



PLANNING COMMISSION STAFF REPORT

Originator Cary Teague Community Development Director	Meeting Date July 22, 2015	Agenda # VI.B.
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INFORMATION/BACKGROUND

Project Description

Edina Community Lutheran Church is proposing to build a sanctuary and kitchen addition, with a new parking lot on the east side of the existing church located at 4113 West 54th Street. (See the property location on pages A1-A2.) To accommodate this request, the parsonage home would be removed and replaced with the new parking lot. The new sanctuary addition would have the same seating capacity as the existing sanctuary. The purpose of the request is to provide larger fellowship and supportive areas to the church. (See the applicant narrative and plans on pages A3-A33 and in the attached 11 x 17 submittal.)

The site is zoned R-1, Single Dwelling Unit District, where a church is a conditionally permitted use. The applicant submitted a request for the conditional use permit in 2013. (See original plans on pages A34-A40.) The applicant did not move forward with the request after concerns were raised in regard to the architecture of the proposed addition, and impact to the steep slopes and mature trees as a result of a new parking lot south of the building and construction of stormwater ponding.

The applicant has now revised the plans in an attempt to address the concerns raised in 2013. They hired a new architect to design the addition to better fit with the neighborhood. The proposed addition uses a pitched roof rather than a flat roof, and has more variety in building material compared to the lap siding originally proposed. The new plan proposes using an underground storage tank for stormwater, rather than the surface pond proposed in 2013. The new plan would preserve the slope and mature tree area.

The applicants also went through the sketch plan process with the proposed plans and have incorporated the feedback received into the proposed plans.

To accommodate the request, the following is requested:

- Conditional Use Permit to construct a new 210 seat sanctuary, a 576 square foot kitchen and a new parking field.
- A Front Yard Setback variance from 50 feet to 23 for the kitchen to match the existing front yard setback of the church.

SUPPORTING INFORMATION

Surrounding Land Uses

- Northerly: Single-family homes; zoned R-1, Single-Dwelling Unit District and guided Low Density Residential.
- Easterly: Single-family homes; zoned R-1, Single-Dwelling Unit District and guided Low Density Residential.
- Southerly: Minnehaha Creek.
- Westerly: Minnehaha Creek.

Existing Site Features

The subject property is 4.15 acres in size, contains the existing church up close to 54th Street and a parsonage. The site is heavily wooded and slopes down toward Minnehaha Creek.

Planning

- Guide Plan designation: LD – Low Density Residential
- Zoning: R-1, Single-Dwelling Unit District

Site Circulation & Traffic

There are currently two curb cuts to the site; one is to the church parking lot, which is located on the south and west side of the building, and one to the church parsonage. The existing parking lot access point would remain. The parsonage driveway would be removed, but replaced with a new access point to the west to serve as a new parking lot. (See page A2 and A20.) With the proposal of having two parking lots in closer proximity to church entrances, there is a greater likelihood of members parking in the lots and not on adjacent streets. The existing parking lot is difficult to navigate.

A traffic and parking study was done by WSB in 2012 and then updated in 2015. (See pages A61-A72.) The study concludes that there may be a

modest increase in traffic, assuming a 130 to 150 person increase; the existing roads however would support the increase.

Parking

The number of parking spaces required is based on the seating capacity of the largest place of assembly which is the sanctuary. The Code requirement is one stall per three seats. The capacity of the existing and proposed sanctuary is 210 people; therefore, the required number of stalls is 70. A parking variance was granted in 1992 to allow 37 spaces when the church last expanded. The proposed parking lot contains 38 spaces. The variance remains in place with the proposal because the sanctuary proposed has a maximum capacity of 210 people. (See page A41-A49 and A53-A60.) In general, parking has not been a problem for the church. The WSB parking study concludes that the existing parking generally works, and there is not a problem with parking in the neighborhood on adjacent streets.

Sec. 36-1274. Sidewalks, trails and bicycle facilities.

- (a) In order to promote and provide safe and effective sidewalks and trails in the city and encourage the use of bicycles for recreation and transportation, the following improvements are required, as a condition of approval, on developments requiring the approval of a final development plan or the issuance of a conditional use permit pursuant to article V of this chapter:
- (1) It is the policy of the city to require the construction of sidewalks and trails wherever feasible so as to encourage pedestrian and bicycle connectivity throughout the city. Therefore, developments shall provide sidewalks and trails which adjoin the applicant's property:
 - a. In locations shown on the city's sidewalk and trail plan; and
 - b. In other locations where the council finds that the provision of such sidewalks and trails enhance public access to mass transit facilities or connections to other existing or planned sidewalks, trails or public facilities.
 - (2) Developments shall provide sidewalks between building entrances and sidewalks or trails which exist or which will be constructed pursuant to this section.
 - (3) Developments shall provide direct sidewalk and trail connections with adjoining properties where appropriate.
 - (4) Developments must provide direct sidewalk and trail connections to transit stations or transit stops adjoining the property.
 - (5) Design standards for sidewalks and trails shall be prescribed by the engineer.

- (6) Nonresidential developments having an off-street automobile parking requirement of 20 or more spaces must provide off-street bicycle parking spaces where bicycles may be parked and secured from theft by their owners. The minimum number of bicycle parking spaces required shall be five percent of the automobile parking space requirement. The design and placement of bicycle parking spaces and bicycle racks used to secure bicycles shall be subject to the approval of the city engineer. Whenever possible, bicycle parking spaces shall be located within 50 feet of a public entrance to a principal building.
- (b) The expense of the improvements set forth in subsection (a) of this section shall be borne by the applicant.

There is an existing sidewalk on the property. Any damage to the sidewalk during construction would have to be repaired by the applicant.

Bike Racks

The applicant has shown five (5) bicycle parking spots on the site plan, located adjacent to the sidewalk connection to the front entrance. (See page A18.) The City Code requirement for bicycle spots in this instance is three (3)

Landscaping

Based on the perimeter of the site, the applicant is required to have 60 over story trees and a full complement of under story shrubs over the entire site. The site currently contains over 200 over story trees. There would be 22 trees removed in the area where the proposed new parking lots would be building would be located. These trees are mostly Elms. (See pages A23-24, which shows the trees and species to be removed.) The applicant proposes 44 new over story trees, including Honey Locust, Maple and Canadian Hemlock coniferous. (See page A18 and A33.) A full complement of understory landscaping is also proposed around the building.

The Canadian Hemlock will be used for year around screening of the parking lots and trash enclosures from the homes to the south. The existing mature deciduous trees also serve as a buffer to the church. A significant area of landscaping is proposed to provide screening from the closest resident to the east. Plantings in this area include year around screening with a row of 8-foot Techny Arborvitae along the lot line; a row of ornamental trees (Serviceberry); and a row of shrubs. (See page A18.) The parking lot is also broken up with landscape islands with shrubs and Maples.

Trash Enclosures

The trash enclosure is located on the west side of the building in the west parking lot. (See page A26a.) The enclosure is to be a six-foot tall wood cedar fence that would be stained a color to match the church. The trash area would be screened by the enclosure, the coniferous plantings and the existing trees. (See page A18.)

Grading/Drainage/Utilities

The city engineer has reviewed the proposed plans and found them to be acceptable subject to the comments and conditions outlined on the attached pages A76-A77. As mentioned earlier, the proposed grading and drainage plan is a big improvement over the previously submitted plans, which proposed to remove trees in the slope area toward the creek to provide stormwater ponding. The applicant has revised the plans to create an underground storage tank under the parking lot, and preserve the trees and slope area. Additionally the east parking ramp would be made of permeable pavers. (See page A20.)

Any approvals should be conditioned on the conditions outline in the director of engineering's memo dated July 14, 2015. The grading and drainage plans are also subject to review and approval of the Minnehaha Creek Watershed District.

Building/Building Material

The building would be constructed of cementitious siding, and stone to match the existing building. Stone from the parsonage would be reused on the kitchen addition. (See renderings on pages A10-A17.) The addition has be redesigned to better match the existing church and fit into the neighborhood. (See originally proposed addition on page A39-A40.) A materials board will be presented at the Planning Commission and City Council meetings.

Historical Review

The Planning Commission requested that the city's historical preservation consultant, Robert Vogel conduct a review of the existing parsonage that is to be removed. Mr. Vogel has provided that review and is attached to this report on pages A73-A75. The review concludes that the church rectory is neither historically nor architecturally significant. Therefore, it does not meet the Edina Heritage Landmark eligibility criteria. No further cultural resource management work is recommended.

The home was built in 1948, about the same time as the church. The home has not be used a residence in several years and has fallen into a very poor

state of repair. As mentioned, the exterior stone on the home would be used on the church addition.

Conditional Use Permit

Per Section 36-305, a conditional use permit shall meet the following:

1. Does not have an undue adverse impact on governmental facilities, utilities, services or existing or proposed improvements;
2. Will generate traffic within the capacity of the streets serving the property;
3. Does not have an undue adverse impact on the public health, safety or welfare;
4. Will not impede the normal and orderly development and improvement of other property in the vicinity;
5. Conforms to the applicable restrictions and special conditions of the district in which it is located, as imposed by this chapter; and
6. Is consistent with the comprehensive plan.

Staff believes that the above criteria are met. A church has been located on this site since the 1940's; and a 210 seat church has been located there since 1992. The site has not had an adverse impact on governmental facilities, utilities or services. The existing roads do, and can support the site. The improvements to the site would not impact development or improvement to property in the area. With the exception of the front street setback variance for the kitchen addition to match the existing setback of the church, the proposal meets all zoning ordinance requirements. A variance for the number of parking stalls has already been granted for the site, and has not been an issue in the past. The use is consistent with the comprehensive plan, and the use is conditionally permitted in the R-1 zoning district, in which this property is located.

The City may impose, conditions and restrictions upon the establishment, location, construction, maintenance, operation or duration of the use, as deemed necessary for the protection of the public interest and adjacent properties, to ensure compliance with the requirements of this chapter and other applicable provisions of this Code, and to ensure consistency with the comprehensive plan.

Compliance Table

	City Standard (R-1)	Proposed
<u>Building Setbacks</u> Front – 54 th Street Side – East Side – West Rear – Creek	50 feet 50 feet 50 feet 50 feet	23 feet* 104 feet 100+ feet 100+ feet
Building Height	3 Stories or 40 feet whichever is less	One story 36 feet tall
Building Coverage	25%	15%+/-
Parking Stalls (Site)	70 required stalls for the sanctuary maximum seating capacity of 210 seats	38 spaces* proposed (37 existing)

* Existing Condition – Variance Granted in 1992

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issues

- **Is the proposed Front Street Setback Variance Justified?**

Yes. Per the Zoning Ordinance, a variance should not be granted unless it is found that the enforcement of the ordinance would cause practical difficulties in complying with the zoning ordinance and that the use is reasonable. As demonstrated below, staff believes the proposal does meet the variance standards, when applying the three conditions.

Minnesota Statutes and Edina Ordinances require that the following conditions must be satisfied affirmatively. The Proposed Variance will:

- 1) *Relieve practical difficulties that prevent a reasonable use from complying with ordinance requirements.***

Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. “Practical difficulties” may include functional and aesthetic concerns.

Staff believes the proposed variance is reasonable. The practical difficulty is the original placement of the church and subsequent additions. (See page A18.) The church is located closer to 54th street to lessen the

impacts to the slope and mature trees located toward Minnehaha Creek. The location of the kitchen therefore, to match the existing front street setback of the church is reasonable. The kitchen addition would be constructed using the stone from the parsonage to match the stone on the existing church.

2) *There are circumstances that are unique to the property, not common to every similarly zoned property, and that are not self-created?*

Yes. The unique circumstance is the location of the existing church which also has a 23-foot setback from 54th Street. This circumstance is not common to every similarly zoned property.

3) *Will the variance alter the essential character of the neighborhood?*

No. The proposed improvements will not alter the essential character of the neighborhood. The kitchen addition is relatively small compared to the size of the church. It matches the existing setback of the church. A church has been located on this property for over 50 years.

• Are the plans proposed reasonable to minimize impacts for the conditionally permitted Use?

Yes, the applicant has minimized impacts on the neighborhood with the revisions that have been made to the plans. Specifically the applicant has:

- Minimized impacts to the slopes and trees by solving the stormwater ponding issue by using an underground system.
- Located the sanctuary addition off the back of the existing church, so not to have so much of the mass of the church on 54th Street.
- Increased the buffer to the home to the east, and provided extensive landscaping to screen the church and the parking lot.
- Provided additional year-round screening south of the improvements to minimize impacts to the south across Minnehaha Creek.
- Proposed downward lighting, and submitted a plan that demonstrates all minimum lighting requirements are met.
- Proposed to use brick from the existing parsonage to match the existing brick on the church.
- Proposed a pitched roof to be in character with the single-family homes in the area, rather than the flat roof previously proposed.

The City may impose, conditions and restrictions upon the establishment, location, construction, maintenance, operation or duration of the use, as deemed necessary for the protection of the public interest and adjacent properties, to ensure compliance with the requirements of this chapter and

other applicable provisions of this Code, and to ensure consistency with the comprehensive plan. Therefore, these items will be made conditions of approval to minimize impacts in the area.

Staff Recommendation

Recommend that the City Council approve the Conditional Use Permit and Front Yard Setback Variance from 50 feet to 23 feet for a new church sanctuary and kitchen at 4113 54th Street West for Edina Community Lutheran Church.

Approval is subject to the following findings:

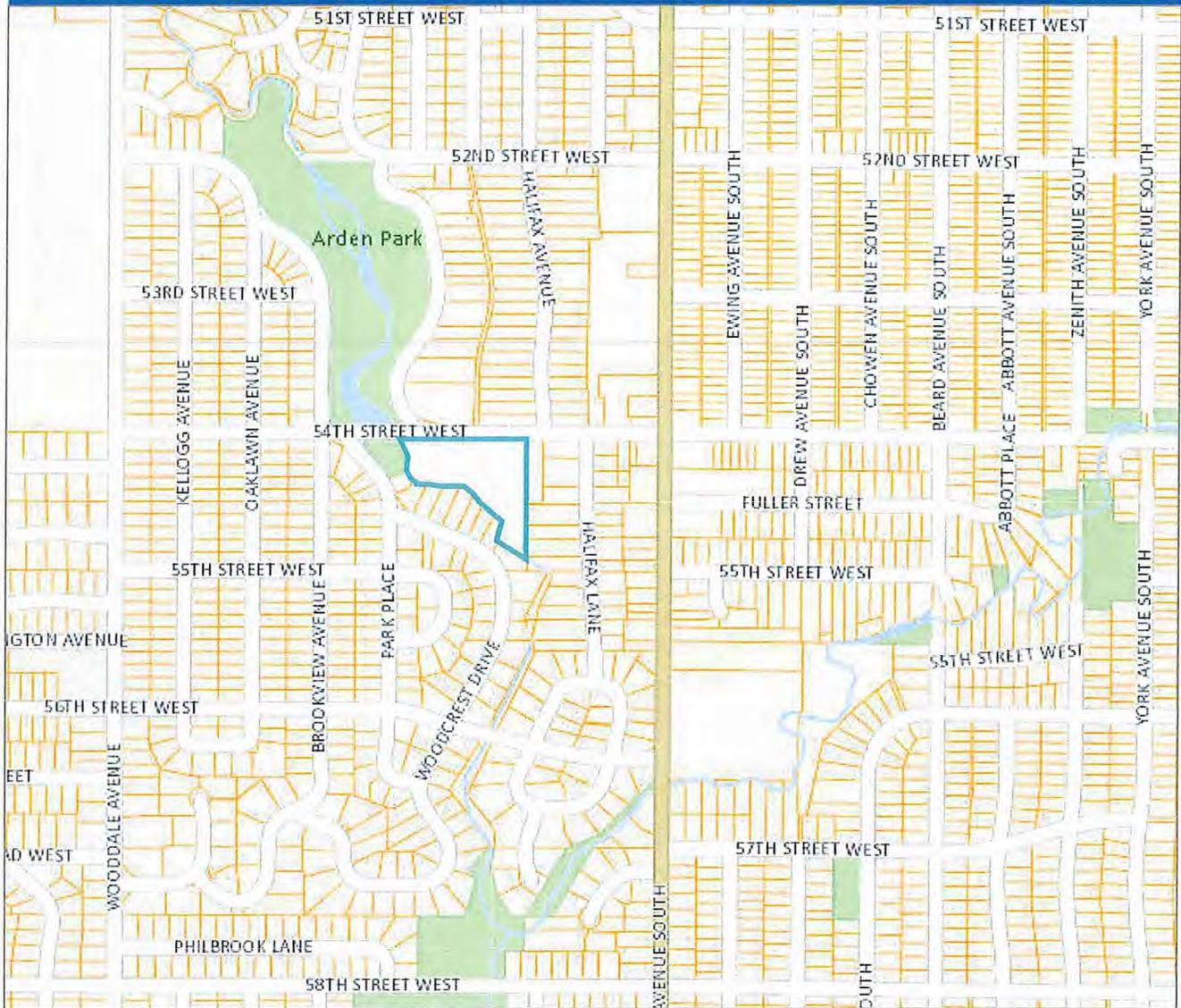
1. The proposal meets the conditional use permit criteria in Section 36-305 of the City Code as follows:
 - a. Does not have an undue adverse impact on governmental facilities, utilities, services or existing or proposed improvements;
 - b. Will generate traffic within the capacity of the streets serving the property;
 - c. Does not have an undue adverse impact on the public health, safety or welfare;
 - d. Will not impede the normal and orderly development and improvement of other property in the vicinity;
 - e. Conforms to the applicable restrictions and special conditions of the district in which it is located, as imposed by this chapter; and
 - f. Is consistent with the comprehensive plan.
2. The proposal would meet the required standards for a variance, because:
 - a. The proposed use of the property is reasonable; the use is conditionally permitted, and the kitchen addition has the same front street setback of the existing church at 23 feet.
 - b. The practical difficulty and unique circumstance is the location of the existing church, which does not meet the required front street setback.
 - c. The kitchen addition would be constructed out of the same stone that was used on the church. The stone from the parsonage would be used on the kitchen.

- d. The proposed improvements will not alter the essential character of the neighborhood. The kitchen addition is relatively small compared to the size of the church. It matches the existing setback of the church. A church has been located on this property for over 50 years.

Final approval is subject to the following Conditions:

1. Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, unless modified by the conditions below:
 - Site plan date stamped June 25, 2015.
 - Grading plan date stamped June 25, 2015.
 - Landscaping plan date stamped June 25, 2015.
 - Building elevations date stamped June 25, 2015
 - Lighting plan date stamped June 25, 2015
 - Utility Plan date stamped June 25, 2015
 - Building materials board as presented at the Planning Commission and City Council meeting.
2. The property owner is responsible for replacing any required landscaping that dies.
3. All trash enclosures shall be screened to meet City Code.
4. Submit a copy of the Minnehaha Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
5. Compliance with all of the conditions outlined in the director of engineering's memo dated July 14, 2015.

Deadline for a city decision: No deadline



Parcel ID: 19-028-24-11-0010

Owner Name: Edina Com Luth Ch

Parcel Address: 4113 54th St W
Edina, MN 55424

Property Type: Residential

Parcel Area: 4.15 acres
180,764 sq ft

A-T-B: -

Code

Map Scale: 1" ≈ 800 ft.

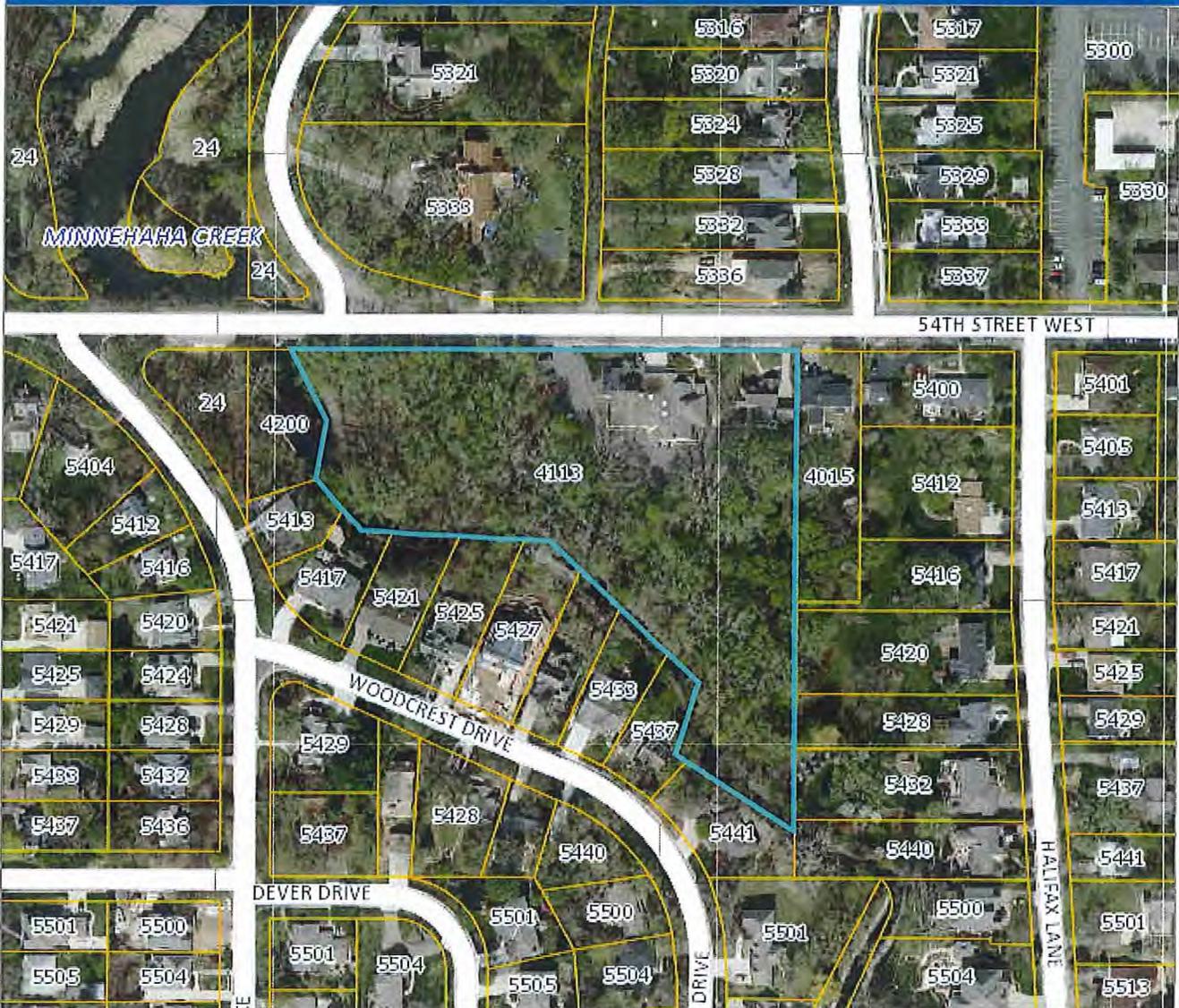
Print Date: 5/6/2015



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Parcel ID: 19-028-24-11-0010

Owner Name: Edina Com Luth Ch

Parcel Address: 4113 54th St W
Edina, MN 55424

Property Type: Residential

Home-stead: Non-Homestead

Parcel Area: 4.15 acres
180,764 sq ft

Map Scale: 1" ≈ 200 ft.

Print Date: 5/6/2015



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BENTZ / THOMPSON / RIETOW

ARCHITECTURE · URBAN DESIGN · PLANNING · INTERIORS

June 24, 2015

EDINA COMMUNITY LUTHERAN CHURCH ADDITION & RENOVATION (CUP Explanation of Request)

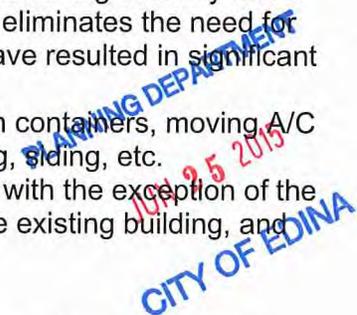
Edina Community Lutheran Church (ECLC) located at 4113 W. 54th St in Edina, MN, hopes to expand their existing church facility in order to better serve their congregation and the surrounding neighborhood and larger community.

The proposed project consists of an addition and remodeling of the existing Edina Community Lutheran Church. The Church has been operating at this site for 67 years as a productive member of the Edina and Twin Cities community and this project will allow them to continue to worship on this site into the future.

The existing sanctuary is approved to seat up to 210 people although on a typical Sunday the sanctuary holds only 175 chairs. Both the sanctuary and fellowship hall are undersized and crowded before, during and after services. The new sanctuary, while also capable of seating 210, provides more space to circulate, process and accommodate choirs and musicians. Classroom and meeting space is expanded. The kitchen serving the fellowship hall is undersized to provide for the needs of funerals, weddings, or community celebrations. Staff members' offices, which currently are on both levels, will be consolidated.

In addition to building a new sanctuary and remodeling existing space, the congregation is proposing:

- 1) **Making the building safer, more accessible and more energy efficient** by adding its first fire sprinkler system, improving energy efficiency with new windows, doors, light fixtures, improved HVAC and additional insulation. The entire project is being planned to closely follow sustainable guidelines.
- 2) **Improving accessibility and livability of streets** by upgrading its elevators and adding accessible door openers, moving the bike parking to the street side of the building and providing an easement to the City to build a sidewalk on the north side of 54th from the west church driveway to the creek. With location on a street marked with bike lanes that is recognized as a key connector between bike routes, combined with a nearby Metro Transit bus stop and parking on both sides of the street, access to the church is broad.
- 3) **Providing stormwater management** where there is very little today, resulting in improved water quality in Minnehaha Creek. The Church property includes a parsonage which no longer serves the needs of the congregation and is slated to be demolished. The resulting large open space adjacent to the east side of the church provides space to add more off-street parking but also to locate an integrated underground storm water management system below the parking lot. While this approach is significantly more expensive than the church's initial proposal two years ago, it eliminates the need for large infiltration ponds in the church's woods, which would have resulted in significant tree removal.
- 4) **Improving aesthetics** by screening the parking lot and trash containers, moving A/C condensers to the roof, repairing or replacing exterior fencing, siding, etc.
- 5) **Zoning code requirements** of the property will be followed, with the exception of the front yard setback which will match the current setback of the existing building, and parking.



Off-street parking is limited by useable buildable area on the heavily wooded site. There is no need to apply for a parking variance, according to Mr. Teague and the city attorney because: 1) the number of approved seats in the sanctuary is not changing; 2) no parking complaints have been received in the 23 years since the City granted the 1992 parking variance and proof-of-parking agreement; and 3) the average Sunday attendance figures have not changed significantly since the Traffic Study performed in 2012 (see 2015 update).

A previous addition and remodeling proposal in 2013 was met with resistance from neighbors and City staff for two primary reasons: 1) the perception of the sanctuary design was that it was too modern and too close to the east neighbors; and 2) the parking lot expansion needed to accommodate the design would have required the removal of too many trees in the Church's wooded area.

