



PLANNING COMMISSION STAFF REPORT

Originator Cary Teague Community Development Director	Meeting Date November 12, 2014	Agenda # VII.C.
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INFORMATION/BACKGROUND

Project Description

Mount Development and Aurora Investments, LLC are requesting final review for redevelopment of 6500 France Avenue. Specifically, the request is to change the use from medical office, which was previously approved for the site, to 100 units of senior assisted and independent living; 54 units of traditional care/skilled nursing; 34 units of memory care, and 7 care suites. (See location on pages A1-A5.) The "care" suites would include short term stays by patients that have had surgery at Fairview Hospital or Twin City Orthopedic (TCO). A pedestrian skyway would connect the building to the hospital parking ramp to the north. The parking would be provided underground. There would be a 15,000 square foot reduction in the structure footprint from the previously approved medical office plan that was proposed for the site. (See applicant narrative and plans on pages A6–A49.)

The applicant has already received the following approvals of the City Council:

- A Comprehensive Plan Amendment to allow senior housing as a permitted use in the Regional Medical District, at a maximum density of 80 units per acre.
- Preliminary Rezoning to amend the PUD, Planned Unit Development-3 Zoning District; and
- Preliminary Development Plan.

The density of the development excluding the transitional care suites for short term stays of patients of the hospital and TCO is 80 units per acre.

The first floor of the new building would contain uses accessory to the senior housing, including a pub, barber shop/beauty salon, coffee/bistro, and fitness center.

The request before the Planning Commission is as follows:

1. Final Development Plan and Final Rezoning to amend the PUD-3 District.
2. Zoning Ordinance Amendment establishing the PUD.

The proposed plans are consistent with the approved preliminary development plans. As noted on pages A14-A15, of the applicant narrative, the following slight modifications have been made:

- A slight reduction in building square footage (2,000 s.f.)
- A slight decrease to west setback (3.5 feet), but green space was added with the elimination of the concrete sidewalk.
- Slight variance in housing unit counts
- Slight parking modifications, but still code compliant.
- Refined the floor and roof plans.
- Due to anticipated damage to the boulevard trees on France, they have been programed to be removed and replaced.
- Refined the exterior of the skyway to be consistent with the new building and parking ramp.
- Worked with the property owner to the West (Cornelia Place Apartments) regarding entrance and exits and providing additional landscape plantings on the Cornelia Place Apartment property to provide additional screening.
- Lowered the loading dock area and provided an overhead structure for screening.

Surrounding Land Uses

- Northerly: Fairview Hospital parking ramp; zoned APD, Automobile Parking District
- Easterly: Fairview Hospital; zoned and guided Regional Medical District.
- Southerly: Point of France condos; Zoned PRD-4, Planned Residential District & High Density Residential
- Westerly: Cornelia Place Apartments; zoned PRD-4, High Density Residential; and guided HDR, High Density Residential

Existing Site Features

The subject property is 2.34 acres in size, is relatively flat. (See pages A1–A3.)

Planning

- Guide Plan designation: RM, Regional Medical. (See pages A5–A6.)
- Zoning: PUD-3, Planned Unit Development District.

Comprehensive Guide Plan/Density

As mentioned, the Comprehensive Plan guides this site for RM, Regional Medical Use. Senior Housing is now allowed in the RM District, and the density range is a maximum of 80 units per acre. The Met Council has approved the City's Comprehensive Plan amendments regarding the density and senior housing as an allowed use in the RMD District.

Amending the PUD-3 District

The applicant is requesting a rezoning of this site to amend the PUD-3 District to allow Senior Independent and Assisted Living/Nursing Home along with Transitional Care associated with the adjacent hospital and other facilities performing surgery. (See attached draft PUD Ordinance.)

Within a PUD District, the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent of the PUD.

The table on the following page demonstrates a comparison of the base zoning (PUD-3) compared to the proposed.

Compliance Table

	City Standard (PUD-3)	Proposed (PUD-3)
Building Setbacks Front – France Avenue Front – 65 th Street Side – West Rear – South	25 feet 25 feet 15 feet (parking structure) 20 feet	25 & 35 feet 25 feet 43 feet 20 feet
Building Height	Five-Stories and 62 feet	Five-Stories & 62 feet
Maximum Floor Area Ratio (FAR)	1.0%	2.2%*
Parking Stalls	133 – Based on: .5 exposed stalls per unit (50) .25 enclosed stall per unit (25) 1 per employee (33) 1 per 4 residents – nursing/memory/care (25)	134 Provided (126 underground + 10 surface)
Parking Stall Size	8.5' x 18'	8.5 x 18'
Drive Aisle Width	24 feet	24 feet

**Would require a variation from Previous approved PUD*

As demonstrated above, the proposed building complies with all setback requirements of the previously approved PUD. The only change proposed is in regard to the FAR.

Per Section 850.04. Subd. 4 D provides the following regulations for a PUD:

- 1. Purpose and Intent. The purpose of the PUD District is to provide comprehensive procedures and standards intended to allow more creativity and flexibility in site plan design than would be possible under a conventional zoning district. The decision to zone property to PUD is a public policy decision for the City Council to make in its legislative capacity. The purpose and intent of a PUD is to include most or all of the following:**
 - a. provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan;**
 - b. promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City;**
 - c. provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian oriented design, and podium height at a street or transition to residential neighborhoods, parks or other sensitive uses;**
 - d. ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;**
 - e. maintain or improve the efficiency of public streets and utilities;**
 - f. preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;**

- g. allow for mixing of land uses within a development;**
- h. encourage a variety of housing types including affordable housing; and**
- i. ensure the establishment of appropriate transitions between differing land uses.**

The general land uses would be consistent with land uses allowed in the area. The adjacent sites to the south and west are guided High Density Residential. With Senior Housing being a permitted use in the Regional Medical District, the land uses area consistent. Staff believes the proposed land uses, Senior Housing and Transitional Care associated with the hospital would be better use of land than the previously approved Medical Office.

As with the previously approved medical project for the site, this proposal would create a more efficient and creative use of the property. The building would be pulled up close to the street, with sidewalks in front, and separated from the street by green space to promote a more walkable environment. This project goes a step further and provides a skyway pedestrian link to the Fairview Southdale Hospital. (See pages A16-A18.)

Parking would be located under the proposed housing; and would not be visible. The previous approved plans included a large parking ramp setback just 15 feet to the west lot line. The new building would expand that setback to 43 feet. (See page A28a.) Landscaping and balconies are proposed in front along France Avenue. The corner of the building still opens up to France and 65th. The individual store fronts have been eliminated; however, the large store-front type windows remain. Uses on this side of the building would be a coffee shop/bistro and fitness room; so activity within these spaces will be evident from the street.

The applicant is again proposing to utilize sustainable design principals. Green building practices are suggested, and green roofs are proposed. (See pages A10–A12, of the applicant's narrative that explains how this project meets the above purpose and intent of the PUD Ordinance.) The applicant has also agreed to attempt to meet an energy savings goal of 15% over the current state energy code guidelines. A plan of how standards are intended to be met must be submitted prior to issuance of a building permit.

The applicant is also proposing to provide affordable per the Planning Commission and City Council recommendation of 10% of all the senior housing units, which would be a total of eighteen (18) units.

2. Applicability/Criteria

- a. Uses. All permitted uses, permitted accessory uses, conditional uses, and uses allowed by administrative permit contained in the various zoning districts defined in Section 850 of this Title shall be treated as potentially allowable uses within a PUD district, provided they would be allowable on the site under the Comprehensive Plan.**

With the amendment of the Comprehensive Plan to allow senior housing, this site would contain uses that are allowed in the Regional Medical District. The Zoning Ordinance amendment, which follows this staff report, lists the uses that would be allowed on this site. WSB and Associates did a parking analysis that determined that the proposed parking would support the uses proposed, and the traffic generated would actually be less than the previously approved medical office. (See pages A61-A88.)

- b. Eligibility Standards. To be eligible for a PUD district, all development should be in compliance with the following:**
 - i. where the site of a proposed PUD is designated for more than one (1) land use in the Comprehensive Plan, the City may require that the PUD include all the land uses so designated or such combination of the designated uses as the City Council shall deem appropriate to achieve the purposes of this ordinance and the Comprehensive Plan;**

The proposal would include a mixture of land uses. It would include senior housing, nursing home/assisted living type housing, memory care, and care suites associated with the adjacent hospital. Retail would also be provided as an accessory use to the residents of the building, including a coffee shop on the corner. A skyway connection is proposed to connect the uses to Fairview Southdale Hospital.

- ii. any PUD which involves a single land use type or housing type may be permitted provided that it is otherwise consistent with the objectives of this ordinance and the Comprehensive Plan;**

As mentioned above, the proposed uses would be senior housing, care suites and limited retail, consistent with Comprehensive Plan as senior housing is acceptable in the Regional Medical District.

- iii. permitted densities may be specifically stated in the appropriate planned development designation and shall be in general conformance with the Comprehensive Plan; and**

The proposed residential density of 80 units per acre is consistent with the Comprehensive Plan, which limits senior housing in the RMD District to 80 units per acre. Senior housing typically does not generate as much traffic as an all age apartment or medical office. Density is typically tied to proximity to low density residential areas, availability to provide utilities to the site (sewer and water), proximity to transit service and impact on roadways.

This site is adequately served by public utilities, there is adequate sewer capacity, the use would generate less traffic than the previously approved office building, and transit service is available on France and 65th. Staff believes the density is appropriate for this site given the adjacent high density land uses, proximity to the hospital, the provision of the skyway connection tying the land uses together and the lesser impact on the roads than the approved medical office. Additional items that might warrant higher density would be the underground parking, affordable housing element, and public art proposed.

iv. the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent described in #1 above.

The proposed project does closely relate to the already approved PUD setbacks for the site as demonstrated above. For the reasons stated above, staff believes the purpose and intent of the PUD Ordinance is met.

Site Access

The primary access to the site would remain off of 65th Street West. There would be no right-out only onto France; as was allowed in the previous PUD.

Parking

Per Section 850.08 Subd. 1, the following are the parking requirements: Senior Housing – 5 exposed stalls per unit; .25 enclosed stalls per unit and one exposed stall per employee & company vehicle. Nursing, Convalescent & Rest Home – 1 enclosed stall per 4 residents. Based on this requirement the project is to provide 50 enclosed spaces and 83 exposed for a total of 133 spaces. The applicant is proposing to provide 126 enclosed and 10 exposed spaces. Residents, employees and visitors can all access the underground parking by an audio/video intercom system. (See pages A8–A13 of the applicant narrative.)

A parking study was done by WSB which concludes that the proposed parking would support the uses. The total demand for parking is anticipated to be 125 spaces.

Traffic

A traffic study was also done by WSB, which concludes that the existing roadways support the proposed uses. (See traffic study on pages A61–A88.) The proposed use would generate less traffic than the approved medical office. The medical office was anticipated to generate 279 trips in the AM peak hour and 216 trips in the PM peak hour. The proposed use would generate 27 trips in the AM peak hour and 40 trips in the PM peak hour.

However, as was conditioned in the approvals for Twin City Orthopedic and Fairview Southdale Hospital, should signal improvements be deemed necessary at 65th Street and France Avenue, the property owner would be required to participate in appropriate cost sharing for signal improvements. This would be a requirement in the Developer's Agreement.

Landscaping

Based on the perimeter of the site, 34 overstory trees and a full complement of understory trees and shrubs are required. The applicant is proposing to plant 43 overstory trees around the perimeter of the site & 200+ understory trees and shrubs. Additionally, the applicant is proposing to plant 22 evergreen trees on the Cornelia Place Apartment site to provide additional screening. (See landscape plan on pages A18 and A37.)

Grading & Utilities

The city engineer has reviewed the plans and found them acceptable and offered comments. (See page A59-A60.)

Noise Study

Based on the issues during preliminary review of this project the chillers have been located on the roof of the building away from the adjacent residential properties to the south and west. They would be screened with a pre-finished metal screen. A noise study has been completed and found that the units would conform to the noise ordinance. (See pages A45-A49.)

Signage

Signage would be allowed per the requirements of the Regional Medical District within the Zoning Ordinance Amendment for the PUD. (See attached draft Ordinance.) This would be consistent with the previous PUD approval.

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issue

- **Is the proposed rezoning to PUD appropriate for the site?**

Yes. Staff believes the proposal to rezone the site to PUD is reasonable for the site for the following reasons:

1. The proposal would create a more efficient and creative use of the property. The building would be pulled up close to the street with a podium height of two-stories, with sidewalks in front, and separated from the street by green space to promote a more walkable environment. The skyway connection adds an element of connectivity not found in the previous project, providing a convenient pedestrian connection for residents of the proposed building and patients of the hospital.
2. Parking would be located under the proposed housing; and would not be visible. The previous approved plans included a large parking ramp setback just 15 feet to the west lot line. This building expands that setback to 43 feet. (See pages A60a-A60d and A43.) Landscaping and balconies are proposed in front along France Avenue. The corner of the building still opens up to France and 65th. The individual store fronts have been eliminated; however, the large store-front type windows remain. Uses on this side of the building would be a coffee shop and fitness room; so activity within these spaces will be evident from the street.
3. The applicant is proposing to utilize sustainable design principals. Green building practices are suggested, and green roofs are proposed. (See pages A10–A12, of the applicant’s narrative that explains how this project meets the above purpose and intent of the PUD Ordinance.) The applicant has also agreed to attempt to meet an energy savings goal of 15% over the current state energy code guidelines. A plan of how standards are intended to be met must be submitted prior to issuance of a building permit.
4. The building includes a podium height of two-stories along the street, which would give pedestrians on the sidewalks in front a feeling that the building is not as tall. Comprehensive Plan contemplates allowing a maximum podium height of two-stories at the street. This two-story podium was also a part of the previously approved medical office plans.
5. The proposed uses would be an even better fit in to the neighborhood. The residential component is consistent with the high density residential apartments to the south and west. The transitional care is consistent with the medical uses to the north and east.

6. The existing roadways would support the project. WSB conducted a traffic impact study based on the proposed development, and concluded that the traffic generated from the project would not impact the adjacent driveways or intersections. In fact the proposed uses would actually generate less traffic than the previously approved medical building. No additional improvements other than those shown on the site plan would be required to accommodate the site redevelopment. (See traffic study on pages A61–A88.)
7. The PUD ensures that the building proposed would be the only building built on the site, unless an amendment to the PUD is approved by City Council.
8. The proposed project would meet the following goals and policies of the Comprehensive Plan:
 - a. Building Placement and Design. Where appropriate, building facades should form a consistent street wall that helps to define the street and enhance the pedestrian environment. On existing auto-oriented development sites, encourage placement of liner buildings close to the street to encourage pedestrian movement.
 - Locate prominent buildings to visually define corners and screen parking lots.
 - Locate building entries and storefronts to face the primary street, in addition to any entries oriented towards parking areas.
 - Encourage storefront design of mixed-use buildings at ground floor level, with windows and doors along at least 50% of the front façade.
 - Encourage or require placement of surface parking to the rear or side of buildings, rather than between buildings and the street.
 - b. Movement Patterns.
 - Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
 - Limit driveway access from primary streets while encouraging access from secondary streets.
 - Provide pedestrian amenities, such as wide sidewalks, street trees, pedestrian-scale lighting, and street furnishings (benches, trash receptacles, etc.)
 - c. Encourage infill/redevelopment opportunities that optimize use of city infrastructure and that complement area, neighborhood, and/or corridor context and character.

9. Higher densities are justified for the following reasons: The project would be connected to the Fairview Hospital by the second level skyway, tying the project to the Hospital; Senior Housing would generate less traffic than the approved medical building with retail on the site; existing roadways would support the project; adequate utilities are available to the site; convenient transit service is available for workers and residents; the building would include sustainable design principles; public art is proposed; affordable housing is offered; and primary parking would be below grade.

Staff Recommendation

*Final Rezoning to Amend the Planned Unit Development-3 District
& Preliminary Development Plan*

Recommend that the City Council approve the Final Rezoning to amend the PUD-3 District, and approve the Final Development Plan.

Approval is based on the following findings:

1. The proposed land uses are consistent with the Comprehensive Plan.
2. The site layout would be an improvement over a site layout required by standard zoning; the building is brought up to the street, provides podium height, and front door entries toward the street, includes sidewalks to encourage a more pedestrian friendly environment along the street, provides underground parking, and provides an indoor pedestrian connection to the hospital.
3. The design of the building is of a high quality brick, architectural precast concrete, and glass, and is compatible with previously approved medical building.
4. Traffic would be improved in the area by eliminating the right-in and out access on France Avenue.
5. Based on the traffic study done by WSB, the existing roadways can support the proposed development. Traffic generated by the proposed project would be less than the approved medical building for the site.
6. The proposed project would meet the following goals and policies of the Comprehensive Plan:
 - a. Building Placement and Design. Where appropriate, building facades should form a consistent street wall that helps to define the street and enhance the pedestrian environment. On existing auto-oriented

development sites, encourage placement of linear buildings close to the street to encourage pedestrian movement.

- Locate prominent buildings to visually define corners and screen parking lots.
- Locate building entries and storefronts to face the primary street, in addition to any entries oriented towards parking areas.
- Encourage storefront design of mixed-use buildings at ground floor level, with windows and doors along at least 50% of the front façade.
- Encourage or require placement of surface parking to the rear or side of buildings, rather than between buildings and the street.

b. Movement Patterns.

- Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
- Limit driveway access from primary streets while encouraging access from secondary streets.
- Provide pedestrian amenities, such as wide sidewalks, street trees, pedestrian-scale lighting, and street furnishings (benches, trash receptacles, etc.)

c. Encourage infill/redevelopment opportunities that optimize use of city infrastructure and that complement area, neighborhood, and/or corridor context and character.

7. The project would be connected to the Fairview Hospital by the second level skyway, tying the project to the Hospital.
8. Convenient transit service is available for workers and residents; the building would include sustainable design principles; public art is proposed; affordable housing is offered; and primary parking would be below grade.

Final approval is subject to the following conditions:

1. Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, unless modified by the conditions below:
 - Site plan date stamped October 15, 2014.
 - Grading plan date stamped May 23, 2014.
 - Utility plan date stamped May 23, 2014.
 - Landscaping plan date stamped May 23, 2014.
 - Building elevations date stamped October 15, 2014

- Building materials board as presented at the Planning Commission and City Council meeting.
2. Prior to issuance of a building permit, a final landscape plan must be submitted, subject to staff approval. The Final Landscape Plan must meet all minimum landscaping requirements per Section 36-1436 through 36-1462 of the City Code. Additionally, a performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures.
 3. The property owner is responsible for replacing any required landscaping that dies.
 4. The Final Lighting Plan must meet all minimum requirements per Section 36-1260 of the City Code.
 5. Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
 6. Sustainable design. The design and construction of the entire project must be done with the Sustainable Initiatives as outlined in the applicant's narrative within the Planning Commission staff report. Attempts must be made meet an energy savings goal of 15% over the current state energy code guidelines. A plan of how standards are intended to be met must be submitted prior to issuance of a building permit.
 7. Compliance with all of the conditions outlined in the director of engineering's memo dated November 5, 2014.
 8. Final Rezoning is subject to a Zoning Ordinance Amendment creating the PUD, Planned Unit Development for this site.
 9. All buildings must be built with sprinkler systems, subject to review and approval of the fire marshal.
 10. As part of a Developers Agreement the property owner would be required to participate in appropriate cost sharing for signal improvements at 65th Street and France Avenue.
 11. Deliveries on the west side of the building shall be limited to 9:00 am to 4:00 pm.
 12. Affordable housing units shall be 10% of the assisted and independent units. (18 units.)

13. Adoption and compliance with a PUD Ordinance for the site.

PUD Ordinance

Recommend the City Council adopt the Ordinance Amendment revising the PUD-3 Zoning District.

Deadline for a city decision: No deadline

ORDINANCE NO. 2014-__
AN ORDINANCE AMENDMENT REVISING PUD-3, EDINA MEDICAL BUILDING
INTO PUD-3, AURORA ON FRANCE A SENIOR HOUSING
AND CARE SUITE FACILITY AT 6500 FRANCE AVENUE

THE CITY COUNCIL OF EDINA ORDAINS:

Section 1. Subsection 36-490 of the Edina City Code. Planned Unit Development District – 3, (PUD-3), Edina Medical Building is amended as follows:

Sec. 36-490. Planned Unit Development District-3 (PUD-3), ~~Edina Medical Building~~ Aurora on France.

(a) *Legal description.*

- (1) All of Lot 4 and the easterly 56.44 feet of Lot 3, Block 2, Southdale Office Park Second Addition, Hennepin County, Minnesota; and
- (2) Lot 3, Block 2, except the easterly 56.44 feet thereof, Southdale Office Park Second Addition, Hennepin County, Minnesota.

(b) *Approved plans.* Incorporated herein by reference are the 6500 France Edina Medical Building Aurora on France plans received by the city on ~~November 6, 2012,~~ **May 23 and October 15, 2014** except as amended by city council Resolution No. ~~2012-176~~ **2014-__**, on file in the office of the planning department under file number ~~2012-003.12a.~~ **2012.003.14a.**

(c) *Principal uses.*

(1) All principal uses allowed in the Regional Medical District (RMD) Zoning District, except drive-through uses.

(2) Senior Independent, Assisted Living/Nursing Home, Transitional Care Suites and Memory Care Suites as proposed in the Approved Plans above.

(d) *Accessory uses.* The following are the accessory uses allowed in the Regional Medical District (RMD):

- (1) Off-street parking facilities.
- (2) Produce stands, pursuant to permit issued by the city manager.
- (3) Signs allowed per the Regional Medical District.

(e) *Conditional uses.* There are no conditional uses for Planned Unit Development District-3 (PUD-3).

(f) *Development standards.* Development standards per the RMD zoning district, except the following:

- (1) Building setbacks.
 - a. Front.
 1. France Avenue: 25 feet.
 2. 65th Street: 25 feet.
 - b. Side—West: ~~100~~ 43 feet.
 - c. Rear—South: 20 & 30 feet.
- (2) Parking ramp setbacks.
 - a. Front.
 1. France Avenue: 80 feet.
 2. 65th Street: 25 feet.
 - b. Side—West: 15 feet.
 - c. Rear—South: 20 feet.
- (3) Building height: five stories or 62 feet.
- (4) Maximum floor area ratio: ~~100~~ 220 percent.
- (5) Chiller/mechanical equipment setbacks. If the footprint is larger than 36 square feet in area or six feet in height, utility and/or mechanical equipment shall be required to meet the front setback requirements in subsection (f)(1) of this section. The side and rear setback requirements shall be six feet. Mechanical equipment must also meet the following conditions:
 - a. All mechanical equipment accessory to any building shall be screened from all lot lines and streets in accordance with section 36-1459.
 - b. Noise from mechanical equipment shall be subject to the city's noise regulations in accordance with article II, division 5 of chapter 16.

(Ord. No. 2012-23, § 1, 12-18-2012)

Section 2. This ordinance is effective immediately upon its passage.

