3.2%	2.9%	-0.9%	-0.4%
* Property sample includes: Crowne Plaza Minneapolis Airport, Clarion Hotel, Wyndham Hotel, Park Inn, Country Inn & Suites, Hampton Inn, Residence Inn, Staybridge Suites, Hilton Garden Inn.						
<i>Source: Smith Travel Research.</i>						

Figure A42
494 Lodging Segment

Focusing on the market in the I-494 corridor, lodging performances are similar to those in the overall Twin Cities market.

This I-494 market segment contains primarily mid-priced properties. Higher-end full-service properties in the southwestern portion of the metropolitan area include the Marriott Southwest, Hotel Sofitel and Sheraton. In general, the entire Twin Cities metro area contains no five-diamond-rated hotels and just four four-diamond properties.¹ While the upscale suburban market has declined along with the overall lodging market in recent years, no new inventory has been added to this niche in recent years, and the Corridor may offer a suitable location for this type of development.

<u>Year</u>	<u>Occupancy</u>	<u>ADR</u>	<u>RevPar</u>	<u>Rm Supply</u>	<u>Rm Demand</u>	<u>Revenue</u>
1998	70.8%	\$100.96	\$71.46	617,875	437,317	\$44,151,974
1999	69.0%	\$105.65	\$72.87	615,755	424,690	\$44,869,692
2000	69.9%	\$111.69	\$78.10	615,755	430,573	\$48,091,090
2001	65.0%	\$112.79	\$73.27	615,755	400,037	\$45,118,242
2002	61.6%	\$109.73	\$67.62	615,755	379,478	\$41,638,260
2003	61.3%	\$101.10	\$62.03	615,755	377,752	\$38,192,372
Avg. Ann. %	-2.8%	0.0%	-2.8%	-0.1%	-2.9%	-2.9%
* Sampled properties include the Hotel Sofitel, Sheraton (Bloomington), Embassy Suites (Bloomington), Hilton Airport and Marriott Southwest.						
<i>Source: Smith Travel Research.</i>						

Figure A43
Southwest Suburban High-End Lodging Performance

Emerging Factors and Issues

Improving Conditions: As of mid-year 2004, both the mid-priced 494 corridor and the up-scale market segments have delivered improved performances over 2003 levels. The former niche has increased its RevPAR by 11 percent over 2003; the latter has improved by 4 percent. The greater improvement among mid-priced properties may reflect their greater ability to attract families and weekend leisure travelers visiting friends as well as attractions such as the Mall of America.

New development adjacent to the Mall of America is likely to include up to three new hotels containing more than 800 rooms. Of these, a 304-room property with an attached indoor waterpark is currently under construction; the other two are also likely to feature waterparks.

Recent events have enhanced lodging development advantages in the Southdale Corridor. These include Edina's removal of prohibitions on the serving of liquor, as well as the development of high-end office uses at Centennial Lakes office park.

Residential Market Overview

General Conditions

Growth will focus on upper-income empty nester households. Under prevailing conditions, absent new property for new development or redevelopment, demographic projections show negative growth for Edina households. Notwithstanding the overall projection, the table below shows that upper-income households, particularly in the empty nester and senior age brackets, will increase. This cohort has provided the primary target market for various townhouse and attached housing formats in the Twin Cities.

Future growth: In 2009 householders age 45 to 54 will represent Edina's largest household age group. As this group moves into the empty nester category in subsequent years, it will provide continued market support for attached housing products.

The Twin Cities apartment market features an overall vacancy rate of 7.4 percent (GVA Marquette Advisors). Vacancies are considerably higher, however, in the highest rent tiers, exceeding 10 percent where monthly rents exceed \$1,300, and approaching 13 percent where