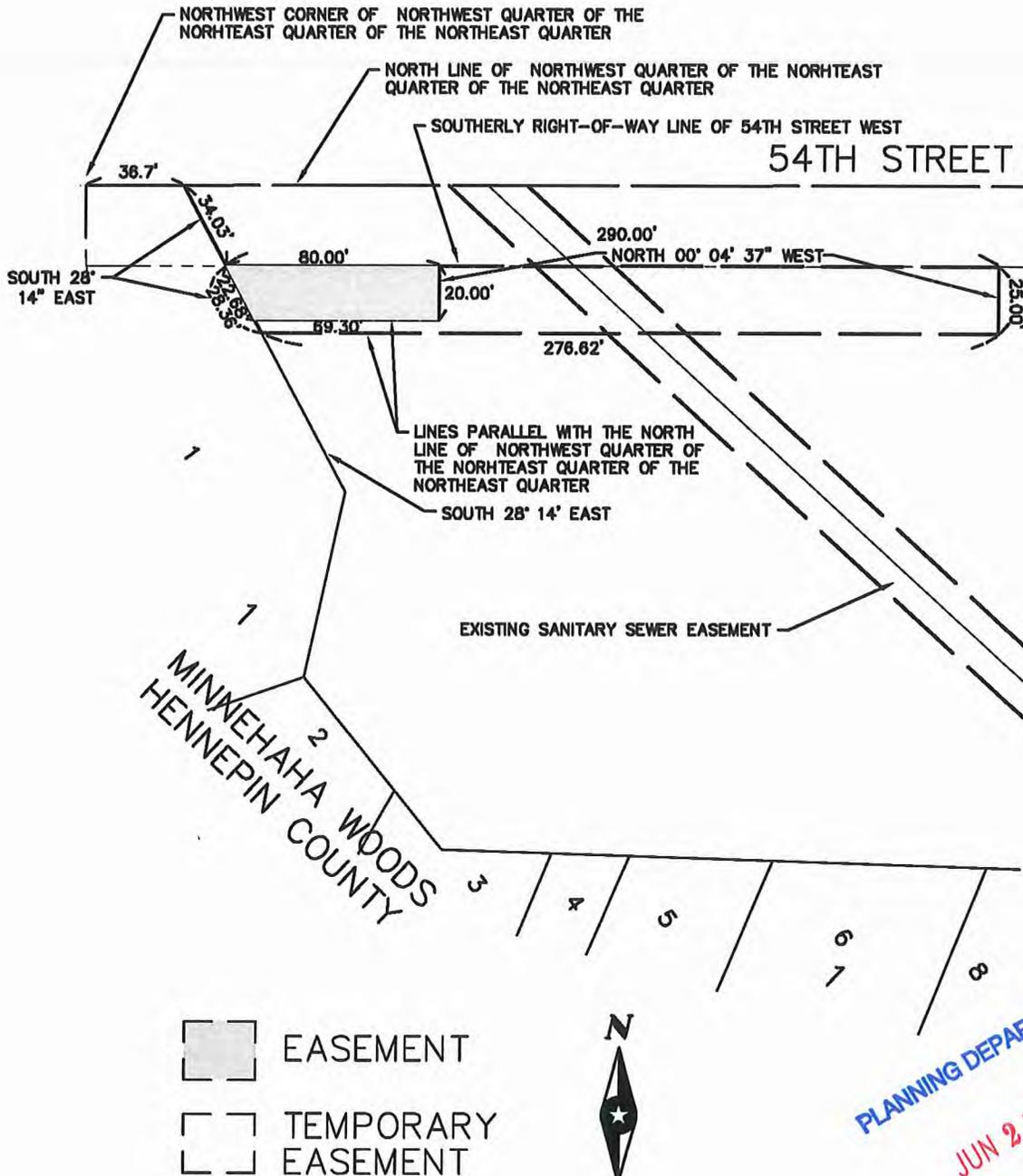
The new addition is designed to fit the neighborhood feel and to take advantage of the view of the woods. It extends to the south rather than toward the eastern neighbors. Because the design creates a second, separate parking lot, cars do not need to be routed across the back of the building, resulting in the removal of far fewer trees. The congregation's consulting arborist has been advising them since 2012 and is working with them on the Tree Preservation Plan submitted here. The congregation's Woods & Creek Task Force is looking at ways to preserve the woods long-term through approaches such as land trust or conservation easement.

At this time, ECLC asks that the City of Edina approve this request as the congregation is not changing the intended use of the site or building, but improving the use that has existed since 1948. The Church is asking to remodel and add space to provide improvements within their facility that assure compliance with current building and accessibility codes, are up to date, and visually integrated with the original building and the surrounding residential neighborhood.

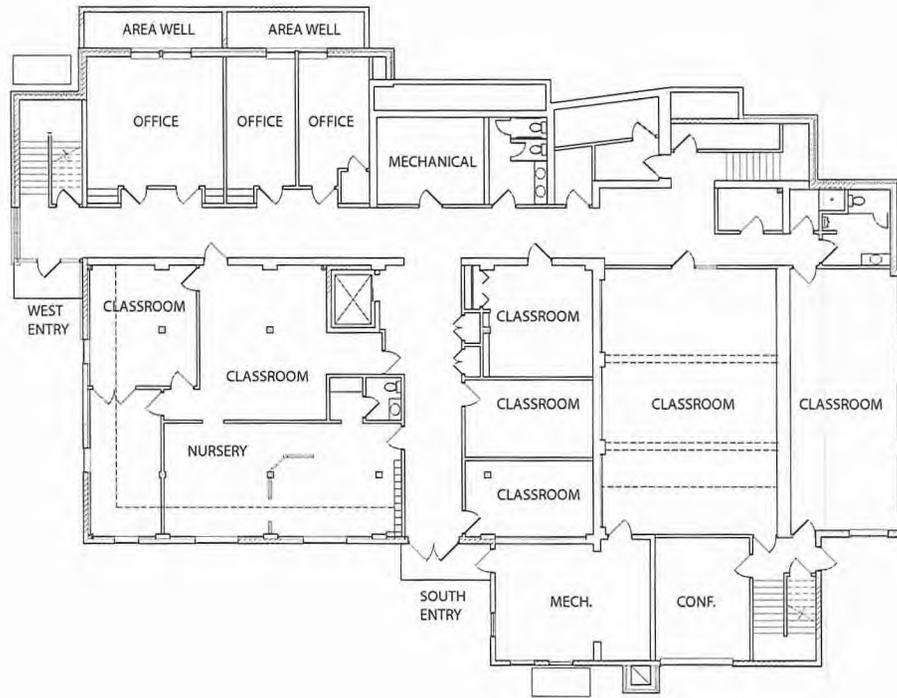
**Depiction of Easement Areas Granted to the City of Edina
by Edina Community Lutheran Church**

Effective June 2, 2015

1. Permanent Utility Easement
2. Temporary Construction Easement; expires November 1, 2016.



15



AC

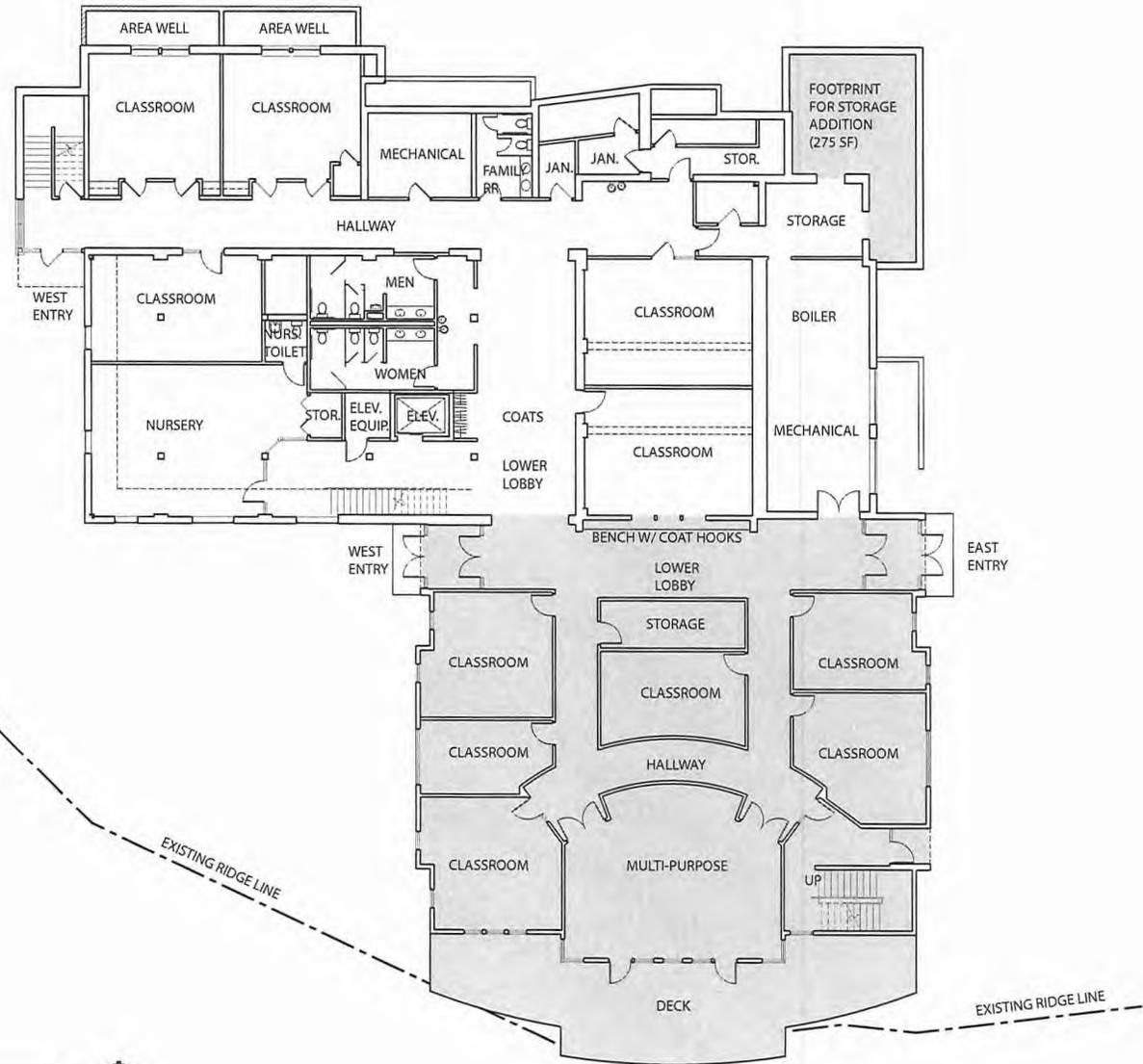
Lower Level Existing
Edina Community Lutheran Church



PLANNING DEPARTMENT
JUN 15 2015
CITY OF EDINA

EXISTING RIDGE LINE

EXISTING RIDGE LINE

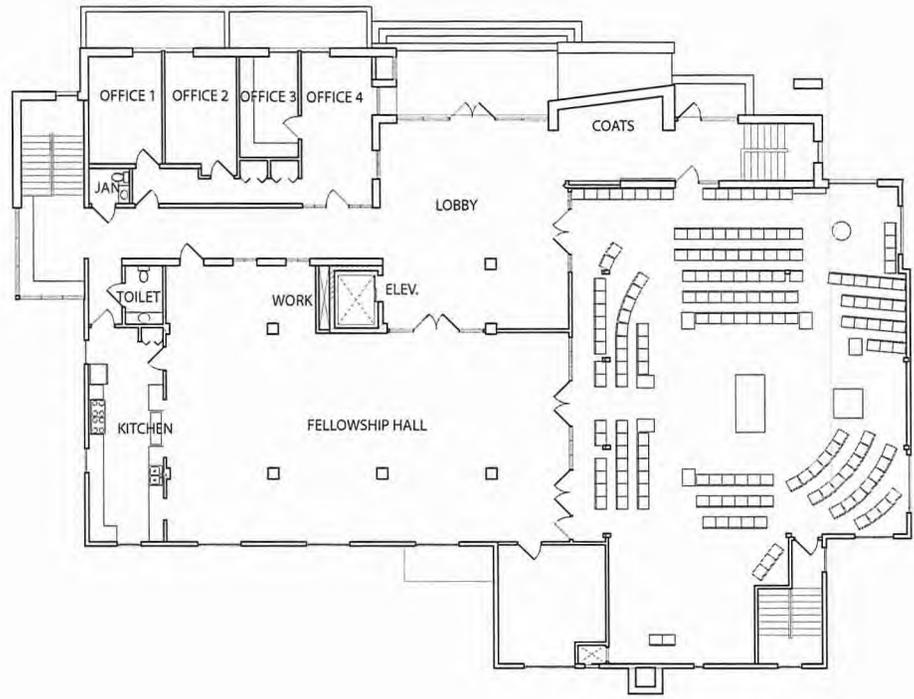


K7

Lower Level - Addition
Edina Community Lutheran Church



PLANNING DEPARTMENT
CITY OF EDINA
JUN 28 2015

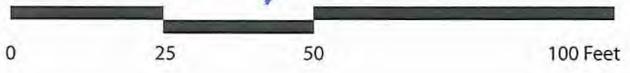


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PLANNING DEPARTMENT
JUN 25 2015
CITY OF EDINA

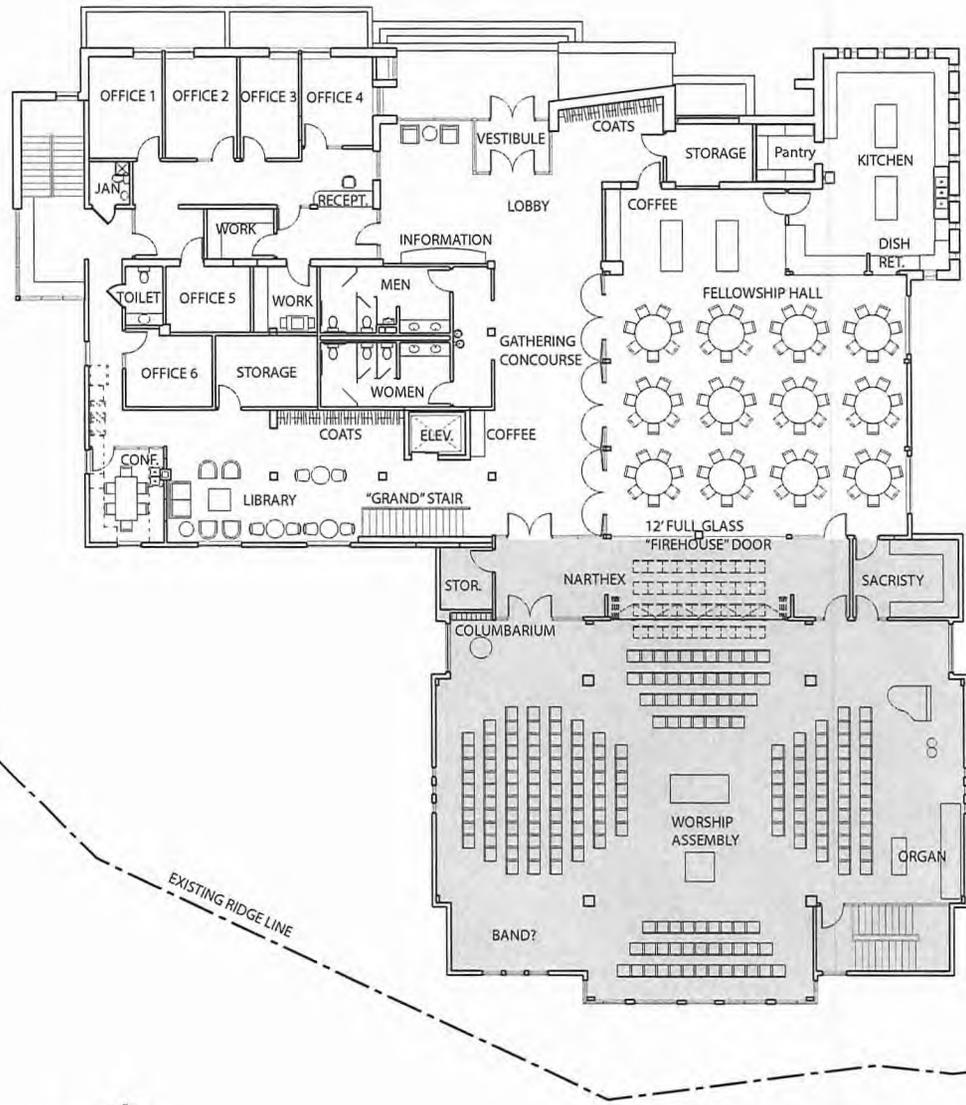
Upper Level - Existing

Edina Community Lutheran Church



EXISTING RIDGE LINE

EXISTING RIDGE LINE



KITCHEN ADDITION (275 SF)

4,446 SF ADDITION

A9

PLANNING DEPARTMENT
 JUN 25 2014
 CITY OF EDINA

Upper Level- Addition
 Edina Community Lutheran Church



EXISTING RIDGE LINE

EXISTING RIDGE LINE



PLANNING DEPARTMENT

JUN 25 2015

CITY OF EDINA

11Y



NORTHELEVATION
Edina Community Lutheran Church

PLANNING DEPARTMENT
JUN 25 2015

CITY OF EDINA

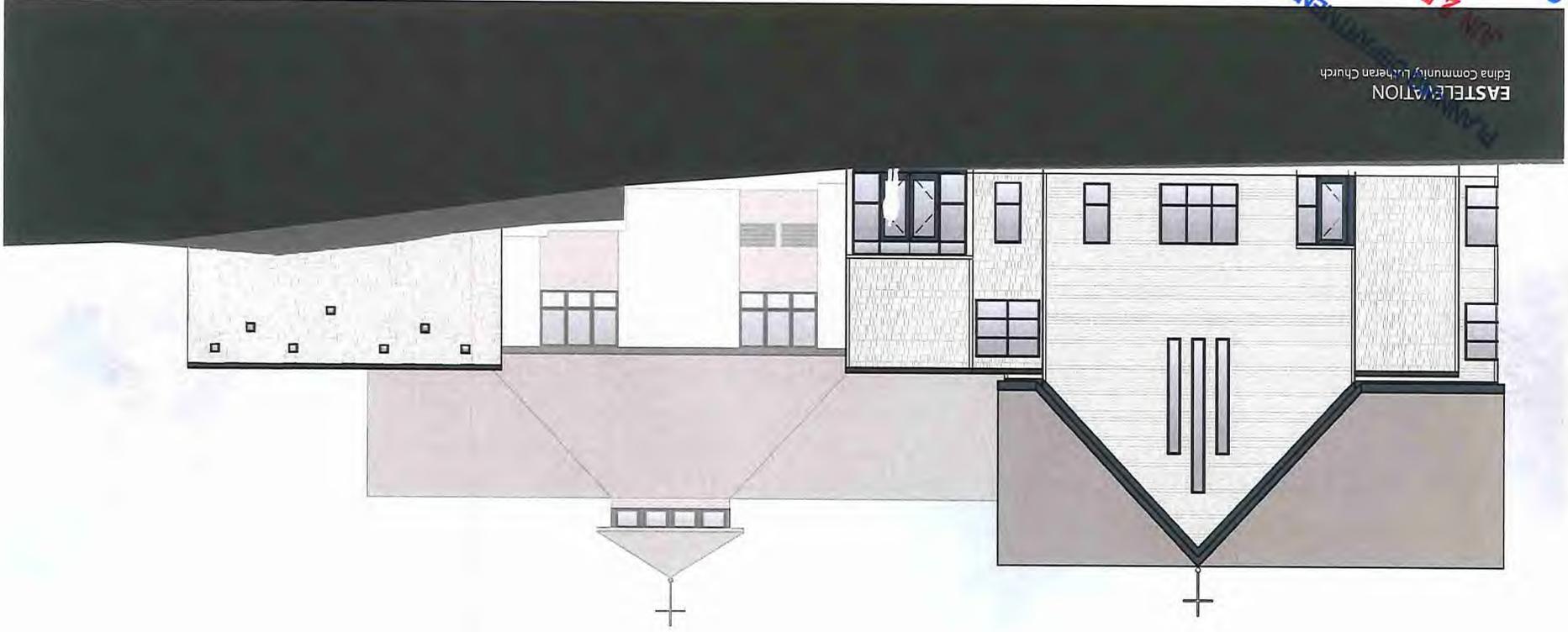
CITY OF EDINA

JUN 25 2015

PLANNING DEPARTMENT

Edina Community Lutheran Church

EAST ELEVATION



A12

A13



SOUTH ELEVATION
Edina Community Lutheran Church

PLANNING DEPARTMENT

JUN 25 2015

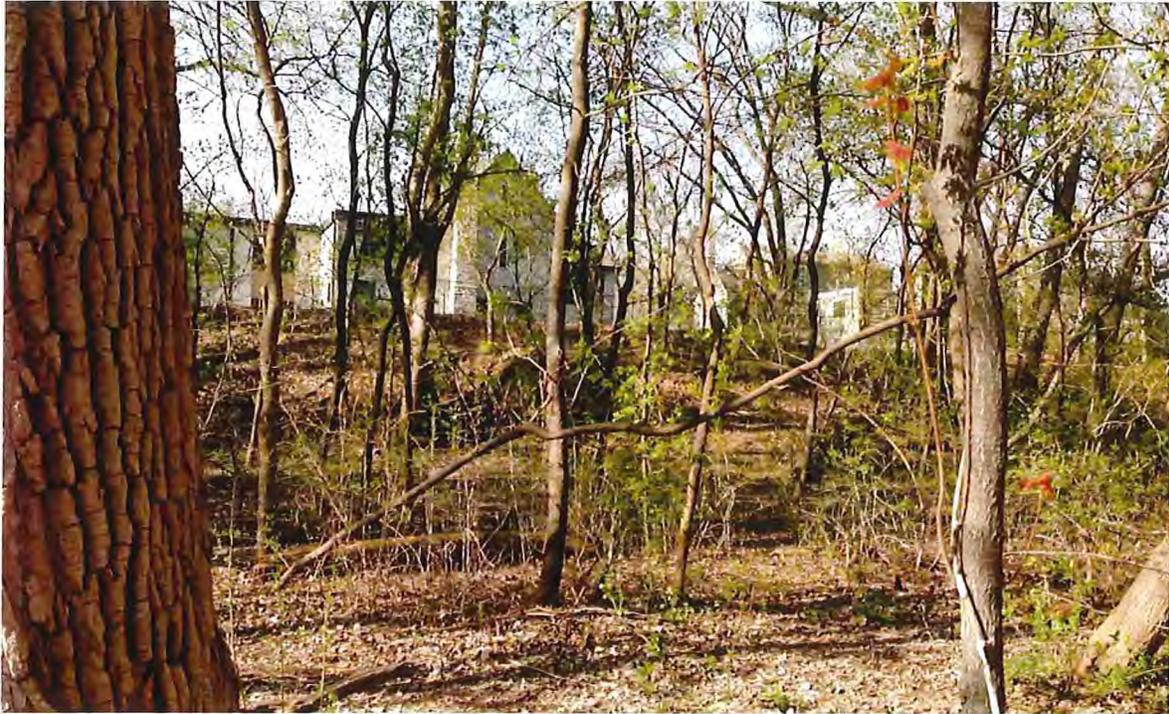
CITY OF EDINA

A14



WEST ELEVATION
Edina Community Lutheran Church

PLANNING DEPARTMENT
JUN 25 2015
CITY OF EDINA



CREEK VIEW: BEFORE

PLANNING DEPARTMENT

JUN 25 2015

CITY OF EDINA



CREEK VIEW: AFTER

A15



54TH ST, LOOKING WEST: BEFORE

PLANNING DEPARTMENT

JUN 25 2015

CITY OF EDINA



54TH ST, LOOKING WEST: AFTER

A16



54TH ST, LOOKING EAST: BEFORE

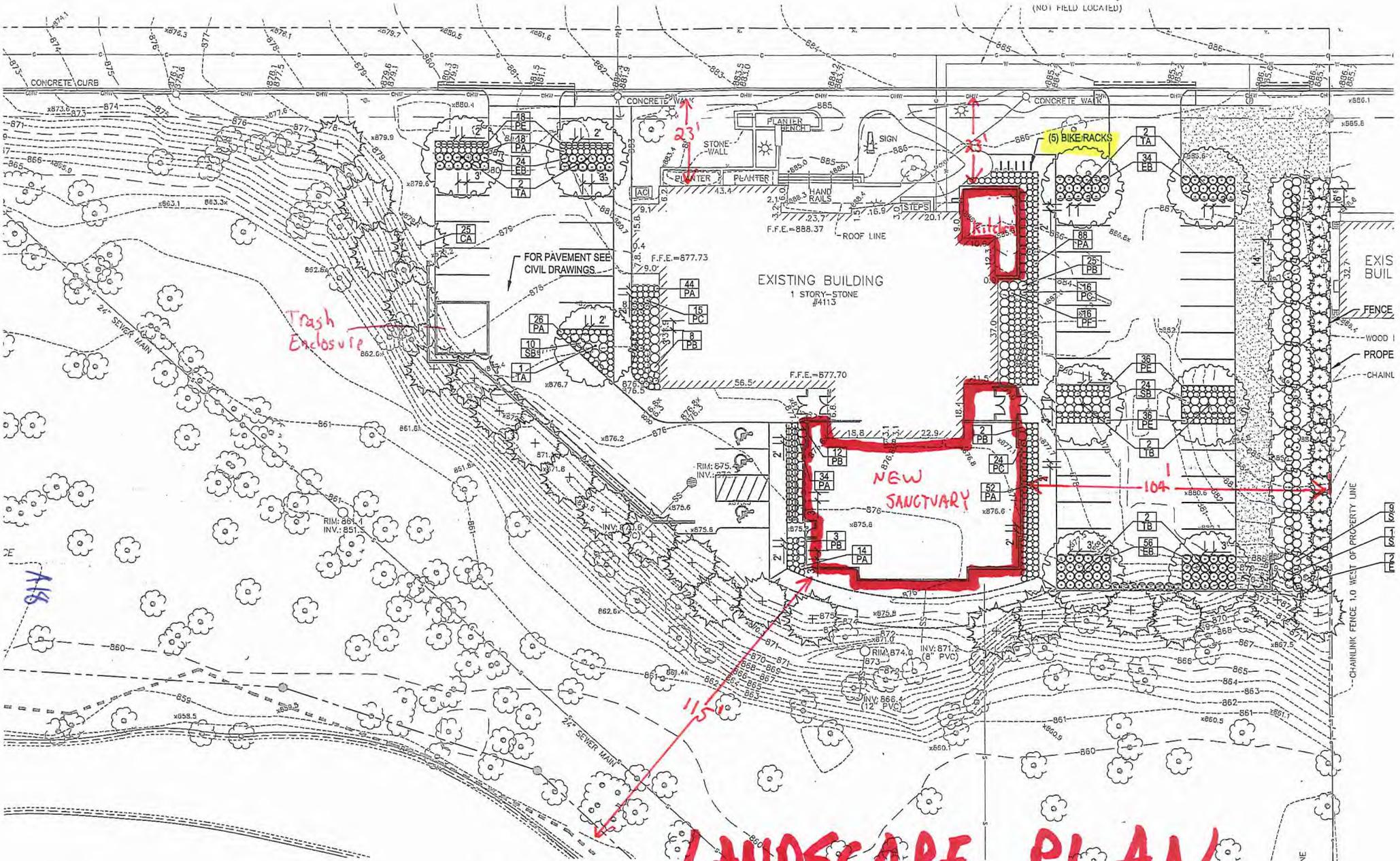
PLANNING DEPARTMENT
JUN 25 2015
CITY OF EDINA



54TH ST, LOOKING EAST: AFTER

A17

(NOT FIELD LOCATED)



LANDSCAPE PLAN

NOTES:

ALL PLANTED AREAS TO HAVE AN IRRIGATION SYSTEM. CONTRACTOR TO SUBMIT IRRIGATION PLAN.