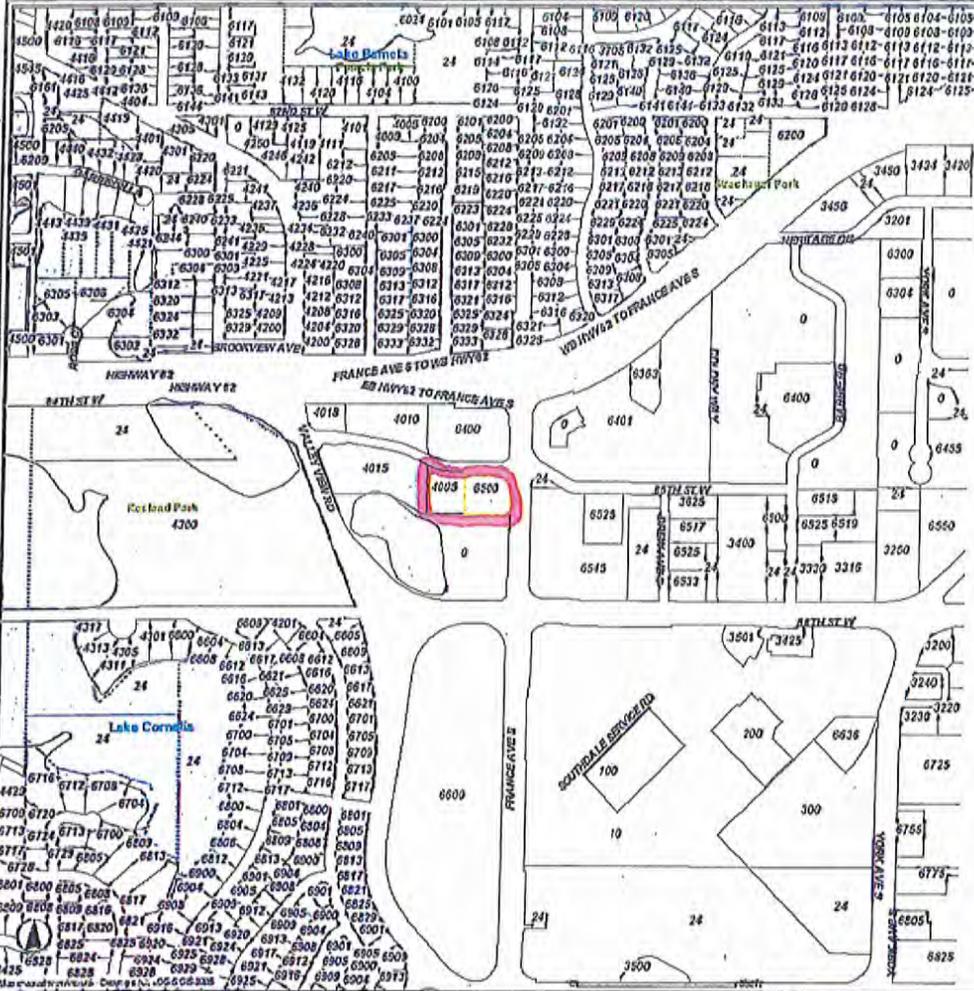
First Reading:
Second Reading:
Published:

Attest

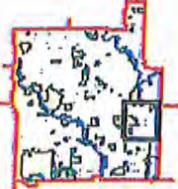
Debra A. Mangan, City Clerk

James B. Hovland, Mayor

City of Edina



- Legend**
- Highlighted Feature
 - House Number Labels
 - Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Parks
 - Parcels

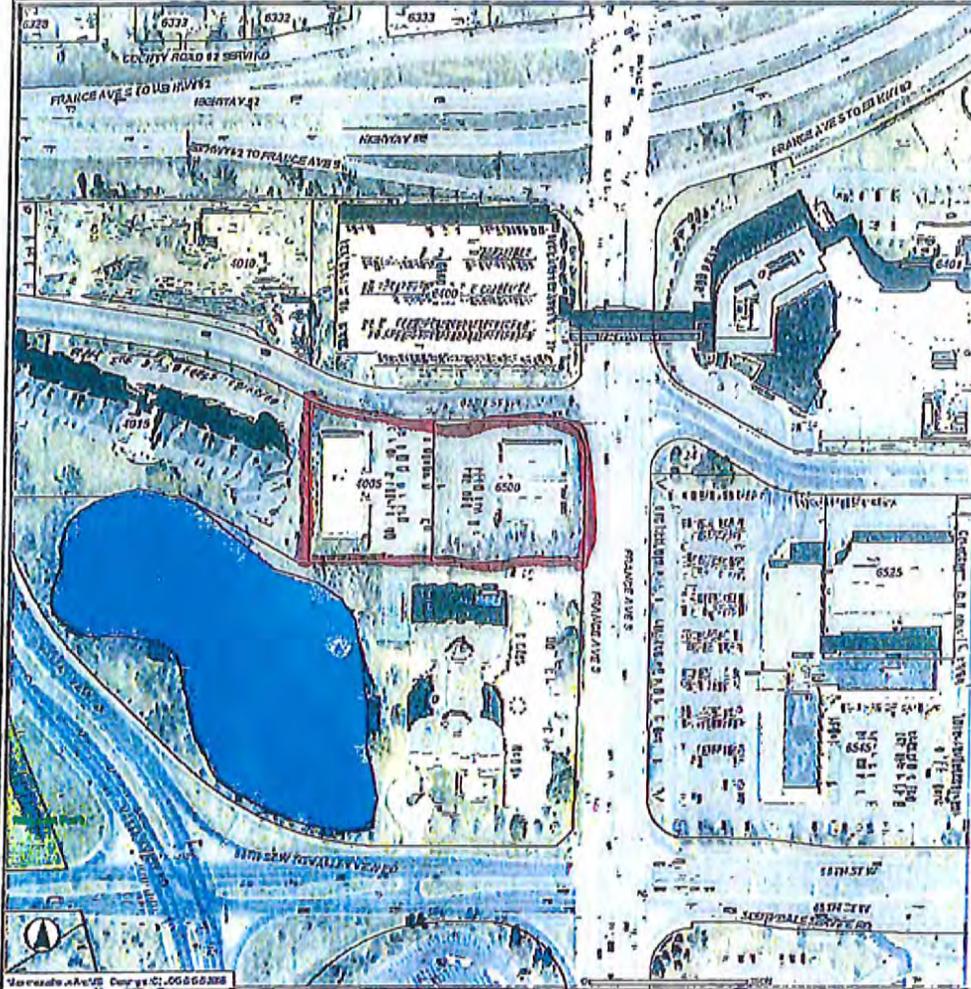


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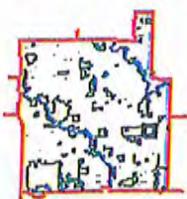
6500 France Ave S
Edina, MN 55435



City of Edina



- Legend**
- House Number Labels
 - Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Parcels
 - 2000 Aerial Photo

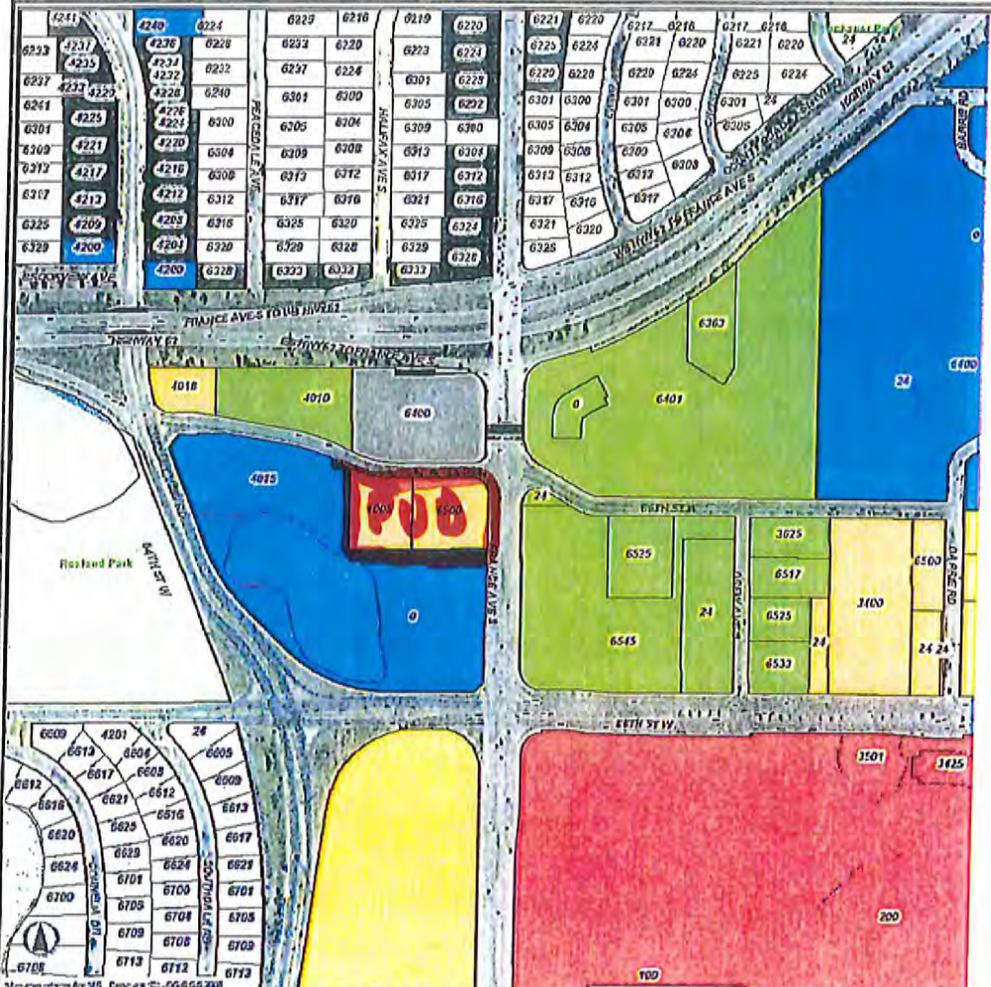


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6500 France Ave S
Edina, MN 55435



City of Edina

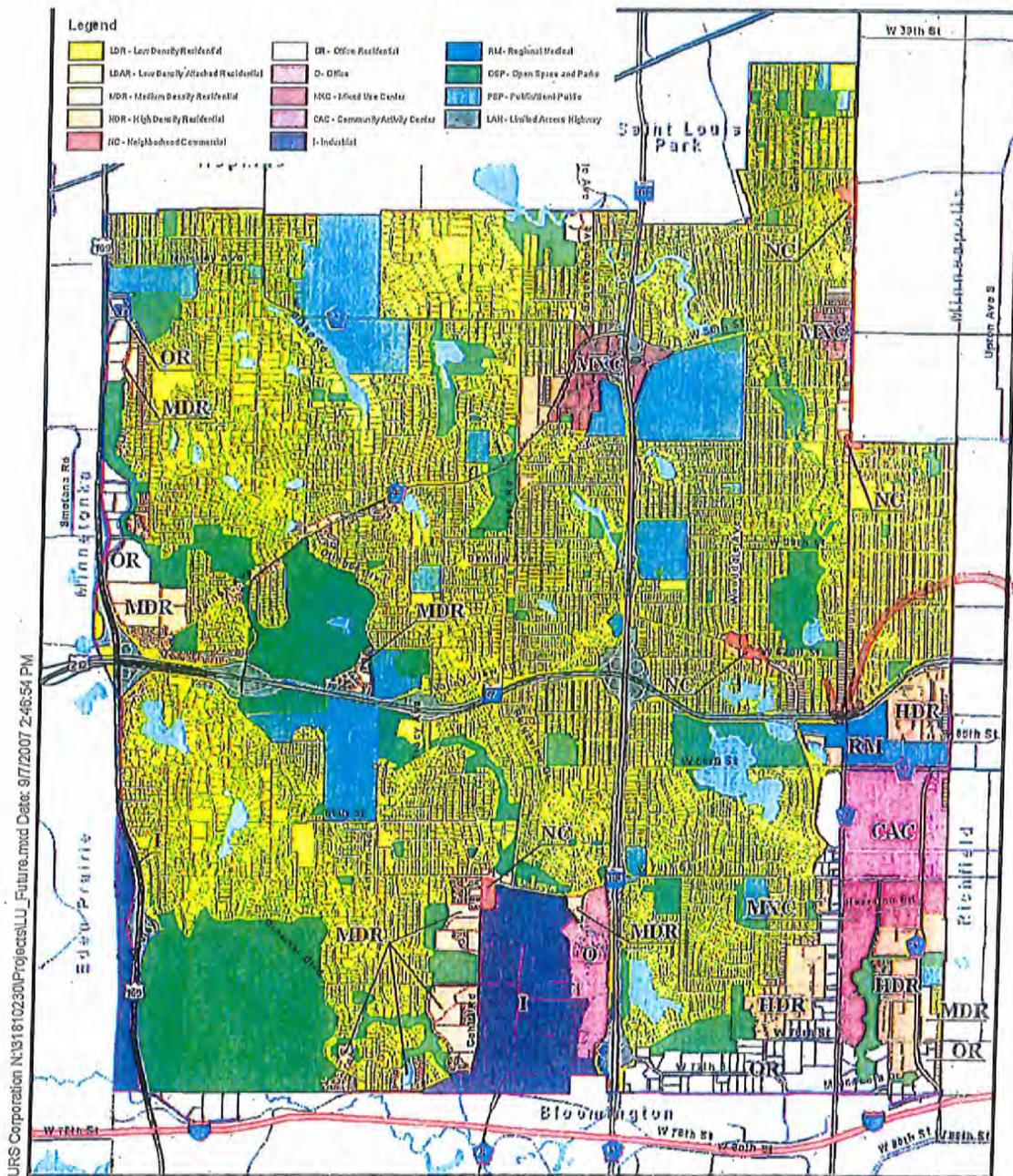


- Legend**
- House Number Labels
 - Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Zoning
 - APD (Automobile Parking District)
 - MDD-1 (Mixed Development District)
 - MDD-2 (Mixed Development District)
 - MDD-3 (Mixed Development District)
 - MDD-4 (Mixed Development District)
 - MDD-5 (Mixed Development District)
 - PCD-1 (Planned Commercial District)
 - PCD-2 (Planned Commercial District)
 - PCD-3 (Planned Commercial District)
 - PCD-4 (Planned Commercial District)
 - PCD-5 (Planned Commercial District)
 - PI (Planned Industrial District)
 - POD-1 (Planned Office District)
 - POD-2 (Planned Office District)
 - PRD-1 (Planned Residential District)
 - PRD-2 (Planned Residential District)
 - PRD-3 (Planned Residential District)
 - PRD-4 (Planned Residential District)
 - PRD-5 (Planned Residential District)
 - PSR-1 (Planned Senior District)
 - R-1 (Single Dwelling Unit)
 - R-2 (Double Dwelling Unit)
 - RMD (Regional Medical District)
 - Parklets
 - 2009 Aerial Photo



PID: 3002824140008
 6500 France Ave S
 Edina, MN 55435





Side

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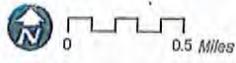
Figure 4.3



City of Edina
2008 Comprehensive Plan Update

Future Land Use Plan

Data Source: URS



AS

Aurora on France
6500 France Avenue South

Project Summary

May 23, 2014

Our development team is looking forward to working with the City of Edina to obtain the final approvals for the **Aurora on France** project. Everyone will strive to make this building an outstanding addition to this regional medical area.

Development Team

- Architect:** Edward Farr Architects, Ed Farr
- Civil Engineering:** Alliant Engineering, Mark Rausch
- Developer:** Mount Development Co., Stephen Michals
- Owner:** Aurora Investments, LLC, Luigi Bernardi
- Tenant/Operator:** Ebenezer Society, Susan Farr

Please contact Stephen Michals with any questions on this new building - 952-941-1383.

Site Area = 102,965 sq ft / 2.34 acres

Property Guided: RM Regional Medical, amended to include Senior Housing (2013 action)

Property Zoned: Planned Unit Development – 3 (2013 action)

Proposed Redevelopment: New Senior Care Building, 227,577 sq ft, 5 stories; plus one level of under-building parking.

Proposed Occupancy:

- Senior Citizen Dwelling Units:
 - Independent & Assisted Living Units – 100 units
- Nursing, Convalescent, Rest Homes:
 - Transitional Care Suites – 54 units
 - Memory Care Suites – 34 units
 - Care Suites – 7 units

Proposed Parking:

8 Surface Stalls + 126 Under-Building Stalls = 134 Stalls total.

Required parking: 133

Proposed Building Setbacks:

North	25' min / 50' max.
South	30' on east end / 20' on west end
East	25' at street level / 35' at podium level
West	44' at grade level / 54' at podium level

History: We received approval of a 5 story medical office building and parking ramp in December, 2012 for this property. Aurora Investments has purchased both land parcels. Demolition is complete on the 6500 France Av structure and the 4005 W. 65th St structure.

Overview: Ebenezer Society will manage this property for a Specialty Senior Care Housing Facility. We feel that this new use is an improvement to our previously approved use in the following ways:

1. The entire block bordered by Valley View Rd on the west, France Av on the east, W. 65th St on the north and W 66th St on the south will become all Housing, for consistency of uses on this block.
2. There will be substantially less traffic generated by this housing development versus the previously approved medical office use – 400 less cars parked on site!
3. There will be approx. 15,000 sq ft less 'roof area' on our building versus the previously approved medical office building and parking ramps roofs.
4. The cubic volume of building enclosure is approx. 25% less than our previously approved medical office building and parking ramp enclosed volume, reducing the visual mass of the structure.

Planning Concept: As per our earlier application, the lot combination of the two parcels (6500 France & 4005 W 65th St) is complete; and it offers many advantages to the site layout. We are still adopting an urban, pedestrian-friendly, streetscape along France Ave by bringing the building forward to the street; as well as incorporating a 'podium' design effect by setting the building back above 2nd floor to maintain a comfortably-scaled pedestrian experience. The streetscape includes over-story trees along the right-of-way, a 10 ft wide sidewalk and decorative planters that contain colorful plantings (annuals, perennials and low evergreen shrubs). Vehicular access is available at two driveways along 65th St W – one at the main entry / visitor drop off area; and a second for parking and delivery vehicles on the west side of

our site.

The City is currently in process to update several intersections along France Avenue at 66th St, 69th St and 70th St. The City's consultant, WSB Associates, has provided the preliminary designs at those intersections for reference. Our proposed project has been refined to incorporate a consistent design in the southwest quadrant of the 65th St and France Avenue intersection including the addition of raised planters between two new relocated pedestrian ramps. There are final traffic signal design details to be done by others that will need to be coordinated with the City Engineer and County regarding the existing traffic signal base, pedestrian crossing buttons and hand-hole relocations, as needed.

Building Design: The building design will be an attractive façade using multiple colors of face brick and warm-toned architectural precast concrete wall panels with a variety of surface finishes. All of the windows will be tinted bronze-tone Low-E glass. Most of the windows will be tan colored prefinished fiberglass windows; and a few of the feature curtainwall windows will be constructed with prefinished champagne color aluminum frames. A small amount of EIFS decorative cornice trim caps the parapet. Multiple façade planes, parapet projections and exterior balconies offer dimensional relief to the façade. The main entrance located off of W 65th St serves as a visitor drop-off area for residents, and features a partially covered drop-off canopy for weather protection. The NE building corner at France Ave and W 65th St has its distinctive glazed crown, backlit at night, to provide a regional point of identity for the building. The corner plaza area will offer outdoor seating, plus a public art piece, and also coordinates with the City's initiative to upgrade the pedestrian experience at these corners. The plaza will have decorative concrete surfacing treatment, planting areas and bollards behind the curb.

Visual Screening for the Adjacent Properties: We will supplement the already mature landscape buffer between Point of France and our site with new trees and bushes along our south yard. The ash trees along this borderline with Point of France are approx. 35' – 50' tall, providing excellent screening for most months of the year.

On the west, facing Cornelia Place Apartments, we are employing significant screening strategies to our garage entrance and service area as follows:

1. We covered the loading dock area with a decorative roof structure to mitigate views into the loading area.
2. We moved the HVAC chiller equipment away from the loading area to minimize any noise for the apartments.
3. We recessed the EXIT garage door in the NW corner so it will be visually screened.
4. We provided a visual screen wall and roof cover at our ENTRY garage door in the SW corner.
5. We are planting 22 evergreen trees on Cornelia Place Apartments property to allow that owner to strategically place those trees where they feel they are best used. In addition, we will be landscaping the property line with a dense row of evergreen trees to provide a visual buffer.
6. We will limit the hours of the loading dock deliveries to the hours of 9am to 4pm (unless otherwise coordinated with neighboring buildings).

Parking: There are 8 exterior surface stalls at the main entrance drop-off, used primarily for short-term visitor parking. The remainder of the parking, 126 stalls, is under the building in an enclosed level that has security access control. Residents, employees and visitors can all access this under-building parking level; via an audio/video intercom system. Refer to the Parking Calculation page of our submittal for parking calculations.

Service Court: Deliveries are quite infrequent for this building, estimated as follows:

- Garbage & Recycling pick up - 3 times a week, max. Short trucks only.
- Food Service deliveries - US Foods (2 times a week) and Bix 6 times a week. Periodically, they may add one delivery per week for special needs.
- Medical and Linen Supply trucks – up to 3 times a week. Step van vehicles, typically.
- Pharmaceutical deliveries - typically weekly, but we require daily when needed. Small vehicles only (car or van).
- Resident Move-in / Move-outs – varies, but a 1 to 3 per week is average.

Sanitary Sewer and Watermain: The property is currently encumbered by two public utilities - an existing 12" ductile iron water main and 12" ductile iron sanitary sewer in the south quarter of the property. The existing water main and sanitary sewer referenced serve other properties and thus the continuity of those mains is required. There will be no disruption to the adjacent roadways.

An existing 8" sanitary service line that previously served the 4005 and 6500 buildings will be removed, capped and abandoned at the west property line. A connection will be made to the existing 12" sanitary sewer main along the southern property line of the 6500 France Ave property to service the developments proposed building.

The existing 12" water main loop cutting through the site will be relocated and re-routed to the south side of the property parallel to the sanitary sewer main. An 8" water service for the development is proposed from a tee off the re-routed 12". A new hydrant is proposed to service the south side of the building. The proposed service will be routed along the east side of the proposed building and connect to the NE corner of the building.

The existing Point of France building south of the development site currently receives water service from the existing 12" trunk watermain within the proposed development site. It is planned that the new watermain will be installed, tested and operational before the existing Point of France service is disconnected and connected to the relocated trunk watermain. This should reduce the length of time required to have water service disrupted to the Point of France.

Stormwater Management: The proposed storm water management storage facilities meet the requirements of Nine Mile Creek Watershed and the City of Edina. The three primary requirements that have been met are:

1. Volume retention onsite equivalent to 1" of runoff over the entire proposed site impervious surface. (Accomplished via a series of rain gardens and an underground infiltration chamber system).
2. Water quality volume from entire site equivalent to runoff from the 2" type II storm event with 25 years of planned sediment storage. (Retention volume counts towards WQ volume and remainder accomplished via the rain gardens and additional underground infiltration chamber).
3. Discharge rate control shall be provided so the proposed conditions do not exceed existing conditions for the 2, 10, and 100 yr storm events. The proposed site plan reduces the amount of impervious surface by 10.5%, reducing proposed discharge rates. The rain gardens and infiltration chamber also contain the entire 2-yr event for their contributing watersheds.

The proposed plan is to maintain the same point of storm water discharge from the properties as is currently present. A private storm sewer collection system will be routed from north to south through the western portion of the site collecting the majority of site runoff. The storm sewer will drain to an offline pretreatment chamber to the proposed underground storage system at the western end of the site which will ultimately outlet to the City pond south of the property. An agreement to construct the storm sewer discharge point to the City pond will be coordinated with the Point of France property owners. Storm water runoff from the south side of the building will sheet drain to a series of two proposed rain gardens. The rain gardens will overflow for larger events to an existing 15" CMP culvert that currently serves the property.

