



PLANNING COMMISSION STAFF REPORT

Originator Cary Teague Community Development Director	Meeting Date July 23, 2013	Agenda # VI.D.
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INFORMATION/BACKGROUND

Project Description

The Planning Commission is asked to consider a redevelopment request of the existing TCF Bank building, located at 3330 66th Street by Beacon Interfaith Housing Collaborative (Beacon). (See property location on pages A1–A8.) The applicant proposes to remodel and expand the building into 39 units of small studio apartments for young adults (age 18-22) who have experienced homelessness. The size of the units would range from 322-451 square feet. Each unit would contain a full kitchen and bathroom. The building would contain offices for on-site service providers and property management. There would also be a community area for residents; a fitness area; a computer lab and a laundry room. (See applicant narrative and plans on pages A9–A45.)

The site is 39,204 square feet in size. The existing bank is 18,179 square feet. The proposed addition would be 10,458 square feet. The building would remain two stories. The remodel of the building would retain the existing brick, and the addition would be brick with metal panels. (See building renderings on pages A41–A44.)

There would be 19 surface parking stalls. Proof of parking would total 37 total surface stalls. No enclosed parking is proposed. The applicants have indicated in their narrative that 18% of their residents have cars. Beacon anticipates that no more than 12 stalls would be required for residents. The maximum need for staff parking is 6 stalls. Therefore, they believe they would have adequate parking. Residents are expected to utilize the Metro Transit bus service available across the street at Southdale.

All of the 39 units would be considered affordable housing, and would apply towards the City and Met Council's goal for affordable housing.

The Comprehensive Plan defines the site and area as RM, Regional Medical. The RM allows for senior housing on a case by case basis, however, does not allow other housing. Therefore, a Comprehensive Plan Amendment is required.

This development proposal is subject to a two-step review process. The first step in the process is to obtain the following approvals:

1. A Comprehensive Guide Plan Amendment to allow affordable housing in addition to Senior Housing in the Regional Medical District. (4/5 Vote of the City Council required.)
2. Preliminary Rezoning from POD-1, Planned Office District-1, to PUD, Planned Unit Development and Preliminary Development Plan. (3/5 Vote of the City Council required.)

If the Comprehensive Plan Amendment, Preliminary Rezoning and Preliminary Development Plan are approved by the City Council, the following is then required for the second step:

1. Final Development Plan and Final Rezoning to a PUD.
2. Zoning Ordinance Amendment establishing the PUD District.

This property is located within an area of the City that is designated as a "Potential Area of Change" within the 2008 Comprehensive Plan. (See page A3.) The Comprehensive Plan states that within the Potential Areas of Change, "A development proposal that involves a Comprehensive Plan Amendment or a rezoning will require a Small Area Plan study prior to planning application. However, the authority to initiate a Small Area Plan rests with the City Council." The City Council did not require a Small Area Plan during the Sketch Plan Review.

Surrounding Land Uses

- Northerly: Office buildings; zoned POD-1, Planned Office District and guided RM, Regional Medical District.
- Easterly: Multi-story office buildings; zoned POD-1, Planned Office District and guided RM, Regional Medical District.
- Southerly: Firestone Tire & Southdale; Zoned PCD-3, Planned Commercial District and guided CAC, Community Activity Center.
- Westerly: Multi-story office buildings; zoned POD-1, Planned Office District and guided RM, Regional Medical District.

Existing Site Features

The subject property is 39,339 square feet in size, is relatively flat and contains a two-story TCF Bank. (See pages A2–A3.)

Planning

Guide Plan designation: RM, Regional Medical. (See page A5.)
 Zoning: POD-1, Planned Office District -1. (See page A6.)

Comprehensive Guide Plan/Density

As mentioned, the Comprehensive Plan guides this site for RM, Regional Medical Use. The only housing allowed in that district is senior housing. The applicant is requesting an amendment to the Guide Plan to allow “affordable housing” in addition to senior housing. The applicant believes that the type of housing proposed is very similar to senior housing, in that the units are very small, and they would generate very little traffic.

Below is the proposed language to consider for the Comprehensive Guide Plan Amendment. Affordable housing was the term recommended to use during the Sketch Plan Review:

Nonresidential and Mixed Use Categories	Description, Land Uses	Development Guidelines	Density Guidelines
<p>RM Regional Medical</p>	<p>Hospitals, senior housing*, <u>affordable housing</u>, medical and dental offices and clinics, and laboratories for performing medical or dental research, diagnostic testing, analytical or clinical work, having a direct relationship to the providing of health services. General office uses are permitted.</p> <p>* Senior housing may include: independent living, assisted living, memory care, and skilled nursing.</p>	<p>Form-based design standards for building placement, massing and street-level treatment. Pedestrian circulation and open space amenities should be provided for larger sites.</p>	<p>12-80 senior residential <u>and affordable</u> dwelling units per acre. Floor to Area Ratio - Per current Zoning Code: maximum of 1.0 for medical office uses. Density for senior housing <u>and affordable housing</u> shall be based on proximity to hospitals, proximity to low density uses, utilities capacity, level of transit service available, and impact on adjacent roads. Other desired items to allow greater density would include: Below grade parking, provision of park or open space, affordable housing, sustainable design principles, and provision of public art.</p>

A case could be made for allowing affordable housing in this location as it would reuse an existing building (sustainability); provide a 100% affordable housing development to help the city achieve its affordable housing goals with the Metropolitan Council; be in close proximity to Metro Transit; be

located on a high visibility arterial roadway; and be completely separated from low density residential uses. Additionally, the unit sizes are small and they would generate very little traffic, similar to senior housing.