PLANT SCHEDULE:

KEY	QTY	BOTANICAL NAME	COMMON NAME	ROOT CONT.	SIZE	SPECIFICATIONS
	9	DECIDUOUS SHADE TREES				
TA	5	GLEDITSIA TRIACANTHOS VAR. INERMIS	HONEYLOCUST, SKYLINE	B&B	2.5" CAL.	12'-14' HT., TRUNK FREE OF BRANCHES 6'-7'
TB	4	ACER X FREEMANII 'JEFFERSRED'	MAPLE, AUTUMN BLAZE	B&B	2.5" CAL. OR CLUMP	14'-16' HT., TRUNK FREE OF BRANCHES 6'-7'
	10	ORNAMENTAL TREES				

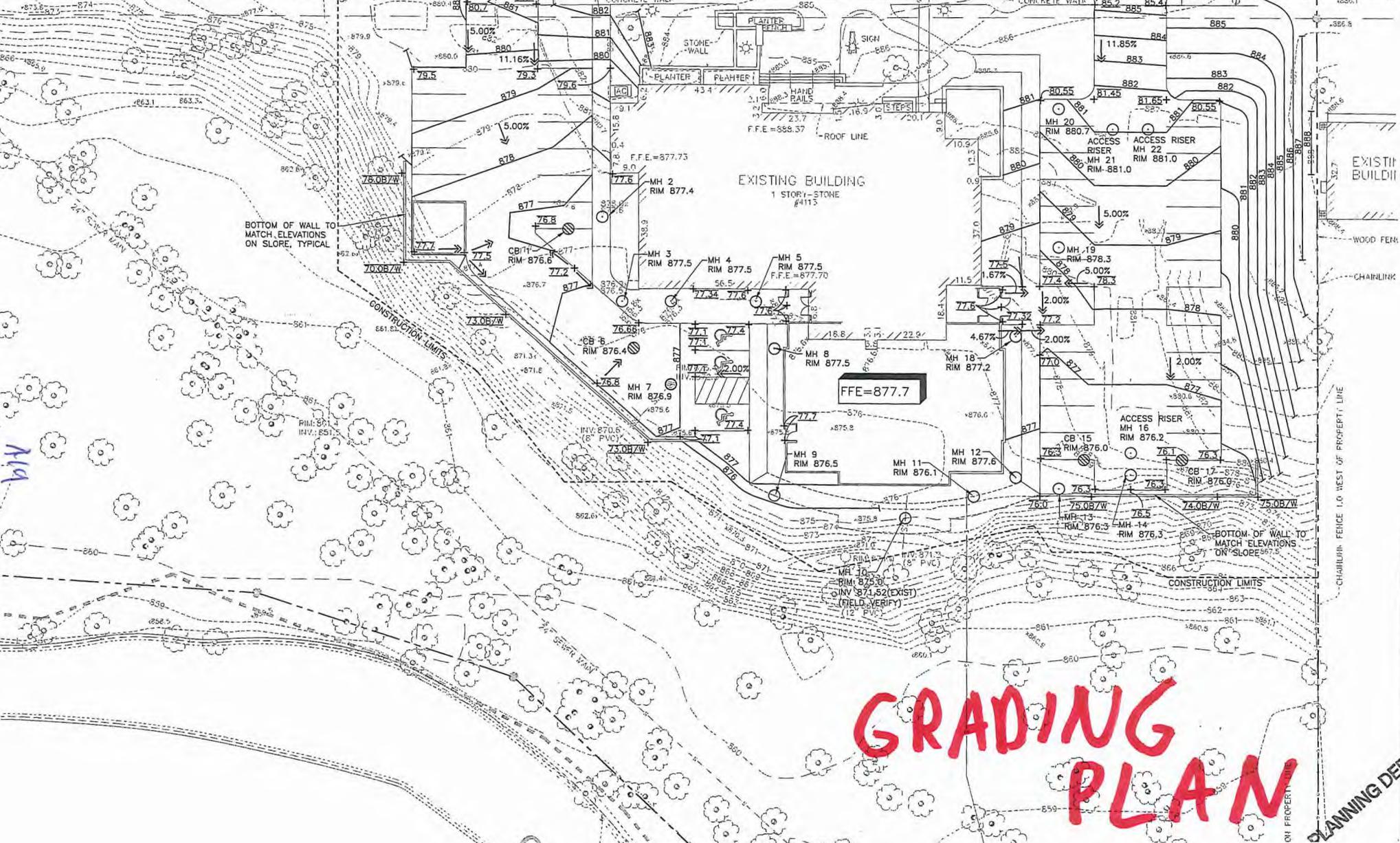
WEST 54th STREET

CONCRETE CURB

LOCATION AS SCALED FROM DRAWING
PROVIDED BY THE CITY OF EDINA
(NOT FIELD LOCATED)

CONSTRUCTION LIMITS

CONCRETE CURB



EXISTING BUILDING
1 STORY - STONE
#4113

FF.E=877.7

BOTTOM OF WALL TO
MATCH ELEVATIONS
ON SLOPE, TYPICAL

BOTTOM OF WALL TO
MATCH ELEVATIONS
ON SLOPE

GRADING PLAN

M19

ON PROPERTY LINE
PLANNING DEPT

WEST 54th STREET

PROPOSED BITUMINOUS PAVEMENT,
MATCH EXISTING ALONG SAWCUT
LINE, TYPICAL SEE DETAILS

CONCRETE CURB

LOCATION AS SCALED FROM DRAWINGS
PROVIDED BY THE CITY OF EDNA.
(NOT FIELD LOCATED)

TAPER CURBS TO BE FLUSH
WITH PAVEMENT, TYPICAL, SEE
DETAILS

PROPOSED #612 CURB AND
GUTTER, MATCH EXISTING ALONG
SAWCUT LINE, TYPICAL, SEE DETAILS

PROPOSED CONCRETE SIDEWALK,
MATCH EXISTING ALONG SAWCUT
LINE, TYPICAL, SEE DETAILS

PROPOSED CONCRETE
DRIVEWAY APRON, MATCH
EXISTING ROADWAY AND
GUTTER, TYPICAL, SEE
DETAILS

CONCRETE WALL

CONCRETE WALL

PROPOSED CONCRETE
CURB AND GUTTER

PROPOSED BITUMINOUS
PAVEMENT,
TYPICAL, SEE
DETAILS

EXISTING BUILDING

WOOD FENCE

CHAINLINK FENCE

ADD ALTERNATE
PERMEABLE PAVERS,
SEE DETAIL 6/C701

CHAINLINK FENCE 1.0 WEST OF PROPERTY LINE

CORNER ON PROPERTY LINE

PLANNING DEPT

JUL

PROPOSED CONCRETE
DRIVEWAY APRON, MATCH
EXISTING ROADWAY AND
GUTTER, TYPICAL, SEE
DETAILS

STONE WALL

PLANTER

EXISTING BUILDING
1 STORY - STONE
#4115

PROPOSED CONCRETE
CURB AND GUTTER

10' CURB TAPER

SIDEWALK FLUSH WITH
PAVEMENT

FFE=877.7

INV: 870.6
(8" PVC)

END CURB

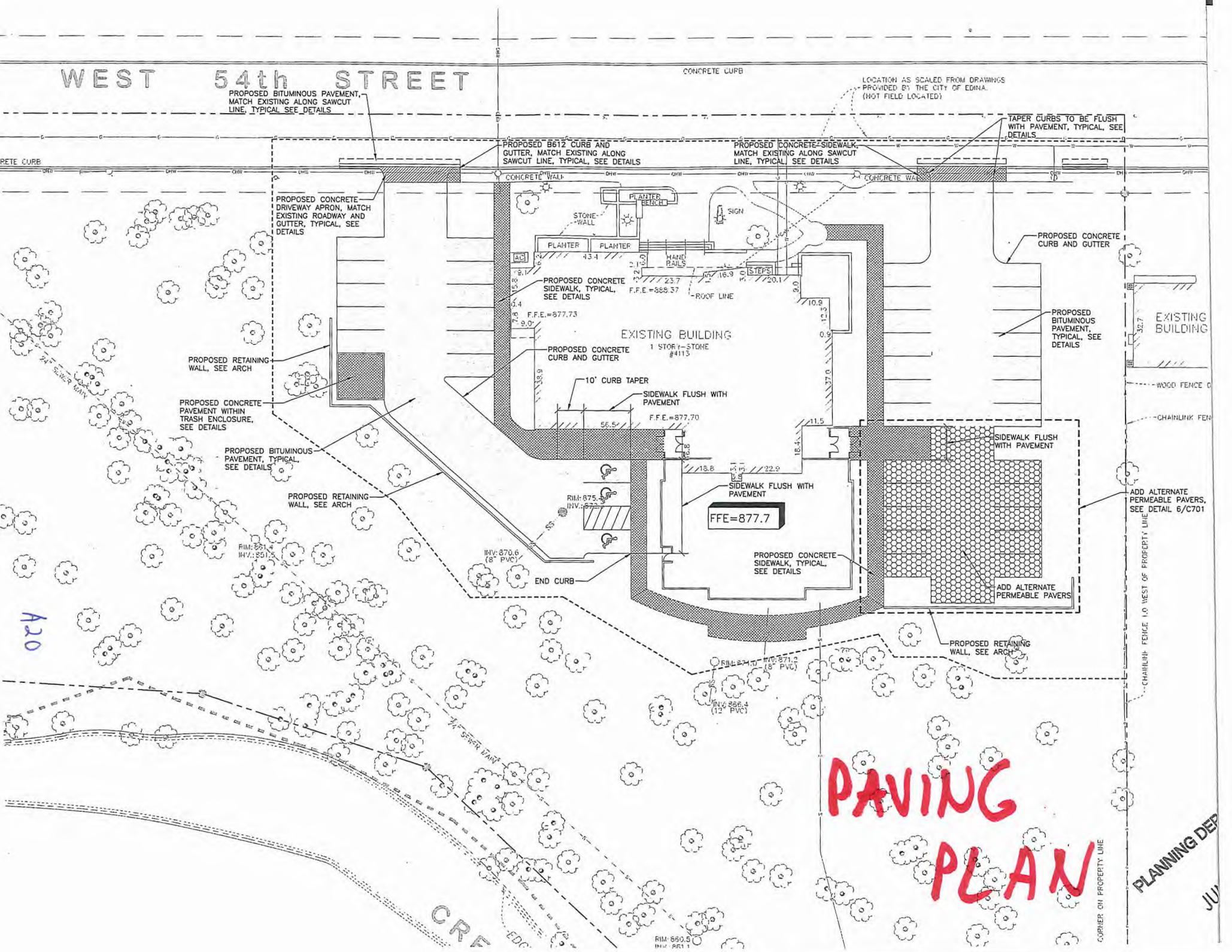
RIM: 871.0
INV: 871.2
(8" PVC)

INV: 866.4
(12" PVC)

RIM: 850.5
INV: 851.1

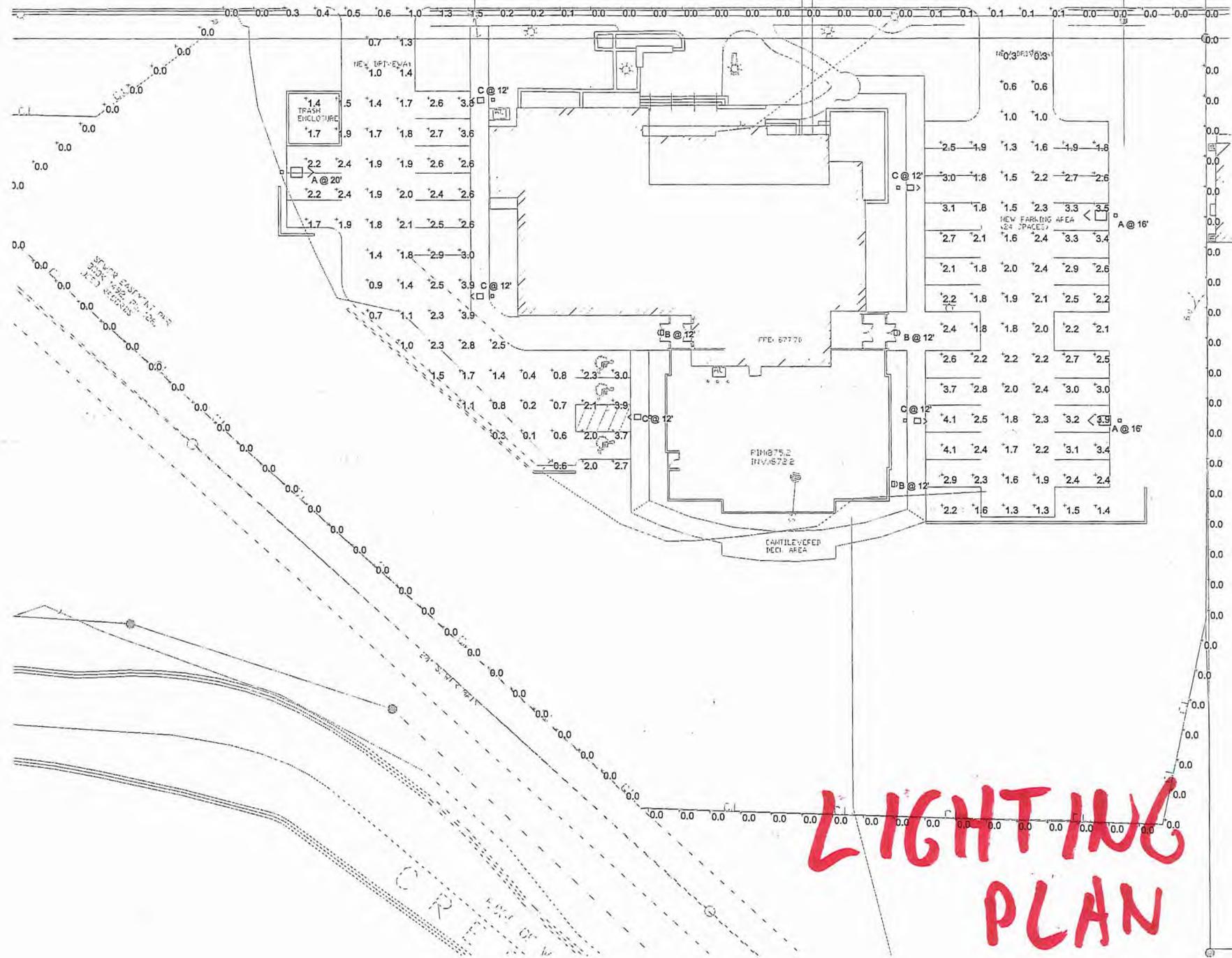
A20

PAVING PLAN



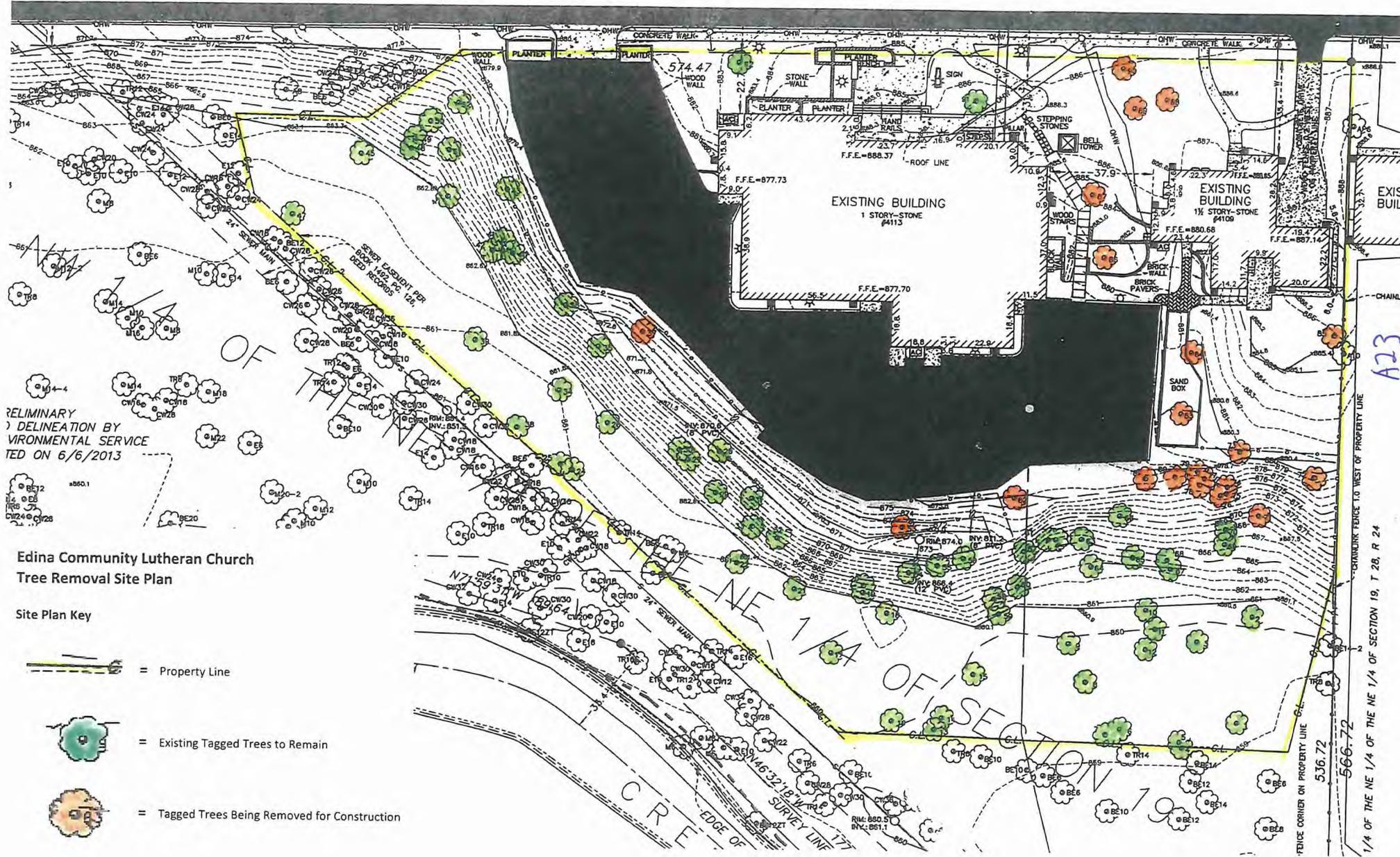
LOCATION SCALED FROM DRAWING PROVIDED BY THE CITY OF EDINA
UNIT FIELD LOCATED

WEST 54+11 FEET



LIGHTING PLAN

A22



PRELIMINARY
 DELINEATION BY
 ENVIRONMENTAL SERVICE
 DATED ON 6/6/2013

**Edina Community Lutheran Church
 Tree Removal Site Plan**

Site Plan Key

-  = Property Line
-  = Existing Tagged Trees to Remain
-  = Tagged Trees Being Removed for Construction

A23

1/4 OF THE NE 1/4 OF SECTION 19, T. 28, R. 24

TREE INVENTORY AS PROVIDED BY : HERITAGE SHADE TREE CONSULTANTS
 Dated July 1, 2013.

Tree Tag #	Species	DBH	Condition	Comments	Tree Tag #	Species	DBH	Condition	Comments
1	Boxelder	9	Fair		47	Boxelder	8	Fair	
2	Boxelder	9	Fair		48	American elm	8	Dead	Next to DED trees
3	Boxelder	9	Fair	Leaning tree	49	American elm	7	Dead	DED
4	Cottonwood	12	Fair	Trunk wounds	50	American elm	7	Dead	DED
5	Cottonwood	13	Poor	Dead top	51	American elm	7	Dead	Next to DED trees
6	Boxelder	7	Poor	Leaning tree	52	Boxelder	15	Fair	
7	Boxelder	8	Fair		53	Boxelder	7	Fair	
8	Boxelder	15	Poor	Leaning tree colonized by vine	54	American elm	9	Dead	DED
9	Boxelder	9	Fair		55	American elm	7	Dead	DED
10	American elm	21	Good		56	Boxelder	10	Fair	
11	Boxelder	7	Fair		57	Boxelder	8	Fair	
12	Boxelder	7	Poor	Trunk wounds, lean	58	Mulberry	12	Good	
13	Boxelder	8	Poor	Leaning tree	59	Boxelder	8	Poor	Lean, stem cankers
14	Silver maple	12	Good		60	Mulberry	10	Fair	
15	Boxelder	15	Fair		61	Cottonwood	40	Fair	Has old lightning strike wound
16	Boxelder	6	Fair	Trunk wound	62	Cottonwood	14	Poor	Dead top
17	Boxelder	6	Fair		63	Cottonwood	27	Fair	
18	Hackberry	7	Good		64	Cottonwood	23	Good	
19	American elm	19	Dead	Structural concerns. Within root graft distance of DED tree	65	Cottonwood	24	Good	
20	American elm	20	Dead	Structural concerns. Within root graft distance of DED tree	66	Boxelder	13	Poor	Leaning into #65
21	American elm	19	Dead	Within root graft distance of DED tree	67	American elm	9	Dead	Within root graft distance of DED tree
22	Mulberry	7	Fair	Trunk wounds	68	American elm	14	Dead	Within root graft distance of DED tree
23	American elm	12	Fair		69	American elm	10	Dead	Within root graft distance of DED tree
24	Boxelder	7	Fair		70	American elm	8	Dead	Within root graft distance of DED tree
25	Cottonwood	18	Fair	Stem cankers, limited crown	71	Mulberry	7	Fair	
26	Boxelder	11	Fair		72	Mulberry	8	Fair	
27	Boxelder	7	Poor	Leaning tree	73	American elm	24	Fair	Leaning tree
28	Cottonwood	32	Good		74	American elm	24	Dead	Within root graft distance of DED tree
29	American elm	10	Dead	Within root graft distance of DED tree	75	American elm	12	Dead	Within root graft distance of DED tree
30	Boxelder	6	Fair		76	American elm	18	Dead	Within root graft distance of DED tree
31	Boxelder	9	Fair		77	American elm	7	Dead	Within root graft distance of DED tree
32	American elm	22	Dead	Has new DED infection	78	American elm	7	Dead	Within root graft distance of DED tree
33	Boxelder	7	Fair		79	American elm	7	Dead	Within root graft distance of DED tree
34	Boxelder	9.7	Fair	Double stemmed tree	80	American elm	10	Dead	Within root graft distance of DED tree
35	Norway maple	13	Good		81	Bur oak	18	Good	
36	Boxelder	15	Fair	Has dieback	82	Bur oak	17	Fair	Leaning tree
37	Boxelder	9	Fair		83	American elm	30	Dead	Within root graft distance of DED tree
38	Boxelder	7	Fair		84	Sugar maple	11	Fair	Trunk wounds
39	Mulberry	8	Poor	Structural concerns	85	Mulberry	9	Fair	
40	Mulberry	8	Fair		86	Green ash	15	Fair	
41	Silver maple	8	Fair		87	Basswood	18	Poor	Structure concerns
42	American elm	14	Good		88	River birch	10, 8, 7.2	Good	Clump tree
43	Boxelder	7	Poor	Overgrown by vine	89	Amur maple	7, 6.5	Good	Clump tree
44	Mulberry	8	Fair		90	Amur maple	9, 6, 6.3	Good	Clump tree
45	Boxelder	9	Fair		91	Amur maple	8, 7, 4	Good	Clump tree
46	Boxelder	7	Fair		92	Autumn blaze maple	9	Good	

 Trees to be Removed

A24

**EDINA COMMUNITY
 LUTHERAN CHURCH**

**ADDITION &
 REMODELING**

EDINA, MINNESOTA

**PIERCE PINI &
 ASSOCIATES**

929B CENTRAL AVENUE NE
 SUITE 312
 BLAINE, MN 55434
 TEL 763.537.1311
 FAX 763.537.1354

**CONDITIONAL USE
 PERMIT SUBMITTAL**

6/24/2015

OWNER CERTIFY THAT THIS FINAL HYDROLOGICAL OR
 REPORT WAS PREPARED BY AN ENGINEER OR OTHER
 SUPERVISOR AND THAT IT IS A TRUE AND CORRECT
 UNDER THE LAWS OF THE STATE OF MINNESOTA.

NAME: RHONDA S. PIERCE
 SIGNATURE: Rhonda S. Pierce
 REG. NO.: 41333
 DATE: 06/24/15

14-045 CONW. 010

JUNE 24, 2015 DATE

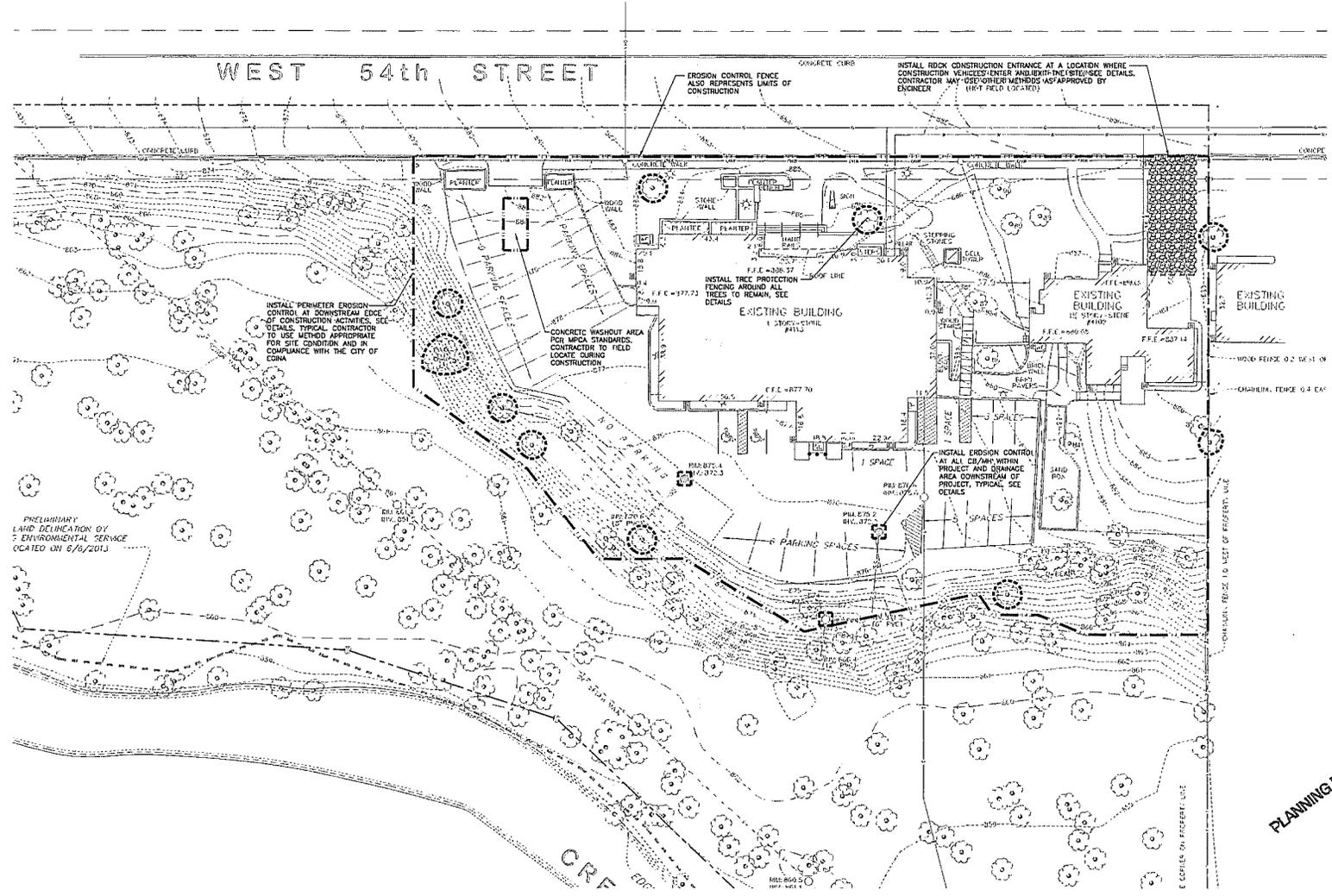
BY: DRAGON

ISP: CHECKED

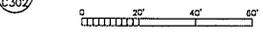
A CITY SUBMITTAL: 06/24/15

**STORMWATER
 POLLUTION PREVENTION
 PLAN - EXISTING
 CONDITIONS**

C302



STORMWATER POLLUTION PREVENTION PLAN - EXISTING CONDITIONS



1"=20'