Landscape Design Strategies: The landscape design provides a mix of over-story, coniferous and ornamental trees, shrubs and perennials to create a vibrant display of color and foliage. The four existing Honeylocust trees along France Avenue and seven of the boulevard Ash trees along West 65th Street will be replaced. Raised curbed planters are to be provided along France Avenue that will be planted with colorful, annual and perennial flowers. Coordination will occur with City staff to provide landscaping along France Avenue that will be consistent with the work proposed by the City at other intersections. The perimeter of the building will be planted with a mixture of plant types to soften and compliment the building architecture. The building's service area and parking entrance will also be screened by existing and proposed conifers on the west and southwest side. The diversity of plantings will provide color variety and year round interest. In addition, the project is proposing to provide a green roof system on top, covering approximately 7,823 sf. This sustainable initiative will help reduce the heat island effect, reduce stormwater runoff quantity, as well as providing a nicer view down on our roof from the residents of Point of France building.

Noise Ordinance Compliance: We are proposing two outside air-cooled chillers for heat rejection, located under the crown feature on the NE corner of our roof. They will be visually screened with an overhead metal trellis structure, and not seen from the public way at all. An acoustical analysis has been performed and attached to demonstrate compliance with zoning noise ordinance limits.

Site Lighting: Decorative wall mounted lighting along France Av and W 65th St frontages, and around the main entry area. Decorative city sidewalk light poles, as prescribed by City Engineering Dept, along the W 65th St and France Av sidewalks. Resident unit balcony wall lights will be low-wattage down lights to provide minimum level of illumination. The glazed crown feature at rooftop level at the corner of France Av and W 65th St will be backlit at night for a nice glow.

Site Signage: Building name / address at corner of France & W 65th St., and address above front entry. Directional signs at both entries along W 65th St.

Hours of Use: 24/7 resident use.

Green Building Practices: Throughout all phases of the project - *Design, Construction and Operation*, we will use best practices of environmental awareness. Ebenezer is fully educated on *Reduce-Recycle-Reuse* operations. We have a Green Building Practices Narrative with our submission. We have retained our green roof area, which can be enjoyed by residents on the north side of Point of France.

Community Benefits from the New Project

1. The previous buildings, parking lots and landscaping had substantial deferred maintenance problems. The property is ready for a fresh, new development.
2. This building provides senior and rehab service to support the regional medical presence of the SW suburbs. This is consistent with the City Guide Plan.
3. The building is sized to have a critical mass of services to allow cross referrals among other senior facilities in Edina. This is a key factor for the success of each specialized service area. The building will offer cost effective services to seniors as a strategic support to the hospital.
4. Edward Farr Architects is known for creating special building design features. The focal point will be the glass and brick detail of the main entry corner creating a Gateway Building to the Southdale area. Numerous surface changes occur on each side of the building to create interest and shadow elements.
5. We are supporting the pedestrian environment along France Avenue with flower gardens and sitting areas. Accent lighting table seating will make this area a pleasant visual experience.
6. The skyway will link the building to the hospital for visitor and patient services.
7. Green Building Practices will be implemented for the three phases: *building design, construction phase and long term operation* of the clinics. The General Contractor has compiled a summary of the *Green Practices* we will pursue for each phase of the project. Periodic reports during construction will demonstrate our progress.
9. A portion of the building roof will have a vegetated green roof, with native wild flowers and sedum, to enhance the view down onto the roof from the neighboring Point of France building, as well as reducing our heat island effect and improving our stormwater runoff quantity. Annual reports will show the reduction in the heat island effect along with estimates of water volume enhanced.
10. There will be ongoing efforts to evaluate methods and new technology for our employees to: – Reduce consumption, Re-use materials, and Recycle waste. The benefit to the building will be reduced operating expenses and good stewardship of our business resources. Reports will be provided on the quantities of recycling.
11. Alternative transportation is a key element of any project. A tangible goal will be to reduce the number of cars coming to the building which will reduce the parking stalls required to service the building. We have provided several successful alternate transportation systems:

Scooter, motorcycle, bike parking will be an enclosed space that is secure, well lit, and air tempered. We want employees and residents to know they are recognized for their efforts.

Employees may participate in the ZAP chip monitor system which will provide a monthly printout of number of rider days. There will be a calculation of carbon footprint poundage saved to each participant.

The MTC #6 bus line services multiple stops for the building. Additional routes around Southdale are 515, 538, 539, 578, 579, 684.

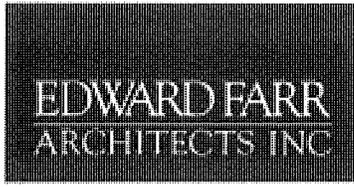
12. There will be over 52 staff positions in the building. In addition, there will be 40-60 construction jobs over 12 months.
13. Storm water management will be improved through *rate control* and *water quality*, including our green roof. The current site has direct, unrestricted runoff. There is almost 13% less hard surface area in the new plan than the current buildings, which means more green space for the community.
14. The new development is using existing utilities and roads in the community. Urban planning considers this good stewardship to reuse existing sites with current infrastructure.
15. The corner plaza will have an area for a significant piece of art. We are planning a special commissioning for this feature.

AURORA ON FRANCE

May 23, 2014

		# of Units	# of Beds	Parking Calculation	Enclosed Parking Required	Exposed Parking Required	Enclosed Parking Provided	Exposed Parking Provided
Nursing Home uses per zone PRD-5								
Traditional Care/Skilled Nursing Licensed Beds	54	63	1 enclosed stall per 4 residents	16				
Memory Care	34	40	1 enclosed stall per 4 residents	10				
Care Suites	7	7	1 enclosed stall per 4 residents	2				
	Subtotal	95	110		28	0		
Senior Citizen Dwelling uses per zone PSR-4								
Independent and Assisted Living	100		.5 exposed stalls per unit			50		
			.25 enclosed stalls per unit	25				
Employees			1 exposed per employee = 29	1	29			
			1 enclosed per management = 1					
	Subtotal	100		26	79			
Totals	195			54	79	126	8	
				Total Required 133		Total Provided 134		

A13



7710 Golden
Triangle Drive
Eden Prairie
Minnesota 55344
Tel: 952-943-9660
Fax: 952-943-9665
www.edfarrarch.com

May 30, 2014

Cary Teague, Community Development Director
City of Edina
4801 W. 50th Street
Edina, MN 55424

Re: **Aurora on France**
6500 France Av S. Redevelopment
Final Development Plan – Supplemental Information

Cary,

This letter itemizes the design changes from the Preliminary Development Plan approved in July 2013 to the current Final Development Plan Application submitted May 23, 2014.

- Slight reduction in building square footage (approx. 2,000 sf less).
- Slight decrease to west side yard setback (3½ ft+/-), but increase in green space by elimination of concrete sidewalk in that west yard.
- Minor change to senior housing unit counts.
- Modified required parking and provided updated parking counts (still compliant).
- Refinements to front plaza (corner of France & 65th) to coordinate grades, skyway columns and planters.
- Interior floor plan refinements, all floors.
- Refined roof plan showing all incidental plumbing vents, roof drains and exhaust vents.
- Refined exterior elevations with updated window pattern and HVAC grille locations.
- The existing boulevard trees along France Ave. are now scheduled for removal and replacement; we would have been excavating too close to their root structures, causing significant root damage.
- We completed the exterior design for the skyway crossing W. 65th St.
- On the west side, the vehicle entrance/exits to the lower level have been redesigned to split the ingress and egress locations for better traffic circulation. We've discussed this design change with our Cornelia Place neighbor; and receive his approval. In return, we have offered additional evergreen trees to be planted off-site on their property, now 22 in total, to further screen their views. The final location of the trees will be field identified in coordination with Cornelia Place representation. Further, architectural wing walls with an overhang have been added to the new southwest vehicle entrance to screen that overhead door.
- The west delivery area has been redesigned with a 4' foot "dropped" truck delivery pavement, so that we could build the overhead dock structure that screens this area, while achieving truck clearances. The grading plan has been revised to lower the delivery area and lower the west driveway entrance to accommodate the elevation change. A small retaining wall has been added along the west side of the driveway for the same reason.
- Relocated the water service connection to northeast building corner.
- Relocated / revised the sanitary sewer connection to the 12" trunk sewer along the south side of the

site. The existing 9" sewer to the west will now be no longer used and abandoned.

- Added a second rain garden system on the south side of the building to provide additional above ground infiltration volume.
- The proposed underground storm water management system was reconfigured to use a 'proprietary' style design. A lower profile storage system was required as a result of the lowered truck dock.

If you have any questions about these design changes, please feel free to contact me at 952.943.9660.
Thank you,

Sincerely,

Edward Farr Architects, Inc.



Edward A. Farr, AIA
President

C: Steve Michals, Mount Development Co
Luigi Bernardi, Aurora Investments, LLC

6500 France Building
Town Center Offerings & Activities

Ebenezer would like to make several common areas of the building accessible to the residents of *Point of France*.

The rooms and activities are designed for all residents in the new building and we welcome your participation in many of the services outlined below.

****Please provide the front desk a 24 hour reservation notice
for the selected activity****

- ❖ **Theater** – 40 reclining seats with a custom screen create a special theater atmosphere with evening star lights in the ceiling that reminds of the outside "drive-in" movies. Presentations will include movies and cable offerings. Popcorn machine and concessions are available!
- ❖ **Convenience Gift Shop** – The main floor will have a retail convenience store with cards, gifts and partial service pharmacy.
- ❖ **Dining Room** – There will be a 160-seat dining room overlooking the south terrace, which will have an upscale traditional theme and servers taking tableside orders. Lunch or dinner may be purchased off the menu. A full-time chef will create daily specials and each month will feature special ethnic dinner parties.
- ❖ **Bistro** – Our northeast corner will have an internet café open to the plaza with seating around the special flower gardens and pedestrian way. Offerings will be premium coffee, ice cream, light meals and other refreshments.
- ❖ **Community Room** – This room is used for music events or special lectures of interest. There will be frequent OSHER Life Long Learning lectures from the University of Minnesota.
- ❖ **Beauty Shop** – Hair Stylist will have a daily schedule and will offer an array of salon services by appointment only.

**All Offerings and Prices will be subject to change without notice. 5/18/13*

A151



Part of Fairview Health Services

Mr. Cary Teague
Community Development Director
City of Edina
4801 W 50th Street
Edina, MN 55424

Ebenezer
2722 Park Avenue
Minneapolis, MN 55407-1009
Tel: 612-874-3460
Fax: 612-874-3465

May 31st, 2013

Dear Mr. Teague,

The 6500 France Avenue location is a gateway into Edina, providing Ebenezer an opportunity to build a senior living community offering Independent Living, Assisted Living, Care suites, Transitional Care Suites, and Observation Rooms for Ebenezer's nearby affiliate Fairview Southdale Hospital and the Twin City Orthopedics center. The new facility would feature Healthsense, state-of-the-art technology, and offer the following: a heated, underground parking garage, full-service salon, exercise room, massage therapist, bistro, club lounge, full-service dining room, gift shops, library/computer lounge with Wi-Fi, theatre and a concierge service, bus lines and bike paths for employees, pharmacies, and shopping. It also faces nature and the lake, providing residents with activity views and beautiful scenery. Ebenezer is looking forward to developing in and partnering with Edina.

Thank you,

Susan Farr

A156



Minnesota Department of **Human Services**

Elderly Waiver Program

What is the Elderly Waiver Program?

The Elderly Waiver (EW) program funds home and community-based services for people age 65 and older who are eligible for Medical Assistance (MA) and require the level of care provided in a nursing home, but choose to reside in the community. The Minnesota Department of Human Services operates the EW program under a federal waiver to Minnesota's Medicaid State Plan. Counties, tribal entities and health plan partners administer the program.

What types of services are available?

Covered services include:

- Adult day service
- Case management
- Chore services
- Companion services
- Consumer-directed community supports
- Home health aides
- Home-delivered meals
- Homemaker services
- Licensed community residential services (customized living services or 24-hour customized living services, family and corporate foster care, residential care)
- Environmental accessibility adaptations
- Personal care
- Respite care
- Skilled nursing
- Specialized equipment and supplies
- Personal Emergency Response Systems
- Training and support for family caregivers
- Transitional supports
- Nonmedical transportation

Who is eligible?

- Those eligible for the EW are 65 or older, eligible for MA and need nursing home level of care as determined by the Long-Term Care Consultation process.
- The EW service cost for an individual cannot be greater than the estimated nursing home cost for that same individual.
- The person chooses to receive home and community-based services instead of nursing facility services.

How many people? How many dollars?

In fiscal year 2012, EW served 31,320 people at a total cost of approximately \$311,817,288. Ninety-two percent of EW participants receive their services through a managed care organization. The managed care program options include the Minnesota Senior Health Options (MSHO) program, an integrated Medicaid/Medicare health and long-term care program; and Minnesota Senior Care Plus (MSC+), a Medicaid health and long-term care option.

The average monthly EW participant population for fiscal year 2012 was 22,357 with an average monthly cost of \$1,504 under fee-for-service purchase and \$1,124 per participant per month under managed care.

What alternatives exist for people who are eligible for EW?

Probable alternatives to EW include Medicaid-certified skilled nursing facilities and certified board-and-care homes. The average cost of these alternative settings is \$5,054 per person, per month, less a resident contribution toward cost of care.

Where can I learn more about the EW program?

The EW program is described in Minnesota Statutes 256B.0915.

Where can I learn more about managed care for seniors?

More information about managed care for seniors is on the DHS website.

How can I enroll?

Contact your county's social services or public health department. If you are already on Medical Assistance and enrolled in a health plan, you should contact your health plan.

How do I obtain more information as a provider of home and community-based services?

See the Elderly Waiver and Alternative Care chapter of the Minnesota Health Care Provider Manual.

Call the Senior LinkAge Line® at 800-333-2433 for more information about the program.

Log on to www.MinnesotaHelp.info for more information. MinnesotaHelp.info is an online directory of services designed to help people in Minnesota identify resources such as human services, information and referral, financial assistance, and other forms of aid and assistance within Minnesota.

This information is available in accessible formats for individuals with disabilities by calling 651-431-2400, toll-free 800-747-5484, or by using your preferred relay service. For other information on disability rights and protections, contact the agency's ADA coordinator.

Final Development Plan



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 CITY OF EDINA
 PLANNING DEPARTMENT

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6500 France Avenue

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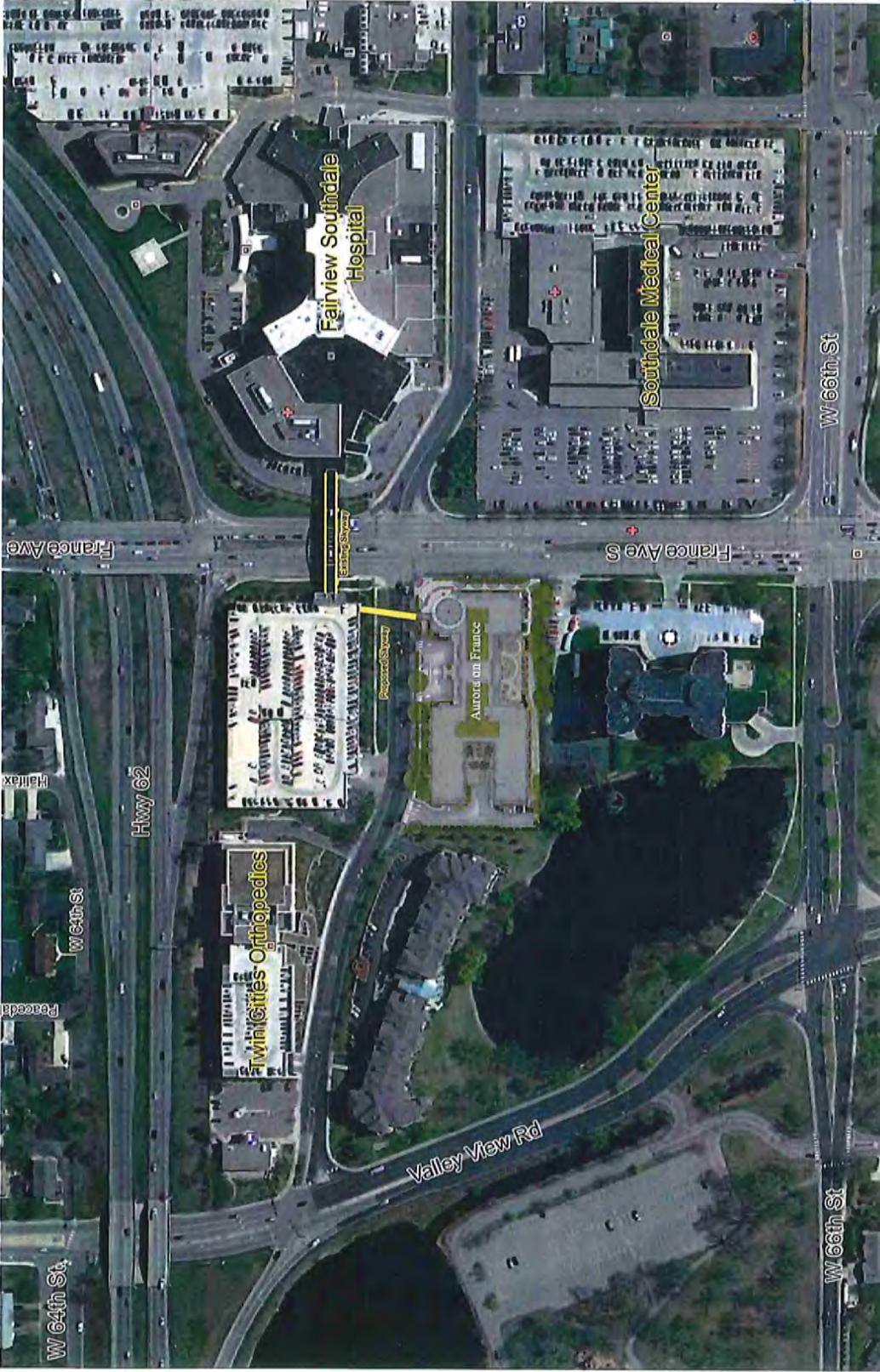
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 Investments, LLC



MOUNT DEVELOPMENT CO.

AURORA ON FRANCE

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Regional Map

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Aerial View Looking Southwest



View Looking Northwest

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View Looking Southeast



View Looking at Front Entry

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 October 14, 2014

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A20



View Looking at Terrace



View Looking Northeast

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 CITY OF EDINA

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 October 14, 2014

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Aa1

THESE ELEVATIONS ARE TO BE USED IN CONJUNCTION WITH THE SPECIFICATION BY VARIOUS AIA DIVISIONS TO THE 2012 IBC. THE ARCHITECT ASSUMES RESPONSIBILITY FOR THE ACCURACY OF THE INFORMATION AND THE DESIGN OF THE ELEVATIONS AND THE MATERIALS LISTED HEREIN. THE ARCHITECT IS NOT RESPONSIBLE FOR THE DESIGN OF THE STRUCTURE OR THE FOUNDATION OF THE BUILDING.

DATE: 10/14/14
 PROJECT: AURORA ON FRANCE
 DRAWING NO.: 13.025
 SHEET: EAST/WEST ELEVATIONS

MATERIALS LEGEND

- 1 FACE BRICK #1 - FIELD
- 2 FACE BRICK #2 - BASE
- 3 FACE BRICK #3 - ACCENT
- 4 EPS
- 5 PRECAST (PCI) CORNICE
- 6A BRONZE TINTED GLASS IN ALUMINUM FRAMES
- 6B BRONZE TINTED GLASS IN PREFINISHED FIBERGLASS FRAMES
- 7 ARCH'L PC CONC #2 - ADD ETCH TO MATCH BRICK #2
- 8 DECORATIVE WALL LIGHT
- 9 CLASS DOWN
- 10 SIGNAGE/ADDRESS NUMBERS
- 11 PREFINISHED METAL PANELS
- 12 PREFINISHED ALUM QUADRAL
- 13 BALCONIES - CONCRETE
- 14 O.H. DOOR - PAINTED
- 15 LOUVERS
- 16 PREFINISHED METAL TRIM
- 17 WALL MOUNTED AREA LIGHTING
- 18 PREFINISHED HVAC GRILLE
- 19 MISCELLANEOUS SIGNAGE

EDWARD FARR ARCHITECTS INC.
 10000 University Ave. S., Suite 100
 Minneapolis, MN 55425
 Tel: 612.338.1100
 Fax: 612.338.1101
 www.edwardfarr.com

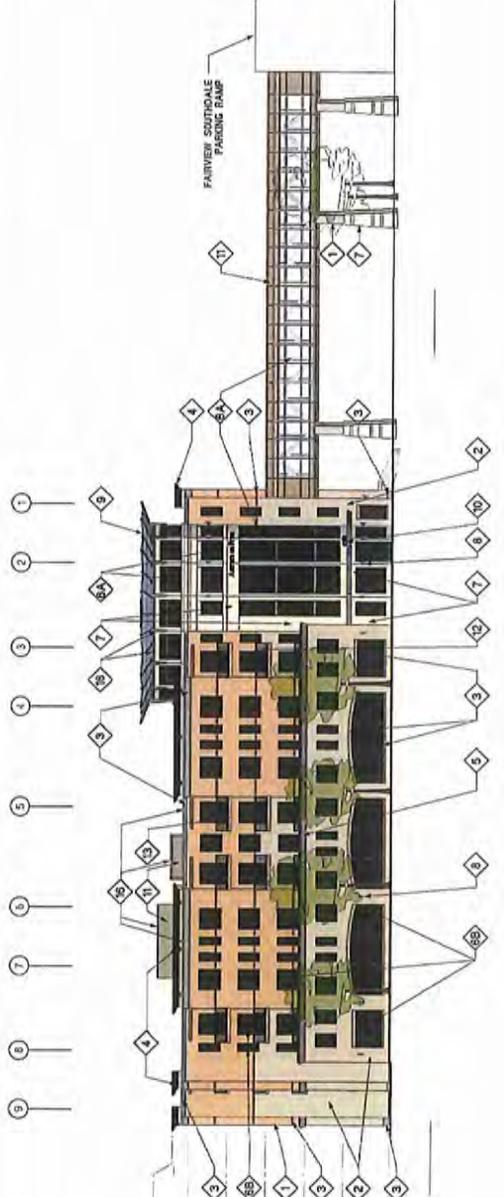
AURORA
 An AIA Member Firm
 8500 FRANCE AVENUE SOUTH
 AURORA, MINNESOTA