Allowing affordable housing as an allowed use within the entire RMD, Regional Medical District would help assist the City of Edina in meeting its goal with the Metropolitan Council of providing 212 new affordable housing units by the year 2020.

However, there are differences between the proposed project and the project that was allowed for senior housing in the RM District (6500 France). The 6500 France project is located adjacent to the hospital and will provide an enclosed skyway connection to the hospital. Also, part of the facility will be used for long term stay patients from the hospital and Twin City Orthopedic. Therefore, there is a direct connection to the Regional Medical District.

The housing proposed by Beacon would not have a direct connection to the RM, Regional Medical District. The structure would be located several blocks from the hospital. There would not be a direct tie in to any medical use in the area.

The Regional Medical Zoning District contemplates a 10 acre minimum lot size. It is intended for larger medical type uses along with senior housing which benefits from being in close proximity to medical uses.

Proposed Density of the project would be 43 units per acre; which would be within the density range currently allowed for senior housing in the RM, Regional Medical District. The following table demonstrates existing density ranges for high density residential development in Edina. There are a variety of housing types here, from market rate housing to senior affordable housing development.

High Density Development in Edina

Development	Address	Units	Units Per Acre
* Yorktown Continental	7151 York	264	45
The Durham	7201 York	264	46
York Plaza Condos	7200-20 York	260	34
York Plaza Apartments	7240-60 York	260	29
Edina Place Apartments	7300-50 York	139	15

* Walker Elder Suites	7400 York	72	40
* 7500 York Cooperative	7500 York	416	36
Edinburgh Condos	76xx York	392	36
* South Haven	3400 Parklawn	100	42
* The Waters	Colonial Drive	139	22
69 th & York Apartments	3121 69 th Street	114	30
* 6500 France – Senior Housing	6500 France	188	80
Lennar	6725 York	240	52

* Senior Housing

PUD Rezoning

The applicant is requesting a rezoning of this site to PUD, Planned Unit Development District to allow affordable housing on the site. (See attached draft PUD Ordinance.)

Within a PUD District, the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent of the PUD.

The following is compliance table that demonstrates how the proposed building would comply with the Regional Medical District standards and show residential densities in Edina. The use is currently not allowed in the existing POD-1 or RMD Zoning District.

Compliance Table

	City Standard (POD-1 Planned Office District)	Proposed
Front – 66 th Street	35 feet	43 feet (existing)
Front – Barrie Road	35 feet	25 feet (existing)
		40 feet (new)
Side – North	20 feet	50+ feet
Side – east	20 feet	25 feet
Building Height	12 stories or	2 stories

	144 feet, whichever is less	
Building Coverage	30%	30%
Floor Area Ratio	.50% Office (1.0 RMD District)	77%**
Density – Comp. Plan	12-80 units per acre	39 units 43 units per acre
Minimum Lot Size	10 acres (RMD Standard) No minimum in the POD District	.9 acres
Parking Stalls	1 enclosed space per unit + guest parking 39 units	19 spaces exposed** (proof-of-parking for 37)

**** Variance Required**

Per Chapter 36 of the City Code the following are the regulations for a PUD:

1. **Purpose and Intent.** The purpose of the PUD District is to provide comprehensive procedures and standards intended to allow more creativity and flexibility in site plan design than would be possible under a conventional zoning district. The decision to zone property to PUD is a public policy decision for the City Council to make in its legislative capacity. The purpose and intent of a PUD is to include most or all of the following:
 - a. provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan;
 - b. promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City;
 - c. provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian

oriented design, and podium height at a street or transition to residential neighborhoods, parks or other sensitive uses;

- d. ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;**
- e. maintain or improve the efficiency of public streets and utilities;**
- f. preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;**
- g. allow for mixing of land uses within a development;**
- h. encourage a variety of housing types including affordable housing; and**
- i. ensure the establishment of appropriate transitions between differing land uses.**

The project would meet some of the goals for a PUD as outline above. Those include:

- Providing a development that is 100% affordable to assist in the City affordable housing goals with the Met Council.
- Improve the efficiency of street by allowing a land use that would generate less traffic than the bank use; and it would eliminate the bank drive-through facility.
- Would utilize sustainable design as described in the applicant narrative on page A13. Most notable elements include: compliance with Minnesota Overlay and Guide to the 2011 Enterprise Green Communities Criteria; utilizing the existing building rather than a tear down; committing to a 15% energy savings; locating the building to make use of Metro Transit; impervious surface would be reduced by 6.9%; enhanced landscaping; making use of special construction material; installing a rain garden for stormwater management; and pedestrian oriented design.