27

PLANNING DEPARTMENT
 JUN 25 2015

EDINA COMMUNITY LUTHERAN CHURCH

ADDITION & REMODELING

EDINA, MINNESOTA

PIERCE PINI & ASSOCIATES

9298 CENTRAL AVENUE NE
 SUITE 312
 BLAINE, MN 55434
 TEL 763.537.1311
 FAX 763.537.1354

CONDITIONAL USE PERMIT SUBMITTAL

6/24/2015

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR SPECIFIC WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

NAME: RHONDA S. PIERCE
 SIGNATURE: Rhonda S. Pierce
 REG. NO.: 41333
 DATE: 06/24/15

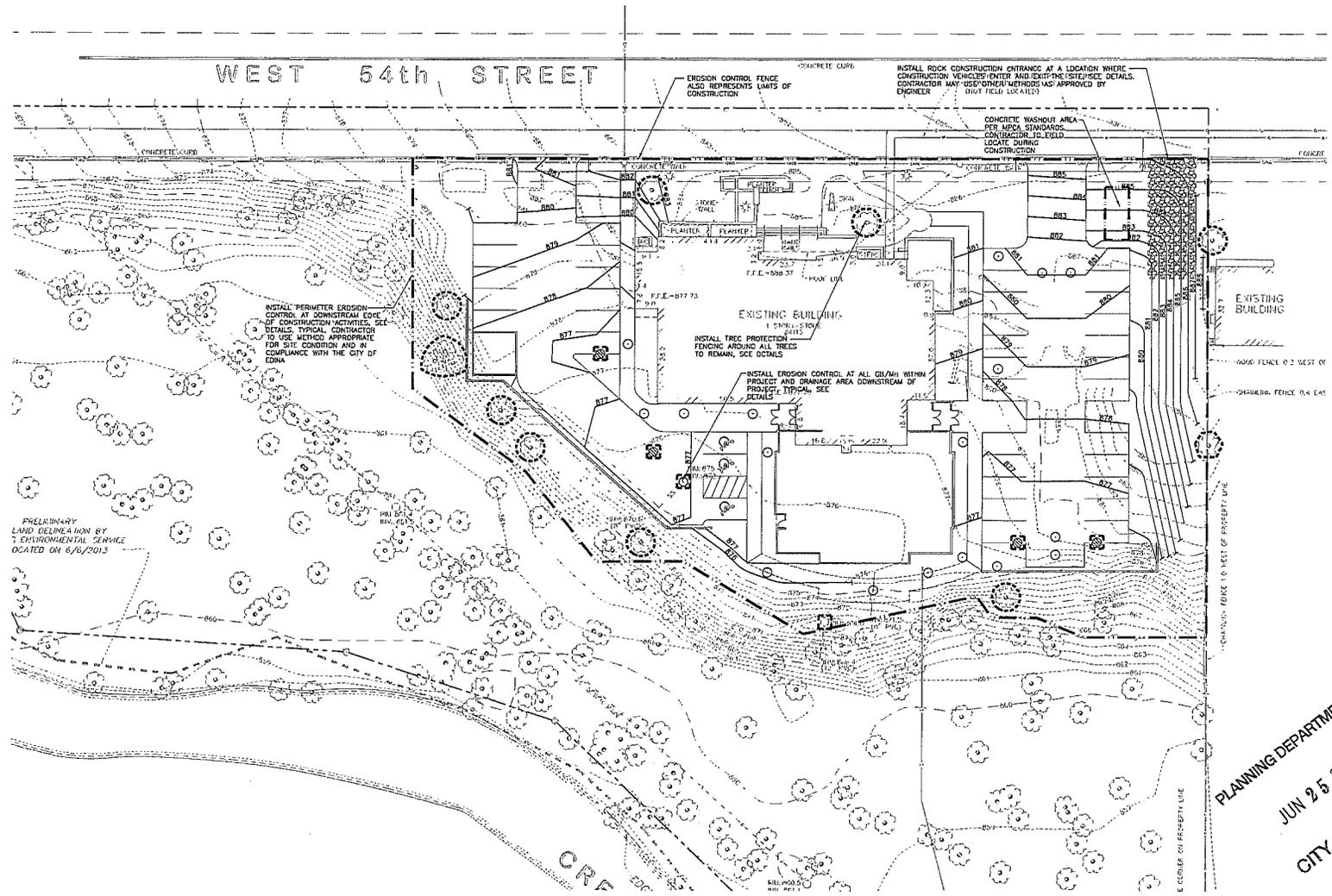
14-045 CD/M. NO.
 JUNE 24, 2015 DATE

ALA: _____ DRAWN
 RSP: _____ CHECKED

REVISIONS
 A CITY SUBMITTAL 06/24/15

STORMWATER POLLUTION PREVENTION PLAN - PROPOSED CONDITIONS

C303



A28



1 STORMWATER POLLUTION PREVENTION PLAN - PROPOSED CONDITIONS

C303



1"=20'

**EDINA COMMUNITY
 LUTHERAN CHURCH**

**ADDITION &
 REMODELING**

EDINA, MINNESOTA

**PIERCE PINI &
 ASSOCIATES**

9298 CENTRAL AVENUE NE
 SUITE 312
 BLAINE, MN 55434
 TEL 763.537.1311
 FAX 763.537.1354

**CONDITIONAL USE
 PERMIT SUBMITTAL**

6/24/2015

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR
 STATEMENT HAS BEEN PREPARED BY ME OR UNDER MY DIRECT
 SUPERVISION AND THAT AS A REGISTERED DESIGNER
 UNDER THE LAWS OF THE STATE OF MINNESOTA.

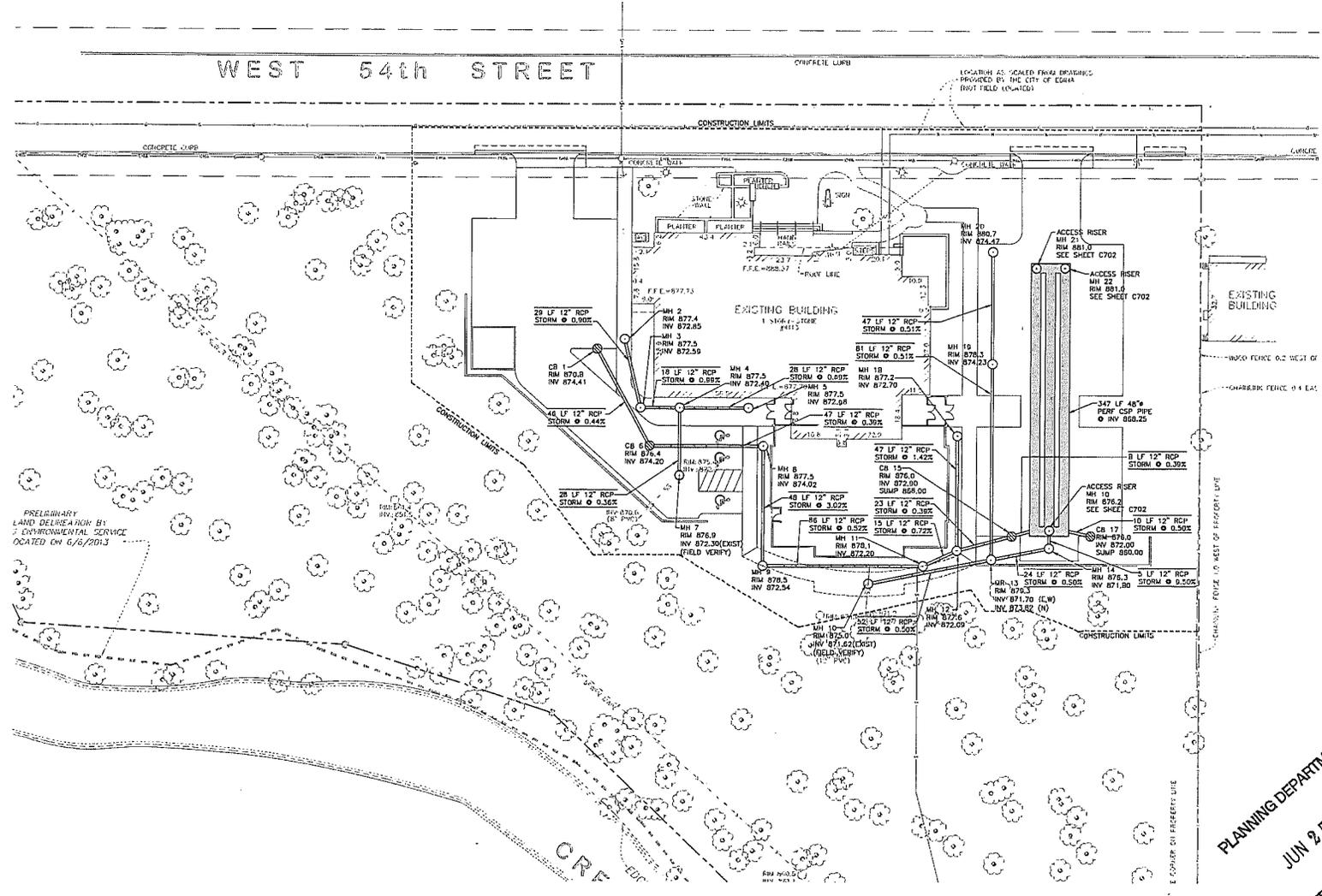
NAME RHONDA S. PIERCE
 SIGNATURE Rhonda S. Pierce
 REG. NO. 413333
 DATE 06/24/15

14-045 COMM. NO.
 JUNE 24, 2015 DATE
 ALA DRAWMAN
 RSP CHECKED

REVISIONS
 4. CIVIL SUBMITTAL 06/24/15

UTILITY PLAN

C500



WEST 54th STREET

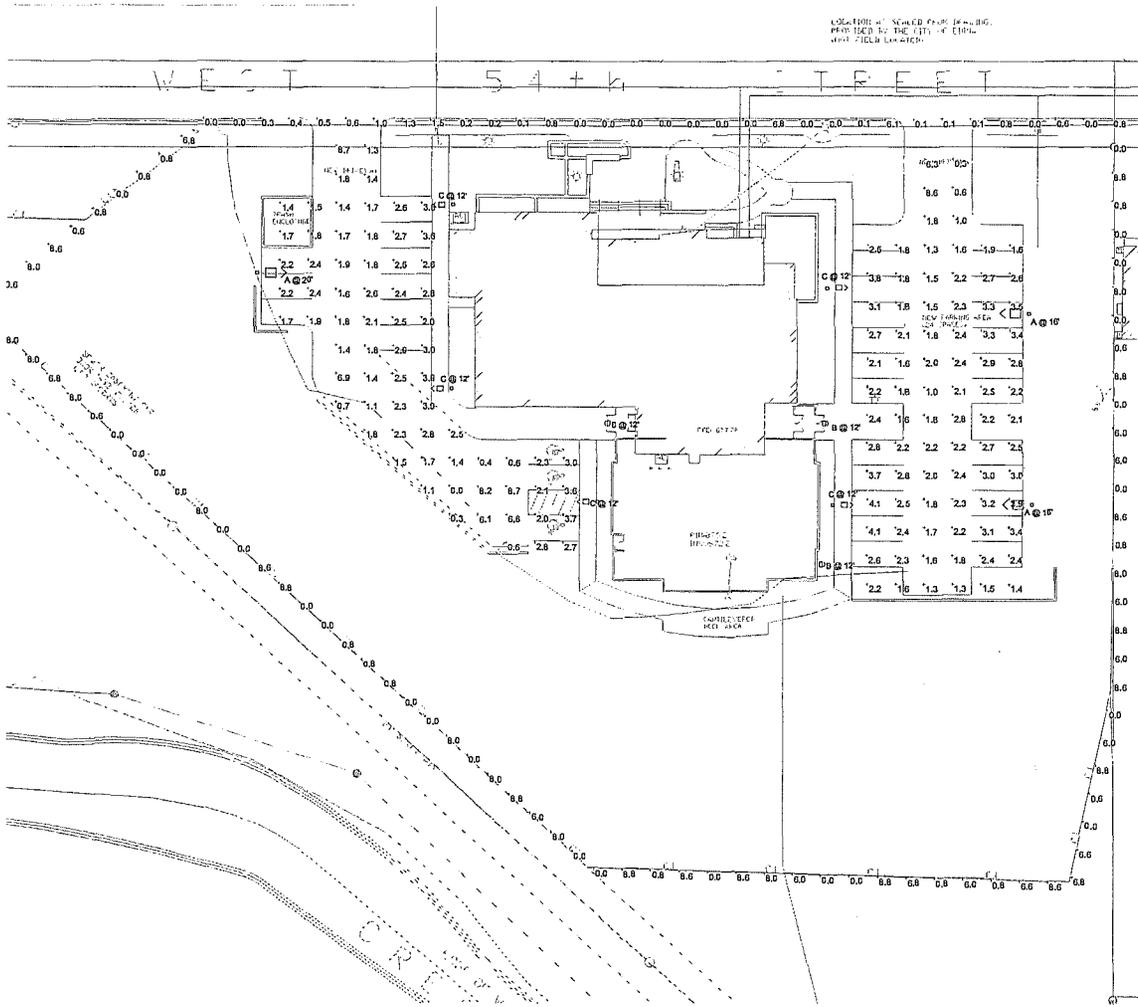
PRELIMINARY
 LAND DELINEATION BY
 ENVIRONMENTAL SERVICE
 DATED ON 6/6/2013



PLANNING DEPARTMENT
 JUN 25 2015
 CITY OF EDINA

A30

LOGS FOR WEST SIDE PARKWAY, INC.
 PROVIDED BY THE CITY OF EDINA
 DATE: 7/20/10



1 PHOTOMETRIC PLAN
 1" = 20'-0"

Symbol	Label	Quantity	Manufacturer	Model Number	Description	Lamp	Number Lamps	Wattage	Lumens Per Lamp	Light Loss Factor	Footcandle
□	A	3	Univex Lighting	OSWY LED 300 300 300 144 INCH (144 INCH) 120V	OSWY LED WITH 300 300 300 LIGHT OPTIC TYPE 144 INCH 120V 144 INCH 120V 144 INCH	LED	1	2400	2400 LM	0.8	181
□	C	3	Univex Lighting	OSWY LED 200 1000 300 120 INCH (120 INCH) 120V	OSWY LED WITH 1000 300 120 INCH LIGHT OPTIC TYPE 120 INCH 120V 1000 300 120 INCH	LED	1	2400	2400 LM	0.8	72
□	B	3	Univex Lighting	OSWY LED 300 300 300 144 INCH (144 INCH) 120V	OSWY LED WITH 300 300 300 LIGHT OPTIC TYPE 144 INCH 120V 144 INCH 120V 144 INCH	LED	1	2400	2400 LM	0.8	20

Classification	Symbol	Area	Min	Max	Max/min	Avg/min	Avg/Max
Property Line	□	2.1 ft	1.5 ft	3.0 ft	1.5 ft	2.25 ft	0.81
Property Line	□	1.1 ft	1.0 ft	1.2 ft	1.1 ft	1.1 ft	0.11
Street Parking Lot	□	1.0 ft	0.5 ft	1.5 ft	0.8 ft	0.8 ft	0.01

D-Series Size 0 LED Area Luminaire

Classification	Symbol	Area	Min	Max	Max/min	Avg/min	Avg/Max
Property Line	□	2.1 ft	1.5 ft	3.0 ft	1.5 ft	2.25 ft	0.81
Property Line	□	1.1 ft	1.0 ft	1.2 ft	1.1 ft	1.1 ft	0.11
Street Parking Lot	□	1.0 ft	0.5 ft	1.5 ft	0.8 ft	0.8 ft	0.01

D-Series Size 1 LED Area Luminaire

Classification	Symbol	Area	Min	Max	Max/min	Avg/min	Avg/Max
Property Line	□	2.1 ft	1.5 ft	3.0 ft	1.5 ft	2.25 ft	0.81
Property Line	□	1.1 ft	1.0 ft	1.2 ft	1.1 ft	1.1 ft	0.11
Street Parking Lot	□	1.0 ft	0.5 ft	1.5 ft	0.8 ft	0.8 ft	0.01

D-Series Size 1 LED Area Luminaire

Classification	Symbol	Area	Min	Max	Max/min	Avg/min	Avg/Max
Property Line	□	2.1 ft	1.5 ft	3.0 ft	1.5 ft	2.25 ft	0.81
Property Line	□	1.1 ft	1.0 ft	1.2 ft	1.1 ft	1.1 ft	0.11
Street Parking Lot	□	1.0 ft	0.5 ft	1.5 ft	0.8 ft	0.8 ft	0.01

BENZ / THOMPSON / RIETOW
 851 WEST GILBERT AVENUE, SUITE 200
 MINNEAPOLIS, MN 55402
 TEL: (612) 332-1234 FAX: (612) 332-1010
 www.btr-architects.com

EDINA COMMUNITY
 LUTHERAN CHURCH

ADDITION &
 REMODELING

EDINA, MINNESOTA

PAULSON & CLARK
 ENGINEERING, INC.
 2352 E. County Road J,
 White Bear Lake, MN 55110
 Office: (651) 487-6556
 Fax: (651) 407-6476
 Email: pac@paulsonclark.com

I hereby certify that this plan, report or specification was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the state of Minnesota.

Date: _____, Page No. _____

CONDITIONAL USE
 PERMIT SUBMITTAL

6/24/2015

1425 _____ CDIAL, US

DATE _____

DRAWN _____

CHECKED _____

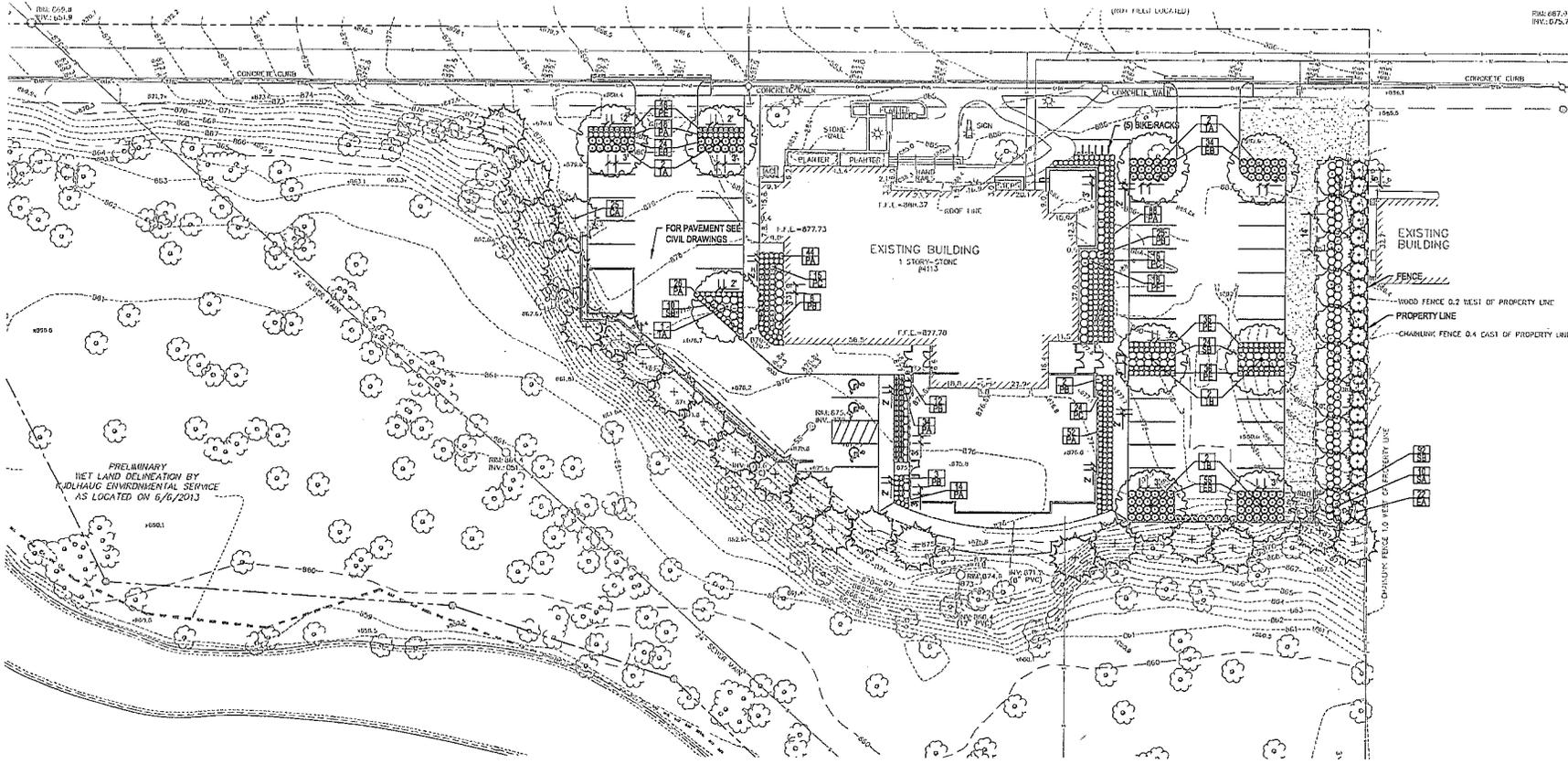
REVISION _____

Site Photometric Plan

E-10

PLANNING DEPARTMENT
 JUN 25 2015
 CITY OF EDINA

A32



BENTZ / THOMPSON / RIETOW
 801 WICOLLET HALL SUITE 4017
 401 N. R. A. P. O. 115 - MINN. 55442-2
 TEL: (612) 332-1234 FAX: (612) 332-1812
 www.btr-architects.com

EDINA COMMUNITY
 LUTHERAN CHURCH
 ADDITION &
 REMODELING

EDINA, MINNESOTA

DAMON FARBER
 LANDSCAPE ARCHITECTS

CONDITIONAL USE
 PERMIT SUBMITTAL

6/24/2015

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR
 REPORT WAS PREPARED BY ME OR UNDER MY DIRECT
 SUPERVISION AND THAT I AM A duly LICENSED ARCHITECT
 UNDER THE LAWS OF THE STATE OF MINNESOTA.

THOMAS WHITLOCK

SIGNATURE

REG. NO. 20282

DATE 6/24/15

14 - 150 COMM. NO.

JUNE 24, 2015 DATE

EM GRANN

TW CHECKED

REVISIONS

LANDSCAPE PLAN &
 SCHEDULE

L100

PLAN LEGEND:

- LANDSCAPE EDITOR
- DECIDUOUS TREE
- ORNAMENTAL TREE
- CONIFEROUS TREE
- CONIFEROUS SHRUB
- DECIDUOUS SHRUB
- PERENNIALS
- TURF

NOTES:

ALL PLANTED AREAS TO HAVE AN
 IRRIGATION SYSTEM. CONTRACTOR TO
 SUBMIT IRRIGATION PLAN.

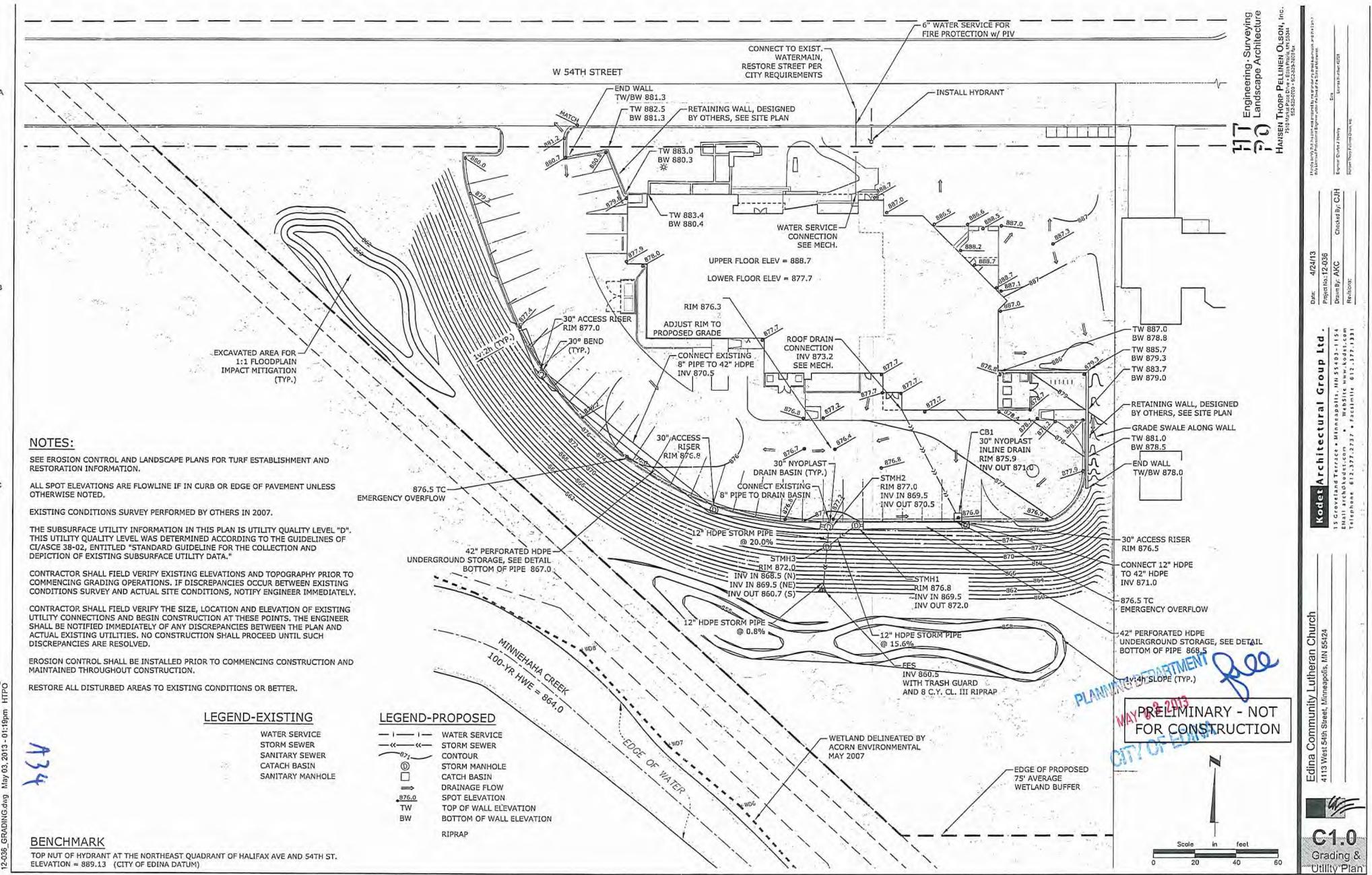
PLANT SCHEDULE:

KEY	QTY	BOTANICAL NAME	COMMON NAME	ROOT CONT.	SIZE	SPECIFICATIONS
	9	DECIDUOUS SHADE TREES				
TA	5/7	GLEDITSIA TRIACANTHOS VAR. INERMIS	HONEYLOCUST, SKYLINE	6BB	2.5' CAL.	12'-14' HT., TRUNK FREE OF BRANCHES 0-7'
TB	4/7	ACER X FREEMANNI 'JEFFERSRED'	MAPLE, AUTUMN BLAZE	6BB	2.5' CAL. OR CLUMP	14'-16' HT., TRUNK FREE OF BRANCHES 0-7'
	10	ORNAMENTAL TREES				
SA	10/1	AMELANCHIER ALNIFOLIA 'REGENT'	SERVICEBERRY, REGENT	CONT.	#5	PLANT AS SHOWN ON THE PLAN
	25	CONIFEROUS TREES				
CA	25/1	CANADIAN HEMLOCK	TSUGA CANADENSIS	8 BB	2.5' CAL.	PLANT AS SHOWN ON THE PLAN
	128	DECIDUOUS SHRUBS				
SB	128	DIERVILLA LONICERA	DWARF BUSH HONEYSUCKLE	CONT.	#5	PLANT AS SHOWN ON THE PLAN
	138	CONIFEROUS SHRUBS				
EA	22	THUJA OCCIDENTALIS 'TECHNY'	ARBORVITAE, TECHNY	CONT.	#15	PLANT AS SHOWN ON THE PLAN
EB	114	JUNIPERUS CHINENSIS	JUNIPER, MINT JULEP®	CONT.	#5	PLANT AS SHOWN ON THE PLAN
	487	PERENNIALS				
PA	278	SPOROBOLUS HETEROLEPIS	GRASS, PRAIRIE DROPSEED	CONT.	#1	PLANT AS SHOWN ON THE PLAN
PB	50	NEPETA X 'FAASENI' WALKER'S LOW'	CATMINT, WALKER'S LOW'	CONT.	#1	PLANT AS SHOWN ON THE PLAN
PC	55	SEDUM X 'AUTUMN JOY'	SEDUM, 'AUTUMN JOY'	CONT.	#1	PLANT AS SHOWN ON THE PLAN
PE	90	HEMEROCALLIS 'SUNDAY GLOVES'	DAYLILLY, 'SUNDAY GLOVES'	CONT.	#1	PLANT AS SHOWN ON THE PLAN
PF	16	RUDBECKIA FULGIDA 'EARLY BIRD GOLD'	RUDBECKIA 'EARLY BIRD GOLD'	CONT.	#1	PLANT AS SHOWN ON THE PLAN



PLANNING DEPARTMENT
 JUN 25 2015
 CITY OF EDINA

A33



NOTES:

SEE EROSION CONTROL AND LANDSCAPE PLANS FOR TURF ESTABLISHMENT AND RESTORATION INFORMATION.