ARCHITECT: EDWARD FARR ARCHITECTS INC.
 PROJECT: AURORA ON FRANCE
 FINAL DEVELOPMENT PLAN

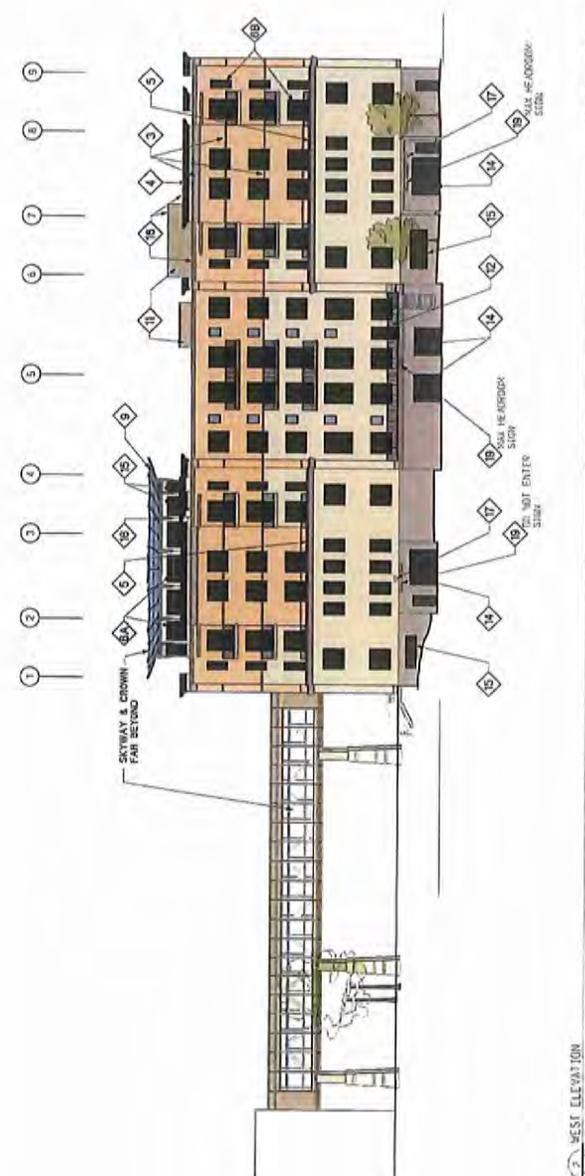
ADDRESS: 8500 FRANCE AVENUE SOUTH
 AURORA, MINNESOTA

OWNER:	AMT DEVELOPMENT CO.
DESIGNER:	EDWARD FARR ARCHITECTS INC.
DATE:	10/14/14
SCALE:	AS SHOWN

SHEET TITLE: EAST/WEST ELEVATIONS
 PROJECT NUMBER: 13.025
 DRAWN: [Name]
 CHECKED: [Name]
 DATE: 10/14/14



1 EAST ELEVATION
 SCALE: 1/8" = 1'-0"



2 WEST ELEVATION
 SCALE: 1/8" = 1'-0"

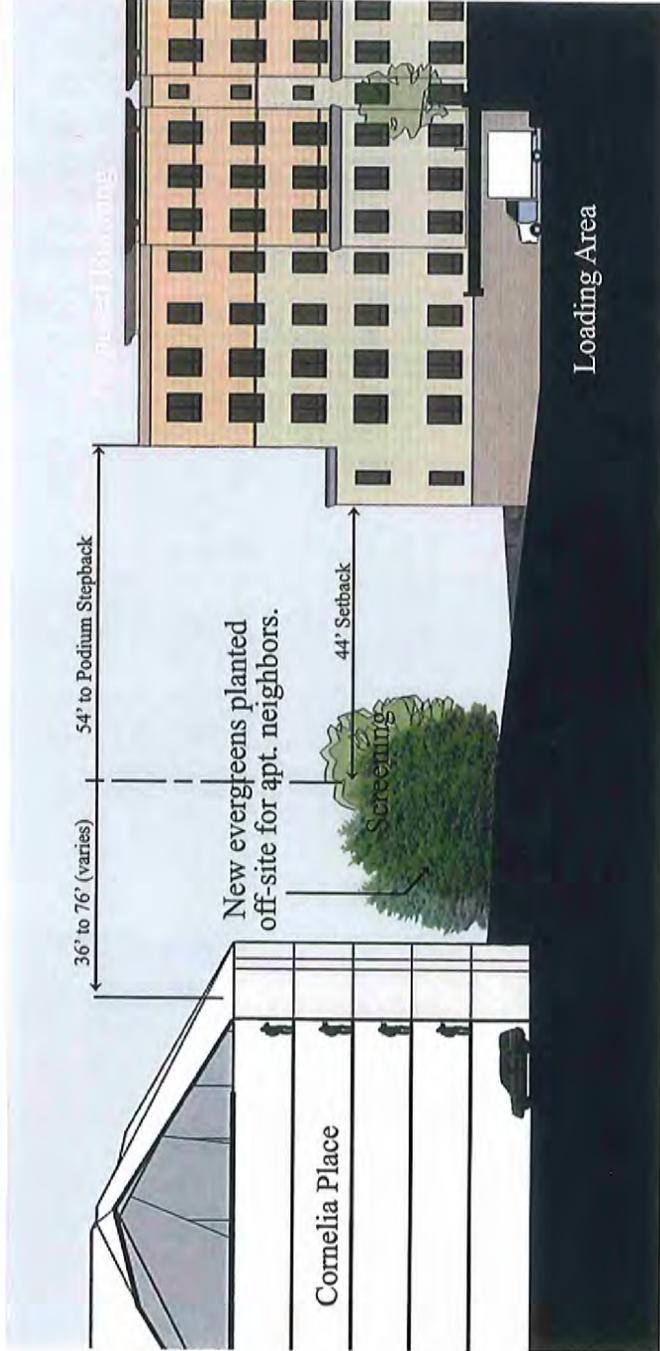
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A23



View Notes

Cornelia Residents no longer have a view of an Office Rooftop (previous condition) nor the view of a Parking Ramp (previously approved condition).



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Screening Strategies West End

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AURORA ON FRANCE



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Investments, LLC



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From 1st Floor Apartment



From 2nd Floor Apartment



From 3rd Floor Apartment



From 4th Floor Apartment

Views from Cornelia Place

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October 14, 2014

CITY OF EDINA
OCT 13 2014

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AURORA ON FRANCE



AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

A25

59.0' Above 1st Floor
 = 942.5'
 = 59.4' Above Curb
 = 59.0' Above Grade

61.5' Above 1st Floor
 = 945.0'
 = 61.2' Above Curb
 = 60.5' Above Grade

71.5' Above 1st Floor
 = 954.0'
 = 70.5' Above Curb
 = 70.0' Above Grade



71.5' Above 1st Floor
 = 954.0'
 = 70.5' Above Curb
 = 70.0' Above Grade

883.5 Building Grade
 883.7 Top of Curb
 883.0 Existing Grade

884.5 Building Grade
 883.8 Top of Curb
 884.0 Existing Grade

1st Floor = 0.0 = 883.5'
 884.5 Existing Grade

Top of Curb Along France Ave

Note: Existing Grade Elevation taken at location of new building footprint

East Elevation - 6500 France Avenue - Primary Architectural Front

59.0' Above 1st Floor
 = 942.5'
 = 59.5' Above Curb
 = 59.0' Above Grade

62.0' Above 1st Floor
 = 945.6'
 = 65.00' Above Curb
 = 62.00' Above Grade

61.5' Above 1st Floor
 = 945.0'
 = 65.2' Above Curb
 = 61.5' Above Grade

59.0' Above 1st Floor
 = 942.5'
 = 64.5' Above Curb
 = 59.5' Above Grade



883.5' Building Grade
 883.0' Top of Curb
 884.0 Existing Grade

883.5' Building Grade
 880.5' Top of Curb
 879.0 Existing Grade

883.5' Building Grade
 879.8' Top of Curb
 878.0 Existing Grade

883.0' Building Grade
 878.0' Top of Curb
 876.0 Existing Grade

879.0' Building Grade
 876.8' Top of Curb
 876.0 Existing Grade

Top of Curb Along 65th St. W.

North Elevation - 65th Street West

EDWARD FARR ARCHITECTS INC

Aurora on France - Building Height Measurements



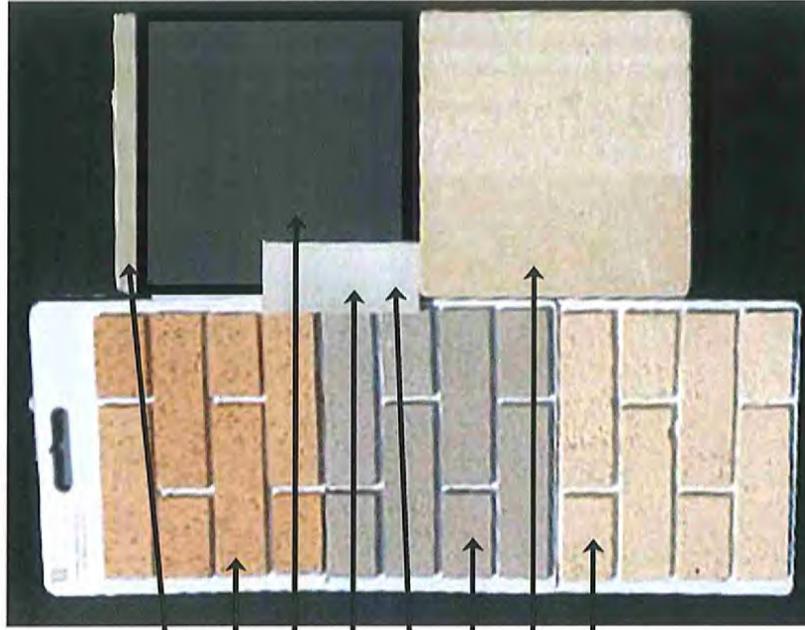
A26

CITY OF EDINA
 OCT 15 2014

Finish materials in shaded area



Finish materials in sunny area



- ① Fiberglass Window Frame
- ② Face Brick #1
- ③ Bronze Tinted Glass
- ④ Champagne Aluminum Window Frame
- ⑤ Prefinished Metal Panels
- ⑥ Face Brick #3
- ⑦ Decorative Precast Concrete
- ⑧ Face Brick #2
- ⑨ Corresponds to Elevation Sheets A5.1 and A5.2

Finish Materials

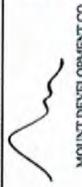
May 23, 2014

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

DRAWING INDEX
 ALL DRAWINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FOLLOWING CODES AND STANDARDS:
 1. IBC 2009
 2. IBC 2009
 3. IBC 2009
 4. IBC 2009
 5. IBC 2009
 6. IBC 2009
 7. IBC 2009
 8. IBC 2009
 9. IBC 2009
 10. IBC 2009
 11. IBC 2009
 12. IBC 2009
 13. IBC 2009
 14. IBC 2009
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 94. IBC 2009
 95. IBC 2009
 96. IBC 2009
 97. IBC 2009
 98. IBC 2009
 99. IBC 2009
 100. IBC 2009

PROJECT: AURORA ON FRANCE
 PROJECT NUMBER: 13.025
 DATE: 11/20/13
 DRAWING: ARCHITECTURAL SITE PLAN
 SCALE: 1" = 30'-0"

Code Required Parking: 1/4 Residents
 Nursing Home, 100 Units = 28 stalls (enclosed)
 Senior Citizen Dwelling, 100 Units = 25 stalls (enclosed)
 Resident = 25 stalls (enclosed)
 Management = 25 stalls (enclosed)
 Required = 54 stalls (enclosed)
 Proposed Parking = 134 stalls
 126 enclosed / 70 exposed = 134 stalls

Building Setbacks:
 North = 25 feet
 South = 20 feet
 East = 20 feet / 35 feet **
 West = 44 feet / 54 feet **
 ** second setback number is above podium level

Proposed Redevelopment: New Senior Care Building
 5 stories, 227,577 gross sq ft.
 Not including 1 level of underground parking 85,554 GSF
 Proposed Occupancy:
 Senior Citizen Dwelling Units, Assisted Living Units
 Independent Living Units, Memory Care Suites, Memory Care Suites, Care Suites
 Property Zoned: PUD-3

EDWARD FARR ARCHITECTS INC.
 2500 1st Avenue South
 Edina, MN 55425
 (763) 933-1111
 www.edwardfarr.com

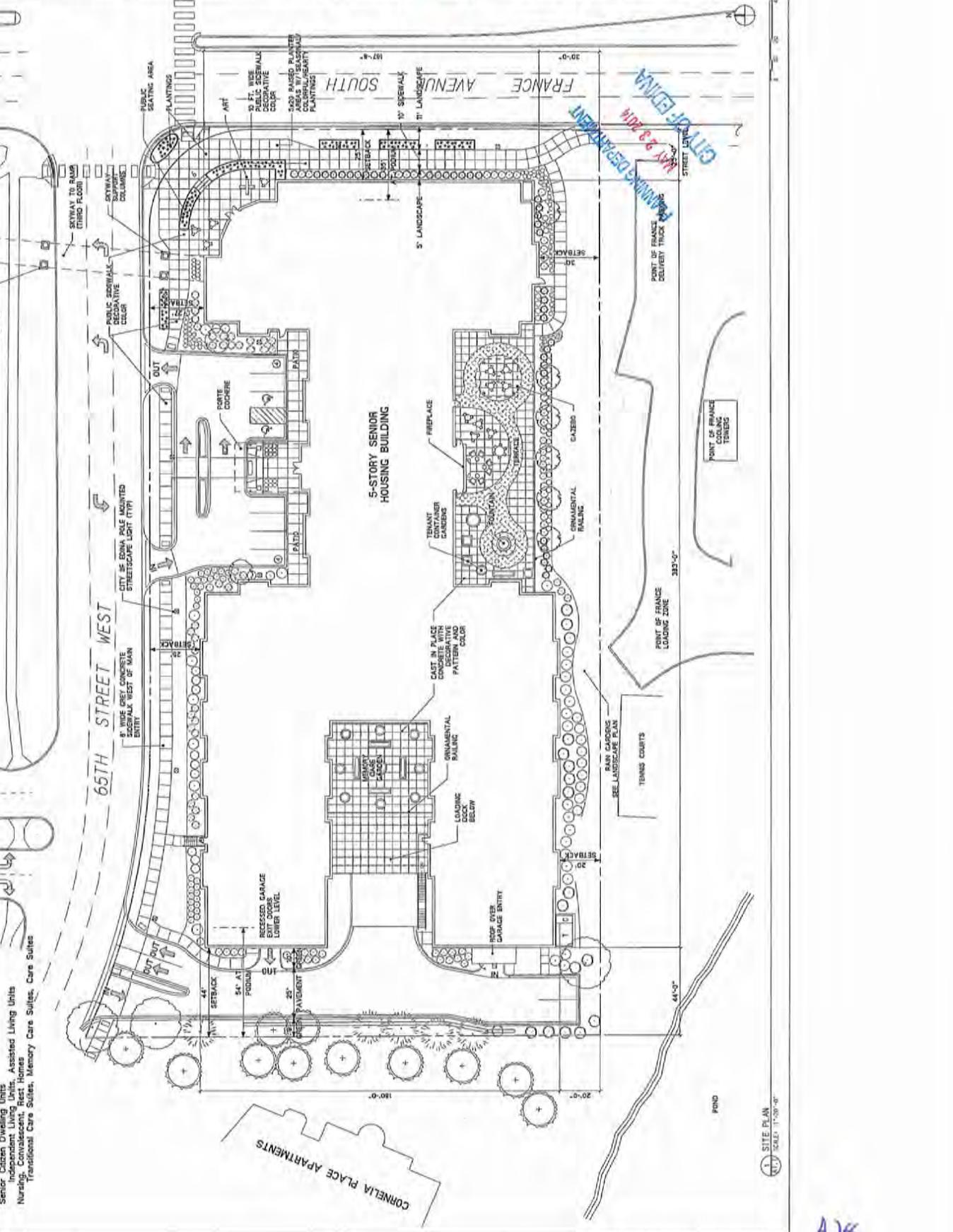
AURORA DEVELOPMENT CO.
 5500 FRANCE AVENUE SOUTH
 EDINA, MINNESOTA

PROJECT: AURORA ON FRANCE
 PROJECT NUMBER: 13.025
 DATE: 11/20/13
 DRAWING: ARCHITECTURAL SITE PLAN
 SCALE: 1" = 30'-0"

Code Required Parking: 1/4 Residents
 Nursing Home, 100 Units = 28 stalls (enclosed)
 Senior Citizen Dwelling, 100 Units = 25 stalls (enclosed)
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 5 stories, 227,577 gross sq ft.
 Not including 1 level of underground parking 85,554 GSF
 Proposed Occupancy:
 Senior Citizen Dwelling Units, Assisted Living Units
 Independent Living Units, Memory Care Suites, Memory Care Suites, Care Suites
 Property Zoned: PUD-3



1 SITE PLAN
 SCALE: 1" = 30'-0"

13.025

NOTIFICATION TO PROPERTY OWNER REQUIRED BY THE CITY OF EDINA. THE CITY ENGINEER HAS REVIEWED THE PLANS AND FOUND THEM TO BE IN ACCORDANCE WITH THE CITY OF EDINA ORDINANCES. THE CITY ENGINEER'S REVIEW IS LIMITED TO THE TECHNICAL ASPECTS OF THE PLANS AND DOES NOT CONSTITUTE AN ENDORSEMENT OF THE PROJECT OR A GUARANTEE OF THE ACCURACY OF THE INFORMATION PROVIDED.

DATE: _____ REG. NO. 10362

PROJECT NUMBER: _____

DATE: _____

CITY OF EDINA, MN

EDWARD FARR ARCHITECTS, INC.
 100 North Washington Ave.
 Edina, MN 55425

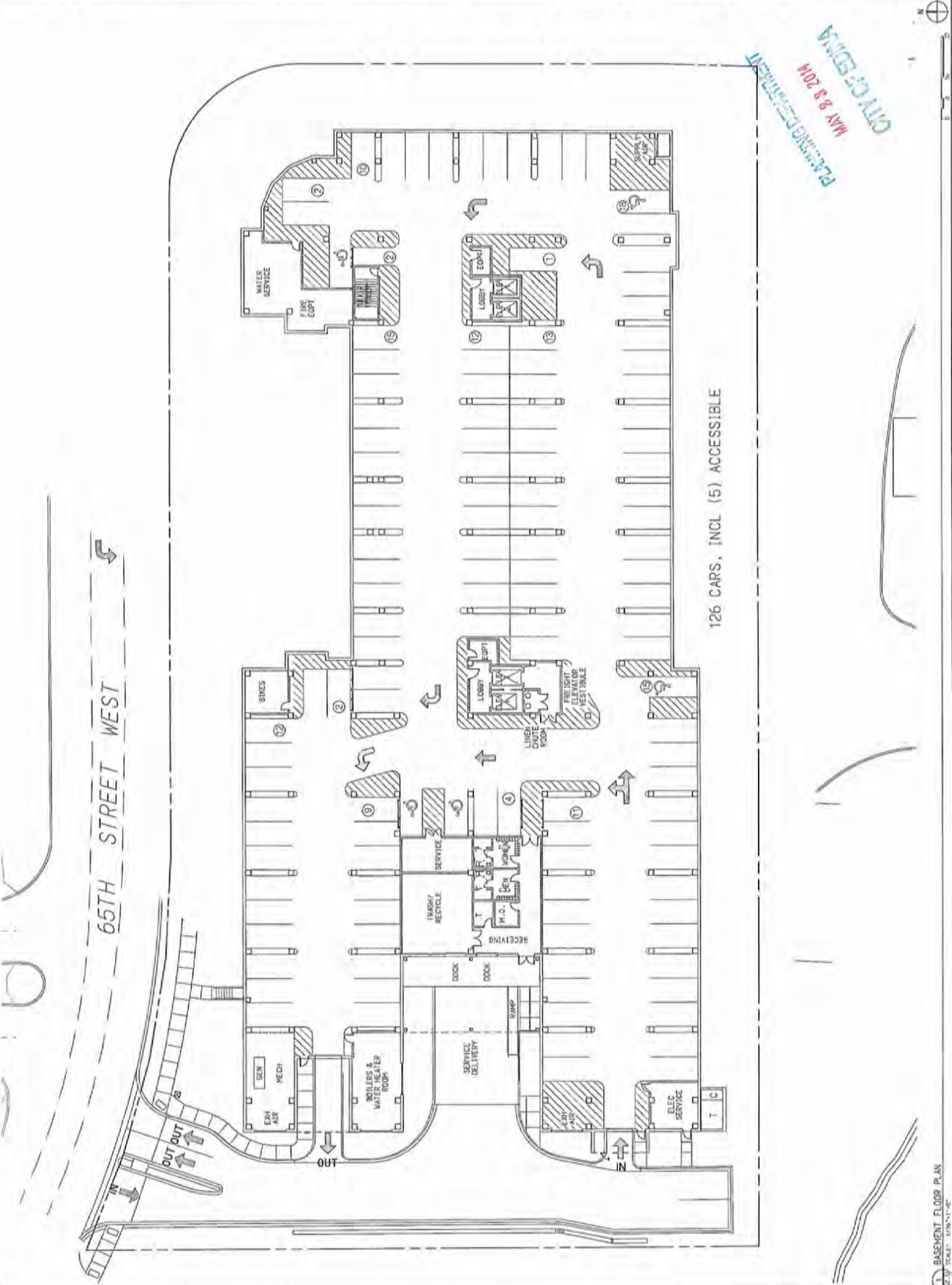
AURORA
 Innovations, LLC

PROJECT: MOUNT DEVELOPMENT CO.
AURORA ON FRANCE
 FINAL DEVELOPMENT PLAN

LOCATION:
 8000 FRANCE AVENUE SOUTH
 EDINA, MINNESOTA

OWNER:	DATE:
DESIGNER:	DATE:
CITY ENGINEER:	DATE:

DATE: 2010
BASEMENT FLOOR PLAN
 PROJECT NUMBER: _____ SHEET NUMBER: _____
 13-025 A2.0



PLANNING DEPARTMENT
 CITY OF EDINA
 MAY 9 2010

BASEMENT FLOOR PLAN
 SCALE: 1/8" = 1'-0"

A40

EDWARD FARR ARCHITECTS INC.
 1000 FRANKLIN AVENUE SOUTH
 SUITE 100
 MINNETONKA, MN 55345
 PHONE: 952.895.1100
 FAX: 952.895.1101
 WWW.EDFARR.COM

DATE: 08/14/12
 REPORT NUMBER: 13.025
 PROJECT NAME: AURORA ON FRANCE

CLIENT: AURORA ON FRANCE
 DEVELOPER: AURORA ON FRANCE
 ARCHITECT: EDWARD FARR ARCHITECTS INC.

PROJECT LOCATION: 13.025
 SHEET NUMBER: A2.1

EDWARD FARR ARCHITECTS INC.
 1000 FRANKLIN AVENUE SOUTH
 SUITE 100
 MINNETONKA, MN 55345
 PHONE: 952.895.1100
 FAX: 952.895.1101
 WWW.EDFARR.COM

AURORA
 INVESTMENTS, LLC

PROJECT: AURORA ON FRANCE
 DEVELOPER: AURORA ON FRANCE
 ARCHITECT: EDWARD FARR ARCHITECTS INC.

PROJECT LOCATION: 13.025
 SHEET NUMBER: A2.1

EDWARD FARR ARCHITECTS INC.
 1000 FRANKLIN AVENUE SOUTH
 SUITE 100
 MINNETONKA, MN 55345
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 WWW.EDFARR.COM

AURORA
 INVESTMENTS, LLC

PROJECT: AURORA ON FRANCE
 DEVELOPER: AURORA ON FRANCE
 ARCHITECT: EDWARD FARR ARCHITECTS INC.

PROJECT LOCATION: 13.025
 SHEET NUMBER: A2.1

EDWARD FARR ARCHITECTS INC.
 1000 FRANKLIN AVENUE SOUTH
 SUITE 100
 MINNETONKA, MN 55345
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 FAX: 952.895.1101
 WWW.EDFARR.COM

AURORA
 INVESTMENTS, LLC

PROJECT: AURORA ON FRANCE
 DEVELOPER: AURORA ON FRANCE
 ARCHITECT: EDWARD FARR ARCHITECTS INC.

PROJECT LOCATION: 13.025
 SHEET NUMBER: A2.1

EDWARD FARR ARCHITECTS INC.
 1000 FRANKLIN AVENUE SOUTH
 SUITE 100
 MINNETONKA, MN 55345
 PHONE: 952.895.1100
 FAX: 952.895.1101
 WWW.EDFARR.COM

AURORA
 INVESTMENTS, LLC

PROJECT: AURORA ON FRANCE
 DEVELOPER: AURORA ON FRANCE
 ARCHITECT: EDWARD FARR ARCHITECTS INC.