The Comprehensive Plan allows senior housing as an allowed land use within the regional medical district. The proposed specialty housing proposal to allow another form of housing that is desirable through a PUD would fit with this site, given its close proximity to the Metro Transit Facility across 66th Street and Southdale Shopping Center. This area would provide employment opportunities

in close proximity for residents. The site is located on an edge of the Regional Medical District and on an arterial roadway.

2. Applicability/Criteria

- a. Uses. All permitted uses, permitted accessory uses, conditional uses, and uses allowed by administrative permit contained in the various zoning districts defined in Section 850 of this Title shall be treated as potentially allowable uses within a PUD district, provided they would be allowable on the site under the Comprehensive Plan.**

If City Council amends the Comprehensive Plan to allow affordable housing in addition to senior housing, this site would otherwise be envisioned to allow uses permitted within the Regional Medical District. The Regional Medical Zoning District contemplates a 10 acre minimum lot size. It is intended for larger medical type uses along with senior housing which benefits from being in close proximity to medical uses.

The Zoning Ordinance amendment, which follows this staff report, lists the uses that would be allowed on this site. Spack Consulting did a parking analysis that determined that the proposed parking would support the uses proposed, and the traffic generated would actually be less than the previously approved medical office. (See pages A48-A110.)

The proposed residential development would generate 20 am peak hour trips and 24 pm peak hour trips. The existing bank generates 45 am peak hour trips and 89 pm peak hour trips.

- b. Eligibility Standards. To be eligible for a PUD district, all development should be in compliance with the following:**
- i. where the site of a proposed PUD is designated for more than one (1) land use in the Comprehensive Plan, the City may require that the PUD include all the land uses so designated or such combination of the designated uses as the City Council shall deem appropriate to achieve the purposes of this ordinance and the Comprehensive Plan;**

The proposal would not include a mixture of land uses. It would include affordable housing for young adults who have experienced homelessness. Within the overall RMD, Regional Medical District, this project would introduce the potential for another use in the district, and would help the City meet its affordable housing goals established with the Metropolitan Council of 212 new affordable housing units by the year 2020.

ii. any PUD which involves a single land use type or housing type may be permitted provided that it is otherwise consistent with the objectives of this ordinance and the Comprehensive Plan;

As mentioned above, the proposed uses would be for housing that is all affordable. Providing affordable housing and sustainable development are goals within the Comprehensive Plan that this project would accomplish. Comprehensive Plan goals and objectives include:

- Promotion of a vision of community that is inclusive of a wide range of ages, incomes, and abilities and offers a wide range of housing options for Edina residents.
- Promote lifecycle housing to support a range of housing options that meet people's preferences and circumstance at all stages of life.
- Variety of Buildings Forms. Encourage an integrated mix of building type, heights and footprints within blocks, rather than single buildings or building groups.
- Incorporate principles of sustainability and energy conservation into all aspects of design, construction, renovation and long-term operation of new and existing development.

iii. permitted densities may be specifically stated in the appropriate planned development designation and shall be in general conformance with the Comprehensive Plan; and

The proposed building density would be 43 units per acre and have an FAR of .77. The Floor Area Ratio contemplated in the Comprehensive Plan for Regional Medical is 1.0; however, the current POD-1 Zoning District allows an FAR of .50. The density range allowed for senior housing in the district is up to 80 units per acre.

Density in the Comprehensive Plan limits senior housing to 12-80 dwelling units per acre. Density for senior housing shall be based on proximity to hospitals, proximity to low density uses, utilities capacity, level of transit service available, and impact on adjacent roads. Other desired items to allow greater density would include: Below grade parking, provision of park or open space, affordable housing, sustainable design principles, and provision of public art.

The site has adequate utilities capacity; would generate less traffic than an office use; would provide affordable housing; would be a sustainable development; and would take advantage of Metro Transit Availability. Staff believes the density is appropriate for this site.

iv. the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent described in #1 above.

The proposed project does closely relate to the already established standards in the POD-1 District, as the existing building is being utilized. Flexibility is requested in regard to parking spaces. For the reasons stated above, staff believes the purpose and intend of the PUD Ordinance is met.

Site Access

The primary access to the site would remain off of Barrie Road. One drive entrance would be eliminated. (See page A19.)

Parking

Per Chapter 36, Article XII, Division 4, the requirement for multi-family residential parking in a commercial area, is one enclosed space per unit plus additional guest parking as required. Therefore, at minimum a requirement of 39 stalls plus guest parking should be provided. The applicant is proposing 19 surface stalls, with a proof-of parking plan to 37 stalls.

Staff has some concern in regard to potential future lack of parking. While the proposed use may be able to get by with the proposed surface parking only, any future conversion of these units for market rate housing would surely be short of parking. A stipulation in a potential PUD Ordinance would be to only allow this type of housing on the site; therefore, any conversion of the units would require a PUD Amendment. Additional parking would be required as part of any PUD Amendment.

A parking study was done by Spack Consulting, which concludes that the proposed parking would support the use. (See page A56.) The total demand for parking is anticipated to be 12 spaces.