ALL SPOT ELEVATIONS ARE FLOWLINE IF IN CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

EXISTING CONDITIONS SURVEY PERFORMED BY OTHERS IN 2007.

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL "D". THIS UTILITY QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF C/ASCE 38-02, ENTITLED "STANDARD GUIDELINE FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."

CONTRACTOR SHALL FIELD VERIFY EXISTING ELEVATIONS AND TOPOGRAPHY PRIOR TO COMMENCING GRADING OPERATIONS. IF DISCREPANCIES OCCUR BETWEEN EXISTING CONDITIONS SURVEY AND ACTUAL SITE CONDITIONS, NOTIFY ENGINEER IMMEDIATELY.

CONTRACTOR SHALL FIELD VERIFY THE SIZE, LOCATION AND ELEVATION OF EXISTING UTILITY CONNECTIONS AND BEGIN CONSTRUCTION AT THESE POINTS. THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY OF ANY DISCREPANCIES BETWEEN THE PLAN AND ACTUAL EXISTING UTILITIES. NO CONSTRUCTION SHALL PROCEED UNTIL SUCH DISCREPANCIES ARE RESOLVED.

EROSION CONTROL SHALL BE INSTALLED PRIOR TO COMMENCING CONSTRUCTION AND MAINTAINED THROUGHOUT CONSTRUCTION.

RESTORE ALL DISTURBED AREAS TO EXISTING CONDITIONS OR BETTER.

LEGEND-EXISTING

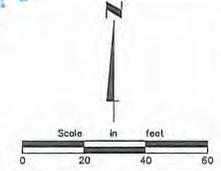
- WATER SERVICE
- STORM SEWER
- SANITARY SEWER
- CATCH BASIN
- SANITARY MANHOLE

LEGEND-PROPOSED

- WATER SERVICE
- STORM SEWER
- CONTOUR
- STORM MANHOLE
- CATCH BASIN
- DRAINAGE FLOW
- SPOT ELEVATION
- TW TOP OF WALL ELEVATION
- BW BOTTOM OF WALL ELEVATION
- RIPRAP

BENCHMARK
TOP NUT OF HYDRANT AT THE NORTHEAST QUADRANT OF HALIFAX AVE AND 54TH ST.
ELEVATION = 889.13 (CITY OF EDINA DATUM)

PRELIMINARY - NOT FOR CONSTRUCTION



Engineering - Surveying
Landscape Architecture
HT PD
HANSEN THORS PELLINEN OLSON, INC.
2119 HALLS ROAD, SUITE 200 • EDINA, MINNESOTA 55424
TEL: 952.925.2222 FAX: 952.925.2223

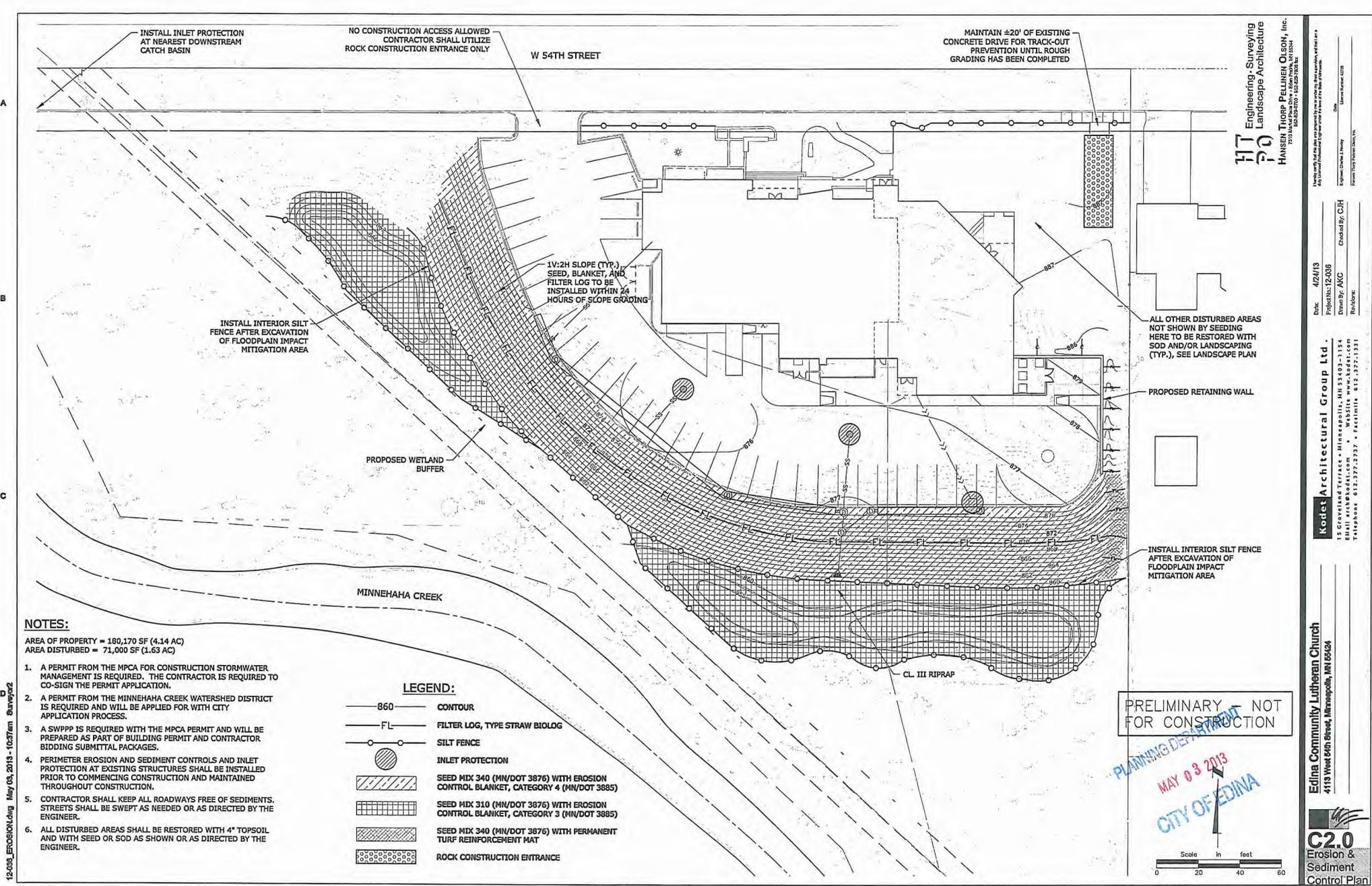
DATE: 4/24/13
PROJECT: 12.036
DRAWN BY: AKC
CHECKED BY: CIH
REVISIONS:

Kodet Architectural Group Ltd.
18 CROFTON TERRACE • MINNEAPOLIS, MN 55412-1111
EMAIL: INFO@KODET.COM • WWW.KODET.COM
TELEPHONE: 612.377.2227 • FACSIMILE: 612.377.1331

Edina Community Lutheran Church
4113 West 54th Street, Minneapolis, MN 55424

C1.0
Grading & Utility Plan

ORIGINAL SUBMITTAL



NOTES:
 AREA OF PROPERTY = 180,170 SF (4.14 AC)
 AREA DISTURBED = 71,000 SF (1.63 AC)

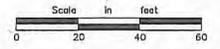
1. A PERMIT FROM THE MPCA FOR CONSTRUCTION STORMWATER MANAGEMENT IS REQUIRED. THE CONTRACTOR IS REQUIRED TO CO-SIGN THE PERMIT APPLICATION.
2. A PERMIT FROM THE MINNEHAHA CREEK WATERSHED DISTRICT IS REQUIRED AND WILL BE APPLIED FOR WITH CITY APPLICATION PROCESS.
3. A SWPPP IS REQUIRED WITH THE MPCA PERMIT AND WILL BE PREPARED AS PART OF BUILDING PERMIT AND CONTRACTOR BIDDING SUBMITTAL PACKAGES.
4. PERIMETER EROSION AND SEDIMENT CONTROLS AND INLET PROTECTION AT EXISTING STRUCTURES SHALL BE INSTALLED PRIOR TO COMMENCING CONSTRUCTION AND MAINTAINED THROUGHOUT CONSTRUCTION.
5. CONTRACTOR SHALL KEEP ALL ROADWAYS FREE OF SEDIMENTS. STREETS SHALL BE SWEEPED AS NEEDED OR AS DIRECTED BY THE ENGINEER.
6. ALL DISTURBED AREAS SHALL BE RESTORED WITH 4" TOPSOIL AND WITH SEED OR SOD AS SHOWN OR AS DIRECTED BY THE ENGINEER.

LEGEND:

- 860 — CONTOUR
- FL — FILTER LOG, TYPE STRAW BIOLOG
- SILT FENCE
- INLET PROTECTION
- ▨ SEED MIX 340 (MIN/DOT 3876) WITH EROSION CONTROL BLANKET, CATEGORY 4 (MIN/DOT 3885)
- ▩ SEED MIX 310 (MIN/DOT 3876) WITH EROSION CONTROL BLANKET, CATEGORY 3 (MIN/DOT 3885)
- ▧ SEED MIX 340 (MIN/DOT 3876) WITH PERMANENT TURF REINFORCEMENT MAT
- ⊠ ROCK CONSTRUCTION ENTRANCE

PRELIMINARY NOT FOR CONSTRUCTION

PLANNING DEPARTMENT
 MAY 08 2013
 CITY OF EDINA



12-008_EROSION.dwg May 05, 2013 - 10:37am Swwy02

A35

ORIGINAL SUBMITTAL

HT PO
 Engineering, Surveying
 Landscape Architecture
 HANSEN THOMPSON PELLERIN OLSEN, Inc.
 2210 Market Street • Eden Prairie, MN 55324
 952.929.9700 • 952.929.9300 fax

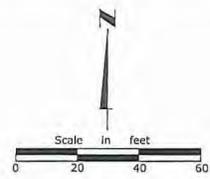
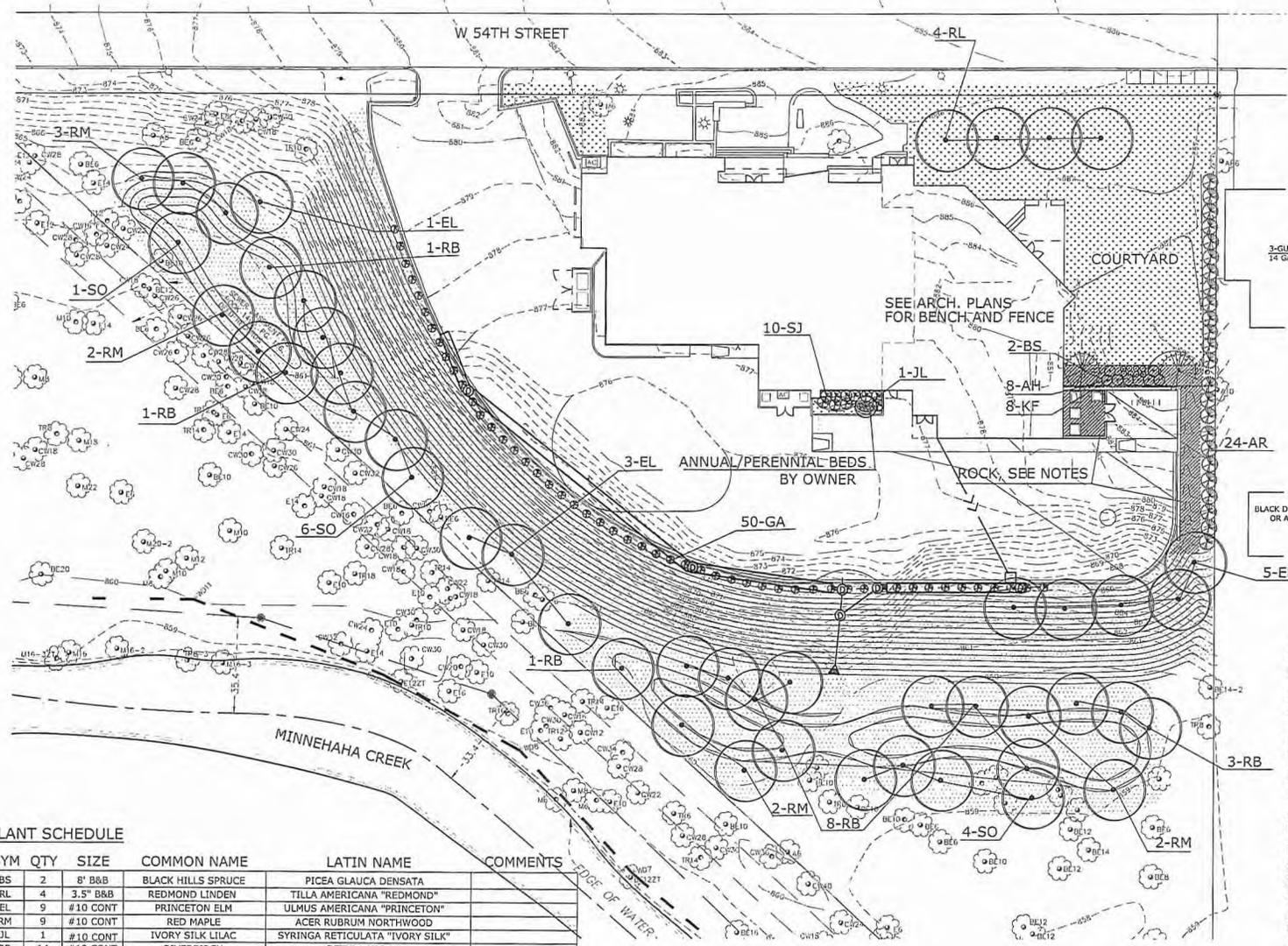
Date: 4/24/13
 Project No: 12-008
 Drawn By: AMC
 Checked By: C/H
 Revision:

Kodet Architectural Group Ltd.
 15 Groveland Terrace • Minneapolis, MN 55409-1154
 Tel: 612.377.1331 Fax: 612.377.1330
 Minneapolis, MN 55409-1154
 Tel: 612.377.1331 Fax: 612.377.1330

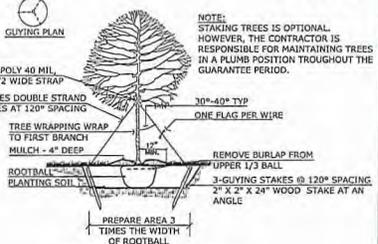
Edina Community Lutheran Church
 4113 Wood 66th Street, Minneapolis, MN 55424

C2.0
 Erosion &
 Sediment
 Control Plan

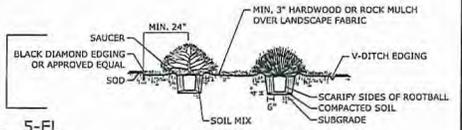
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HTPO
Engineering - Surveying
Landscape Architecture
HANSEN TRAPP PELLNER OLSON, Inc.
1914 W. Lake Street, Suite 200, Minneapolis, MN 55424
612.232.0200 • 612.232.1514



TREE INSTALLATION
DECIDUOUS OR CONIFEROUS TREE
BALLED AND BURLAPPED



SHRUB INSTALLATION
DECIDUOUS OR CONIFEROUS SHRUB BALLED
AND BURLAPPED OR CONTAINER GROWN

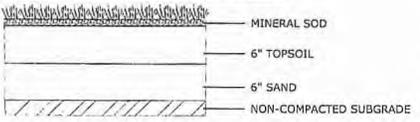
GENERAL NOTES:

- REFER TO EROSION CONTROL PLAN FOR SEEDING INFORMATION SHOULD A PLANT BE UNAVAILABLE AT THE TIME OF INSTALLATION. ALL SUBSTITUTIONS ARE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT AND THE OWNER.
- ANY EXISTING VEGETATION TO BE REMOVED FROM THE SITE SHALL BE DISPOSED OF IN A MANNER THAT IS APPLICABLE WITH LOCAL CITY AND STATE REQUIREMENTS.
- SEE ENGINEERS GRADING AND UTILITY PLANS FOR EXACT LOCATIONS OF BERMS, SWALES, BASINS, AND PIPE LOCATIONS. SEE ARCHITECTURAL SITE PLAN FOR FENCE, WALLS, LIGHTING, ETC.
- ALL TREES SHALL RECEIVE SAUCERS AND MULCH WITH SHREDDED HARDWOOD INDIVIDUALLY AS PER PLANTING DETAIL. ALL SHRUB GROUPS SHALL RECEIVE A CONTINUOUS BEDLINE A MIN. 18" FROM THE ROOTBALL OF THE PROPOSED PERIMETER PLANTS OF THAT GROUPING. FINAL SHAPE OF THE BEDLINES ARE TO BE FIELD APPROVED BY THE LANDSCAPE ARCHITECT. PLANTS SHALL BE MULCHED AS A CONTINUOUS MASS. ALL SHRUBS BEDS SHALL BE EDGED WITH A STEEL EDGING OR APPROVED EQUAL.
- ROCK OVER LANDSCAPE FABRIC SHALL BE APPLIED TO ALL PLANTING BEDS AREAS TO A 3" DEPTH.
- ROCK SHALL BE 1.5-2" CRUSHED GRANITE, PROVIDE OWNER WITH ROCK SAMPLES FOR THEIR APPROVAL PRIOR TO CONSTRUCTION. SPRING EXCAVATION AND PLANTING PREFERRED.
- ALL AREAS WITHIN THE PROPERTY THAT ARE NOT SHRUB BEDS, SEEDED OR HARD SURFACED EXCEPT AS NOTED SHALL BE SODDED.
- ALL SOD AREA SHALL HAVE AN IRRIGATION SYSTEM PER CITY REQUIREMENTS. IRRIGATION SHALL BE DESIGN BUILD.
- 67 TREES TO BE REMOVED AND 67 REPLACEMENT TREES ARE PROPOSED. QUANTITIES ARE BASED ON TREES SHOWN ON THE SURVEY PROVIDED.

PLANT SCHEDULE

SYM	QTY	SIZE	COMMON NAME	LATIN NAME	COMMENTS
BS	2	8' B&B	BLACK HILLS SPRUCE	PICEA GLAUCA DENSATA	
RL	4	3.5" B&B	REDMOND LINDEN	TILIA AMERICANA "REDMOND"	
EL	9	#10 CONT	PRINCETON ELM	ULMUS AMERICANA "PRINCETON"	
RM	9	#10 CONT	RED MAPLE	ACER RUBRUM NORTHWOOD	
JL	1	#10 CONT	IVORY SILK LILAC	SYRINGA RETICULATA "IVORY SILK"	
RB	14	#10 CONT	RIVERBIRCH	BETULA NIGRA	
SO	11	#10 CONT	SWAMP OAK	QUERCUS BICOLOR	
AR	24	#20 CONT	TECHNY ARBORVITAE	THUJA OCCIDENTALIS "TECHNY"	SPACE 5 FEET O.C.
GA	50	#5 CONT	GLOBE ARBORVITAE	THUJA OCCIDENTALIS "TECHNY GLOBE"	SPACE 6 FEET O.C.
SJ	10	#5 CONT	SCANDA JUNIPER	JUNIPEROUS SABINA "SCANDA"	
AH	8	#5 CONT	ANNABELLE HYDRANGEA	HYDRANGEA ARBORESCENS "ANNABELLE"	
KF	8	#1 CONT	KARL FOERSTER GRASS	CALAMAGROSTIS KARL FOERSTER	
			INDICATES MINERAL SOD		
			INDICATES SEEDED AREA		
			INDICATES ROCK MULCH		

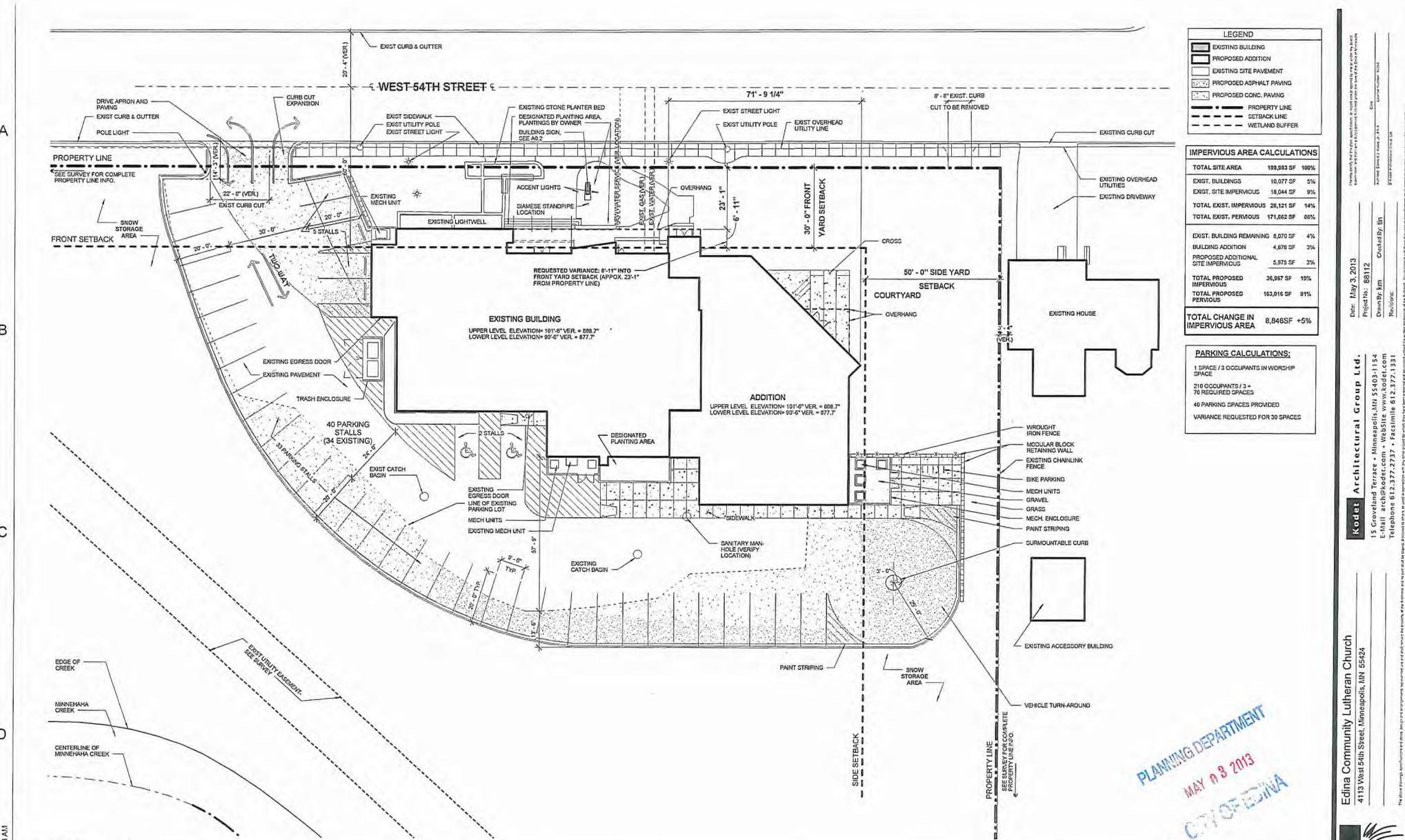
SOD SECTION FOR COURTYARD



PRELIMINARY - NOT FOR CONSTRUCTION

ORIGINAL
SUBMITTAL

Edina Community Lutheran Church
4113 West 54th Street, Minneapolis, MN 55424
Kodes Architectural Group Ltd.
13 Grosvenor Terrace • Minneapolis, MN 55409-1154
Phone: 612.377.2337 • Fax: 612.377.1331
E-mail: kodes@kodes.com
Project No: 12-036
Checked By: JWA
Revised:



LEGEND

- EXISTING BUILDING
- PROPOSED ADDITION
- EXISTING SITE PAVEMENT
- PROPOSED ASPHALT PAVING
- PROPOSED CONC. PAVING
- PROPERTY LINE
- SETBACK LINE
- WETLAND BUFFER

IMPERVIOUS AREA CALCULATIONS

Category	Area (SF)	Percentage
TOTAL SITE AREA	199,883 SF	100%
EXIST. BUILDINGS	10,077 SF	5%
EXIST. SITE IMPERVIOUS	18,044 SF	9%
TOTAL EXIST. IMPERVIOUS	28,121 SF	14%
TOTAL EXIST. PERVIOUS	171,862 SF	86%
EXIST. BUILDING REMAINING	8,070 SF	4%
BUILDING ADDITION	4,876 SF	3%
PROPOSED ADDITIONAL SITE IMPERVIOUS	5,875 SF	3%
TOTAL PROPOSED IMPERVIOUS	36,897 SF	19%
TOTAL PROPOSED PERVIOUS	163,016 SF	81%
TOTAL CHANGE IN IMPERVIOUS AREA	8,846SF	+5%

PARKING CALCULATIONS:

- 1 SPACE / 3 OCCUPANTS IN WORSHIP SPACE
- 216 OCCUPANTS / 3 = 72 REQUIRED SPACES
- 40 PARKING SPACES PROVIDED
- VARIANCE REQUESTED FOR 30 SPACES

D1 SITE PLAN
1/16" = 1'-0"

PLANNING DEPARTMENT
MAY 08 2013
CITY OF EDINA

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Kodet Architectural Group Ltd.
15 Groveland Terrace • Minneapolis, MN 55403-1154
E-mail: arch@kodet.com • WebSite: www.kodet.com
Telephone: 612.372.7237 • Facsimile: 612.372.1331

Date: May 3, 2013
Project No: 88112
Drawn By: km
Checked By: jh
Revised:

Edina Community Lutheran Church
4113 West 54th Street, Minneapolis, MN 55424

A0.1
SITE PLAN

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ORIGINAL
SUBMITTAL

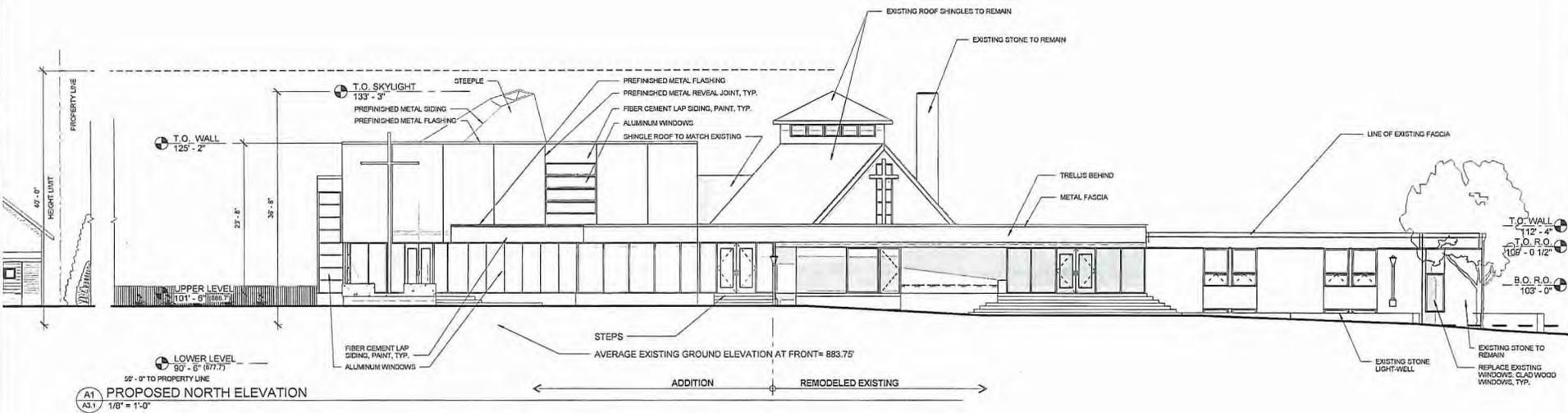
A

B

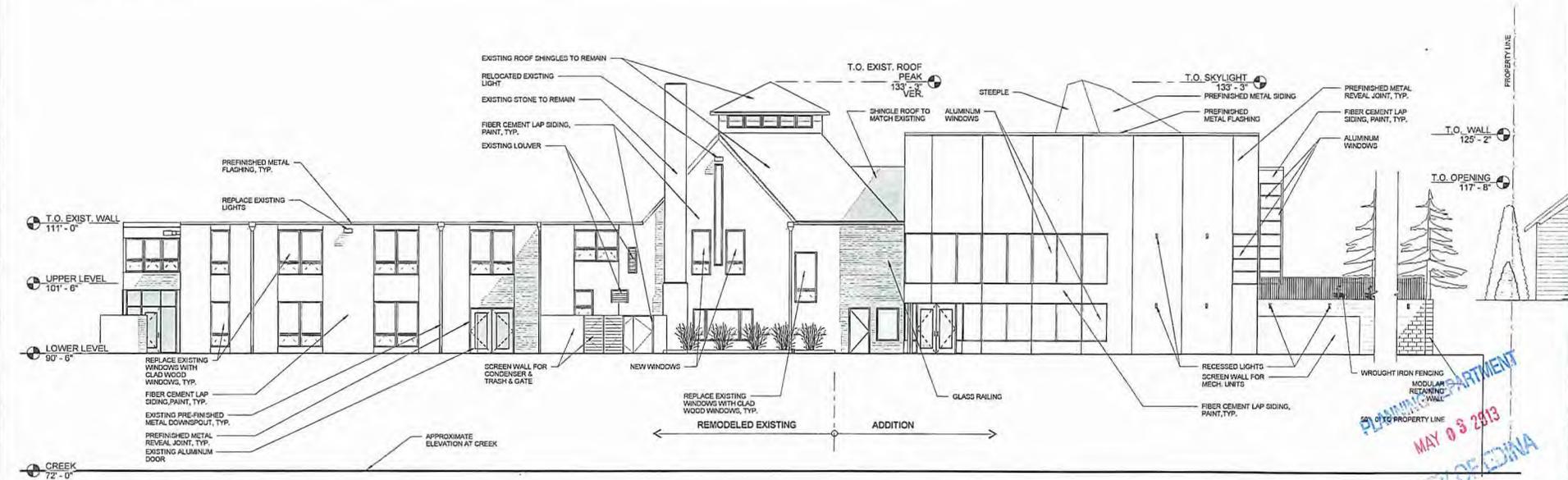
C

D

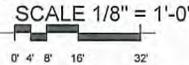
5/3/2013 9:28:30 AM



A1 PROPOSED NORTH ELEVATION
 A3.1 1/8" = 1'-0"



D1 PROPOSED SOUTH ELEVATION
 A3.1 1/8" = 1'-0"



NOT FOR CONSTRUCTION
 Kodet Architectural Group, Ltd. © 2013

Date: May 3, 2013
 Project No.: 88112
 Designer: JZ
 Checker: B. W. G.
 Plot No.:
 Revision:
 Kodet Architectural Group Ltd.
 15 Groveland Terrace • Minneapolis, MN 55403-1154
 E-Mail: arch@kodet.com • Website: www.kodet.com
 Telephone: 612.377.2737 • Facsimile: 612.377.1311

Edina Community Lutheran Church
 4113 West 54th Street, Minneapolis, MN 55424

A3.1
 Exterior Elevations

PLANNING DEPARTMENT
 MAY 03 2013
 CITY OF EDINA

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ORIGINAL
 SUBMITTAL



Edina Community Lutheran Church

Exterior Street Level Perspective

Kodet Architectural Group Ltd.

15 Cleveland Terrace • Minneapolis, MN 55403-1154
E-Mail arch@kodet.com • WebSite www.kodet.com
Telephone 612.377.2737 • Facsimile 612.377.1331

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PLANNING DEPARTMENT
MAY 08 2013
CITY OF EDINA

May 3, 2013

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Edina Community Lutheran Church

SW Perspective

Kodet Architectural Group Ltd.

15 Groveland Terrace • Minneapolis, MN 55403-1154
E-Mail arch@kodet.com • WebSite www.kodet.com
Telephone 612.377.2737 • Facsimile 612.377.1331

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PLANNING DEPARTMENT
MAY 03 2013
CITY OF EDINA

May 3, 2013

A40

ORIGINAL
SUBMITTAL

heretofore caused notice of hearing to be duly published and mailed to owners of each parcel within the area proposed to be assessed on the following proposed improvements:

- PERMANENT STREET SURFACING, CURB & GUTTER, STORM SEWER AND SIDEWALK IMPROVEMENT NO. BA-293 (S.A.P. 120-159-03) - Valley View Road from West 69th Street/France Avenue to Crosstown Highway
- PERMANENT STREET SURFACING, CURB & GUTTER AND SIDEWALK IMPROVEMENT NO. BA-299 (S.A.P. 130-144-06) - West 66th Street from France Avenue to Southdale Lane
- STORM SEWER IMPROVEMENT NO. STS-219
- TRAFFIC SIGNAL IMPROVEMENT NO. TS-21 - Valley View Road & West 66th Street
- STREET LIGHTING IMPROVEMENT NO. L-36 (S.A.P. 120-150-07)

and at the hearing held at the time and place specified in said notice, the Council has duly considered the views of all persons interested, and being fully advised of the pertinent facts, does hereby determine to proceed with the construction of said improvement including all proceedings which may be necessary in eminent domain for the acquisition of necessary easements and rights for construction and maintenance of such improvements; that said improvements are hereby designated and shall be referred to in all subsequent proceedings as:

- PERMANENT STREET SURFACING, CURB & GUTTER AND SIDEWALK IMPROVEMENT NO. BA-293
- PERMANENT STREET SURFACING, CURB & GUTTER AND SIDEWALK IMPROVEMENT NO. BA-299
- STORM SEWER IMPROVEMENT NO. STS.-219
- TRAFFIC SIGNAL IMPROVEMENT NO. TS-21
- STREET LIGHTING IMPROVEMENT NO. L-36

The area proposed to be assessed for a portion of the cost of the proposed improvements includes: Unplatted Parcel (Cornelia Park Pool), Lot 1, Blk. 2, Southdale Office Park Second Addn.; Meets & Bounds Description Southdale Office Center Unplatted, Commencing at NE corner of SE 1/4 thence South to NE Corner of South Office Park 1st Addn., thence Westerly along Northerly line of said Addn. to its intersection with the centerline of Valley View Road, thence Northerly along said centerline to North line of SE 1/4. Thence Easterly to Beginning Except Roads; Lots 1 thru 10, Blk. 3, Southdale First Addn.; Lots 1 thru 13, Blk. 4, Southdale First Addn.; Lot 2, Blk. 1 South Office Park First Addn.; Tract A, RLS No. 1365; and Apartment Ownership #79 - Point of France Condominiums. All units contained with Apartment Ownership #79 (Unit 102 thru 1210 and Penthouse 1 thru Penthouse 12).

Motion was seconded by Member Kelly.

Rollcall:

Ayes: Kelly, Paulus, Smith, Richards

Nays: Rice

Resolution adopted.

CONDITIONAL USE PERMIT GRANTED FOR BUILDING EXPANSION AND REMODELING - EDINA COMMUNITY LUTHERAN CHURCH

Planner Larsen reminded Council that at the March 16, 1992, meeting the request for a conditional use permit for building expansion and remodeling at Edina Community Lutheran Church, 4113 West 54th Street, was continued for additional information on two issues: 1) sidewalk along the front of the church property, and 2) parking bays along the south side of W. 54th Street adjacent to the church. Following that meeting, architects for the church have submitted two site plan for both sidewalk and parking bays. Exhibit "A" illustrates a seven foot wide sidewalk just inside the existing curb line. The sidewalk would extend from the parking lot curb cut on the west to the parsonage driveway on the east, with no parking bay. Exhibit "B" illustrates both a sidewalk and a parking bay. The parking bay would stop before the parsonage and would provide space for six cars.

Staff would recommend the Exhibit "A" sidewalk only alternative. The bay should

C.C.
4/6/92

A41

be included in the Proof of Parking Agreement and considered in the future if conditions warrant. Reasons presented in support were:

- 1) The existing street meets the local and state width standard for a low volume collector street with parking on both sides. The state standard is 38 feet and the existing street is 40 feet wide.
- 2) The parking bay would not provide additional parking spaces. There is presently parking on both sides of 54th Street.
- 3) Due to existing grades, additional steps would be required if the parking bay is constructed.
- 4) The Proof of Parking Agreement will allow the City to address future parking and safety problems.

Ben Crabtree, 5428 Woodcrest Drive, neighbor and church member, submitted that one parking space would be lost if the bay is constructed. With sidewalk up to the curb, snow left by plowing could be more easily moved onto the church property. He concurred with the staff recommendation for Exhibit "A" sidewalk only. No further comment or objection was heard.

**RESOLUTION GRANTING CONDITIONAL USE PERMIT
FOR EDINA COMMUNITY LUTHERAN CHURCH**

WHEREAS, the procedural requirements of Ordinance No. 825 (The Zoning Ordinance) have been met; and

WHEREAS, it has been determined that the Findings as required by Ordinance No. 825 have been satisfied;

NOW, THEREFORE, BE IT RESOLVED that the Edina City Council hereby grants a Conditional Use Permit to Edina Community Lutheran Church at 4113 West 54th Street for building expansion and remodeling.

Rollcall:

Ayes: Kelly, Paulus, Rice, Smith, Richards.

Resolution adopted.

SECOND READING GRANTED; ORDINANCE NO. 812-A3 ADOPTED (REGULATING TELEVISION AND RADIO ANTENNAS AND TOWERS) Planner Larsen recalled that the Council granted first reading for Ordinance No. 812-A3 on March 16, 1992. The only changes to the final draft are to the height of the residential radio antennas in Subsection 815.06. Staff would recommend second reading and adoption.

Ray Voss, 5716 Benton Avenue, representing the amateur radio group, and Dick Casey, 6120 Ashcroft Avenue, said they concurred with the ordinance amendment as presented.

Member Kelly offered Ordinance No. 812-A3 for Second Reading and moved adoption as follows:

**ORDINANCE NO. 812-A3
AN ORDINANCE AMENDING ORDINANCE NO. 812
TO REGULATE CONSTRUCTION AND MAINTENANCE OF TELECOMMUNICATION,
TELEVISION AND RADIO ANTENNAS, EXTERIOR DISH ANTENNAS
AND SUPPORTING TOWERS**

THE CITY COUNCIL OF THE CITY OF EDINA, MINNESOTA, ORDAINS:

Section 1. Ordinance No. 812 is amended to read as follows:

815.01 Definitions. Words and phrases used in this Section which are defined in Section 850 of this Code shall be construed in this Section according to their definitions contained in Section 850. The following words and terms shall have the following meanings in this Section:

Antenna. Equipment used for transmitting or receiving telecommunication, television or radio signals, which is located on the exterior of, or outside of, any building or structure. For purposes of this Section, "antenna" does not include "dish antenna".

Dish Antenna. A parabolic shaped antenna (including all supporting

bathroom/restroom located within a non residential building or within the common areas of a multiple residential building.)

Section 460 - Signs (Scheduled for April 6 hearing.)

* Section 470 - Dangerous and Substandard Buildings (Incorporates State Law whereby Council may order repair or removal of dangerous or substandard buildings.)

Section 475 - Parking Ramps (Scheduled for April 20 hearing.)

Section 480 - Exterior of Single Dwelling Unit and Double Dwelling Unit Buildings

No public comment or objection was heard on Chapter 4.

CHAPTER 5 - CIVIL DEFENSE AND EMERGENCIES

The following provisions in the existing code have been deleted:

Section 500 - Air Raid Precautions

Sections marked with an asterisk have significant changes noted:

Section 505 - Civil Defense

No public comment or objection was heard on Chapter 5. Mayor Richards then declared the public hearing closed.

REQUEST FOR CONDITIONAL USE PERMIT CONTINUED TO APRIL 6, 1992, FOR BUILDING EXPANSION/REMODELING - EDINA COMMUNITY LUTHERAN CHURCH, 4113 WEST 54TH STREET

Affidavits of Notice were presented, approved and ordered place on file.

Presentation by Planner

Planner Larsen recalled that in August, 1988, the Edina Community Lutheran Church, 4113 West 54th Street, requested and received a Conditional Use Permit to construct a new sanctuary addition and to generally remodel the church. The new sanctuary would have seated 210 persons compared to 197 in the existing sanctuary. Following approval of the permit, the church decided not to proceed with the addition and renovation.

The church has now reapplied for a Conditional Use Permit for a revised and reduced in size plan for expansion and renovation of the church. Phase I would be construction of a 22 x 24 foot addition to the north side of the church for relocation of church offices. The fellowship hall (located in the lower level) would be moved to the main floor and the present fellowship hall would be converted to classrooms. Phase II would be construction of a 16 x 48 foot addition to the sanctuary on the east side to provide improved circulation within the building with no increase in seating capacity.

The church presently maintains a setback of approximately 27 feet from West 54th Street. The proposed addition would extend four feet in front of the existing wall and would provide a setback of approximately 23 feet. The Zoning Ordinance requires a 50 foot minimum setback; thus a 27 foot setback variance is requested. All other existing and proposed setbacks comply with ordinance requirements.

The existing parking is located west and south of the church with a capacity ranging from 25 to 35 vehicles. The lot is improved with a blacktop surface but is not striped. The plan anticipates organizing and striping the lot to provide 37 spaces. The Zoning Ordinance requires one space for each three seats in the largest place of assembly. The 197 seat sanctuary would require 66 spaces. The plan requests a 37 space parking variance. The church has submitted an alternate parking plan which would increase the total parking count to 44 spaces. This plan would require substantial fill and retainage along the creek bank.

The church site is 4.73 acres in size, with a majority of the site either flood plain or wetland; the useable area is relatively small. In 1988 the church prepared plans to construct parking in the floodplain area. The plan received approval of the Minnehaha Creek Watershed District. This plan was not viewed

CC
3/16/92

A43

favorably by the City or neighbors along the creek. The approved Conditional Use Permit instead included a proof of parking agreement obligating the church to pursue other parking solutions if a problem arose in the future. The plan addressed modified service schedules and possible parking bays along 54th Street. Staff believes this approach remains valid. On-street parking has not presented a problem for the neighborhood and continued on-street parking seems to be preferable to disturbance of natural areas adjacent to the creek.

Staff would recommend approval of the Conditional Use Permit including the requested parking space variance and building setback variance, subject to a proof of parking agreement for these reasons: 1) The project has been reduced in size from the 1988 plans, 2) Given the characteristics of the site, the plan is the best solution with minimal impact on the neighborhood and the environmental features on the site, 3) The project will be an improvement to the neighborhood.

The Planning Commission heard the request at its meeting of February 26, 1992, and unanimously recommended approval of the Conditional Use Permit, subject to a proof of parking agreement with the following elements if warranted: 1) Parking bays along West 54th Street, 2) Off-site parking implemented on City property, 3) Additional parking constructed behind the parsonage, 4) Rearrangement of existing parking stalls. The Commission specifically recommended that the alternative parking plan for parking within the floodplain not be considered.

It was noted that written correspondence in support of the project had been received from Pastor Erik Strand, Edina Community Church; Steven B. Edwins, of Sovik Mathre Sathrum Quanbeck Architects, 205 So. Water Street, Northfield, MN; and Burton W. Grimes, 5400 Halifax Lane. Correspondence in opposition was received from Merideth/John Hale, 5504 Halifax Lane; Martin/Marion Donnelly, 5332 Halifax Avenue So.; Kathleen Wetherall, 5328 Halifax Avenue So.; Anne/John Crist, 5324 Halifax Avenue So.; Amy/Tom Donnelly, 5333 Halifax Avenue So.; and Teresa Forliti, 5336 Halifax Avenue So.

Presentation for Proponent

Erik Strand, pastor of Edina Community Church, submitted that the congregation felt that the plan proposed in 1988 was too ambitious and not in keeping with its desire to maintain a modest size church. He elaborated on the current proposal emphasizing that it would allow for better internal flow of the building and better utilization of space. Further, a sidewalk along the front of the church property is being considered if the City and neighborhood concur.

Public Comment

Burt Grimes, 5400 Halifax Lane, said he favored the sidewalk on church property only. Ray Voss, 5716 Benton Avenue, said he was a councilmember at Edina Community Lutheran Church and referred to a letter from neighboring property owners regarding problems with church activities. He stated that none of the issues mentioned have come to the attention of the church, that they would not intend to impinge upon the neighborhood in any way, and would follow-up on the concerns that have been raised. Jim Grotz, 5513 Park Place, asked if the building was used for day care and also asked about the parking bays. Pastor Stand said space is rented to a nursery school which uses the facilities from 9:00 A.M. to 3:00 P.M. on weekdays. The church council will be evaluating whether this rental should continue. Planner Larsen said parking bays would essentially be a widening of the street in front of the church property to allow parallel parking. Tom McCusker, 5413 Woodcrest Avenue, said he felt the proposal accommodates the wetlands area and asked about the proposed parking on City property. Planner Larsen explained that, if a parking problem is perceived in the future, one of the solutions may be to improve the City property adjacent to the creek for church parking.

Council Comment/Action

In response to Mayor Richards, Planner Larsen said there is no code requirement nor a commission recommendation that sidewalks or parking bays be included in the proposal, but could be made a condition of the permit. Answering Member Rice about what triggers a proof of parking agreement, Planner Larsen said there are agreements in force with some churches now. However, with the exception of Christ Presbyterian Church, none have been implemented. A proof of parking agreement would give the City the right to determine and institute any action necessary to bring non-conforming parking requirements into compliance. Engineer Hoffman explained that if a sidewalk were installed the church would be responsible for its maintenance. He added that safety would be the main reason for parking bays and/or a sidewalk in the area.

Member Smith made a motion to grant a Conditional Use Permit to Edina Community Lutheran Church, 4113 West 54th Street, for building expansion and remodeling, subject to a Proof of Parking Agreement. Motion was seconded by Member Rice.

Council discussion ensued regarding implementation of parking bays and sidewalk into the proposal at this time.

Member Paulus amended the motion to continue the public hearing on a Conditional Use Permit for Edina Community Lutheran Church, 4113 West 54th Street, for building expansion and remodeling, to April 6, 1992, to allow the church to present plans for parking bays and sidewalk as a condition for issuance of the permit. Motion was seconded by Member Rice.

Ayes: Paulus, Rice, Smith, Richards
Motion carried.

ORDINANCE NO. 812-A3 (REGULATING CONSTRUCTION AND MAINTENANCE OF RADIO AND TELEVISION ANTENNAS AND SUPPORTING TOWERS) GRANTED FIRST READING Subsequent to the public hearing on the draft City Code, and draft Section 815 specifically, the Council considered an amendment to existing Ordinance No. 812 to incorporate language from draft Section 815 as amended.

Member Smith offered Ordinance No. 812-A3 for First Reading incorporating the language contained in draft Section 815 as amended in the public hearing on the draft City Code. Motion was seconded by Member Rice.

Rollcall:
Ayes: Paulus, Rice, Smith
Nays: Richards
First Reading granted.

AMENDMENT TO REPEAL ORDINANCE NO. 114 (MORATORIUM ON CONSTRUCTION OF TOWERS, ANTENNAS AND DISH ANTENNAS) CONTINUED TO 04/06/92 Member Smith made a motion to continue the hearing on an amendment to repeal Ordinance No. 144 (Moratorium on Construction of Towers, Antennas and Dish Antennas) to April 6, 1992. Motion was seconded by Member Rice.

Ayes: Paulus, Rice, Smith, Richards
Motion carried.

VACATIONS GRANTED FOR DRAINAGE AND UTILITY EASEMENT (LOT 1, BLOCK 2, INTERLACHEN HILLS 3RD ADDITION) AND EASEMENT FOR PUBLIC PARK LANDS, PUBLIC OPEN SPACE, STORMWATER POND AND HOLDING AREA (OUTLOT A, INTERLACHEN HILLS 3RD ADDITION); AGREEMENT APPROVED FOR USE OF UTILITY EASEMENT Affidavits of Notice were presented, approved and ordered placed on file.

Presentation by Engineer

Engineer Hoffman advised that the developer of the Lincoln Apartments on Lincoln Drive has petitioned the City to either vacate certain public interests or grant

permission for encroachment on public easements on the Lincoln Apartments project. Staff would recommend the following actions by the Council:

- A. Vacate the southerly two feet of a 35 foot utility easement above the elevation of 889.7, National Geodetic Vertical Datum of 1929.
- B. Grant execution of an agreement to permit a ventilation shaft on the utility easement.
- C. Vacate all public interest on east 20 feet of Outlot A, Interlachen Hills 3rd Addition, except to reserve drainage and utility easement rights over the east 22 feet.

The first recommendation involves vacating air rights on the southerly two feet of a 35 foot utility easement running parallel to the north wall of the Lincoln Apartments north building. The north building footprint was constructed just south of the easement but after field confirmation it was determined that the bay windows protruded over the easement area. The developer has requested that air rights be granted over this two foot area to resolve issues with the title company resulting from the encroachment.

The second recommendation results from the encroachment of a ventilation shaft on the utility easement for sanitary sewer. In this case, staff would recommend not granting a vacation of the easement to allow the ventilation shaft but would recommend granting the execution of an agreement to use the utility easement. The terms of the agreement would hold the City harmless for any damage done by the City during its use of the utility easement for repair or construction work on the utility system.

The third recommendation involves vacating any public interest except for utility and drainage rights over the east 22 feet of Outlot A, Interlachen Hills 3rd Addition. This area is part of the parking lot for the south building of the Lincoln Apartments. During a title examination it was unclear what the City's intent was in 1990 when it earlier vacated all public interests but retained utility interests over the east 20 feet.

Council Comment/Action

Mayor Richards raised the issue of who would pay for the relocation and legal costs if in the future the City must relocate the public utility within the easement. Attorney Gilligan explained that language could be added to the draft agreement to include the City's right to relocate the utility and that all costs incurred would be paid by the developer. Norm Bjornnes, Lincoln Drive Partners, affirmed that the partnership would indemnify and hold the City harmless from any loss under the terms of the Agreement and would be liable for any and all costs. No public comment or objection to the proposed Council action was heard.

Member Paulus introduced the following resolutions and moved adoption:

**RESOLUTION VACATING EASEMENT
FOR DRAINAGE AND UTILITY PURPOSES**

IN THE CITY OF EDINA, HENNEPIN COUNTY, MINNESOTA

WHEREAS, a resolution of the City Council, adopted the 18th day of February, 1992, fixed a date for a public hearing on a proposed vacation of easement for drainage and utility purposes; and

WHEREAS, two weeks published and posted notice of said hearing was given and the hearing was held on the 16th day of March, 1992, at which time all persons desiring to be heard were given an opportunity to be heard thereon; and

WHEREAS, the Council deems it to be in the best interest of the City and of the public that said easement vacation be made; and

WHEREAS, the Council has considered the extent to which the vacation affects existing easements within the area of the vacation and the extent to which the vacation affects the authority of any person, corporation, or municipality owning

AAG

ABH552

View Road contain between two and five units. Lot sizes range between 10,000 and 15,000 square feet. The R-2 lot immediately to the south of the subject property has an area of 11,500 square feet. Planner Larsen explained that despite the minor deficiencies from the ordinance requirements, staff believes there are several reasons to support the proposal. Lot Size - The area of the double dwelling unit lot is below the requirement, however, it is compatible with the other R-2 properties in the vicinity. The R-1 lot exceeds the ordinance minimum as proposed and is larger than several other R-1 lots on Brookview. Character and Symmetry - The streetscape along Brookview Avenue will remain unchanged. The R-1 properties along Brookview are not impacted by the proposal. The new double dwelling on Valley View Road will appropriately fill in a noticeable gap. The new unit will be developed within all required setbacks and orients logically to Valley View Road. Community Planning - The proposed plat and rezoning is consistent with the principles and land use designations set forth in the Comprehensive Plan. Planner Larsen advised that the proposal was considered by the Community Development and Planning Commission on September 7, 1988 and recommended preliminary rezoning and plat approval, subject to final rezoning, final plat, subdivision dedication, curb cut location approval and separate utility connections. He noted that the proponents, Ronald and Robert Erhardt were present to answer questions. Ronald Erhardt gave a brief history of the subject property which was the home of his maternal grandparents. He stated that in the summer of 1987 they made inquiries as to the possibility of constructing a double bungalow on the rear of the lot. With the decease of their mother earlier this year, they are now pursuing the project with plans to live on this property when they retire. Member Turner asked if the Planning Commission discussed the substantial variances that are involved. Mr. Erhardt said the variances were discussed and the Commission's questions were answered. Member Turner then asked Mr. Ehrhardt to support their request for variances. Mr. Erhardt explained that most of the lots along Valley View square footage wise are smaller than the variance on the square footage on the proposed lot fronting on Valley View. In discussions with City staff and the surveyor it was determined that the proposal was the best use of that lot. Member Turner said the use makes sense but that she had trouble finding justification for the variances and that she was concerned about the impact on the single family neighborhood. Member Smith suggested that if the lot line were drawn perpendicular to Brookview Avenue instead of diagonally that the variance needed for the single family lot would not be as great and it would maintain a rectangular lot on Brookview. Member Richards commented that he did not feel there is justification for the requested variances so that there could be three dwelling units where now just one exists. Mr. Erhardt said that one of the questions raised at the Planning Commission meeting was how many units would the square footage of the subject property handle and the answer was a minimum of three. The issue then was should the existing house be removed in order to construct three units. The reponse was where there is an existing single family house that fits Brookview why not build a double on the back portion of the lot which would front on Valley View. There was further discussion on re-orienting the proposed lot line to eliminate the setback variance for the single family lot and how a double could be sited on the new lot fronting Valley View Road. Member Richards made a motion to continue the hearing for approval of the rezoning to R-2 Double Dwelling Unit District and preliminary plat for Erhardt Addition to the meeting of October 17, 1988 to give the proponents time to revise the preliminary plat and provide information on the sizes and dimensions of other lots in the area. Motion was seconded by Member Turner.