PROJECT LOCATION: 13.025
 SHEET NUMBER: A2.1

EDWARD FARR ARCHITECTS INC.
 1000 FRANKLIN AVENUE SOUTH
 SUITE 100
 MINNETONKA, MN 55345
 PHONE: 952.895.1100
 FAX: 952.895.1101
 WWW.EDFARR.COM

AURORA
 INVESTMENTS, LLC

PROJECT: AURORA ON FRANCE
 DEVELOPER: AURORA ON FRANCE
 ARCHITECT: EDWARD FARR ARCHITECTS INC.

PROJECT LOCATION: 13.025
 SHEET NUMBER: A2.1

EDWARD FARR ARCHITECTS INC.
 1000 FRANKLIN AVENUE SOUTH
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 MINNETONKA, MN 55345
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AURORA
 INVESTMENTS, LLC

PROJECT: AURORA ON FRANCE
 DEVELOPER: AURORA ON FRANCE
 ARCHITECT: EDWARD FARR ARCHITECTS INC.

PROJECT LOCATION: 13.025
 SHEET NUMBER: A2.1

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AURORA
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PROJECT: AURORA ON FRANCE
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AURORA
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PROJECT: AURORA ON FRANCE
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65TH STREET WEST



FRANCE AVENUE SOUTH

13.025 A2.1

141



KVERNSTOEN, RÖNNHOLM & ASSOCIATES, INC.
CONSULTANTS IN ARCHITECTURAL ACOUSTICS

14 May 2014

Stephen Michals
Mount Development Co.
10400 Viking Dr. Suite 160
Eden Prairie, MN. 55344

NOISE REPORT: 6500 FRANCE AVENUE SOUTH

Kvernstoen, Rönholm & Associates, Inc. was retained by Mount Development Co. to model mechanical noise levels from the building at the south property line of the site at 6500 France.

To that end I gathered sound data for the two chillers on the roof as well as the Magic Pac units located on the 3rd, 4th, and 5th floors of the proposed new building. I then calculated the expected noise levels from the equipment at the loudest locations at the south line of the property.

For my calculations I used manufacturer's sound data from Magic Pac for Model MGEA-09-241 and from York for the two Model YVAA0183ABV46 chillers equipped with low-sound fans and low-sound-level kit. All units are calculated at the 100% level, which is the loudest expected sound level.

Chillers

The chillers are located at the NE corner of the 6500 France building in the circular element referred to as "the crown". The direct distance from the chillers to a person standing at the south property line is approximately 175 ft. However, the building and parapet acts as a very effective sound barrier for the chillers. I have calculated the noise level at the property line closest to the chillers to be below 25 dBA. This is very well within the 50 dBA maximum required by Minnesota Rule 7030. See Fig 1 for the location of this calculated

sound level. This location was chosen because it is expect to exhibit the loudest sound levels from the chillers on the roof.

Magic Pac Units

These units are located on exterior of the south side of the 6500 France building on the 3rd, 4th, and 5th floors, as shown schematically in Figs 1 & 2.

The loudest point from the 6 chillers facing the south will be on-axis with one of the sets of 3 vertical chillers. I have calculated the noise level to be an even 50.0 dBA at this location. At the midway point between the two vertical rows of Magic Pac units, the sound level would be approximately 1 dB lower. (See Figure 2). At other locations along the south property line the sound levels from the Magic Pac units will be lower than 49 dBA. Noise levels from the chillers on the roof will not have any impact on the noise levels at these locations.

In order to achieve these low sound levels, the Magic Pac units for the apartments on the East and West sides must be located on the East and West faces of the building.

Thus, the sound levels at the property line from the York chillers and the Magic Pac units will meet the daytime and nighttime MPCA rules. Given that the measured L_{50} is 55.6 dBA at that location, and that the spectrum from the Magic Pac units will be similar to that of the traffic noise, the Magic Pac units will not figure significantly in the soundscape at the south property line of the 6500 France building.

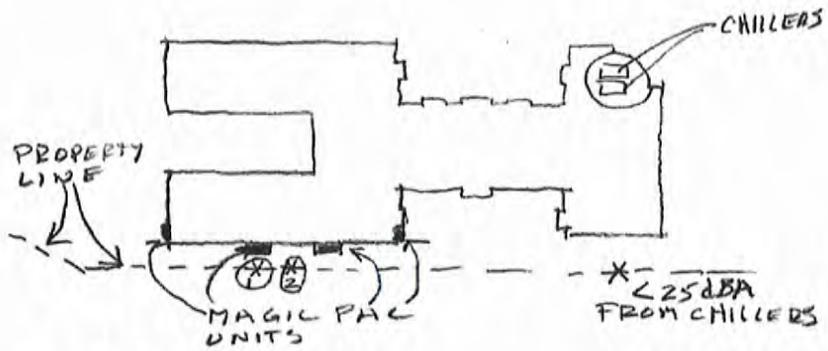
Please contact me with any comments or questions.



Steven Kvernstoen

FIGURE 1

6500 FRANCE 5/14/14



- 1. 50 dBA AT THIS LOCATION
- 2. 49 dBA AT THIS LOCATION

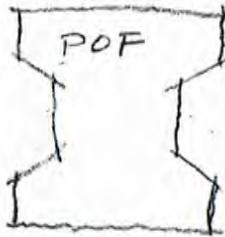
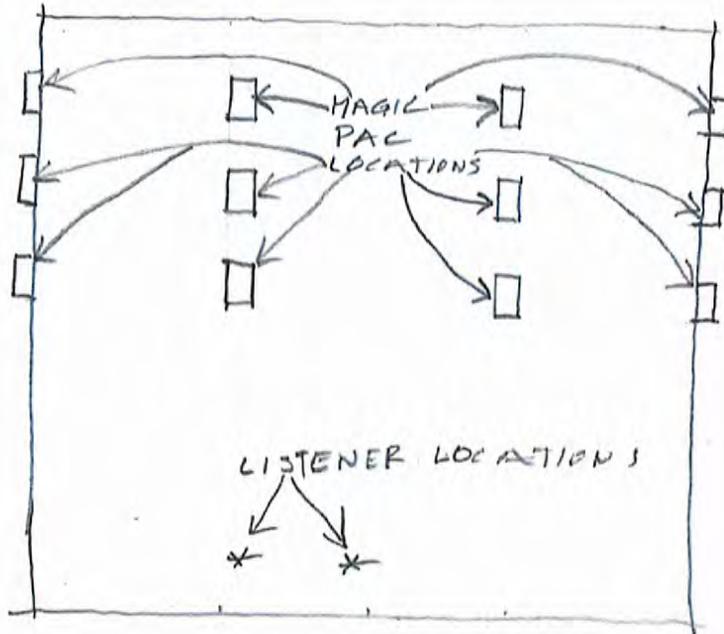


FIGURE 2

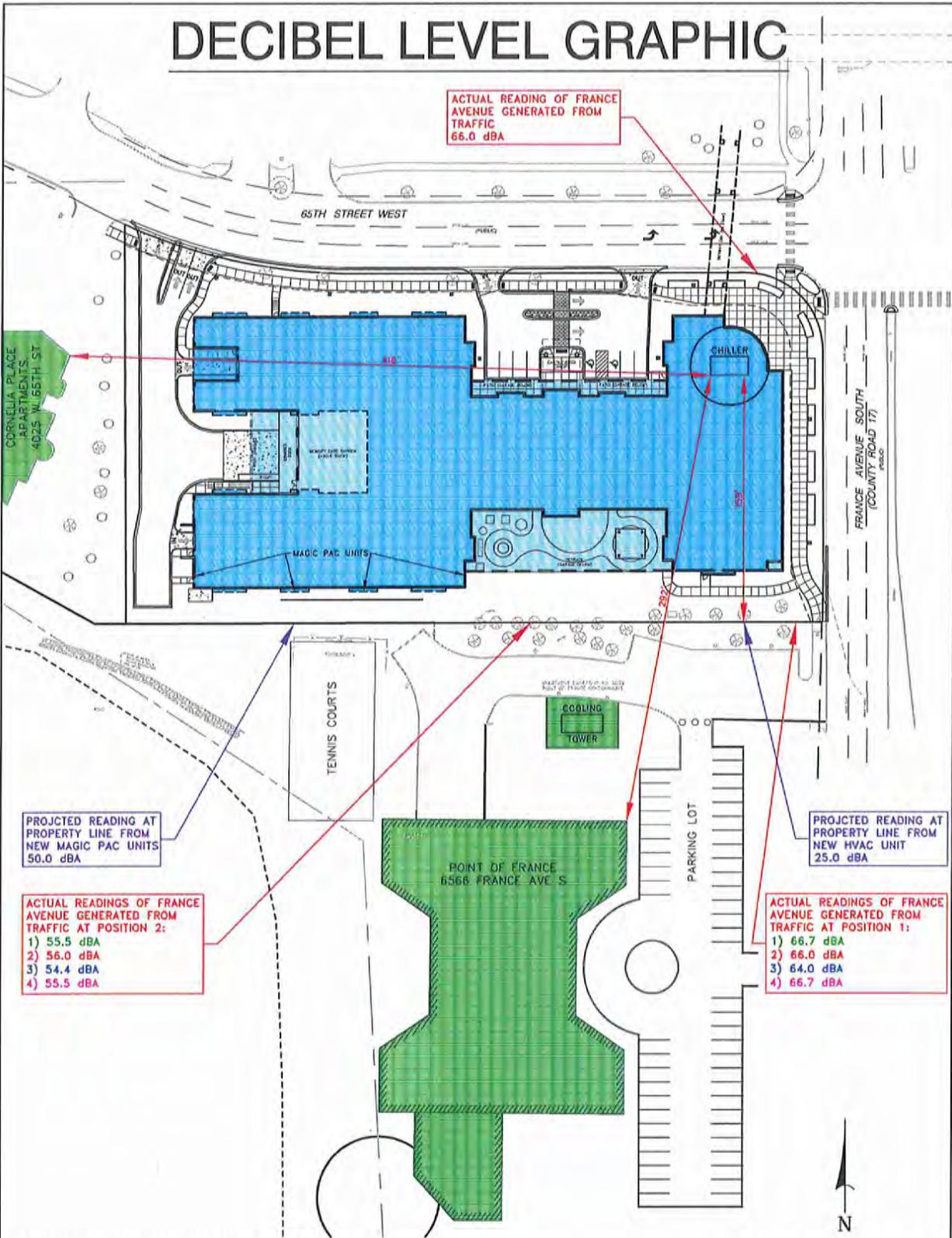
6500 FRANCE
SOUTH ELEVATION

5/14/14



A45

DECIBEL LEVEL GRAPHIC



ACTUAL READING OF FRANCE AVENUE GENERATED FROM TRAFFIC
66.0 dBA

PROJECTED READING AT PROPERTY LINE FROM NEW MAGIC PAC UNITS
50.0 dBA

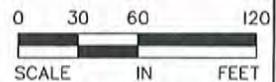
ACTUAL READINGS OF FRANCE AVENUE GENERATED FROM TRAFFIC AT POSITION 2:
1) 55.5 dBA
2) 56.0 dBA
3) 54.4 dBA
4) 55.5 dBA

PROJECTED READING AT PROPERTY LINE FROM NEW HVAC UNIT
25.0 dBA

ACTUAL READINGS OF FRANCE AVENUE GENERATED FROM TRAFFIC AT POSITION 1:
1) 66.7 dBA
2) 66.0 dBA
3) 64.0 dBA
4) 66.7 dBA

DATA COLLECTION LOCATION NOTES:

1. DATA COLLECTION PROVIDED BY KVERNSTOEN, RÖNNHOLM & ASSOCIATES.
2. COLLECTION TIMES ARE AS FOLLOWS:
7-9AM WEEKDAY, 11:15AM-1:15PM WEEKDAY, 6-8PM WEEKDAY, 11AM-1PM SATURDAY
3. POSITION 1 IS JUST WEST OF THE SIDEWALK AT THE PROPERTY LINE BETWEEN 6500 FRANCE AND POINT OF FRANCE. (L50 AVERAGE IS 65.9 dBA. 60 dBA IS THE MPCA RULE FOR MAXIMUM DAYTIME NOISE LEVELS.)
4. POSITION 2 IS ON THE PROPERTY LINE BETWEEN 6500 FRANCE AND POINT OF FRANCE NORTHWEST OF THE COOLING TOWER. (L50 AVERAGE IS 55.4 dBA, WHICH IS WITHIN THE MPCA DAYTIME GUIDELINES.)
5. THE DECIBEL LEVEL NUMBERS ARE NOT CUMULATIVE.

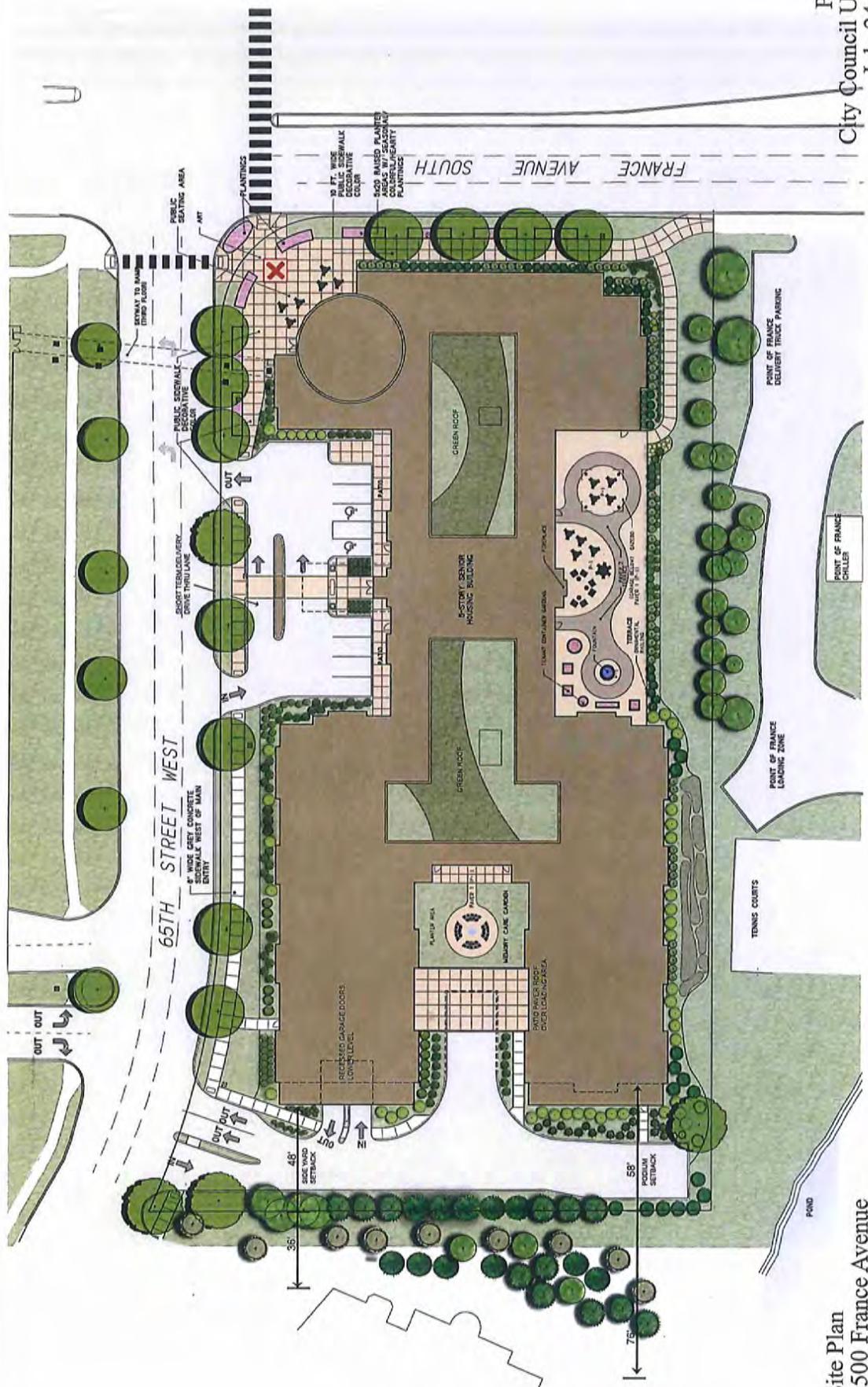


Design File: 130063	Checked By: DBB	I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Land Surveyor under Minnesota statutes 326.02 to 326.16, 326.05 & 326.07.
Scale: 1" = 50'	Drawn By: DE	
Date: 5/15/14	Book No.:	Signature: Date: License Number:

POINT OF FRANCE
DECIBEL EXHIBIT
EDINA, MINNESOTA

ALLIANT
ENGINEERING, INC.
233 PARK AVE. SOUTH, SUITE 300
MINNEAPOLIS, MN 55415
PHONE 612 259-0900
FAX 612 259-0901

HA



Site Plan
6500 France Avenue

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

A51

PRELIMINARY PLAN



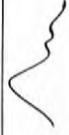
View Looking Southwest

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

PRELIMINARY PLAN

A52



From 1st Floor Apartment



From 2nd Floor Apartment



From 3rd Floor Apartment



From 4th Floor Apartment

Views from Cornelia Place

Page 20
July 26, 2013

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

A53

PRELIMINARY PLAN



Aerial View Looking Southwest

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



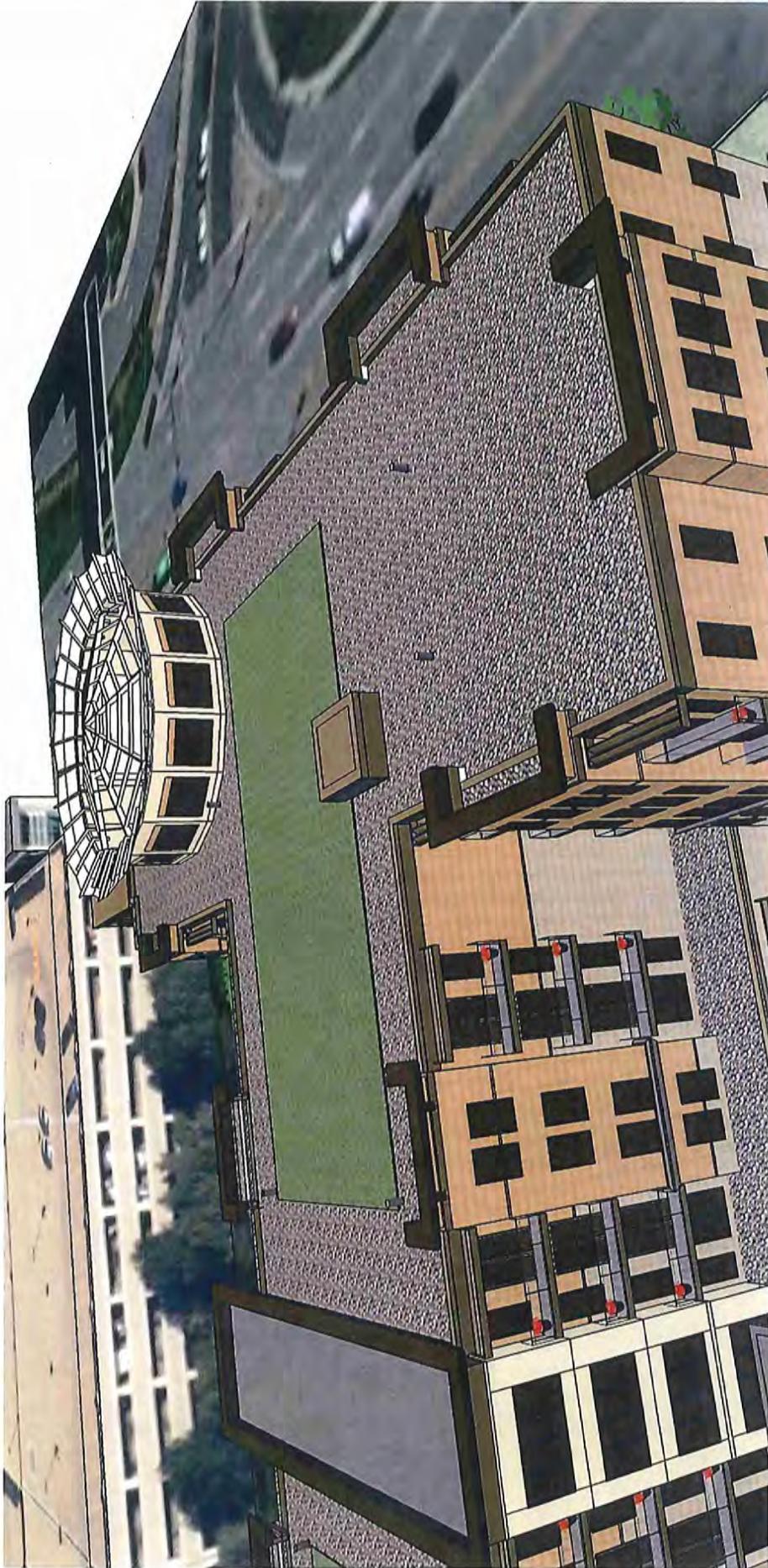
AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

PRELIMINARY PLAN

A54



View From Point Of France Looking at Top Crown

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

PRELIMINARY PLAN

A55



View Looking at Terrace

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
INVESTORS, LLC



MOUNT DEVELOPMENT CO.

A56

PRELIMINARY PLAN



View Looking Northwest

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
INVESTMENTS, LLC



MOUNT DEVELOPMENT CO.

AS7

PRELIMINARY PLAN



View Looking Southeast

EDWARD FARR
ARCHITECTS INC

AURORA ON FRANCE



AURORA
Investments, LLC



MOUNT DEVELOPMENT CO.

A58

PRELIMINARY PLAN

9. Provide private maintenance agreement for skyway.
8. Propose modifications to signal as necessary.
7. Traffic signal sight lines, and Emergency Vehicle Preemption system (EVP) on traffic signal on France may be blocked by skyway. Provide design schematics showing sight lines approaching from west, and and France Avenue and shall include installation of ADA pedestrian ramps.
6. Crosswalk intersection improvements shall include white thermoplastic blocks across both 65th Street http://edina.gov/edinafiles/files/City_Offices/Public_Works/CurbCutApplication.pdf in curb cut permit application.
5. Curb cut permit will be required for relocation or modification of driveway entrances. Follow standards luminaires, the lower one directed to the sidewalk.
4. The pedestrian lighting along France Avenue shall match the pedestrian lighting proposed for the France Avenue Pedestrian Improvements Project. See the Canto G detail attached with two http://edina.gov/edinafiles/files/City_Offices/Engineering/Construction%20Standards%20Plates/ConstructionStandards_625.pdf.
3. Provide pedestrian lighting along France Avenue and also along West 65th Street. (Example Attached)
2. The pedestrian lighting along West 65th Street shall match the pedestrian lighting used on West 66th Street from France Avenue to York Avenue, see City Standard Plate 625:
1. A traffic and parking study (WSB and Associated 7/26/2013) shows adequate parking, and no undue burden on the transportation network.
0. Update all City standard plates to current http://edina.gov/index.php?section=construction_standards

Traffic and Street

Details

Soils

Survey

The Engineering Department has reviewed the subject property for street and utility connections, grading, storm water, erosion and sediment control.