Traffic

A traffic study was also done by Spack, which concludes that the existing roadways support the proposed uses. (See traffic study on pages A48–A110.) The proposed use would generate less traffic than the existing bank on the site. The existing use generates 45 trips in the am peak hour and 889 trips in the pm peak hour. The proposed use would generate 20 trips in the am peak hour and 24 trips in the pm peak hour.

Landscaping

Based on the perimeter of the site, 21 overstory trees and a full complement of understory trees and shrubs are required. The applicant is proposing to plant 24 overstory trees around the perimeter of the site & understory trees and shrubs. (See landscape plan on pages A19 and A34-A35.) A more detailed review of the landscape plan would be done at final development plan.

Grading & Utilities

The city engineer has reviewed the plans and found them generally acceptable and offered comments. (See pages A46-A47.) A more detailed review would be done as part of the Final Development Plan and Final Rezoning.

Signage

Signage would be allowed per the requirements of the POD-1, Planned Office District within the Zoning Ordinance Amendment for the PUD. (See attached draft Ordinance.)

Bike Racks

The applicant is proposing 26 bicycle parking spots and indoor bicycle storage. Outdoor racks would be located in front of the building near the main entrance off the parking lot. (See page A19.)

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issues

- **Is the proposed Comprehensive Plan Amendment to allow affordable housing as a permitted use in the RM District reasonable?**

Yes. Staff believes the proposed Comprehensive Plan Amendment is reasonable for the site for the following reasons:

1. Affordable housing is identified as a need in the Comprehensive Plan; and the proposed amendment would assist the City in meeting its established affordable housing goal with the Met Council of providing 212 new affordable housing units by the year 2020. This project would include 39 new affordable housing units toward that goal. That would make up 100% of the total units in the project.
2. The proposed density of 43 units per acre is reasonable, and within the density range suggested in the Comprehensive Plan of between 12-80 units per acre.
3. The RM District allows senior housing currently. The proposed affordable housing project would include units that are small in size generally similar to senior housing; and the residents within the proposed project typically do not drive, similar to senior housing.
4. The proposed affordable housing project would generate less traffic than the existing bank facility.
5. The project would utilize sustainability principles. Most notable elements include: compliance with Minnesota Overlay and Guide to the 2011 Enterprise Green Communities Criteria; utilizing the existing building rather than a tear down; committing to a 15% energy savings; locating the building to make use of Metro Transit; impervious surface would be reduced by 6.9%; enhanced landscaping; making use of special construction material; installing a rain garden for storm water management; and pedestrian oriented design.
6. Project would meet the following additional Comprehensive Plan goals and objectives:
 - a) Promotion of a vision of community that is inclusive of a wide range of ages, incomes, and abilities and offers a wide range of housing options for Edina residents.
 - b) Promotion of lifecycle housing to support a range of housing options that meet people's preferences and circumstance at all stages of life.
 - c) Encourage an integrated mix of building type, heights and footprints within blocks, rather than single buildings or building groups.
 - d) Incorporate principles of sustainability and energy conservation into all aspects of design, construction, renovation and long-term operation of new and existing development.

However, as mentioned earlier, there are differences between the proposed project and the project that was allowed for senior housing in the RM District (6500 France). The 6500 France project is located adjacent to the hospital and will provide an enclosed skyway connection to the hospital. Also, part of the facility will be used for long term stay patients from the hospital and Twin City Orthopedic. Therefore, there is a direct connection to the Regional Medical District.

The housing proposed by Beacon would not have a direct connection to the RM, Regional Medical District. The structure would be located several blocks from the hospital. There would not be a direct tie in to any medical use in the area.

The Regional Medical Zoning District contemplates a 10 acre minimum lot size. It is intended for larger medical type uses along with senior housing which benefits from being in close proximity to medical uses.

- **Is the proposed rezoning to PUD appropriate for the site?**

Yes. Staff believes the proposal to rezone the site to PUD is reasonable for the site for the following reasons:

1. The project would meet many of the goals of for a PUD as outline above. Those include:
 - Providing a development that is 100% affordable to assist in the city affordable housing goals with the Met Council.
 - Improve the efficiency of street by allowing a land use that would generate less traffic than the bank use; and it would eliminate the bank drive-through facility.
 - Would utilize sustainable design as described in the applicant narrative on page A13. Most notable elements include: compliance with Minnesota Overlay and Guide to the 2011 Enterprise Green Communities Criteria; utilizing the existing building rather than a tear down; committing to a 15% energy savings; locating the building to make use of Metro Transit; impervious surface would be reduced by 6.9%; enhanced landscaping; making use of special construction material; installing a rain garden for stormwater management; and pedestrian oriented design.
2. The Comprehensive Plan allows senior housing as an allowed land use within the regional medical district. The proposed specialty housing proposal to allow another form of housing that is desirable through a PUD would fit in well with this site, given its close proximity to the Metro Transit Facility

across 66th Street and Southdale Shopping Center. This area could provide employment opportunity in close proximity for residents. The site is located on an edge of the Regional Medical District and on an arterial roadway.