Ayes: Kelly, Smith, Richards, Turner, Courtney
 Motion carried.

*PRELIMINARY PLAT APPROVAL FOR EDINA HIGHLANDS 2ND ADDITION CONTINUED TO 10/3/88. Motion was made by Member Kelly and seconded by Member Turner to continue the hearing for preliminary plat approval for Edina Highlands 2nd Addition to October 3, 1988, as requested by the proponent.

Motion carried on rollcall vote, five ayes.

*CONDITIONAL USE PERMIT GRANTED FOR EDINA COMMUNITY LUTHERAN CHURCH. Motion was made by Member Kelly and seconded by Member Turner for adoption of the following resolution, subject to an executed Proof of Parking Agreement prior to issuance of a building permit:

RESOLUTION GRANTING CONDITIONAL USE PERMIT

WHEREAS, the procedural requirements of Ordinance No. 825 (The Zoning Ordinance) have been met; and
 WHEREAS, it has been determined that the Findings as required by Ordinance No. 825 have been satisfied;
 NOW, THEREFORE, BE IT RESOLVED that the Edina Ctiy Council hereby grants a Conditional Use Permit to Edina Community Lutheran Church, 4113 West 54th Street, for construction of a new sanctuary and conversion of the existing sanctuary to a fellowship hall.

Motion carried on rollcall vote, five ayes,

PUBLIC HEARING HELD ON ISSUE OF DIVERTING TRAFFIC ON HALIFAX AVENUE. Engineer Fran Hoffman advised that approximately 800 notices were mailed to residents in northeast Edina regarding the hearing on diverting traffic on Halifax Avenue. The

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RESOLUTION APPROVING PRELIMINARY PLAT
FOR OAK PONDS OF INTERLACHEN 2ND ADDITION

BE IT RESOLVED by the City Council of the City of Edina, Minnesota, that that certain plat entitled "OAK PONDS OF INTERLACHEN 2ND ADDITION, platted by Michael Halley Homes, Inc. and presented at the regular meeting of the City Council of August 15, 1988 be and is hereby granted preliminary plat approval. Motion for adoption of the resolution was seconded by Member Turner.

Rollcall:
Ayes: Kelly, Richards, Turner, Courtney.
Nays: Smith
Resolution adopted.

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PRELIMINARY PLAT APPROVED FOR HED ADDITION. Affidavits of notice were presented, approved and ordered placed on file. Planner Larsen presented the request for preliminary plat approval for the Hed Addition, located at 6625 West Trail, Lot 1, Block 10, Indian Hills. The subject property is a developed single family lot with an area of 79,207 square feet. The proponent has submitted a preliminary plat which would create one new buildable lot. The existing house would remain as it is today. The new lot would contain 34,277 square feet, and the lot for the existing house would contain 44,930 square feet. The new house is proposed to front on Iroquois Trail. Both lots comply or exceed all Zoning Ordinance requirements for single family lots. A graphic of the proposed plat was shown indicating the existing house and driveway and proposed house to be constructed on the new lot. In support of their application, the proponents have submitted an analysis and survey of 40 lots in the western portion of Indians to determine lot size. The existing lot (Lot 1, Block 10, Indian Hills) is the largest in the area and following the subdivision the resulting lots would remain among the largest lots in this area of Indian Hills. Planner Larsen explained that, normally, staff would want to see the property split more evenly than is proposed. However, in this case a more even division of the property would require removing the existing house. The present proposal maximizes the size of Lot 2 while saving the existing house. The entire property is heavily wooded and the existing house is barely visible from the street. A division which would require removing the existing house may disturb the site more than the present proposal. He advised that at its meeting of July 27, 1988, the Community Development and Planning Commission reviewed the proposal and recommended preliminary approval subject to: 1) final plat approval, 2) subdivision dedication, and 3) utility connection charges. He stated that Virgil Hed, proponent, was present to answer questions. Member Smith asked questions about the retaining wall adjacent to Iroquois Trail. Planner Larsen said that it was an existing private retaining wall on the property. Member Smith commented that the lots in the proposed plat are consistent in size with the neighborhood with the exception of the large lots to the east. No other comment being heard, Member Smith introduced the following resolution and moved adoption, subject to: 1) final plat approval, 2) subdivision dedication, and 3) utility connection changes:

RESOLUTION APPROVING PRELIMINARY PLAT
FOR HED ADDITION

BE IT RESOLVED by the City Council of the City of Edina, Minnesota, that that certain plat entitled "HED ADDITION", platted by Virgil and Sharon Hed, husband and wife, and presented at the regular meeting of the City Council of August 15, 1988 be and is hereby granted preliminary plat approval.

Motion for adoption of the resolution was seconded by Member Turner.
Rollcall:
Ayes: Kelly, Richards, Smith, Turner, Courtney
Motion carried.

CONDITIONAL USE PERMIT FOR EDINA COMMUNITY LUTHERAN CHURCH EXPANSION CONTINUED.

Affidavits of notice were presented, approved and ordered placed on file. Planner Larsen presented the request of Edina Community Lutheran Church, 4113 West 54th Street, for a conditional use permit. Edina Community Lutheran Church, generally located south of West 54th Street and west of Halifax Avenue, has applied for a conditional use permit to build a new sanctuary and convert the existing sanctuary into a fellowship hall. The new sanctuary will seat 210 people which is an increase of 23 seats over the present sanctuary. The project includes remodeling the interior of the existing building. The exterior of the new sanctuary will be finished with cedar shakes and stone trim to match the stone on the existing church. The existing building will be re-sided with cedar shakes to match the new addition. Discussion at the Community Development and Planning Commission meeting centered upon where additional parking would be located on the church site. The church property measures 4.73 acres in size. However, much of the area is within the flood plain of Minnehaha Creek. No building or other obstructions may be placed in the flood plain. Consequently, the only buildable area is the northeasterly portion of the site. All of the proposed construction is above the flood plain elevation. Under certain circumstances, parking could be developed within the flood plain area. However, from staff's point of view it is not a desirable alternative. In looking at other alternatives, it was determined to develop as much parking as possible on the upper area of the church ground and if parking overflowed the capacity of that lot it could locate on West 54th Street where there is adequate street width for parking on both sides and very few homes.

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The church's proposal calls for rebuilding the existing parking area to the south and west of the church to accommodate 41 cars. The existing parking area is unstriped and accommodates 25-35 cars. With seating for 210 persons, the Zoning Ordinance requires 70 parking spaces. In order to provide the required parking the church would need to locate the additional parking within the flood plain area. The church has elected to request a parking variance of 29 spaces to avoid disturbing the natural conditions existing adjacent to the creek. According to church officials the intent of the addition and renovation project is to better accommodate existing needs and not to prepare for any significant increase in congregation size. The seating capacity increase is very modest and the existing building would benefit from the proposed renovation. The building design and the soft, natural materials seem appropriate for the site. The Planning Commission members discussed at length whether to recommend the parking variance and the conditional use permit or to require a proof of parking agreement. The question arose if there is a proof of parking agreement in place and additional parking is required in the future, where would those 29 spaces be located. The answer would be in the flood plain which both the church and staff have been trying to avoid. At its meeting of July 27, 1988, the Planning Commission recommended approval of the conditional use permit, with a 29 space parking variance as recommended by staff, and that the City and church enter into a proof of parking agreement. Planner Larsen stated that the Planning Commission, staff, the church, and neighbors all supported the proposed parking variance to avoid disturbing the flood plain areas on the church property. He explained that staff did not recommend the proof of parking agreement for these reasons: 1) The increase in seating capacity is small, only 23 seats. 2) The flood plain area is the only place to develop more parking. This area is approximately 16 feet lower than the existing parking lot make it undesirable parking. 3) Cars can park on both sides of West 54th Street without disrupting traffic flow. 4) Present church activities have not caused problems for the neighborhood. Planner Larsen said that representatives of the church were present, as well as John Cunningham, project architect. Member Turner asked if all alternatives for parking have been considered. Planner Larsen said there may be room for additional parking in the future if the existing parsonage were removed. Staff also looked at the possibility of a parking bay adjacent to West 54th Street. However, the level of traffic and the existing width of the street did not seem to warrant that alternative. Staff also looked at public park land on the south side of West 54th Street and west of Minnehaha Creek as off-site parking; however, that is rather remote to the church. Member Smith asked how a proof of parking agreement would be handled. Planner Larsen said the standard procedure with regard to a proof of parking agreement is that the City would hope it would not need to be enforced. The problem with an agreement is if additional parking is proved to be needed the City would require that parking be provided somehow on the site. Here the only place would be down on the flood plain. Planner Larsen added that the City and church could enter into a general agreement, similar to that with Colonial Church, whereby the church would work with the City to solve parking problems should they occur in the future. Ann Bishop, 5324 Halifax Avenue, asked about the impact of traffic in the neighborhood if both sanctuaries are used to capacity. Planner Larsen explained that the Zoning Ordinance says you calculate the demand on traffic and parking based on the largest use assembly which in this case would be so modest that there would be no noticeable increase in traffic. If in the future, both structures were used concurrently, it could have some impact. When the City reviews requests for conditional use permits from schools and churches, staff relies on those institutions telling staff how they operate. Mark Brethein, 5429 Woodcrest Drive, said he was supportive of the church's plan and in favor of the parking variance. He said he was concerned about any option to build parking in the flood plain as that would be across the creek from his property. Member Smith commented that he would like to see some kind of agreement with the church regarding future parking needs. Member Turner said she would not support putting parking in the flood plain. She added that the Council has been tough with churches in recent years regarding their parking requirements; that we should not make an exception here and that there should be some type of agreement. Member Richards made a motion that approval of the Conditional Use Permit for Edina Community Lutheran Church be continued to September 19, 1988 and that staff be directed to bring back an agreement regarding future parking needs for approval before the Conditional Use Permit is granted. Motion was seconded by Member Smith.

Ayes: Kelly, Richards, Smith, Turner, Courtney
Motion carried.

TRAFFIC SAFETY COMMITTEE MINUTES OF AUGUST 9, 1988 APPROVED. Because of the number of persons present regarding Agenda Item VIII.A (Approval of Traffic Safety Committee Minutes), Mayor Courtney declared this the next item to be heard. Engineer Hoffman reviewed the discussion held at the Traffic Safety Committee meeting of August 9, 1988 regarding the traffic issues on Halifax Avenue, West 51st through West 54th Streets and the temporary traffic barricade that had been installed at Halifax and West 51st Street. He recalled that at the July meeting the Committee had recommended that the Council conduct a public hearing on this matter to get input from the residents of Halifax Avenue and the affected surrounding neighborhoods. At its meeting of August 9, 1988, the members

Mr. Ackerberg told the Commissioners he too would like better pedestrian access to the patio; however, liquor license restrictions prevents it and it is difficult to control seating order.

Chair Platteter said he supports the project; adding in his opinion it's a great project. Platteter said the Commission viewed this in a "small scale"; however, as redevelopment occurs in the area the Commission may need to think about how these small redevelopments fit into the rest of the area and bigger picture. Platteter noted it appears throughout Edina and the greater Southdale area infills are occurring.

Commissioner Carr stated she supports the project; however, was a little hesitant with the exterior materials.

Commissioner Forrest also indicated her support for the project. She said in her opinion the SE corner may need enhancement to keep the public engaged.

A discussion ensued on the importance of creating an active streetscape and developing an inviting pedestrian experience. There was some discussion on the location of the door; however, Commissioners acknowledged the challenge with door placement when the majority of visitors come by car. All in all, Commissioners agreed it was a nice project.

Public Hearing

Chair Platteter asked if anyone would like to speak to the issue; being none, Commissioner Thorsen moved to close the public hearing. Commissioner Strauss seconded the motion. All voted aye; motion carried.

Motion

Commissioner Carr moved to recommend final rezoning and final development plan approval based on staff findings and subject to staff conditions; with the additional comment that the applicant do everything to help encourage engagement with the street. Commissioner Hobbs seconded the motion. All voted aye; motion carried. 9-0.

VII. REPORTS AND RECOMMENDATIONS

A. Sketch Plan. Edina Community Lutheran Church – 4113 54th St West.

Planner Comments

Planner Teague reported the Commission is asked to consider a sketch plan proposal to build a new sanctuary, kitchen addition and new parking lot on the east side of the existing church. Teague explained the parsonage home would be removed and replaced with the new parking lot. The new sanctuary addition would provide the same seating capacity as the existing

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sanctuary. Teague gave a brief history on the previous proposal, adding the revised plans address concerns previously raised. Teague asked for Commission comments.

Appearing for the Applicant

Thomas Whitlock and Randy Mo

Discussion

Commissioner Nemerov asked how close the proposed addition is to the creek. Planner Teague responded that it appears to be over 100-feet from the creek.

Applicant Presentation

Randy Mo told the Commission that the site is striking and with graphics explained the layout of the proposed addition(s) and site drainage.

Thomas Whitlock presented the landscaping plan and asked the Commission to note that the proposed addition ends at the edge of the existing parking lot. He noted permeable pavers would be implemented throughout where appropriate. Whitlock explained that the neighbor to the east will be heavily buffered through the planting of evergreens, adding this form of landscaping buffer also screens neighbors from headlight wash from vehicles coming or leaving the parking lots. Concluding, Whitlock said the development team continues to work with the neighbors on landscaping and tree retention.

Discussion

Commissioner Hobbs noted that drainage and water shed are important issues facing this redevelopment and asked the applicants if they believe the proposal can adequately handle the run-off. Mr. Whitlock said they are working with the Watershed District and believe the plan will address all drainage issues. He explained that the demolition of the parsonage allows for the location of an integrated underground storm water management system below the parking lot. Whitlock said this storm water management system is superior because it eliminated the need for retaining ponds which would result in additional tree loss.

Commissioner Carr noted that the parsonage earmarked to be demolished was built in 1949, adding the Heritage Preservation Board may be interested in the demolition. Carr suggested that the applicant take photos of the parsonage to document it. There was a brief discussion on if the Heritage Preservation Board should weigh in on the demolition of the parsonage.

Chair Platteter asked the applicants the square footage of the build-out. Mr. Mo responded that total build-out square footage is 26,000 square feet. Chair Platteter commented that this addition creates an opportunity to work with the Watershed District that could provide a better storm water management situation especially since the structure is in close proximity to the creek. Continuing, Platteter said special attention needs to be paid to screening the addition from the properties to the west. Platteter said he likes the handling of the trash

enclosures He further recommended that the development team look at the project from all angles and consider the build-out in all seasons, leafs, no leafs, minimal leafs etc. to ensure adequate screening from adjoining neighbors. Concluding Platteter said if done correctly the new addition should blend well.

Commissioner Forrest commented that the addition of bike parking stations should be added to the plan. She further noted she agrees with Chair Platteter in supporting the location of the trash enclosure. Continuing, Forrest stated she has a couple issues; one is lighting, two, parking. She asked the applicant to make sure they are sensitive to light wash into the residential neighborhood, pointing out parking lot lighting needs to be done correctly. Forrest also noted that she is concerned with parking on West 54th Street. She pointed out West 54th Street is a bike route and the parking of vehicles along a bike route could raise some issues.

Commissioner Nemerov commented that it is difficult finding balance and encouraged the applicants to continue to work with the neighbors in addressing all their concerns. Mr. Whitlock agreed, adding they will continue to work on tree placement, landscaping, access and parking.

Commissioner Thorsen commented that parking is an issue and will continue to be an issue; however, circulation and viable access can help mitigate the issue.

Chair Platteter thanked Mr. Whitlock and Mo for their presentations.

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PLANNING DEPARTMENT

JUN 25 2015

CITY OF EDINA

AGREEMENT

THIS AGREEMENT, Made and entered into this 18th day of May, 1992, by and between EDINA COMMUNITY LUTHERAN CHURCH, INCORPORATED, a Minnesota non-profit corporation (the "Church") and CITY OF EDINA, a Minnesota municipal corporation (the "City");

WITNESSETH, THAT:

WHEREAS, The Church is the Owner of certain real property ("Property") located in the City of Edina, County of Hennepin, State of Minnesota, legally described on Exhibit A attached hereto and hereby made a part hereof; and

WHEREAS, The Church proposes to construct an addition to the building currently existing on the Property ("Building"), which addition is to include parking and landscaping to be used in connection therewith as shown on the plan prepared by Sövik Mathre Sathrum Quanbeck Edwins dated February 20, 1992 ("Plan") (the addition and said parking and landscaping being hereinafter called the "Improvements"); and

WHEREAS, the Plan provides 37 parking spaces on site but would need 70 parking spaces, following completion of the Improvements, to comply with the City's zoning ordinances; and

WHEREAS, additional on site parking spaces could only be provided by locating them on the flood plain of Minnehaha Creek; and

WHEREAS, the Church has sought approval from the City for construction of the Improvements and requested a parking variance necessary to allow construction of the Improvements as shown on the Plan; and

WHEREAS, the City did, on April 6, 1992, in Case No. C-92-1, approve the construction of the Improvements and grant the requested variance because strict enforcement of the City ordinances would, in this case, cause undue hardship because of circumstances unique to the Property, and the approval of construction and grant of such variance has been determined by the City to be in keeping with the spirit and intent of the applicable ordinances, but the approval of construction and grant of the variance was conditioned upon the execution, delivery, and recording of this Agreement, and upon the conditions hereinafter set out in this Agreement, which the City deems necessary to impose to ensure compliance with the applicable City ordinances and to protect adjacent properties; and



APPROVED FOR FILING *as to Variance*
Quinn White in *Church's*
Deputy Examiner of Titles *Name*

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WHEREAS, The Church is agreeable to the approval of construction and the granting of the variance being subject to the conditions hereinafter set out, and is willing, and represents that it has the power and authority, to enter into this Agreement.

NOW, THEREFORE, in consideration of the approval given by the City and the granting of the above requested variance, by the City, and of the mutual covenants and agreements hereinafter contained, it is hereby agreed by and between the parties hereto as follows:

1. The City hereby confirms that it did, as above stated, approve construction of the Improvements, and did grant variance from its applicable ordinances concerning parking requirements and building setback requirements, subject, however, to the terms and conditions of this Agreement.

2. If the City Manager and the City Planner shall hereafter determine, in their sole and absolute discretion, that additional parking spaces are required on the Property, the Church will, at its sole cost and expense, submit to the City a plan for providing the additional parking spaces as the City Planner and City Manager shall then require, up to the maximum number of parking spaces required by the then applicable City Ordinances. The City Planner and City Manager need not require that all of the additional parking spaces be provided at any one time, but may require additional parking spaces be provided from time to time as they deem them necessary, again in their sole and absolute discretion, until the maximum number of parking spaces as required by the then applicable City Ordinances have been provided. The plan for additional parking spaces may provide for additional parking spaces on the Property, on the City property used for parking and located West of Minnehaha Creek and South of West 54th Street, on West 54th Street following reconstruction and widening of the street to provide for parking bays, or on other off-street locations or combinations thereof. The plan shall also include methods for implementing the use of any such additional parking areas by Church patrons. Any such plan shall avoid use of the floodplain area for additional parking spaces. The plan must be approved by the City, and if approved by the City (which approval may be withheld for any reason or cause), such additional parking spaces may then be utilized by the Church pursuant to said new parking plan, as approved, and subject to the then applicable ordinances of the City, except as such ordinances may be waived by variances, if any, then granted. As above stated, the City Manager and City Planner shall be the sole judges of whether or not additional parking is required, from time to time, and if so, how much is to be constructed at any given time.

The City Manager and the City Planner shall give written notice to the Church of their determination that additional parking spaces are then required,

setting forth in said notice the number of spaces then required to be provided, up to the maximum required by the then applicable City Ordinances. The Church must submit its proposed plan for additional parking spaces within thirty (30) days after such notice is given.

3. If any term, condition, or provision of this Agreement, or the application thereof to any person or circumstance, shall, to any extent, be held to be invalid or unenforceable, the remainder hereof and the application of such term, provision, and condition to persons or circumstances other than those as to whom it shall be held invalid or unenforceable shall not be affected thereby, and this Agreement, and all the terms, provisions, and conditions hereof, shall, in all other respects, continue to be effective and to be complied with to the full extent permitted by law.

4. In the event that the Church fails or refuses to fully comply with all of its obligations under this Agreement, or violates any of the provisions hereof, and such failure, refusal or violation continues for a period of thirty (30) days after written notice thereof is given to the Church, then, in that event, in addition to any other remedies then available to the City at law or in equity, the City shall have the right to:

(a) Prohibit any parking on West 54th Street (which City may or may not whether or not agreed to herein by the Church).

(b) Obtain enforcement of this Agreement by court order for mandatory injunction or other appropriate relief; and

(c) Withhold, deny, or revoke any building permits, certificates of occupancy, utility connection permits and any other permits and approvals, now or hereafter issued or granted or to be issued or granted by the City for the construction or occupancy of all or any part of the Property, or Improvements, until such failure or refusal ends and the Church fully complies with its obligations hereunder.

All of the foregoing remedies shall be usable and enforceable by the City separately or concurrently as the City shall determine, and the use of one remedy shall not waive or preclude the use of any one or more of the other remedies. Also, the failure to exercise, or delay in exercising, any remedy hereunder in the event of a failure or refusal by the Church, shall not preclude City from thereafter exercising any of its remedies for the same or a subsequent failure or refusal. The Church agrees to pay to City any and all costs and expenses incurred by City in enforcing this Agreement by the use of the remedies above set out or by other remedies or means available to the City at law or in equity, including attorneys' fees whether suit be brought or not, and with interest on all such costs and expenses at the highest rate permitted by law, or, if no maximum rate is

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CITY OF EDINA

applicable, then at the rate of twelve percent (12%) per annum from the dates incurred by the City until paid.

The Church also agrees to pay all costs of collection of any monies due to the City from the Church pursuant hereto, and of such costs and expenses incurred in enforcing this Agreement, with interest thereon, again including attorneys' fees and whether suit be brought or not, with interest at the highest rate permitted by law, or if no maximum rate is applicable, then at the rate of twelve percent (12%) per annum from the dates such costs of collection were incurred until paid.

5. All notices, reports, or demands required or permitted to be given under this Agreement shall be in writing and shall be deemed to be given when personally delivered to any officer of the party to which notice is being given, or when deposited in the United States mail in a sealed envelope, with registered or certified mail postage prepaid thereon, addressed to the parties at the following addresses:

To City: 4801 West 50th Street
Edina, Minnesota 55424
Attn: City Manager

To the Church: 4113 West 54th Street
Edina, Minnesota 55424

Such addresses may be changed by any party upon notice to the other party given as herein provided.

5. The terms and provisions hereof shall be binding upon and inure to the benefit of the parties hereto and their respective successors and assigns, and shall run with the title to the Property and be binding upon all present and future owners of the Property. If, for any reason, the provisions hereof should be determined by the legal counsel for the City, or by a court of competent jurisdiction, not to be binding upon and fully enforceable against any owner of the Property, then the variance granted by the City in Case No. C-92-1 shall wholly cease and terminate and the Property shall be used and usable only in full compliance with all then applicable ordinances of the City. If there be at any future time more than one owner of the Property, all of such then owners, while they are such owners, shall be jointly and severally liable for all obligations under this Agreement.

EXHIBIT A

CTF
All of the Northwest Quarter (NW $\frac{1}{4}$) of the Northeast Quarter (NE $\frac{1}{4}$)
of the Northeast Quarter (NE $\frac{1}{4}$) in Section Nineteen (19), Township
Twenty-eight (28), Range Twenty-four (24), except that part thereof
platted as Minnehaha Woods

PLANNING DEPARTMENT
JUN 25 2015
CITY OF EDINA

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OFFICE OF THE REGISTRAR OF TITLES
HENNEPIN COUNTY, MINNESOTA
CERTIFIED FILED ON

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JUN 22 1992

BY *B. D. Carlson* REGISTRAR OF TITLES
DEPUTY

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Memorandum

DATE: *July 14, 2015*

TO: *Mr. Cary Teague, Planning Director
City of Edina*

FROM: *Charles Rickart, P.E., PTOE*

RE: *REVISED Edina Community Lutheran Church Expansion
Traffic and Parking Study
City of Edina, MN
WSB Project No. 1686 - 32*

Background

In 2013 a Traffic and Parking Study was completed for a proposed expansion of the Edina Community Lutheran Church (ECLC). The proposed expansion at that time included additional seating capacity for the main sanctuary, additional classroom space and removal of the existing residential building on the east side of the site. A new proposal has been submitted to build a sanctuary and kitchen addition and a new parking lot on the east side of the existing church. To accommodate the request, the parsonage home would be removed and replaced with the new parking lot. The new sanctuary addition would have the same seating capacity as the existing sanctuary. The purpose of this study is to determine potential traffic and parking impacts the proposed expansion would have on the adjacent roadway system.

The site is located on the south side of West 54th Street between Minnehaha Blvd and Halifax Avenue. The project location is shown on **Figure 1**. The new plan includes the reconfiguration of the existing parking east of the building maintaining access to 54th Street, and; construction of a new parking lot with full movement access to 54th Street west of the building. The proposed site plan is shown on **Figure 2**.

The traffic and parking impacts of the proposed expansion were evaluated at the following locations.

- 54th Street west of Minnehaha Blvd to east of Halifax Ave
- Halifax Avenue north of 54th Street
- Intersection of 54th Street and Minnehaha Blvd
- Intersection of 54th Street and the site entrances
- Intersection of 54th Street and Halifax Avenue
- ECLC Site Parking Lot

The following sections of this report document the analysis and anticipated impacts of the proposed ECLC expansion.

Existing Traffic Characteristics

The existing lane configuration and traffic control include:

54th Street is an east/west city local street with no turn lanes. An all-way stop is provided at Minnehaha Blvd and a side street stop is provided at Halifax Avenue. Access to adjacent developments and residential property including the Edina Community Lutheran Church site is provided directly from this street.

The City recently completed the Bike Blvd project that modified the existing street configuration by restriping the street with parking lanes, advisory bike lanes and shared vehicle lanes.

Parking is currently allowed on both sides of the street except: west of Minnehaha Blvd where no parking is allowed; on the south side of the street from the Church driveway east for approximately 30 feet where no parking is allowed for site sight lines, and; on the south side of the street for the drop off area in front of the Church where parking is restricted to 15 minutes. The speed limit posted on 54th Street is 30 mph.

Halifax Avenue is a north/south city 2-lane (on lane in each direction) local street with no turn lanes. Access to adjacent residential property is provided directly from this street. Parking is currently allowed on both sides of the street except directly at the intersection of 54th Street where no parking is allowed approximately 30 feet back from the intersection on both sides of the street. The speed limit posted on Halifax Avenue is 30 mph.

Turning movement counts and parking utilization data was collected on Sunday June 10th, 2012 from 7:30am to 1:00pm. In addition hourly directional counts were collected on 54th Street, Halifax Avenue and Minnehaha Blvd beginning on Friday June 8th through Monday June 11th.

Figure 3 shows the existing conditions in the area including: lane configurations and traffic control; average daily and average weekend daily traffic counts; weekday and Sunday peak hour traffic counts, and; the Sunday "Church" peak hour traffic turning movement counts.

Site Trip Generation

In order to determine the impacts the proposed expansion will on the adjacent roadway system the number of trips from the site needs to be determined. For the Church this is based on attendance. Reviewing the Churches attendance records the average attendance (not including Christmas or Easter) for a Sunday is approximately 130 persons at the 11:00 am service. The 8:30 am service has a lower average Sunday attendance, therefore the 11:00 am service was used for the analysis. The attendance at the June 10th 2012 service was 125 persons. It can therefore be concluded that the traffic and parking counts for that day would represent an average event for the Church.