1. Utility easements were drafted for a prior building version, an update will be required.
2. Submit soils, soil boring and geotechnical report.
3. Update all City standard plates to current http://edina.gov/index.php?section=construction_standards

RE: 6500 France Avenue – Development Review

FROM: Ross Bintner P.E. - Environmental Engineer

CC: Chad Millner – City Engineer

TO: Cary Teague – Planning Director

DATE: November 5, 2014



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ENGINEERING DEPARTMENT

Sanitary and Water Utilities

- 10. Developer's agreement will be required for the realignment of public water main and the installation of public sidewalk.
- 11. City requires realignment of watermain at SE corner of property in France Avenue.
 - a. With realignment, watermain is not necessary along east property line.
 - b. Make water service connection off of France Avenue watermain near the NE corner of the property.
 - c. Extend watermain and a fire hydrant to the front entrance off 65th Street, near the out driveway.
- 12. Remove sanitary sewer to downstream manhole on southwest property corner.
- 13. Expose sanitary manhole 4680 on south property line, confirm location and bring to grade.
- 14. A revised SAC unit determination will be required at building permit application.

Storm Water Utility

- 15. Provide hydraulic and hydrologic calculations.
- 16. Point of France pond outlet elevation is 862.9, 10 year elevation is 864.4 and 100 year elevation is 865.9. Provide summary of hydrologic and hydraulic modeling to confirm proper capacity under backwater conditions. Confirm no backflow potential from drains near garage that may cause sanitary inflow.
- 17. A separate permit is required from 9MCWD

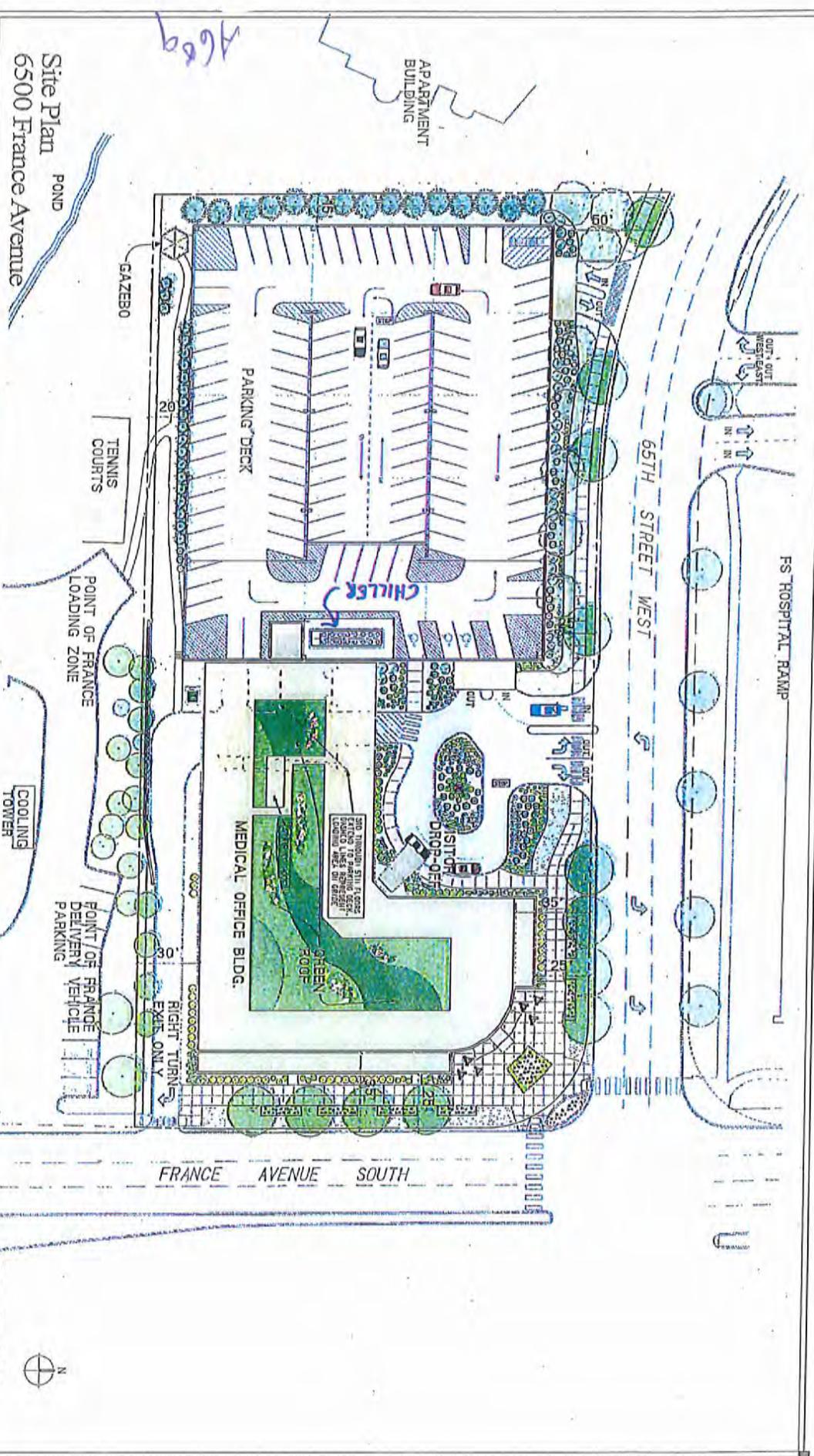
Grading, Erosion and Sediment Control

- 18. Provide erosion, sediment control plan that meets provisions of MPCA construction site general permit. Reference notes on detail sheet on ESC plan. Provide permanent erosion control for down spout locations.

Other Agency Coordination

- 19. Nine Mile Creek Watershed permit is required. Hennepin County, MDH, MPCA and MCES permits are required. These permits may have been started under the previous iteration, but need to be revised and resubmitted.





Site Plan
6500 France Avenue

EDWARD FARR
ARCHITECTS INC

EDINA MEDICAL PLAZA



MOUNT DEVELOPMENT CO.

November 6, 2012

APPROVED MEDICAL PROJECT

AG06

Site Plan
6500 France Avenue

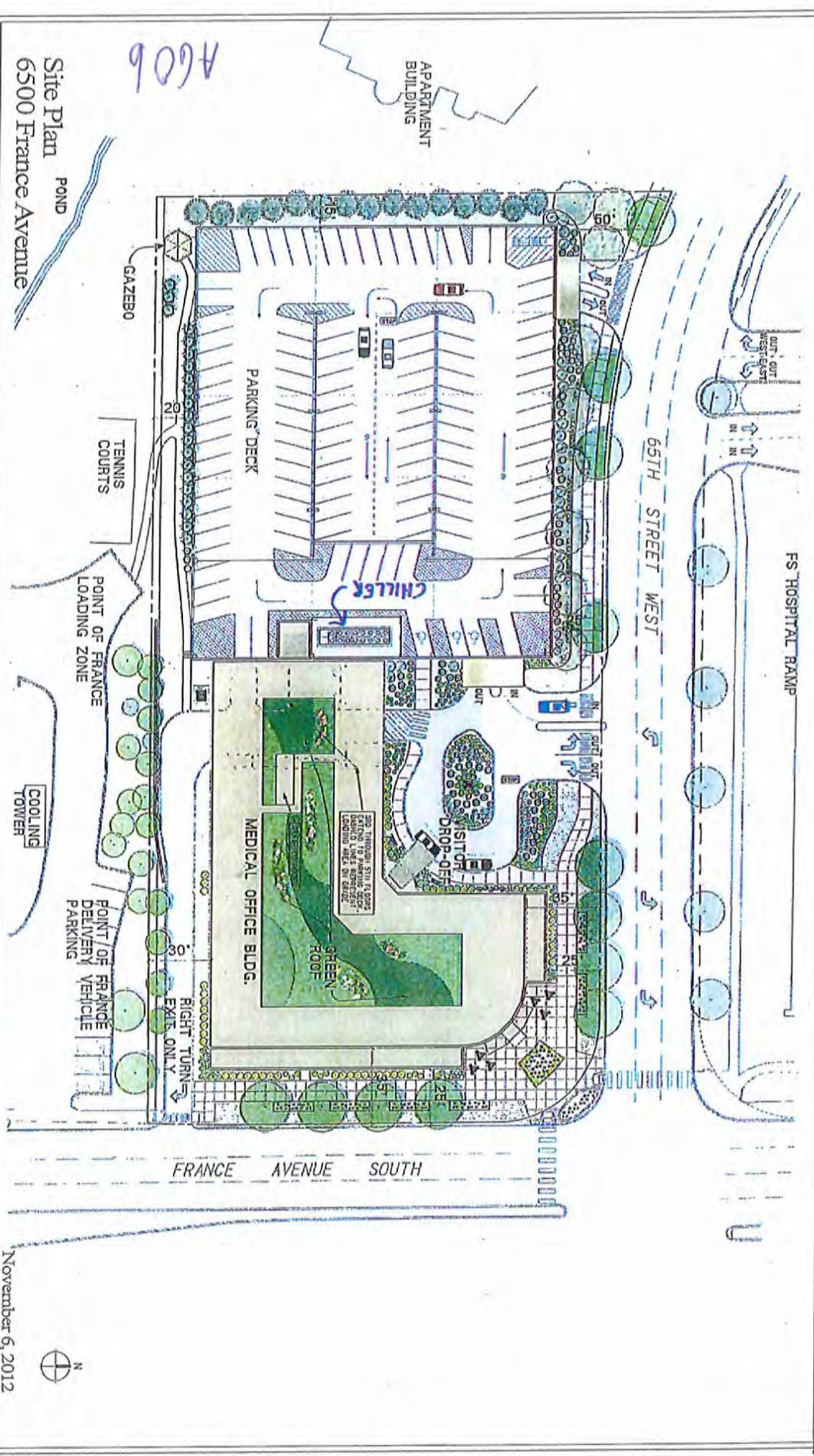
EDWARD FARR
ARCHITECTS INC

EDINA MEDICAL PLAZA



AURORA
Investments, LLC

MOUNT DEVELOPMENT CO.



APPROVED MEDICAL
PROJECT

November 6, 2012

460c



Northeast Aerial

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November 6, 2012

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Northwest Aerial

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INVESTMENT, LLC



MOUNT DEVELOPMENT CO.

The revised development plan includes a change in the proposed use on the site from medical office to a senior housing and skilled care facility. The current approved plan for the site includes a 102,965 sq ft medical office building with a 548 vehicle parking ramp. The new proposed senior housing and skilled care facility will include 209 units with 144 parking spaces.

In addition, the current approved plan included two full movement access locations on 65th Street and a right out only access to southbound France Avenue. Access to the existing sites is currently provided at two (2) full movement driveway locations from 65th Street to each property (four driveways). All access to the proposed development plan will be from 65th Street at two locations. The primary access to the underground parking will be on the west side of the site. A second access will be located in the center of the site and will be utilized for visitor drop-offs.

The proposed site plan is shown on *Figure 2*.

The traffic impacts of the existing and anticipated development were evaluated at the site access locations as well as the primary impacted intersections and driveway along 65th Street between France Avenue and Valley View Road. The following sections of this report document the analysis and anticipated traffic and parking impacts for the proposed redevelopment.

Traffic and Parking Studies were completed in April, June and October 2012 documenting the anticipated impacts the proposed redevelopment of both 6500 France Avenue and 4005 65th Avenue sites would have on the adjacent roadway system. The site and proposed redevelopment has again been revised. This memorandum provides an updated review of the traffic and parking impacts, based on the revised site plan and development proposal. The project location is shown on *Figure 1*.

Background

RE: Aurora on France Senior Housing Development (6500 France Avenue)
 Traffic and Parking Study
 City of Edina, MN
 WSB Project No. 1686-43

FROM: Charles Rickart, P.E., PTOE

TO: Mr. Cary Teague, Planning Director
 Mr. Wayne Houle, Public Works Director
 City of Edina

DATE: June 26, 2013

Memorandum

701 Xenia Avenue South
 Suite #300
 Minneapolis, MN 55416
 Tel: 763 541-4800
 Fax: 763 541-1700

Infrastructure ■ Engineering ■ Planning ■ Construction



Existing Traffic Characteristics

France Avenue (CSAH 17) is north/south a 6-lane divided Arterial roadway from south of 66th Street to north of 65th Street. Primary access to York Avenue is by local streets and development driveways. The posted speed limit on France Avenue in the vicinity of the site is 40 mph.

65th Street 65th Street is an east/west City street with numerous access driveways. The existing roadway configuration includes a single lane in each direction with a continuous center left turn lane (three lane section). All the driveway access points are controlled with stop signs, stopping the exiting movements from the developments. A 30 mph speed limit is posted on this roadway.

The two primary intersections along the 65th Street corridor are at France Avenue and Valley View Road. Both France Avenue and Valley View Road are classified as "A" Minor Arterials providing regional access to the area. Both intersections are controlled with traffic control signals. The lane configurations at each are as follows.

65th Street at France Avenue
SB France Ave approaching 65th St – one through/right, two through, one left
NB France Ave approaching 65th St – one through/right, two through, one left
EB 65th St approaching France Ave – one through/right, one left
WB 65th St approaching France Ave – one right, one through/left

65th Street/TH 62 off ramp at Valley View Road
SB Valley View Rd approaching 65th St – one through, one left
NB Valley View Rd approaching 65th St – one through/right, two through
EB TH 62 off ramp approaching Valley View Rd – one free right, one through/left
WB 65th St approaching Valley View Rd – one right/left

AM and PM peak hour turning movement counts were conducted along 65th Street at each access driveway, the intersection of France Avenue at 65th Street and the intersection of Valley View Road at 65th Street/TH 62 off ramp the week of February 20th, 2012.

Figure 3 shows the intersections and driveways along the corridor that were analyzed as part of this traffic study and, Figure 4 shows the existing 2012 AM and PM peak hour traffic volumes with the existing lane configuration. The traffic count data is included in the *Appendix*.

Background (Non Development) Traffic Growth

Traffic growth in the vicinity of a proposed development will occur between existing conditions and any given future year due to other growth and development within the region. This background growth must be accounted for and included in future year traffic forecasts. Reviewing the historical traffic counts on in the area traffic has stayed constant or dropped in the past few years. In order to account for some background growth in traffic a .05% per year factor was applied to the through traffic on 65th Street, France Avenue and Valley View Road to the 2014 and 2030 analysis years.

In addition to the regional background traffic, other specific none development related traffic was determined and included with the overall background traffic.

Fairview Southdale Hospital Expansion – The proposed plan includes the expansion of the emergency center, urgent care, behavioral health and observation area. The proposed expansion consists of a 77,500 sf (gross area), two-story building located on the north side of the existing hospital building. This project has been approved by the City Council. It is assumed that it will be completed in 2014 and included in the background traffic for the 2014 and 2030 analysis.

Southdale Residential - The City recently approved the addition of 232 apartment units with associated parking in the existing Southdale Shopping Center parking lot. The site is located in the northwest quadrant of 69th Street and York Avenue. It is assumed that this project will not be open and included as part of the 2014 analysis but, will be fully leased and included in the 2030 background traffic.

Additional Southdale Mall Development - Based on the information received from Southdale Center about the current vacancy rates and plans for renovations, it was determined that following the renovations, the mall would have an additional 143,880 sf of leasable space available. This includes leasable retail and food court space. The analysis assumes that all space will be occupied by 2014 and included in the background traffic for the 2014 and 2030 analysis.

Future Restaurant Development – A future restaurant is proposed in northeast quadrant of France Avenue and 69th Street in the Southdale Center Parking lot. The restaurant was assumed to be 8,000 sf in size with approximately 300 seats. The analysis assumes the restaurant will not be developed by 2014 but, will be open and included as part of the 2030 background traffic.

The estimated trip generation for the additional background traffic is shown below in **Table 1**. The trip generation rates used to estimate the additional development traffic is based on extensive surveys of the trip-generation rates for other similar land uses as documented in the Institute of Transportation Engineers *Trip Generation Manual, 9th Edition*. The table shows the AM and PM peak hour trip generation for the proposed uses.

Table 1 - Estimated Additional Background Trip Generation

Use	Size	AM Peak Hour		PM Peak Hour	
		Total	In	Out	Total
Hospital Expansion	77,500 sf	36	21	15	24
Apartments	232 units	118	24	94	144
Shopping Center	143,880 sf	138	86	52	533
Restaurant	8000 sf	87	48	39	79
Total New Trips		379	179	200	780
					407
					373

Source: Institute of Transportation Engineers Trip Generation Manual, 9th Edition

Site Trip Generation

The estimated trip generation from the proposed ultimate redevelopment is shown below in *Table 2*. The trip generation rates used to estimate the proposed site traffic are based on extensive surveys of the trip-generation rates for other similar land uses as documented in the Institute of Transportation Engineers *Trip Generation Manual, 9th Edition*. The table shows the total daily, AM peak hour and PM peak hour trip generation for the proposed site.

Table 2 - Estimated Site Trip Generation

Use	Size (units)	ADT			AM Peak			PM Peak		
		Total	In	Out	Total	In	Out	Total	In	Out
Assisted Living	59	157	78	79	9	6	3	13	6	7
Independent Living	50	101	51	50	3	2	1	9	4	5
Skilled Nursing Care	40	96	48	48	6	4	2	7	3	4
Memory Care	40	96	48	48	6	4	2	7	3	4
Observation Care Rooms	20	48	24	24	3	2	1	4	2	2
Total New Trips		498	249	249	27	18	9	40	18	22

Source: Institute of Transportation Engineers Trip Generation Manual, 9th Edition

Trip Distribution

Site-generated trips were distributed to the adjacent roadway system based on the population distribution relative to the site and the travel sheds for the major routes that serve it. The Trip Distribution was assumed as follows:

- 25% north on France Avenue
- 20% south on France Avenue
- 10% from the Hospital across France Avenue
- 10% north on Valley View Road
- 5% south on Valley View Road
- 30% from west on TH 62

Future Year Traffic Forecasts

Traffic forecasts were prepared for the year 2014 which is the year the proposed development would be completed and for the 2030 conditions which represents the City's Comprehensive Plan development time frame.

AG4

The traffic forecasts were prepared by adding the projected annual background traffic growth and the projected non-development background traffic growth to the existing 2013 traffic counts to determine the "No-Build" traffic conditions. The anticipated Aurora on France traffic was then added to the no-build to determine the "Build" traffic conditions. *Figures 5 and 6* show the projected 2014 and 2030 Build AM and PM peak hour traffic volumes.

Traffic Operations

Existing and/or forecasted traffic operations were evaluated for the impacted intersections and access driveway adjacent to the hospital. The analysis was conducted for the following scenarios:

1. Existing Conditions
2. Projected 2014 Build
3. Projected 2030 Build

This section describes the methodology used to assess the operations and provides a summary of traffic operations for each scenario. Existing and/or forecasted traffic operations were evaluated for the intersections of 65th Street at France Avenue, 65th Street at Valley View Road, the existing driveways on 65th Street, the proposed development site driveways on 65th Street and the right-out only site access to France Avenue. This section describes the methodology used to assess the operations and provides a summary of traffic operations.

Analysis Methodology

The traffic operations analysis is derived from established methodologies documented in the *Highway Capacity Manual 2000* (HCM). The HCM provides a series of analysis techniques that are used to evaluate traffic operations.

Intersections are given a Level of Service (LOS) grade from "A" to "F" to describe the average amount of control delay per vehicle as defined in the HCM. The LOS is primarily a function of peak traffic hour turning movement volumes, intersection lane configuration, and the traffic controls at the intersection. LOS A is the best traffic operating condition, and drivers experience minimal delay at an intersection operating at that level. LOS E represents the condition where the intersection is at capacity, and some drivers may have to wait through more than one green phase to make it through an intersection controlled by traffic signals. LOS F represents a condition where there is more traffic than can be handled by the intersection, and many vehicle operators may have to wait through more than one green phase to make it through the intersection. At a stop sign-controlled intersection, LOS F would be characterized by exceptionally long vehicle queues on each approach at an all-way stop, or long queues and/or great difficulty in finding an acceptable gap for drivers on the minor legs at a through-street intersection.

The LOS ranges for both signalized and un-signalized intersections are shown in *Table 3*. The threshold LOS values for un-signalized intersections are slightly less than for signalized intersections. This variance was instituted because drivers' expectations at intersections differ with the type of traffic control. A given LOS can be altered by increasing (or decreasing) the number of lanes, changing traffic control arrangements, adjusting the timing at signalized intersections, or other lesser geometric improvements. LOS also changes as traffic volumes increase or decrease.

Table 3 - Intersection Level of Service Ranges

Control Delay (Seconds)		
Signalized	Un-Signalized	
≤ 10	≤ 10	A
10 – 20	10 – 15	B
20 – 35	15 – 25	C
35 – 55	25 – 35	D
55 – 80	35 – 50	E
> 80	> 50	F

Source: HCM

LOS, as described above, can also be determined for the individual legs (sometimes referred to as “approaches”) or lanes (turn lanes in particular) of an intersection. It should be noted that a LOS E or F might be acceptable or justified in those cases where a leg(s) or lane(s) has a very low traffic volume as compared to the volume on the other legs. For example, improving LOS on such low-volume legs by converting a two-way stop condition to an all-way stop, or adjusting timing at a signalized intersection, could result in a significant penalty for the many drivers on the major road while benefiting the few on the minor road. Also, geometric improvements on minor legs, such as additional lanes or longer turn lanes, could have limited positive effects and might be prohibitive in terms of benefit to cost.

Although LOS A represents the best possible level of traffic flow, the cost to construct roadways and intersection to such a high standard often exceeds the benefit to the user. Funding availability might also lead to acceptance of intersection or roadway designs with a lower LOS. LOS D is generally accepted as the lowest acceptable level in urban areas. LOS C is often considered to be the desirable minimum level for rural areas. LOS D or E may be acceptable for limited durations or distances, or for very low-volume legs of some intersections.

The LOS analysis was performed using Synchro/SimTraffic:

- Synchro, a software package that implements Highway Capacity Manual (HCM) methodologies, was used to build each signalized intersection and provide an input database for turning-movement volumes, lane geometrics, and signal design and timing characteristics. In addition, Synchro was used to optimize signal timing parameters for future conditions. Output from Synchro is transferred to SimTraffic, the traffic simulation model.

- SimTraffic is a micro-simulation computer modeling software that simulates each individual vehicle’s characteristics and driver behavior in response to traffic volumes, intersection configuration, and signal operations. The model simulates drivers’ behaviors and responses to surrounding traffic flow as well as different vehicle types and speeds. It outputs estimated vehicle delay and queue lengths at each intersection being analyzed.

Existing Level of Service Summary

Table 4, below, summarizes the existing LOS at the two primary intersections on 65th Street as well as the existing site driveways and adjacent development driveway based on the current lane geometry and traffic volumes. The table shows that all intersection are operation at LOS C or better during both the AM and PM peak hours with all movements operating at LOS D or better. A table showing the LOS and delays by approach is included in the *Appendix*.

Table 4 - Existing Level of Service

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	Delay (sec/veh)	LOS	Delay (sec/veh)
65 th Street at France Ave	C (D)	20.3	B (D)	19.8
65 th Street at Valley View Rd	B (D)	13.8	B (D)	12.5
65 th Street at 6500 Site Driveway	A (A)	1.0	A (A)	1.2
65 th Street at 4005 Site Driveway	A (A)	0.5	A (A)	0.4
65 th Street at Adjacent Development Driveways	A (A)	0.5 - 2.2	A (A)	0.4 - 2.7

C = Overall LOS, (D) = Worst movement LOS Source: WSB & Associates, Inc.

Forecast Traffic Operations

A capacity and LOS analysis was completed for the study area intersections for 2014 which is the year proposed Aurora on France would be developed and for the 2030 conditions which are discussed below and shown in *Tables 5*. All of the intersections are expected to continue to operate at similar levels of service after the redevelopment as prior to the redevelopment. A table showing the LOS and delays by approach is included in the *Appendix*.