3. The existing roadways and parking lot would support the project. Spack Consulting conducted a traffic impact study based on the proposed development, and concluded that the traffic generated from the project would not impact the adjacent driveways or intersections. In fact the proposed uses would actually generate less traffic than the previously approved medical building. No additional improvements other than those shown on the site plan would be required to accommodate the site redevelopment. (See traffic study on pages A48–A110.)
4. The PUD ensures that the building proposed would be the only building built on the site, unless an amendment to the PUD is approved by City Council.
5. The PUD ensures that this is the only use allowed within the building. Any change in use would require an amendment to the PUD Ordinance.

Staff Recommendation

Comprehensive Plan Amendment

Recommend that the City Council approve the request for a Comprehensive Plan Amendment to allow affordable housing in the RMD District.

Approval is subject to the following findings:

1. Affordable housing is identified as a need in the Comprehensive Plan; and the proposed amendment would assist the City in meeting its established affordable housing goal with the Met Council of providing 212 new affordable housing units by the year 2020. This project would include 39 new affordable housing units (100% of the projects units) toward that goal.
2. The proposed density of 43 units per acre is reasonable, and within the density range suggested in the Comprehensive Plan of between 12-80 units per acre.
3. The RM District allows senior housing currently. The proposed affordable housing project would include units that are small in size generally similar to senior housing; and the residents within the proposed project typically do not drive, similar to senior housing.
4. The proposed affordable housing project would generate less traffic than the existing bank facility.

5. The project would utilize sustainability principles. Most notable elements include: compliance with Minnesota Overlay and Guide to the 2011 Enterprise Green Communities Criteria; utilizing the existing building rather than a tear down; committing to a 15% energy savings; locating the building to make use of Metro Transit; impervious surface would be reduced by 6.9%; enhanced landscaping; making use of special construction material; installing a rain garden for storm water management; and pedestrian oriented design.
6. The project would meet the following additional Comprehensive Plan goals and objectives:
 - a) Promotion of a vision of community that is inclusive of a wide range of ages, incomes, and abilities and offers a wide range of housing options for Edina residents.
 - b) Promotion of lifecycle housing to support a range of housing options that meet people's preferences and circumstance at all stages of life.
 - c) Encourage an integrated mix of building type, heights and footprints within blocks, rather than single buildings or building groups.
 - d) Incorporate principles of sustainability and energy conservation into all aspects of design, construction, renovation and long-term operation of new and existing development.

***Preliminary Rezoning from POD-1,
Planned Office District to Planned Unit Development
District & Preliminary Development Plan***

Recommend that the City Council approve the Preliminary Rezoning and approve the Preliminary Development Plan.

Approval is based on the following findings:

1. Affordable housing is identified as a need in the Comprehensive Plan; and the proposed amendment would assist the City in meeting its established affordable housing goal with the Met Council of providing 212 new affordable housing units by the year 2020. This project would include 39 new affordable housing units toward that goal.

2. The proposed density of 43 units per acre is reasonable, and within the density range suggested in the Comprehensive Plan of between 12-80 units per acre.
3. The proposed affordable housing project would generate less traffic than the existing bank facility.
4. The project would utilize sustainability principles. Most notable elements include: compliance with Minnesota Overlay and Guide to the 2011 Enterprise Green Communities Criteria; utilizing the existing building rather than a tear down; committing to a 15% energy savings; locating the building to make use of Metro Transit; impervious surface would be reduced by 6.9%; enhanced landscaping; making use of special construction material; installing a rain garden for storm water management; and pedestrian oriented design.
5. Project would meet the following additional Comprehensive Plan goals and objectives:
 - a) Promotion of a vision of community that is inclusive of a wide range of ages, incomes, and abilities and offers a wide range of housing options for Edina residents.
 - b) Promotion of lifecycle housing to support a range of housing options that meet people's preferences and circumstance at all stages of life.
 - c) Encourage an integrated mix of building type, heights and footprints within blocks, rather than single buildings or building groups.
 - d) Incorporate principles of sustainability and energy conservation into all aspects of design, construction, renovation and long-term operation of new and existing development.

Preliminary approval is subject to the following conditions:

1. The Final Development Plan must be generally consistent with approved Preliminary Development Plans dated June 20, 2014.
2. Sustainable design. The design and construction of the entire project must be done with the Sustainable Initiatives as outlined in the applicant's narrative within the Planning Commission staff report.
3. All buildings must be built with sprinkler systems, subject to review and approval of the fire marshal.

4. Compliance with all of the conditions outlined in the director of engineering's memo dated July 15, 2014. (See pages A46-A47.)
5. The Final Landscape Plan must meet all minimum landscaping requirements per Chapter 36 of the Zoning Ordinance.
6. The Final Lighting Plan must meet all minimum landscaping requirements per Chapter 36 of the Zoning Ordinance.
7. Final Rezoning is subject to a Zoning Ordinance Amendment creating the PUD, Planned Unit Development for this site.
8. Final Rezoning is subject to review and approval of the Metropolitan Council on the Comprehensive Plan Amendment.