<u>Age Group</u>	<u>2004</u>	<u>2009</u>	<u>#</u>	<u>Change Compnd. avg. %</u>
<u>15 - 24</u>	<u>406</u>	<u>451</u>	<u>45</u>	<u>2.1%</u>
< \$35,000	149	145	-4	-0.5%
\$35,000 - \$74,999	191	206	15	1.5%
\$75,000 - \$99,999	45	62	17	6.6%
\$100,000+	21	38	17	12.6%
<u>25 - 34</u>	<u>1,709</u>	<u>1,596</u>	<u>-113</u>	<u>-1.4%</u>
< \$35,000	307	243	-64	-4.6%
\$35,000 - \$74,999	597	530	-67	-2.4%
\$75,000 - \$99,999	311	281	-30	-2.0%
\$100,000+	494	542	48	1.9%
<u>35 - 44</u>	<u>3,521</u>	<u>3,106</u>	<u>-415</u>	<u>-2.5%</u>
< \$35,000	458	367	-91	-4.3%
\$35,000 - \$74,999	945	817	-128	-2.9%
\$75,000 - \$99,999	529	444	-85	-3.4%
\$100,000+	1,589	1,478	-111	-1.4%
<u>45 - 54</u>	<u>4,600</u>	<u>4,615</u>	<u>15</u>	<u>0.1%</u>
< \$35,000	475	424	-51	-2.2%
\$35,000 - \$74,999	1,062	992	-70	-1.4%
\$75,000 - \$99,999	578	556	-22	-0.8%
\$100,000+	2,485	2,643	158	1.2%
<u>55 - 64</u>	<u>3,373</u>	<u>3,631</u>	<u>258</u>	<u>1.5%</u>
< \$35,000	600	589	-11	-0.4%
\$35,000 - \$74,999	851	841	-10	-0.2%
\$75,000 - \$99,999	467	505	38	1.6%
\$100,000+	1,455	1,696	241	3.1%
<u>65 - 74</u>	<u>2,886</u>	<u>2,830</u>	<u>-56</u>	<u>-0.4%</u>
< \$35,000	852	794	-58	-1.4%
\$35,000 - \$74,999	1,078	1,024	-54	-1.0%
\$75,000 - \$99,999	286	278	-8	-0.6%
\$100,000+	670	734	64	1.8%
<u>75+</u>	<u>4,358</u>	<u>4,436</u>	<u>78</u>	<u>0.4%</u>
< \$35,000	2,223	2,109	-114	-1.0%
\$35,000 - \$74,999	1,380	1,445	65	0.9%
\$75,000 - \$99,999	353	384	31	1.7%
\$100,000+	402	498	96	4.4%
TOTAL	20,853	20,665	-188	-0.2%

Source: Claritas, Inc.

Figure A44
Household Age By Income Group: Edina, 2004-2009

rents exceed \$1,500. Vacancies at new properties (built since 1999) are even higher, at 17.1 percent.

New rental apartment development in Edina and the Southdale corridor has been limited; the 100-unit Cornelia Place apartment property represents the only substantial development in recent years. Cornelia Place occupies one of the highest rent tiers in the Twin Cities. Monthly rents range from roughly \$1,300 to \$1,400 for one-bedroom units and from \$1,600-\$2,300 for two-bedroom units. The property's extensive amenities include heated parking spaces, fitness center, meeting rooms, car wash and unit features such as 9' ceilings, ceramic tile finishes, granite countertops, built-in glass cabinets, in-unit washer/dryers and walk-in closets. Open approximately one year, the property maintains an 85 percent occupancy rate.

Other major rental properties in and around the corridor include The Durham, York Plaza, Edina Towers and Cedars of Edina. None of these were built within the last twenty years. Among these, the Durham and Edina Towers achieve rents of \$900 to \$1,500 for one- and two-bedroom units; the other properties occupy a lower rent tier. All of these properties report occupancy rates in the 90 percent range, and most (including Cornelia Place) report that empty nesters and retirees comprise their largest resident components.

Condominium properties (non-age-restricted) in the Southdale Corridor include the Edinborough, Village Homes and Coventry developments. These occupy three different price tiers. At the low end, Edinborough's small (less than 1,000 square feet) units are priced at roughly \$200,000. At the high end, Coventry, located adjacent to Centennial Lakes Park, offers larger (e.g., 3,000 square feet) and more upscale townhouse units, generally priced above \$500,000. Brokers report that the strongest source of demand for these properties include empty nester households as well as some younger singles and couples.

Emerging Factors and Issues

Suburban condominium products have increasingly gained acceptance in the regional market. Townhouse properties such as those in the Corridor have achieved successful absorptions and strong price appreciation, and recent midrise developments with multi-family configurations have proven successful. In the latter category, successful marketing of projects such as Grandview Square (Edina), MarketPlace Lofts (Hopkins), Excelsior and Grand (St. Louis Park), City Bella (Richfield) and Cloud 9 (Minnetonka) demonstrate the market's acceptance of such products.

Both for both condominiums and rental apartments, recent suburban projects have encountered market resistance in the higher price tiers; the depth of these market tiers may be limited.

Transportation Appendix

Study Area Trip Generation

Figure A45 shows the relative magnitude of trip generation from the Greater Southdale study area for the weekday evening commute peak hour for the land uses that currently exist in the area. Evaluation of this trip generation in relation to traffic volumes on the study area street system indicates that trips with one or both ends in the study area account for the majority of travel on the non-freeway street network in the study area. Of these trips approximately 10% would be expected to have both ends in the study area (local trips), while 90% of them would be coming into the study area (for work or shopping) or leaving the study

area (residents going to/from work/shop). Because of the proportionately larger amount of retail in the study area in relation to other uses, the direction of travel in the study area is biased toward outbound or away from the study area in the evening peak. This bias results in an imbalance in traffic flow that places more pressure on the capacity on the outbound side of roadways (e.g. southbound France Avenue at I-494, northbound France at TH 62).