Table 5 shows that all intersection will continue to operate at overall LOS D or better in 2014 and 2030 during both the AM and PM peak hours. However, with the increase in traffic, some additional movements will be operating at LOS E. Overall delays will also increase slightly from the existing conditions to the 2030 conditions, especially at the intersection of France Avenue at 65th Street.

By 2030 the analysis indicates that at the intersection of 65th Street and France Avenue potential issues on the 65th Street approaches and France Avenue left turns may exist. With minor intersection and signal improvements (additional turn lane length and signal phasing changes), these issues would be minimized, improving the overall intersection LOS back to a C with 20 to 25 sec delays.

Table 5 – Forecast Build with Development

Intersection	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	LOS	Overall Delay (sec/veh)						
65 th St at France Ave	C (D)	21.3	B (D)	19.8	C (D)	23.2	D (E)	37.4
	B (D)	14.3	B (D)	14.8	B (D)	14.5	B (D)	17.8
65 th St at Valley View Rd	A (C)	1.1	A (A)	1.3	A (C)	1.2	A (C)	1.8
65 th St at West Site Access	A (A)	0.4	A (A)	0.5	A (A)	0.8	A (A)	1.0
65 th St at East Site Access	A (A)	0.8 - 2.2	A (A)	0.5 - 2.7	A (A)	0.8 - 2.4	A (A)	0.5 - 2.8
65 th St at Adjacent Development Driveways	A (A)		A (A)		A (A)		A (A)	

C = Overall LOS, (D) = Worst movement LOS Source: WSB & Associates, Inc.

Vehicle Queuing Analysis

A queuing analysis for the existing and future 2014 and 2030 conditions was prepared evaluating the anticipated vehicle queuing impacts at the driveways and intersections on 65th Street between France Avenue and Valley View Road. The analysis was conducted using the SimTraffic simulation software.

The results found that during both the AM and PM peak hours, for the existing, and future no-build and build 2014 and 2030 conditions, the average queues in the corridor do not exceed any of the available turn lanes storage. In some cases however, the maximum queues were exceeded. The maximum queue represents the longest length of queue that was observed during the analysis period. The observations were identified just one time during the peak periods with an extremely short duration of less than 2 seconds. In most cases the queues exceed the storage in the continuous left turn lane, therefore only blocking the adjacent driveway and not impacting through traffic.

The potential future mitigation at the 65th Street and France Avenue intersection discussed above will improve the flow of traffic on 65th Street and also minimize traffic blocking the hospital entrance. Additional signage could also be added indicating “do not block intersection” and/or “no left turns during peak hours” at the hospital entrance should this become an issue.

Tables showing the average and maximum queue lengths by movement and approach are included in the *Appendix*.

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Parking Demand

The parking demand for the proposed development was analyzed based on anticipated uses on the site. The parking generation rates used to estimate the parking demand was based on surveys of the parking generation for other similar land uses as documented in the Institute of Transportation Engineers *Parking Generation Manual*, 4th Edition.

Table 6 below shows a summary of each potential uses, the estimated parking generation rate and what the anticipated peak parking demand would be for a typical weekday. This would represent the worst case condition for the parking on the site assuming the proposed uses.

Table 6 – Site Parking Demand per ITE

Use	Size	Rate	Spaces
Assisted Living	59	0.41 spaces/unit	25
Independent Living	50	1.00 spaces/unit	50
Skilled Nursing Care	40	0.48 spaces/unit	20
Memory Care	40	0.48 spaces/unit	20
Observation Care Rooms	20	0.48 spaces/unit	10
Total Parking Demand			125

The current City Code would require a total of 138 parking spaces for the proposed development. Currently the proposed site is estimating 144 spaces available. *Table 7* shows a breakdown of the parking required per City Code.

Table 7 – Parking Required per City Code

Use	Size	Rate	Spaces
Assisted Living	59	0.75 per unit + 1 per employee	60
Independent Living	50	1 per employee	53
Skilled Nursing Care	40	1 per 4 beds	10
Memory Care	40	1 per 4 beds	10
Observation Care Rooms	20	1 per 4 beds	5
Total Parking Demand			138

Based on the results of the parking analysis, it can be concluded that the parking proposed with the site plan would be adequate for the proposed senior housing and skilled care facility.

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Conclusions / Recommendation

Based on the analysis documented in this memorandum, WSB has concluded the following:

- The proposed Senior Housing and Skilled Care Facility project including planned 209 units is anticipated to generate an additional 27 trips in the AM peak hour and 40 trips in the PM peak hour.
- Additional trips will be generated from other approved or anticipated development in the surrounding area. These uses will generate an additional 379 trips in the AM peak hour and 780 trips in the PM peak hour.
- Existing traffic operations at the intersections and driveways in the study area on 65th Street are all operating at overall LOS D or better for the both the AM and PM peak hours.
- Traffic operations at the intersections and driveway on 65th Street between France Avenue and Valley View Road with or without the proposed site development for the forecasted conditions in 2014 and 2030 will continue to operate at an overall LOS D or better during the AM and PM peak hours.

- The intersection of 65th Street and France Avenue may have potential delay issues with full development in 2030. With the minor intersection and signal improvements (additional turn lane length and signal phasing changes), these issues would be minimized, improving the overall intersection LOS back to a C.
- The results of the queuing analysis found that during both the AM and PM peak hours, for the existing, and future no-build and build 2014 and 2030 conditions, the average queues in the corridors do not exceed any of the available turn lane storage.

- The proposed site plan provides parking to meet both City Code and anticipated parking generation based on ITE guidelines.

Based on these conclusions the following is recommended.

1. Provide the proposed roadway internal roadway improvements as shown on the proposed site plan (*Figure 2*).

2. Although no improvements to the France Avenue at 65th Street intersection are specifically required at this time. Should delays and queuing become an issue in the future, minor intersection turn lane and phasing improvements may be necessary. Should these improvements be required in the future the Aurora on France Senior Housing development will be responsible for their share of those improvements.

3. No additional roadway improvements or additional parking would be required to accommodate the proposed Aurora on France Senior Housing project.

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APPENDIX

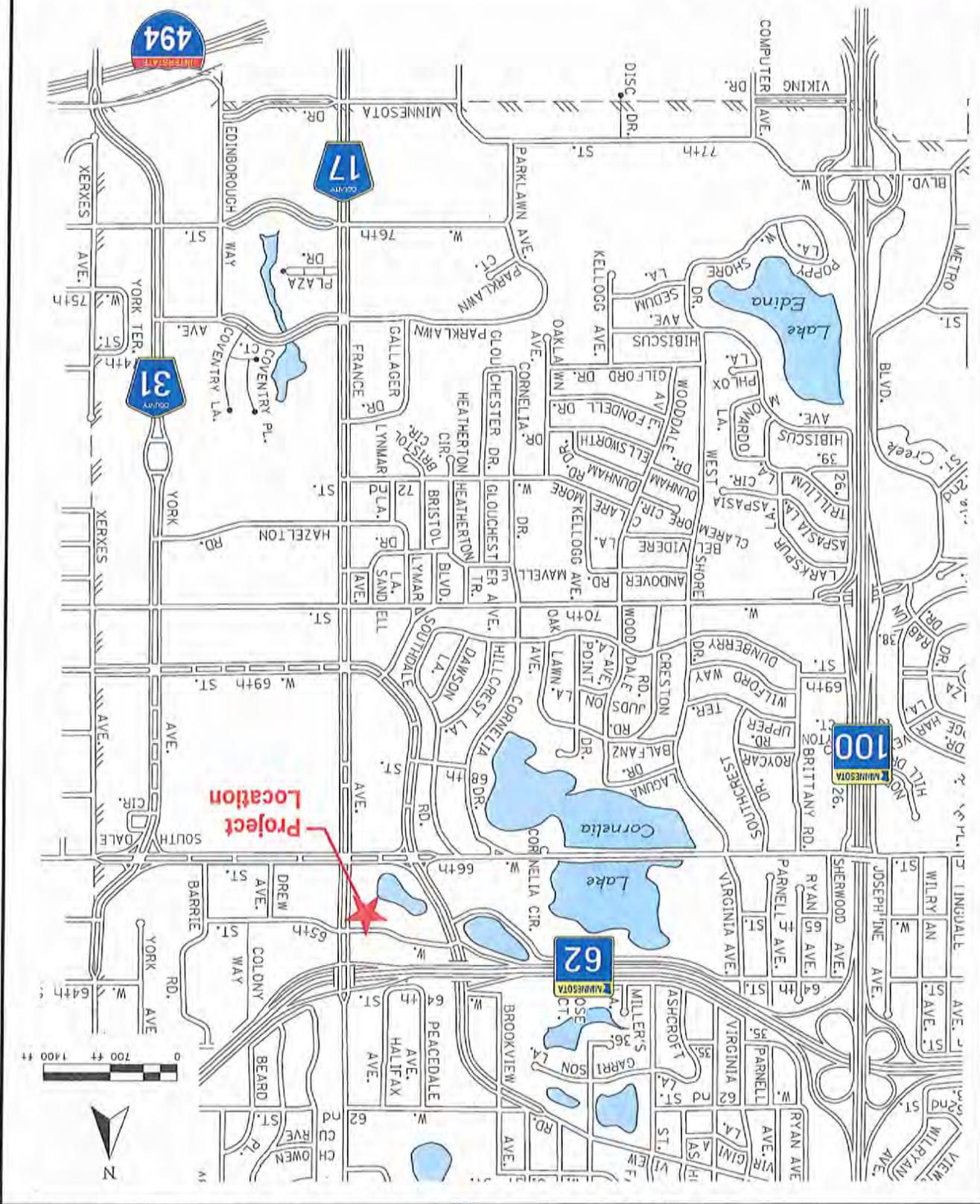


Traffic Impact Study

Aurora on France (6500 France Avenue)
Senior Housing Development
City of Edina, Minnesota

Project Location Map
Figure 1

Date: Print Date: 6/29/2016
Project File Name: K:\10586-430\Coord\Exhibit\10586-43 719-01 - Project Location.dgn



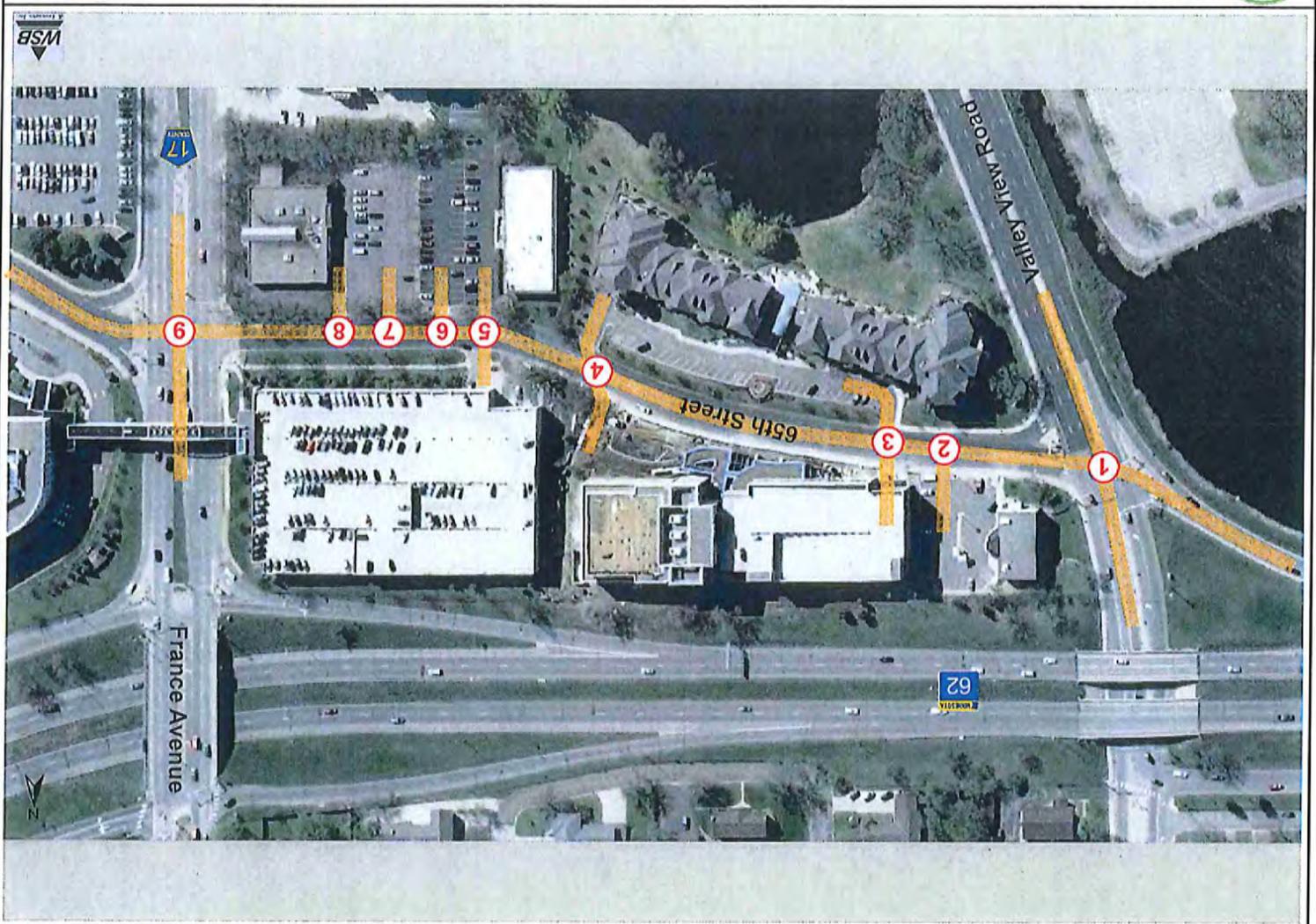
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Traffic Impact Study
Aurora on France Senior Housing Development (6500 France Avenue)
City of Edina, Minnesota

Figure 3
Intersection Location Map

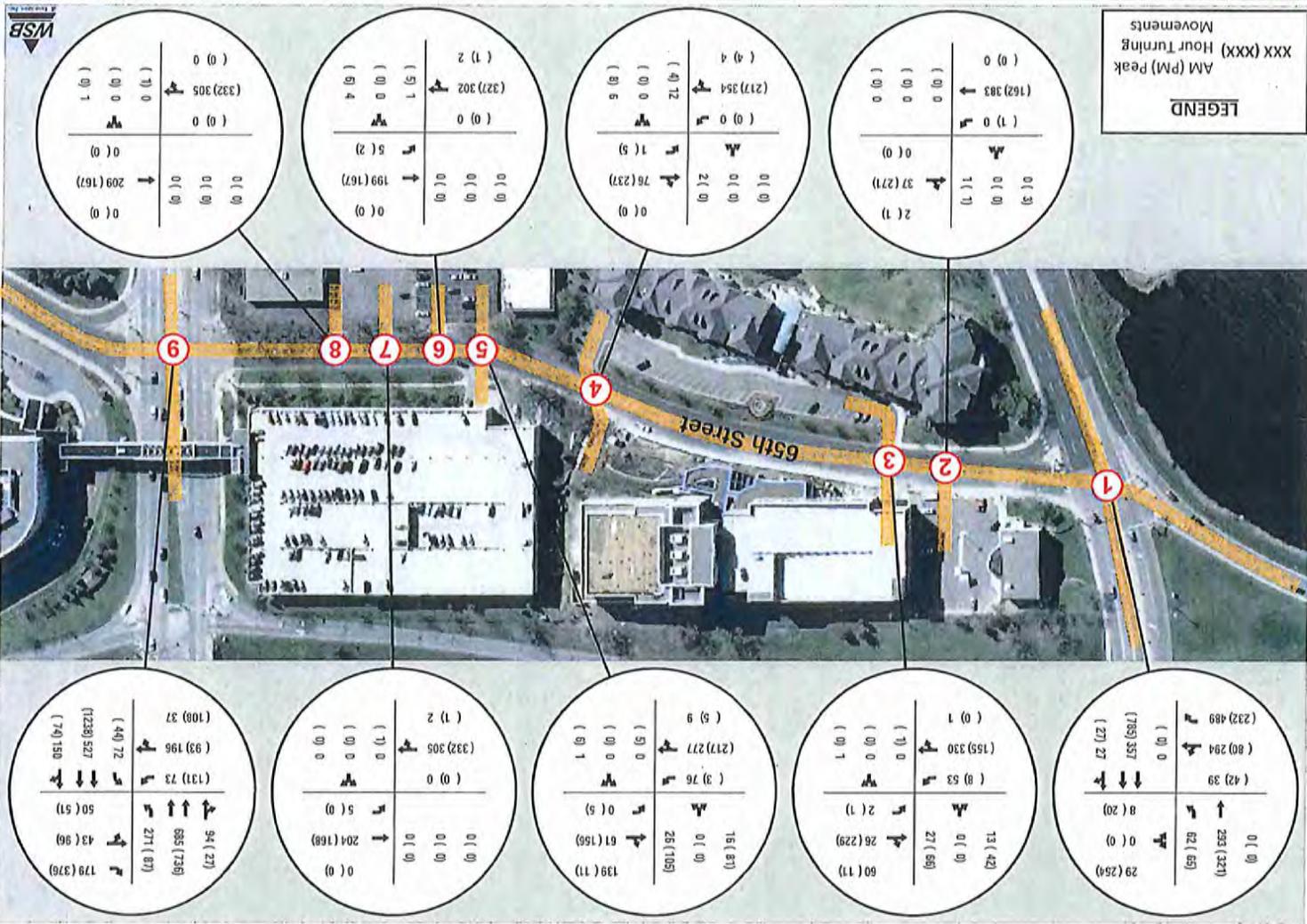




Traffic Impact Study
Aurora on France Senior Housing Development (6500 France Avenue)
City of Edina, Minnesota

Figure 4
Existing Peak Hour Turning Movements

LEGEND
AM (PM) Peak
XXX (XXX) Hour Turning
Movements



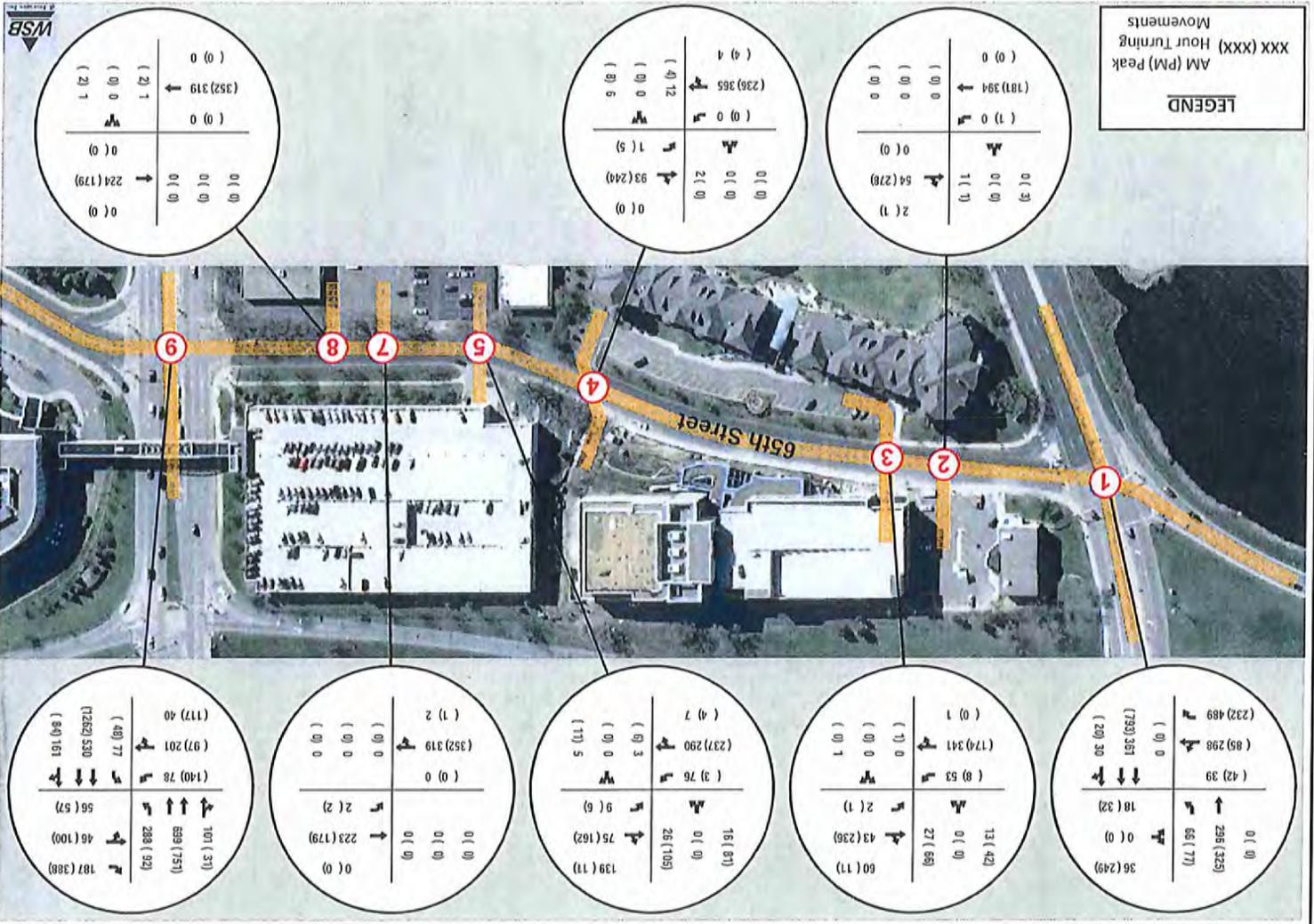
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Traffic Impact Study
 Aurora on France Senior Housing Development (6500 France Avenue)
 City of Edina, Minnesota