Deadline for a city decision: October 15, 2014

ORDINANCE NO. 2014-__

AN ORDINANCE AMENDING THE ZONING ORDINANCE
TO ESTABLISH THE PUD-7, PLANNED UNIT DEVELOPMENT-7
DISTRICT AT 3330 66th STREET

The City Of Edina Ordains:

Section 1. Chapter 36, Article VIII, Division 4 is hereby amended to rezone the below described property to PUD, Planned Unit Development in accordance with the following:

Sec. 36-494 Planned Unit Development District-7 (PUD-7) – 66th West Apartments

(a) *Legal description:*

The South 300 feet of Lot 2, as measured along the West line of said lot from the Southwest corner thereof in Block 3, Southdale Acres, Hennepin County, Minnesota. Torrens Property Certificate of Title No: 361393.

(b) **Approved Plans.** Incorporated herein by reference are the re-development plans received by the City on June __, 2014 except as amended by City Council Resolution No. 2014-__, on file in the Office of the Planning Department.

(c) **Principal Uses:**

Affordable housing as proposed and described by Beacon Interfaith Housing collaborative in their project description on file in the Office of the Planning Department. Any change in use of the site will require an amendment to this PUD-7 Ordinance.

(d) **Accessory Uses:**

None

(e) **Conditional Uses:**

None

(f) **Development Standards.** Development standards per the POD-1 Zoning District, except the following:

Building Setbacks

<u>Building Setbacks</u> Front – 66 th Street Front – Barrie Road Side – North Rear – South	43 feet 25 feet 50 feet 25 feet
Building Height	2 stories
Maximum Floor Area Ratio (FAR)	.77%
Building Coverage	30%
Parking Stalls	19 surface Proof-of-parking for 37

(g) Signs shall be allowed per the POD-1 standards in Sec. 36-1714.

Section 3. This ordinance is effective immediately upon Met Council review and decision on the Comprehensive Plan Amendment.

First Reading:

Second Reading:

Published:

ATTEST:

Debra A. Mangen, City Clerk

James B. Hovland, Mayor

Please publish in the Edina Sun Current on:

Send two affidavits of publication.