SCENARIO	PM PEAK			PM PEAK - INTERNAL TRIPS			PM PEAK - EXTERNAL TRIPS		
	TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL	IN	OUT
Existing	22,020	8,904	13,115	2,310	1,155	1,155	19,709	7,749	11,960
Future	29,370	12,759	16,611	4,326	2,163	2,163	25,044	10,596	14,448
Change	7,350	3,855	3,496	2,016	1,008	1,008	5,335	2,847	2,488
Percent change	33%	43%	27%	87%	87%	87%	27%	37%	21%

Figure A45
Existing Weekday Evening Commute Peak Hour

Appendix B
Existing & Planned
Land Use, GBA, and
Dwelling Units By
Block

Block		EXISTING							PLANNED											Notes
		Residential Dwelling Units				Retail, Office and Civic GBA			Residential Dwelling Units				Mixed Use Housing Emphasis		Mixed Use Commercial Emphasis		Retail	Office	Civic	
Block Number	Block Acres	Low Density Residential	Medium Density Residential	High Density Residential	Total	Retail	Office	Civic	Low Density Residential	Medium Density Residential	High Density Residential	Total Housing Units (includes Mixed Use Units)	Residential	Commercial (Retail and Office)	Residential	Commercial (Retail and Office)	Retail	Office	Civic	
1	5.56				0							0								Change 2.71 Acres to Office
2*	13.65				0							0								No Change
3	15.81			501	501						501	501								No Change
4*	6.52			306	306						306	306								No Change
5	15.17			328	328						328	328								No Change
6	6.59		12	92	104						297	297								Change to HD Res
7	5.02		14	57	71						226	226								Change to HD Res
8	1.18				0	20,202						0					20,202			No Change
9	12.16				0							547			547	304,000				Change to Mixed Use C
10	8.07				0							363			363	201,750				Change to Mixed Use C
11	9.16				0							0								Add 225,000 SF Office
12	14.06			261	261						261	366			105	58,500				2.34 Acres office changed to Mixed Use C
13	21.71				0							0								Add 200,000 SF Office
14	80.46				0						518	518								Add 11.5 HD Res 45 DU Acre
15	21.49	9			9							967	967	214,900						Change to Mixed Use H
16	6.86				0	25,550						309	309	68,600						Change to Mixed Use H
17	23.83				0	517,478					63	63								Add 1.4 Ac HD Housing or 63 Units
18	7.52				0	8,639						338	338	75,200						Change to Mixed Use H
19	15.16	7	17	148	172	67,165				183		453	270	60,000						Change HD Res to MD Res and other to Mixed Use H
20	14.8				0	211,017						666			666	370,000				Change to Mixed Use C
21	14.85				0							0								New Total 225,000 SF Com.
22*	7.69				0							0								No Change
23	2.92				0							0						51,301		No Change
24	11.57			528	528						528	528								No Change
25	24.96			524	524						921	921								Replace Existing Com.(8.82 Acres) With HD Res
26	18.93				0	156,470						852	852	189,300						Change to Mixed Use H
27	21.88			284	284						445	985	540	120,000						Change HD Res to HD res and Office to Mixed Use C
28	7.64				0	49,684						189			189	104,750				Change theater site (4.9 Acres) to Mixed Use C
29	9.39		189	0	189					189		189								No Change
30	13.42			311	311						311	311								No Change
31*	10.32				0							0							75,000	No Change
32	15.24	20		90	110				20		90	110								No Change
33	11.61			450	450						450	450								No Change
34	6.59			250	250						250	250								No Change
35	12.78											575			575	319,500				Change to Mixed Use C
36	11.87											0								No Change
37	23.69											0								No Change
38	29.24											0								No Change
39	23.56		392	201	593	91,123				392	269	661					91,123			Add 1.5 Acres HD
40	9.41	10			10				10			10								No Change

2* Office is medical office, units are 390 hospital beds	Planned Land Use Assumptions:	
4* Units for PID 29-028-24-21-0011 VOA Care Facility estimated at 48 units (80x200' footprint, 3 floors, 1,000 sf per unit)	Medium Density Residential	20 Du/Acre
22* GBA for PID 32-028-24-21-0001 Library & Gov Center estimated at 140,000 SF	High Density Residential	45 Du/Acre
31* GBA for PID 32-028-24-24-0005 Fire Station estimated at 10,000 SF	Mixed Use, Commercial Emphasis	45 Du/Acre
		25,000 SF/Acre
	Mixed Use, Housing Emphasis	45 Du/Acre
		10,000 SF/Acre