Figure 5
 2014 Peak Hour Turning Movements

LEGEND
 AM (PM) Peak
 XXX (XXX)
 Movements



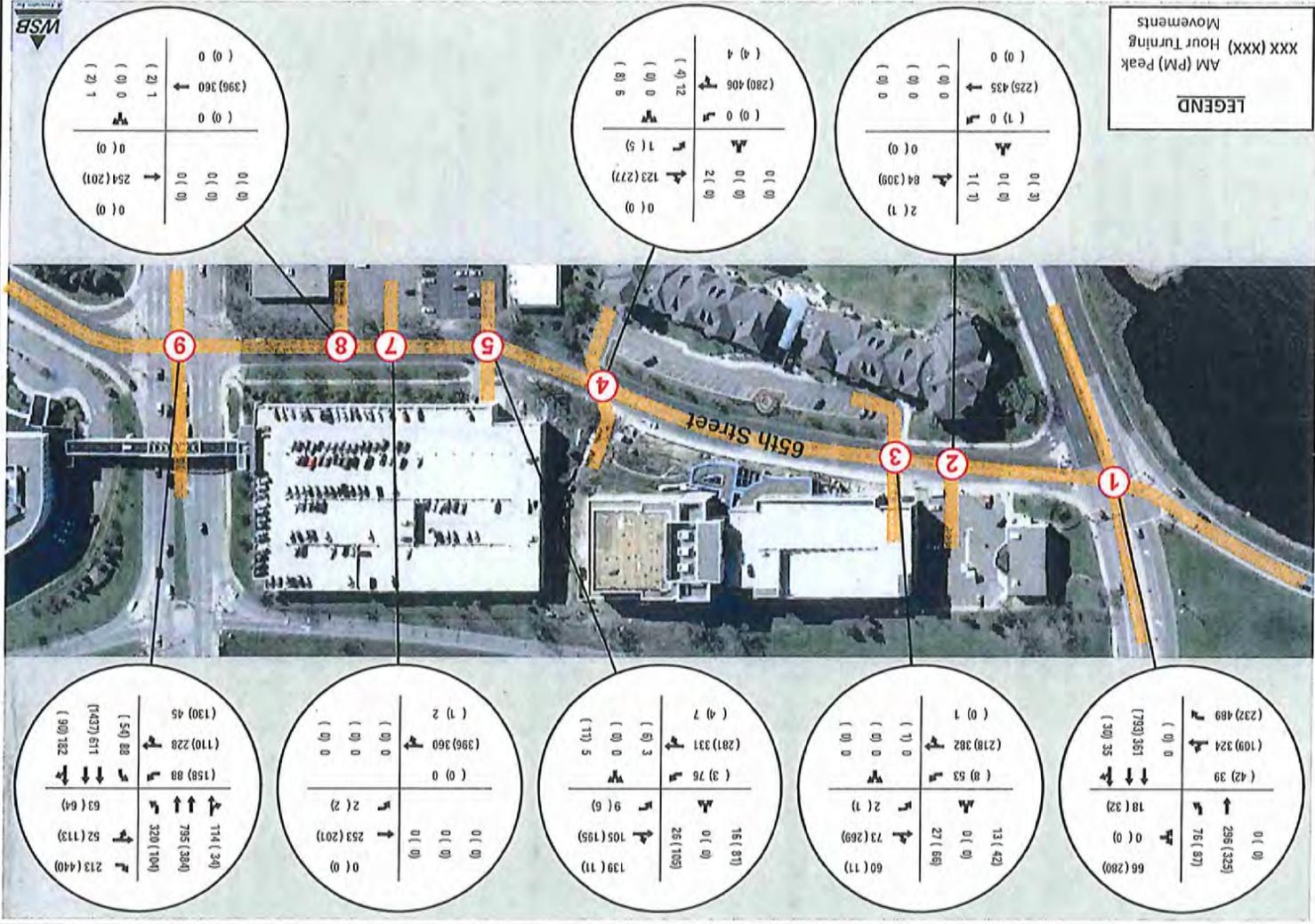
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Traffic Impact Study
 Aurora on France Senior Housing Development (6500 France Avenue)
 City of Edina, Minnesota

Figure 6
 2030 Peak Hour Turning Movements

LEGEND
 AM (PM) Peak
 XXX (XXX)
 Movements



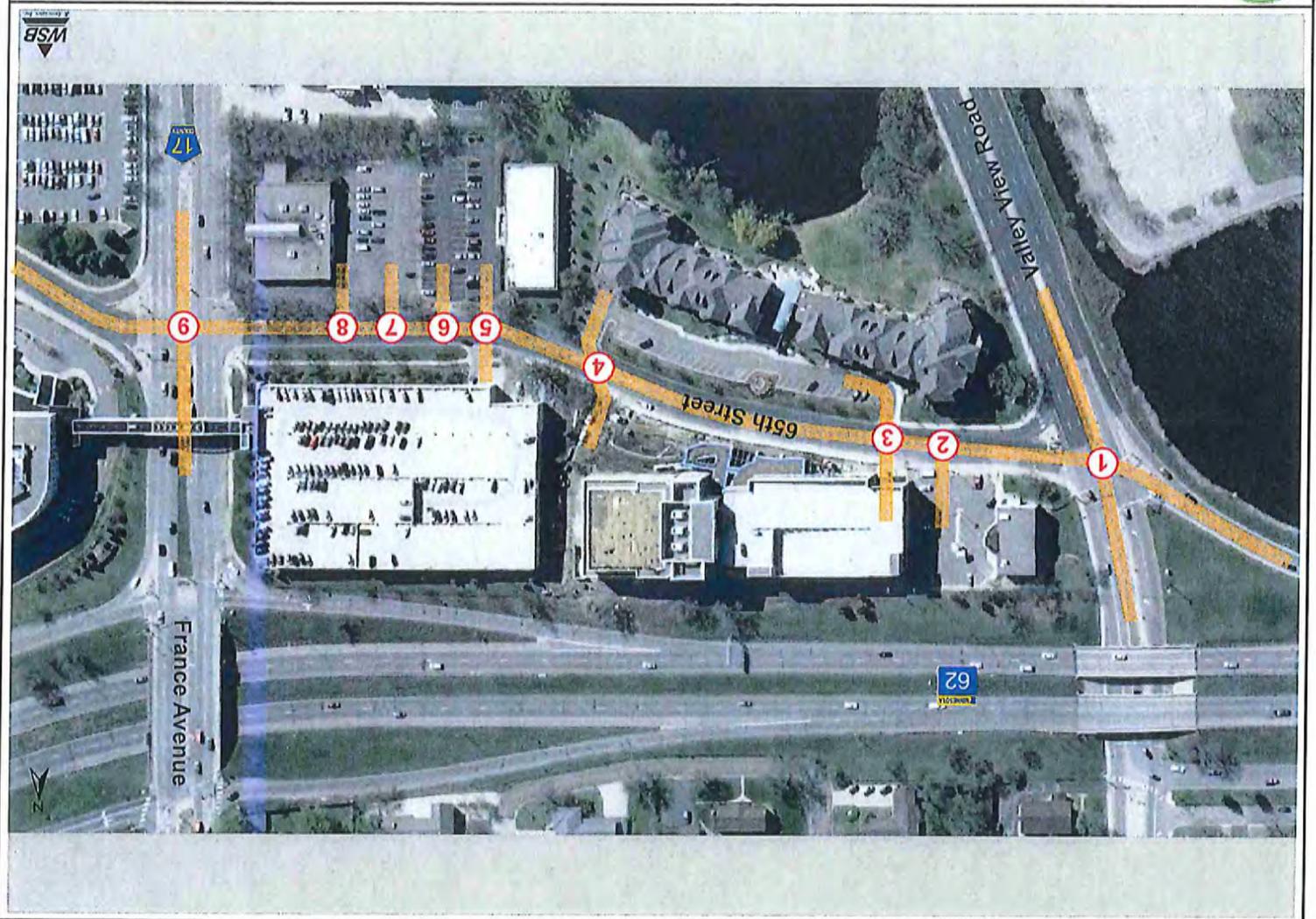
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Traffic Impact Study
Aurora on France Senior Housing Development (6500 France Avenue)
City of Edina, Minnesota

Figure 3
Intersection Location Map

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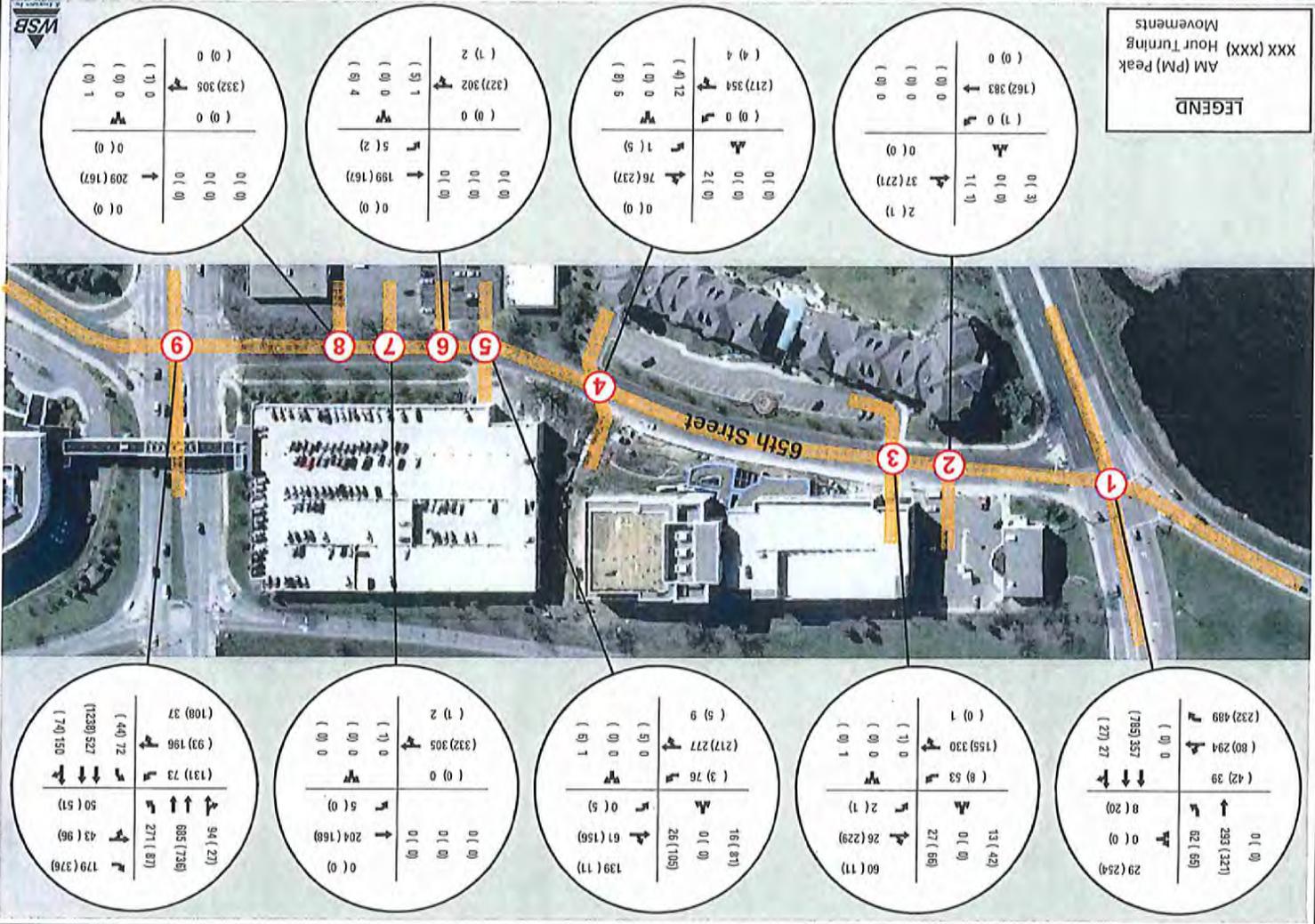




Traffic Impact Study
Aurora on France Senior Housing Development (6500 France Avenue)
City of Edina, Minnesota

Figure 4
Existing Peak Hour Turning Movements

LEGEND
AM (PM) Peak
XXX (XXX) Hour Turning
Movements



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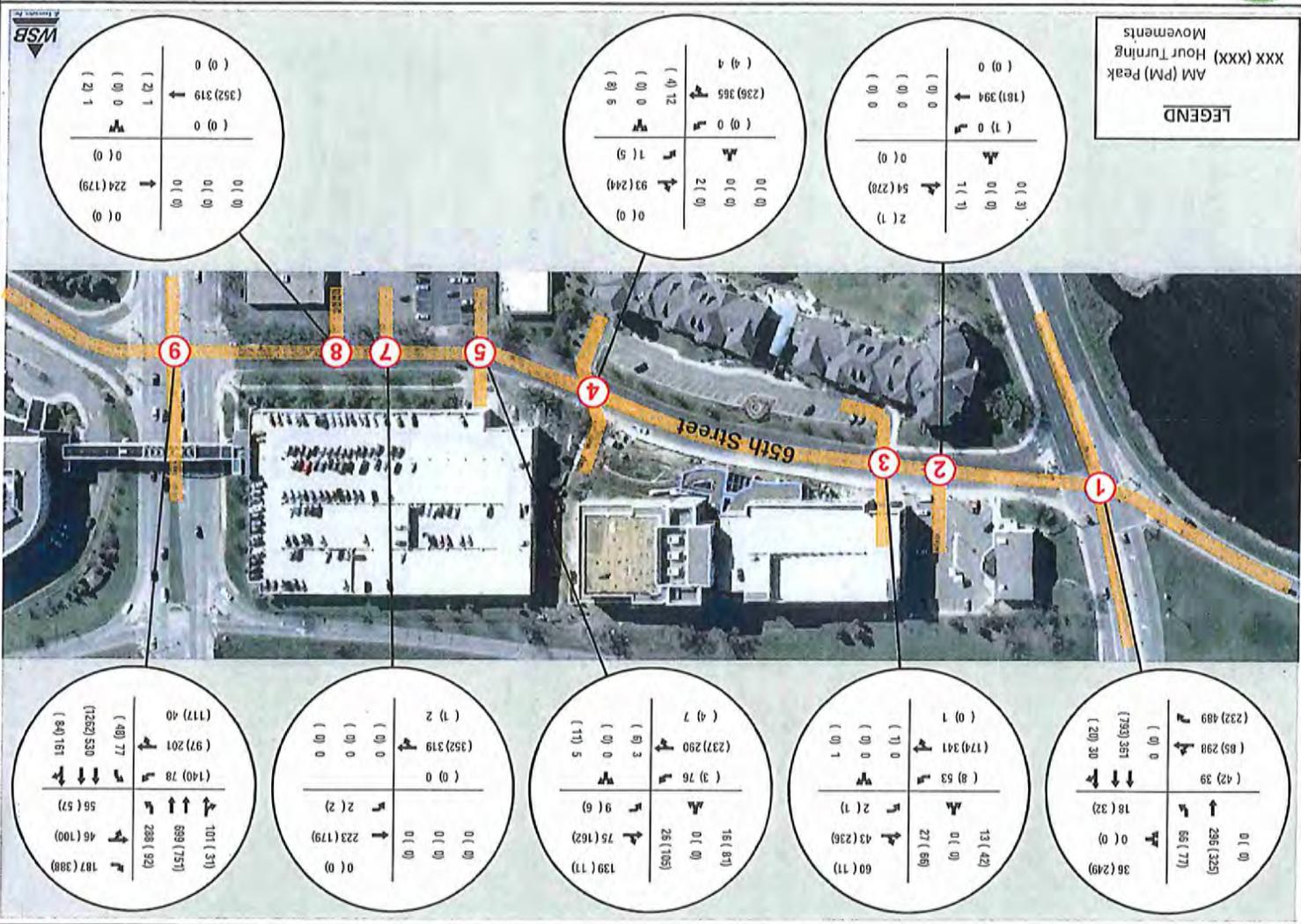


Traffic Impact Study
Aurora on France Senior Housing Development (6500 France Avenue)
City of Edina, Minnesota

2014 Peak Hour Turning Movements

Figure 5

LEGEND
AM (PM) Peak
XXX (xxx)
Movements



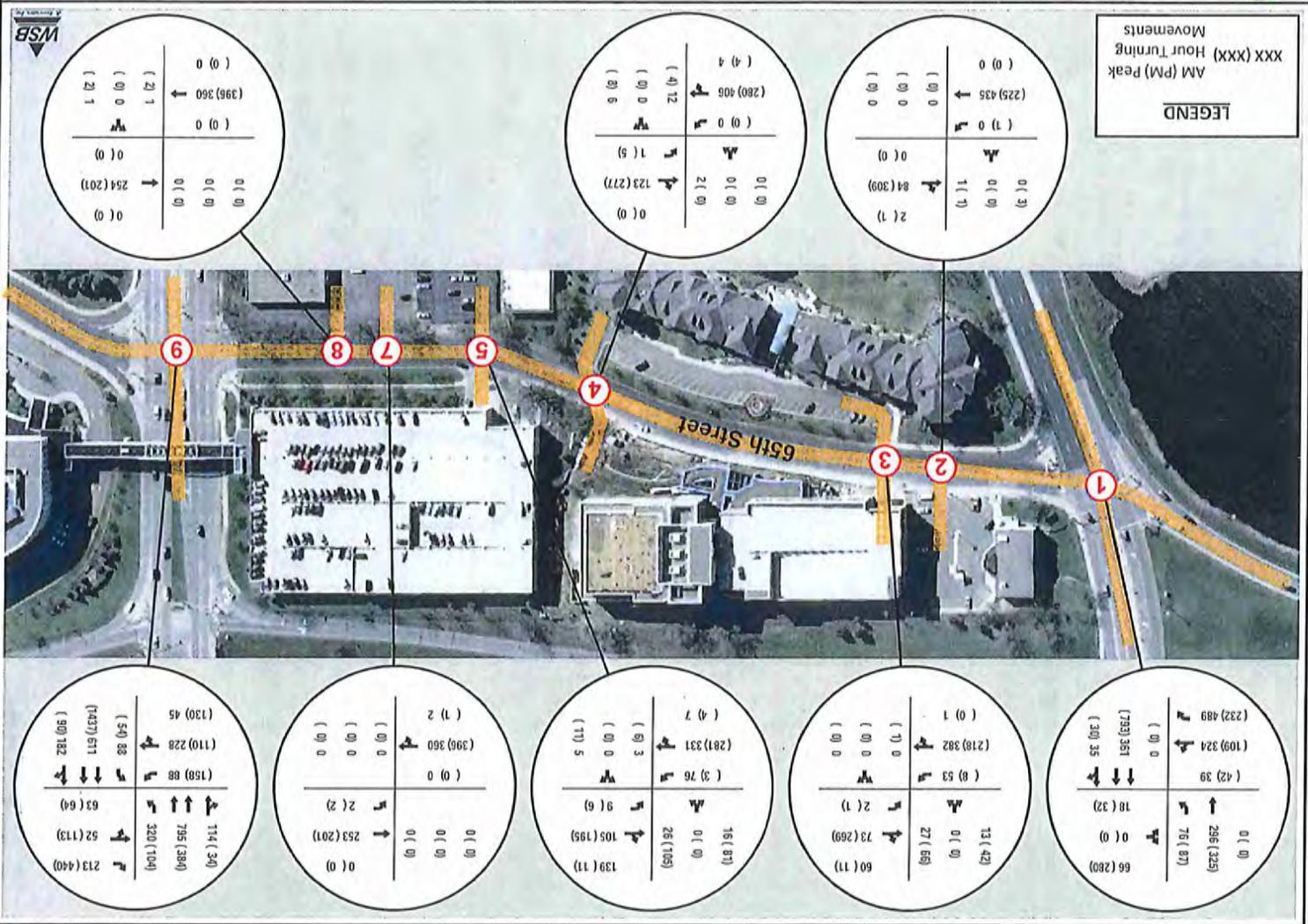
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Traffic Impact Study
 Aurora on France Senior Housing Development (6500 France Avenue)
 City of Edina, Minnesota

Figure 6
 2030 Peak Hour Turning Movements

LEGEND
 AM (PM) Peak
 XXX (XXX) Hour Turning
 Movements



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Location	Signalized		Thru-Stop		Signalized											
	L	R	L	R	L	R	L	R	L	R	L	R	L	R	L	R
1: 65th Street & Valley View Rd	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2: 65th Street & Crosswain Medical Ramp	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4: 65th Street & Condo Garage	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5: 65th Street & Fairview Ramp	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7: 65th Street & 6500 - West Dwy	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8: 65th Street & 6500 - East Dwy	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
9: 65th Street & France Ave	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

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6/26/2013

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Vehicle Queuing Analysis

A queuing analysis for both the existing and future 2014 conditions was prepared evaluating the anticipated vehicle queuing impacts at existing driveways and intersections on 65th Street between France Avenue and Valley View Road. The analysis was conducted using the SimTraffic simulation software.

The results found that during both the AM and PM peak hours, for the existing and future 2014 conditions, the average queues in the corridor do not exceed any of the available turn lanes storage. In some cases however, the maximum queues were exceeded. The maximum queue represents the longest length of queue that was observed during the analysis period. The observations were identified just one time during the peak periods with an extremely short duration of less than 2 seconds. In most cases the queues exceed the storage in the continuous left turn lane, therefore only blocking the adjacent driveway and not impacting through traffic.

Based on the analysis none of the anticipated average queues or maximum queues on 65th Street will back up or block either the France Ave or the Site Access intersections. Tables showing the average and maximum queue lengths by movement and approach are included in the Appendix.

Conclusions / Recommendation

Based on the analysis documented in this memorandum, WSB has concluded the following:

- The proposed site redevelopment is proposed to include primarily medical office and supporting uses and is anticipated to generate 2280 trips in a day, 279 trips in the AM peak hour and 216 trips in the PM peak hour.
- Traffic operations at the intersections and driveway on 65th Street between France Avenue and Valley View Road will remain the same with or without the proposed redevelopment.
- Traffic operations at the proposed site driveway will operate at overall LOS A in the AM and PM peak hours with the worst movement operating at a LOS C.
- The queuing analysis indicates that no significant impact to the adjacent driveways or intersections will occur as a result of the proposed redevelopment.

Based on these conclusions no additional improvements other than those shown on the site plan would be required to accommodate the proposed site redevelopment.

AGS

Jackie Hoogenakker

From: Penny Smith <Penny.smith@att.net>
Sent: Saturday, June 14, 2014 8:19 PM
To: Jackie Hoogenakker
Subject: Aurora project

I live at #603 at Point of France. Our condo overlooks the project. I support the development and feel it will be an asset to the area.

I just received your recent mailing with the latest architect drawing. If my recollection is correct, the exterior originally looked more like the Twin Cities Orthopedics with rich looking shades of brown. This current Aurora on France building seems to show a cheap yellow look as shown in the latest drawing. I hope it is ONLY the printer shade and not reality.

That Red 8 on front of the building seems to cheapen the exterior look. I note that those huge beautiful flower pot containers originally included on the corner are now missing and replaced with rather pathetic looking low planters. Unfortunately, I no longer have the previous drawings of the building, but seems there was also beautiful area (perhaps on the green roof) toward the POF side. I hope it is still there.

Last year, Point of France owners were told that they would be invited to use Aurora's dining facilities, be invited movies that would be shown in their movie theater (with a ceiling of twinkling lights to remind folk of Drive in movies) etc. That is one reason why I strongly supported the project. I hope this offer has not been eliminated due to any change in concept.

Thank you all for your work on this and other Edina issues.

Sincerely,
Penny Smith

Jackie Hoogenakker

From: Tree <twedin@ncscor.com>
Sent: Tuesday, November 04, 2014 10:19 AM
To: Jackie Hoogenakker
Subject: Aurora project

Hi!

The project is very nice looking and the skyway to the ramp is nice. From the previous Aurora presentations they were touting many amenities that there would be at this facility and with this basic outline it doesn't give enough detail to determine exactly but in practical terms only parking for 30 employees seems short.

If there are more than 30 staff or vendors there where will they park?

Jones Harris in Minneapolis and the addition to the Coop at 7600 Xerxes both have parking issues for staff with overflow parking all over have you ever driven it buy these places? Are they going to park in the lot across from valley view at the park? The streets surrounding the development are no parking, 65th France, valley view, what is the plan? Will they be allowed to use the Fairview ramp?

I guess my concern is our parking lots.....will we have to add staff for a parking guard on our two lots? Our receiving lot could be full or empty on any given day and I just see it as a natural progression for the Aurora folks to use our lots without some kind of a plan.

How do I see the rest of the plans noted on the drawing index?

Thanks!

Teresa Wedin

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Jackie Hoogenakker

From: Judie Mattison <judiematt@gmail.com>
Sent: Sunday, November 02, 2014 7:17 PM
To: Jackie Hoogenakker
Subject: 6500 France Av S

I am a resident of Point of France, 6566 France Av. S. I write to support construction of a senior care building at 6500 France Av. S. which is being considered for final rezoning. This building can be a valuable asset to our neighborhood as it serves the growing number of aging people in our community. The neighborhood has other medical services into which these services would fit and with whom they can possibly cooperate. I encourage you to approve this beneficial addition to our area.

Judith Mattison
Point of France #908