Based on the traffic and parking counts the 125 person attendance was represented by approximately 73 vehicles. Although the expansion is not anticipated to generate additional attendance on an average Sunday, a growth in attendance was assumed for this analysis. Assuming a modest growth in attendance to an average of 150 persons the corresponding traffic growth would be approximately 15 vehicles.

Background Traffic Growth

Traffic growth in the vicinity of the site will occur between existing conditions and any given future year due to other growth and development within the region. This background growth must be accounted for and included in future year traffic forecasts. Reviewing the historical traffic counts in the area, traffic has stayed constant or dropped in the past few years. In order to account for some background growth in traffic a .05% per year factor was applied to the through traffic on 54th Street.

Trip Distribution

Site-generated trips were distributed to the adjacent roadway system based on existing travel patterns, the population distribution relative to the site and the travel sheds for the major routes that serve it. The Trip Distribution was assumed as follows:

- 65% east on 54th Street
- 35% west on 54th Street

Future Year Traffic Forecasts

Traffic forecasts were prepared for the year 2016, which is the year after the proposed expansion would be completed. The traffic forecasts were developed by adding the projected annual background traffic to the existing traffic counts then adding the anticipated additional site traffic to the system based on the traffic distribution outlined above. **Figure 4** shows the projected 2016 Sunday peak hour traffic volume.

Traffic Operations

Existing and forecasted traffic operations were evaluated for each of the study area intersections. This section of the study describes the methodology used to assess the operations and provides a summary of traffic operations.

Analysis Methodology

The traffic operations analysis is derived from established methodologies documented in the *Highway Capacity Manual 2000* (HCM). The HCM provides a series of analysis techniques that are used to evaluate traffic operations.

Intersections are given a Level of Service (LOS) grade from “A” to “F” to describe the average amount of control delay per vehicle as defined in the HCM. The LOS is primarily a function of peak traffic hour turning movement volumes, intersection lane configuration, and the traffic controls at the intersection. LOS A is the best traffic operating condition, and drivers experience minimal delay at an intersection operating at that level. LOS E represents the condition where the intersection is at capacity, and some drivers may have to wait through more than one green phase to make it through an intersection controlled by traffic signals. LOS F represents a condition where there is more traffic than can be handled by the intersection, and many vehicle operators may have to wait through more than one green phase to make it through the intersection. At a stop sign-controlled intersection, LOS F would be characterized by exceptionally long vehicle queues on each approach at an all-way stop, or long queues and/or great difficulty in finding an acceptable gap for drivers on the minor legs at a through-street intersection.

The LOS ranges for both signalized and un-signalized intersections are shown in **Table 1**. The threshold LOS values for un-signalized intersections are slightly less than for signalized intersections. This variance was instituted because drivers’ expectations at intersections differ with the type of traffic control. A given LOS can be altered by increasing (or decreasing) the number of lanes, changing traffic control arrangements, adjusting the timing at signalized intersections, or other lesser geometric improvements. LOS also changes as traffic volumes increase or decrease.

Table 1 - Intersection Level of Service Ranges

	Control Delay (Seconds)	
	Signalized	Un-Signalized
A	≤ 10	≤ 10
B	10 – 20	10 – 15
C	20 – 35	15 – 25
D	35 – 55	25 – 35
E	55 – 80	35 – 50
F	> 80	> 50

Source: HCM

LOS, as described above, can also be determined for the individual legs (sometimes referred to as “approaches”) or lanes (turn lanes in particular) of an intersection. It should be noted that a LOS E or F might be acceptable or justified in those cases where a leg(s) or lane(s) has a very low traffic volume as compared to the volume on the other legs. For example, improving LOS on such low-volume legs by converting a two-way stop condition to an all-way stop, or adjusting timing at a signalized intersection, could result in a significant penalty for the many drivers on the major road while benefiting the few on the minor road. Also, geometric improvements on minor legs, such as additional lanes or longer turn lanes, could have limited positive effects and might be prohibitive in terms of benefit to cost.

Although LOS A represents the best possible level of traffic flow, the cost to construct roadways and intersection to such a high standard often exceeds the benefit to the user. Funding availability might also lead to acceptance of intersection or roadway designs with a lower LOS. LOS D is generally accepted as the lowest acceptable level in urban areas. LOS C is often considered to be the desirable minimum level for rural areas. LOS D or E may be acceptable for limited durations or distances, or for very low-volume legs of some intersections.

The LOS analysis was performed using Synchro/SimTraffic:

- Synchro, a software package that implements Highway Capacity Manual (HCM) methodologies, was used to build each signalized intersection and provide an input database for turning-movement volumes, lane geometrics, and signal design and timing characteristics. In addition, Synchro was used to optimize signal timing parameters for future conditions. Output from Synchro is transferred to SimTraffic, the traffic simulation model.
- SimTraffic is a micro-simulation computer modeling software that simulates each individual vehicle's characteristics and driver behavior in response to traffic volumes, intersection configuration, and signal operations. The model simulates drivers' behaviors and responses to surrounding traffic flow as well as different vehicle types and speeds. It outputs estimated vehicle delay and queue lengths at each intersection being analyzed.

Existing Level of Service Summary

Table 2, below, summarizes the existing LOS at each of the study area intersections based on the current lane geometry and traffic volumes. The table shows that all intersections currently operate at an overall LOS A during Sunday peak hour with all movements operating at LOS B or better.

Table 2 - Existing Level of Service

Intersection	Sunday Peak Hour	
	LOS	Delay (sec/veh)
54 th Street at Minnehaha Ave	A (B)	4
54 th Street at Church Entrance	A (A)	2
54 th Street at Halifax Ave	A (B)	5

C = Overall LOS, (D) = Worst movement LOS

Forecast Traffic Operations

A capacity and LOS analysis was completed for the study area intersections for 2014 which represents the year after the proposed expansion is planned for completion. The results of the analysis are shown below in **Table 3**. All of the intersections are expected to continue to operate at similar levels of service with the proposed increase in attendance as before the expansion.

Table 4 – Expansion (2014) Level of Service

Intersection	Sunday Peak Hour	
	LOS	Delay (sec/veh)
54 th Street at Minnehaha Ave	A (B)	6
54 th Street at Church West Entrance	A (A)	3
54 th Street at Church East Entrance	A (A)	4
54 th Street at Halifax Ave	A (B)	7

C = Overall LOS, (D) = Worst movement LOS

Parking Demand

The parking demand for the site was analyzed based on the existing and anticipated attendance for the Church. Based on the parking inventory and count conducted on June 10th, 2012 there are 37 parking spaces available in the existing Church parking lot (including 3 handicapped spaces) and approximately 35 spaces on 54th Street from Minnehaha Blvd to Halifax Ave south. This represents a total of 72 spaces available on site or adjacent to the Church. There are also an additional 25+ spaces on Halifax Avenue north of 54th Street that could be used during peak attendance days. The peak parking demand on June 10th, 2012 was 73 vehicles between 10:30 and 11:30 am. All of these vehicles were parked in either the existing Church parking lot or on 54th Street. No vehicles were parked on Halifax Avenue. **Figure 5** shows the number of parking spaces available and used based on the parking count conducted.

As discussed in the Traffic Analysis section, if attendance would grow to an average of 150 persons, this would represent an additional 15 vehicles, raising the parking demand to 88 vehicles. The proposed revised site plan includes 14 spaces west of the building and 24 spaces east of the building for a total of 38 spaces. This represents an increase of only one (1) space in the available parking.

It is estimated that on an average attendance day there would not be any change in the parking demand or operations in the parking lot or on 54th Street. However, with any increase in attendance vehicles would be parking on Halifax Avenue. Assuming an increase of 15 vehicles requiring parking, an additional 100 to 200 feet of parking on both sides of Halifax Avenue north of 54th Street would be needed.

Based on the current and proposed seating capacity of 210 people, the current City Code and previous parking agreement requires 70 parking spaces be provided on site for the Church. The Church received a variance for the parking requirement in 1992 for 37 spaces using proof of parking as a justification.

Conclusions / Recommendation

Based on the analysis documented in this memorandum, WSB has concluded the following:

- The proposed Church Expansion will include construction of a new sanctuary and kitchen, removal of the existing residential building, revisions to the existing parking lot on the west side of the building and construction of a new parking lot on the east side of the building.
- Although the expansion is not expected to generate new attendance, assuming a modest growth from 130 persons to 150 persons, the site would generate an additional 15 vehicles / hour on an average Sunday (11:00 am service).
- Traffic operations at the study area intersections and driveways on 54th Street will remain the same with or without the proposed Church expansion.
- Based on the analysis documented in this memorandum, WSB has concluded that, although the available parking does not meet the City's Code, based on the expansion of the Church parking lot and the availability of on street parking, adequate parking spaces are available for the anticipated parking demand using on-street parking on 54th Street and Halifax Avenue.

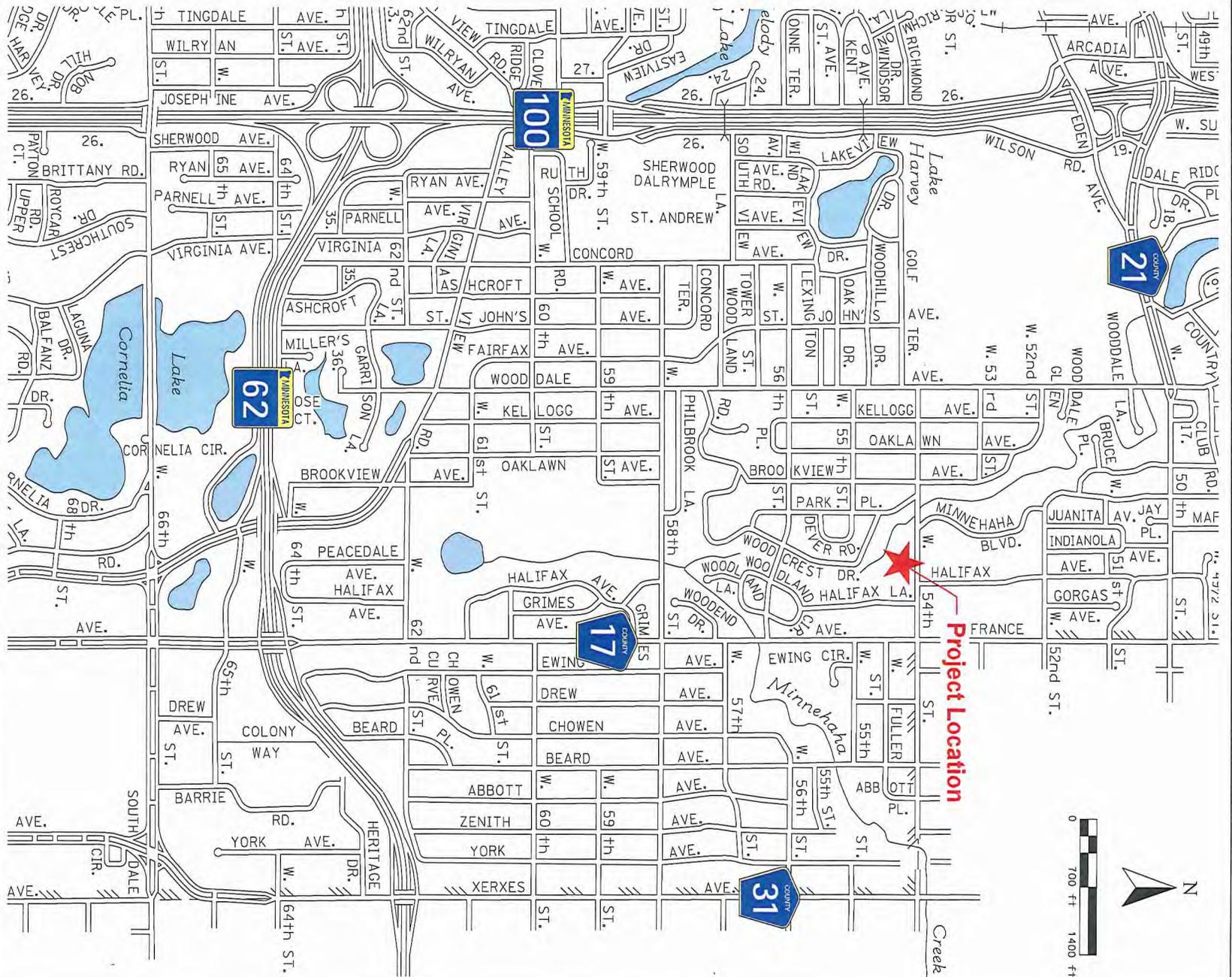
Based on these conclusions no additional improvements other than those shown on the site plan would be required to accommodate the proposed Church expansion.



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 Edina Community Lutheran Church Expansion
 City of Edina, Minnesota

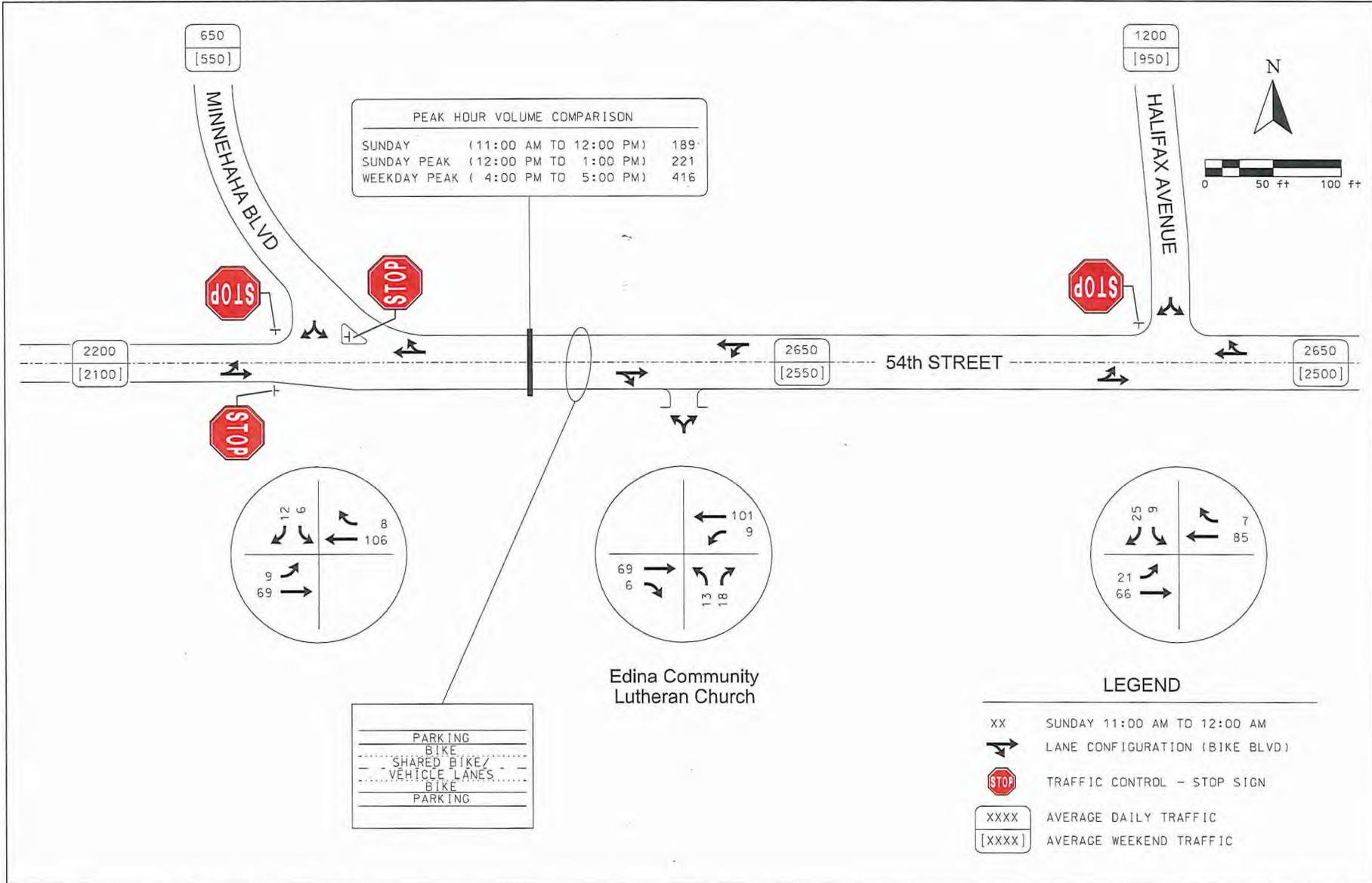
Project Location Map

Figure 1



A68

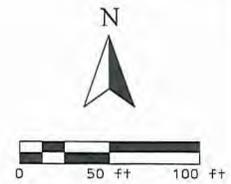
A70



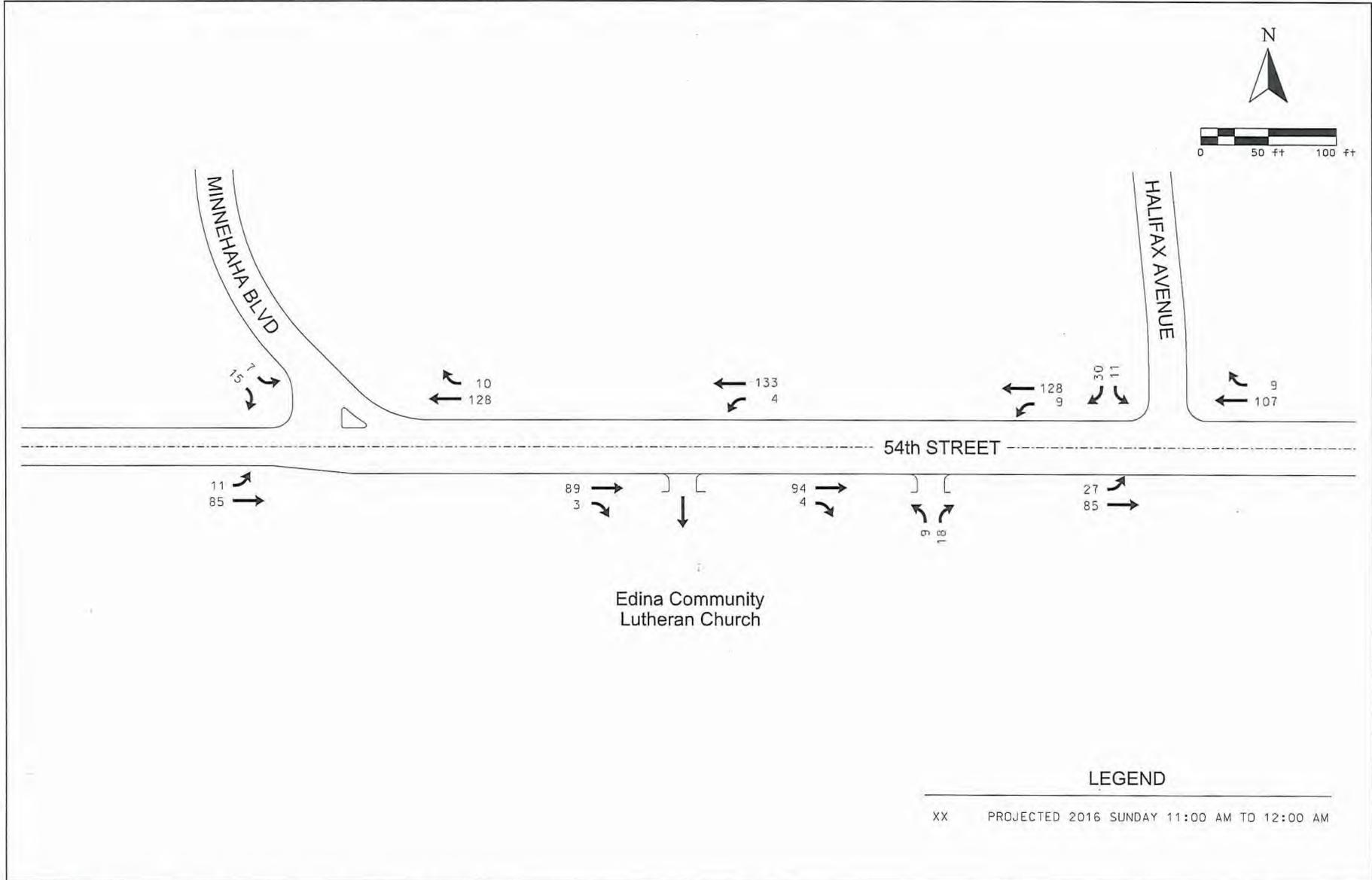
Revised Traffic and Parking Study - July 14, 2015
 Edina Community Lutheran Church Expansion
 City of Edina, Minnesota

Figure 3

Existing Conditions



K71



LEGEND

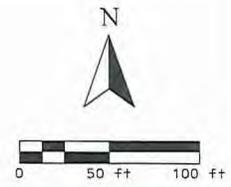
XX PROJECTED 2016 SUNDAY 11:00 AM TO 12:00 AM



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 City of Edina, Minnesota

Figure 4

Projected 2016 Traffic Volumes



A72



Edina Community Lutheran Church

LEGEND

- XX AVAILABLE PARKING SPACES
- (XX) PEAK PARKING USE (10:30 TO 11:30)



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 City of Edina, Minnesota

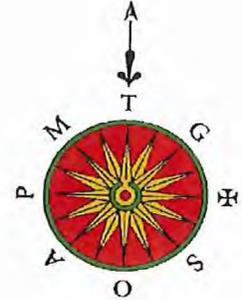
Figure 5

Existing Parking Demand

Pathfinder CRM, LLC

Cultural Resource Management & Heritage Preservation Consultants

319 South Division Avenue
PO Box 503
Spring Grove, Minnesota 55974-0503
507-498-3810 800-206-8704 (toll free)
www.pathfindercrm.com



TO: Cary Teague, Community Development Director
FROM: Robert Vogel, Preservation Planning Consultant
DATE: July 14, 2015
RE: Cultural resources survey of the Rectory at Edina Community Lutheran Church

This letter report provides a summary of the results of a cultural resources survey of the rectory of the Edina Community Lutheran Church, located at 4113 W. 54th Street. The purpose of the investigation was to identify the subject property and gather the information needed to evaluate its eligibility for designation as an Edina Heritage Landmark. Survey activities included background research and field inspection.

Description

The subject property is located on the south side of West 54th Street a short distance east of Minnehaha Creek. The rectory is on the east side of the church and the two buildings occupy the same 4.15 acre lot (Parcel ID 19-028-24-11-0010). Historically, the building functioned as the residence of the church pastor and as rental housing.

The rectory is a one-and-one-half story, four bedroom, frame house with a gable roof and an attached, front-facing garage. The architectural classification is Minimal Traditional but the design incorporates elements of Ranch and other modern movement styles. This type of house was built in great numbers throughout the Twin Cities in the years immediately preceding and following World War II. It was probably the most popular suburban house form used by Edina builders during the 1940s. Very few of these houses were architect designed. The decorative cut stone wall cladding on the street elevation matches that of the church next door. The house, which contains approximately 3000 square feet of living space, is currently vacant.

Historical data relating to the rectory is somewhat sparse. It was built in 1947-48 and remodeled in the 1970s, according to building permit records. Edina Community Lutheran Church was founded in 1948. The property appears as vacant land on the 1940 aerial survey of Hennepin County but the rectory and the church are clearly shown on the aerial photographs flown in 1951.

Evaluation

For a property to qualify for the registry of Edina Heritage Landmarks it must meet one of the heritage landmark criteria for eligibility by being associated with an important historic context and retaining historic integrity of those physical features necessary to convey its significance. Unlike the National Register of Historic Places, the Edina Heritage Landmark program does not disqualify religious properties; as a matter of policy, the city considers churches and related religious buildings as heritage preservation resources when they meet established criteria for historical or architectural significance. A specific property may meet more than one of the eligibility criteria and it may be significant within one or more historic contexts; a property is only required, however, to be documented as significant under a single eligibility criterion and historic context.

For purposes of evaluating its heritage resource value, the church rectory was evaluated individually, and not as a functional component of the church building, within the local historic context "Midcentury Modern Architecture and Landscapes," a thematic planning unit that represents an important aspect of Edina history. Recently developed as part of the city's ongoing preservation planning process, this context is applicable to all residential buildings constructed in Edina between ca. 1935 and 1975. (The Heritage Preservation Board is in the process of developing a city-wide historic context for religious properties that would provide a more detailed framework for evaluating their historical, architectural, artistic and cultural significance. This study unit would also include landmark registration requirements applicable to properties such as rectories and other church-owned residences.)

To be eligible under the Heritage Landmark criteria on the basis of its associative value, the rectory would have to be historically significant for its association with a specific event marking an important moment in Edina history or a pattern of events that represent an important trend in community development. Mere association with historic events or trends is not enough, however, to qualify a building as eligible for designation—the specific association would have to be considered important as well. Although it shows the history and development of the church as a community institution and its history reflects the broad pattern of

postwar suburban life, the rectory cannot be said to have acquired historical significance in its own right.

The rectory would be considered significant for its architectural value if it embodied the distinctive characteristics of a type, period or method of construction, or if it represented the work of a master architect. Based on the results of background research and physical examination, the subject property is not architecturally distinguished when compared with other 1940s era residences in Edina. It was not designed by a notable architect or master builder. Viewed from the perspective of its design and construction values, the rectory is not considered historically significant.

Recommendation

Evaluated from the perspective of its historic context, the church rectory is neither historically nor architecturally significant. Therefore, it does not meet the Edina Heritage Landmark eligibility criteria. No further cultural resource management work is recommended.

cc: Joyce Repya, Senior Planner



DATE: July 15, 2014
TO: Cary Teague – Planning Director
CC: Chad Millner – City Engineer
FROM: Ross Bintner P.E. - Environmental Engineer
RE: 4113 W 54th Street / ECLC – Development Review

The Engineering Department has reviewed the subject property for street and utility connections, grading, storm water, erosion and sediment control.

1. City Standard Plates available here: http://edinamn.gov/index.php?section=construction_standards
2. A separate permit is required from Minnehaha Watershed District: <http://www.minnehahacreek.org/>

Survey

3. None

Soils

4. Submit soils, soil boring and geotechnical report.

Details

5. No comments.

Traffic and Street

6. Commercial entrance should follow standard plate 400 and 410. Please coordinate entrance location with Engineering Department and the currently active 54th Street reconstruction project.
7. A curb cut permit will be required at time of building permit.

Sanitary and Water Utilities

8. Show existing utility connections.
9. A revised SAC unit determination will be required at building permit application.

Storm Water Utility

10. Provide hydraulic and hydrology calculations that meet Minnehaha Creek Watershed District standards. This majority of this site drains directly to Minnehaha Creek, that portion that drains to W 54th Street will not affect available capacity, as the street is currently under construction and a new stormwater system will be installed.
11. Provide copies of maintenance agreement for private stormwater systems.
12. Show existing pipe on utility plan (Sheet C500)

Grading, Erosion and Sediment Control

13. Provide erosion, sediment control plan that meets provisions of MPCA construction site general permit.

ENGINEERING DEPARTMENT

7450 Metro Boulevard • Edina, Minnesota 55439
www.EdinaMN.gov • 952-826-0371 • Fax 952-826-0392

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14. Building permit and engineered structure design will be required for retaining wall over 4' in height at time of building permit.

Other Agency Coordination

15. Minnehaha Creek Watershed permit is required. MDH, MPCA and MCES permits may be required.

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Jackie Hoogenakker

From: Denis Mitchell <djjm99@gmail.com>
Sent: Tuesday, July 14, 2015 4:35 PM
To: Jackie Hoogenakker
Subject: case file 2015.007

Please note that we am TOTALLY in favor of the
Edina Community Lutheran Church
being granted a conditional use permit & variance

Regards
Denis & Faye Mitchell