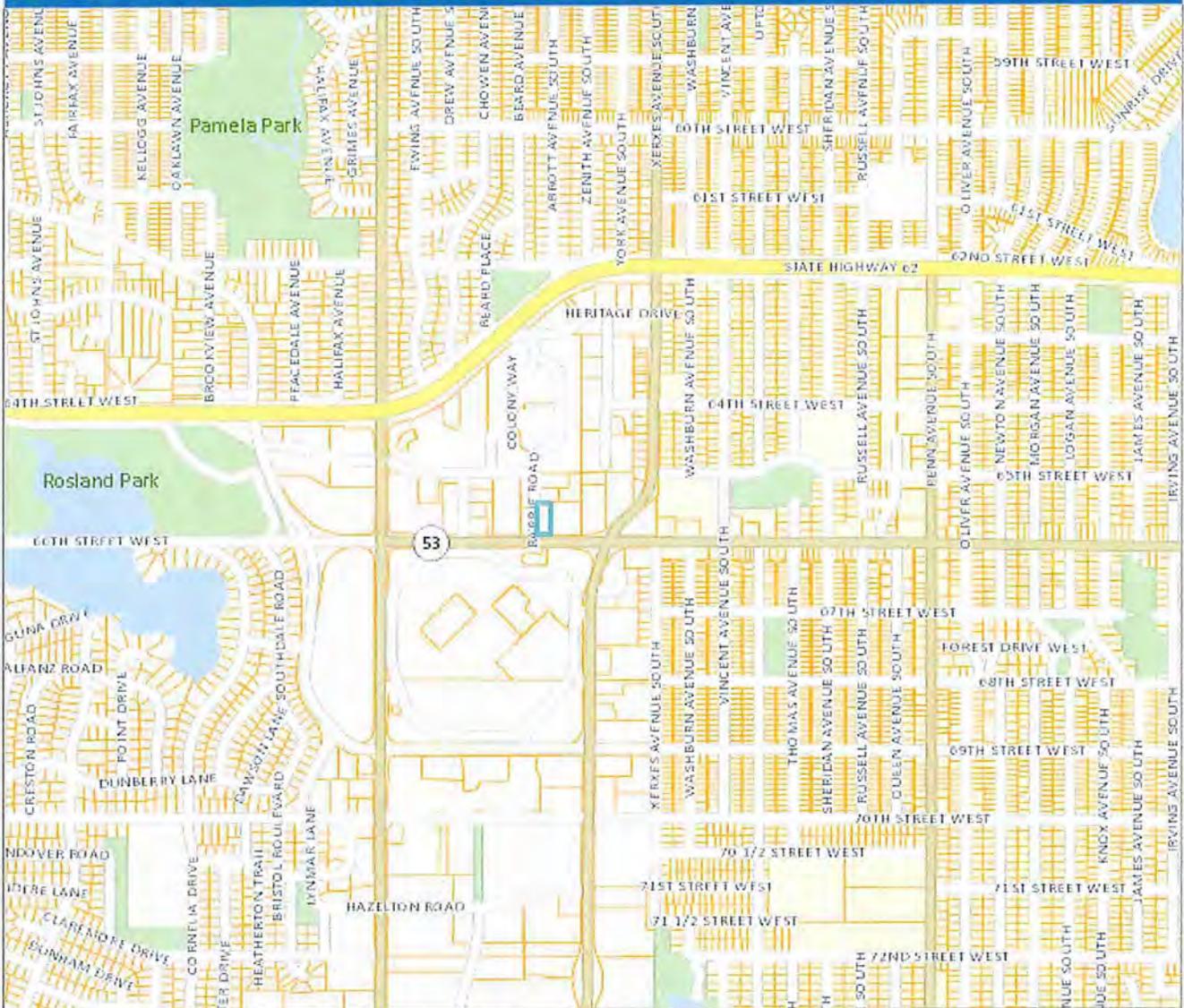
Bill to Edina City Clerk

CERTIFICATE OF CITY CLERK

I, the undersigned duly appointed and acting City Clerk for the City of Edina do hereby certify that the attached and foregoing Ordinance was duly adopted by the Edina City Council at its Regular Meeting of June 17, 2014, and as recorded in the Minutes of said Regular Meeting.

WITNESS my hand and seal of said City this _____ day of _____, 2014.

City Clerk



Parcel ID: 29-028-24-24-0030

Owner Name: Twin City Fed Sav

Parcel Address: 3330 66Th St W
Edina, MN 55435

Property Type: Commercial-Preferred

Home-stead: Non-Homestead

Parcel Area: 0.9 acres
39,339 sq ft

A-T-B:

Market Total:

Tax Total:

Sale Price:

Sale Date:

Sale Code:

Map Scale: 1" ≈ 1600 ft.
Print Date: 4/17/2014

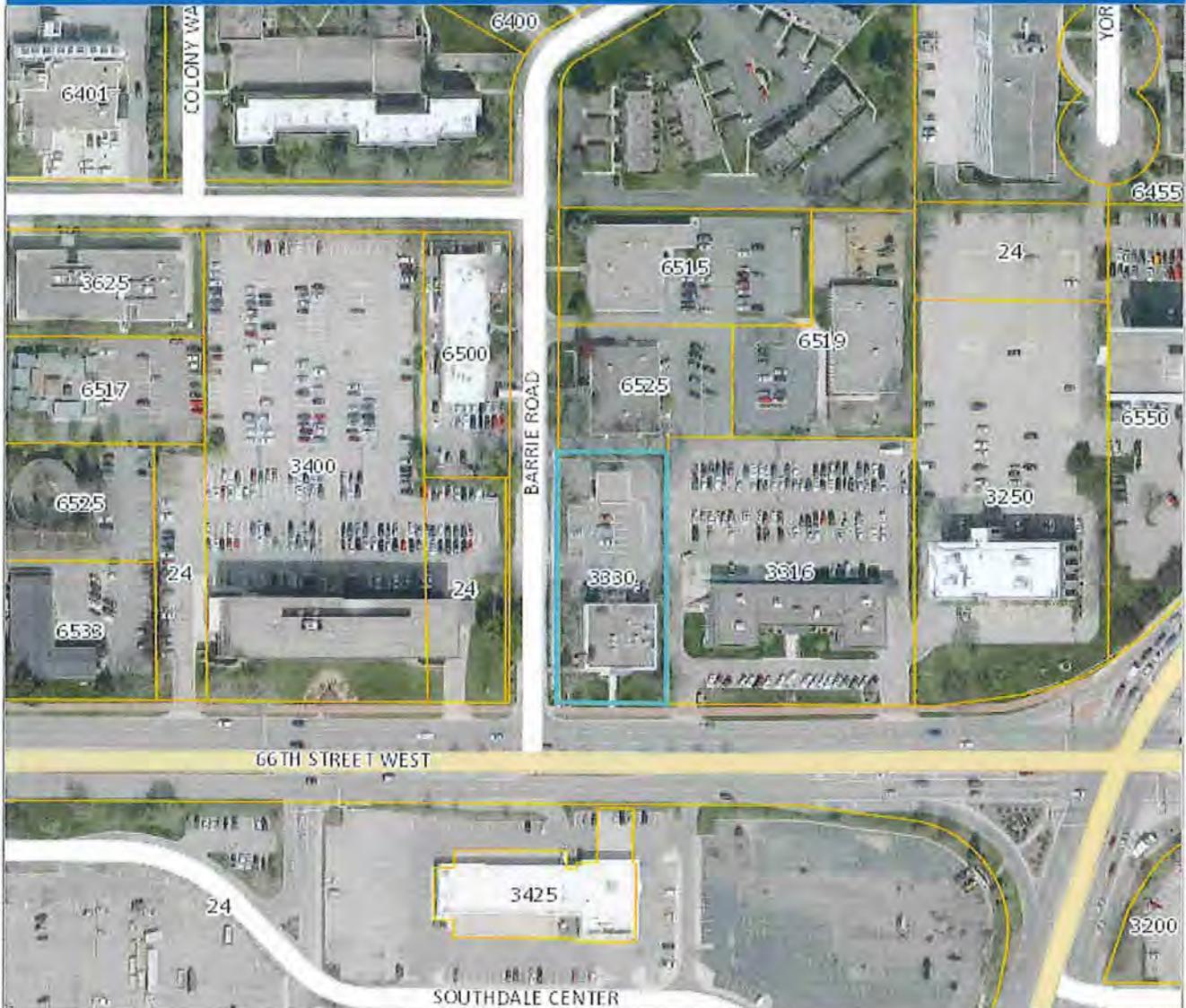


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Parcel ID: 29-028-24-24-0030 Owner Name: Twin City Fed Sav Parcel Address: 3330 66Th St W Edina, MN 55435 Property Type: Commercial-Preferred Home-stead: Non-Homestead Parcel Area: 0.9 acres 39,339 sq ft	A-T-B: Market Total: Tax Total: Sale Price: Sale Date: Sale Code:	Map Scale: 1" ≈ 200 ft. Print Date: 4/17/2014  <p>This map is a compilation of data from various sources and is furnished "AS IS" with no representation or warranty expressed or implied, including fitness of any particular purpose, merchantability, or the accuracy and completeness of the information shown.</p> <p>COPYRIGHT © HENNEPIN COUNTY 2014</p> 
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Google earth



A3

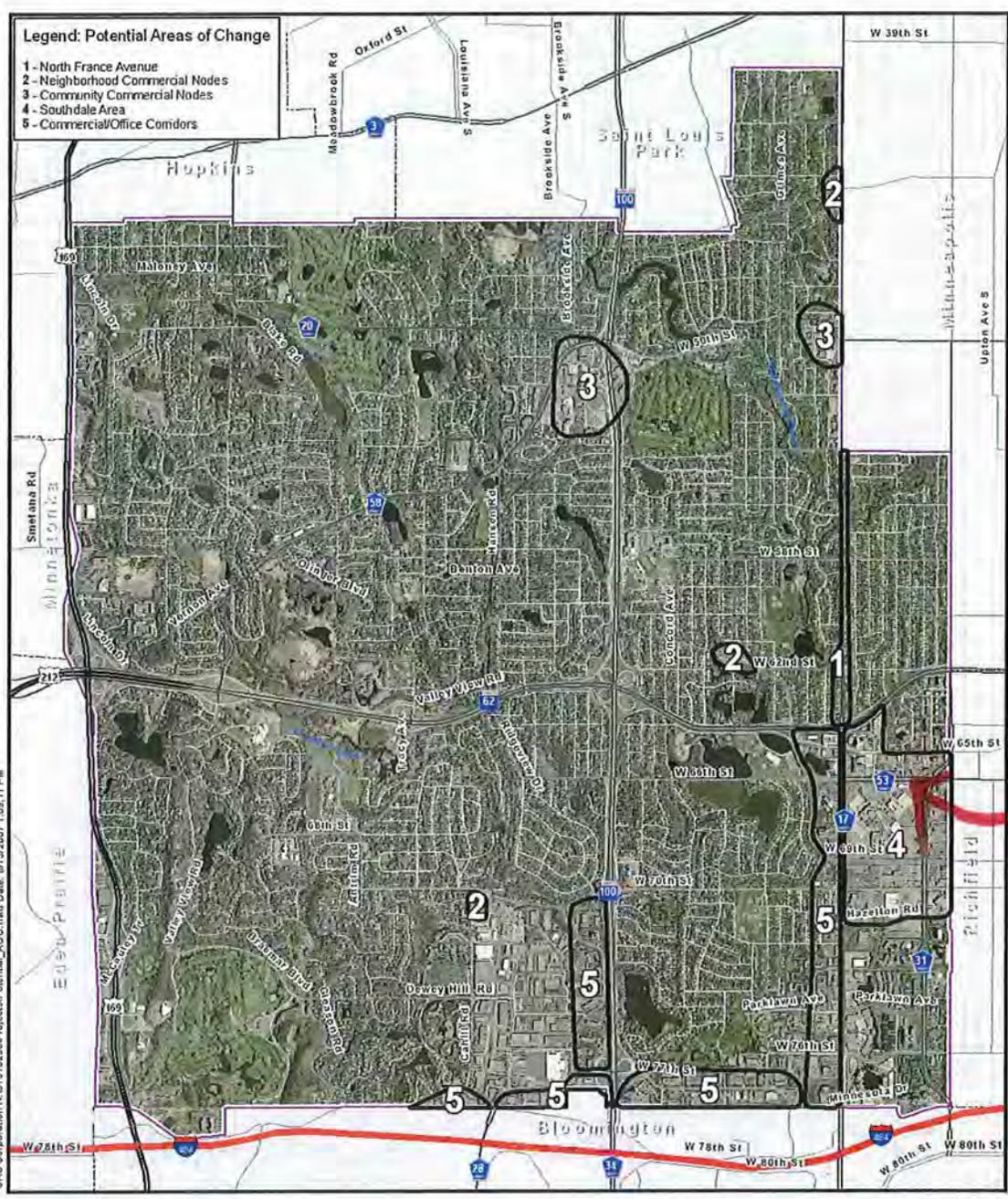


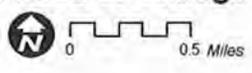
Figure 4.4



City of Edina
2008 Comprehensive Plan Update

**Conceptual Land Use Framework:
Potential Areas of Change**

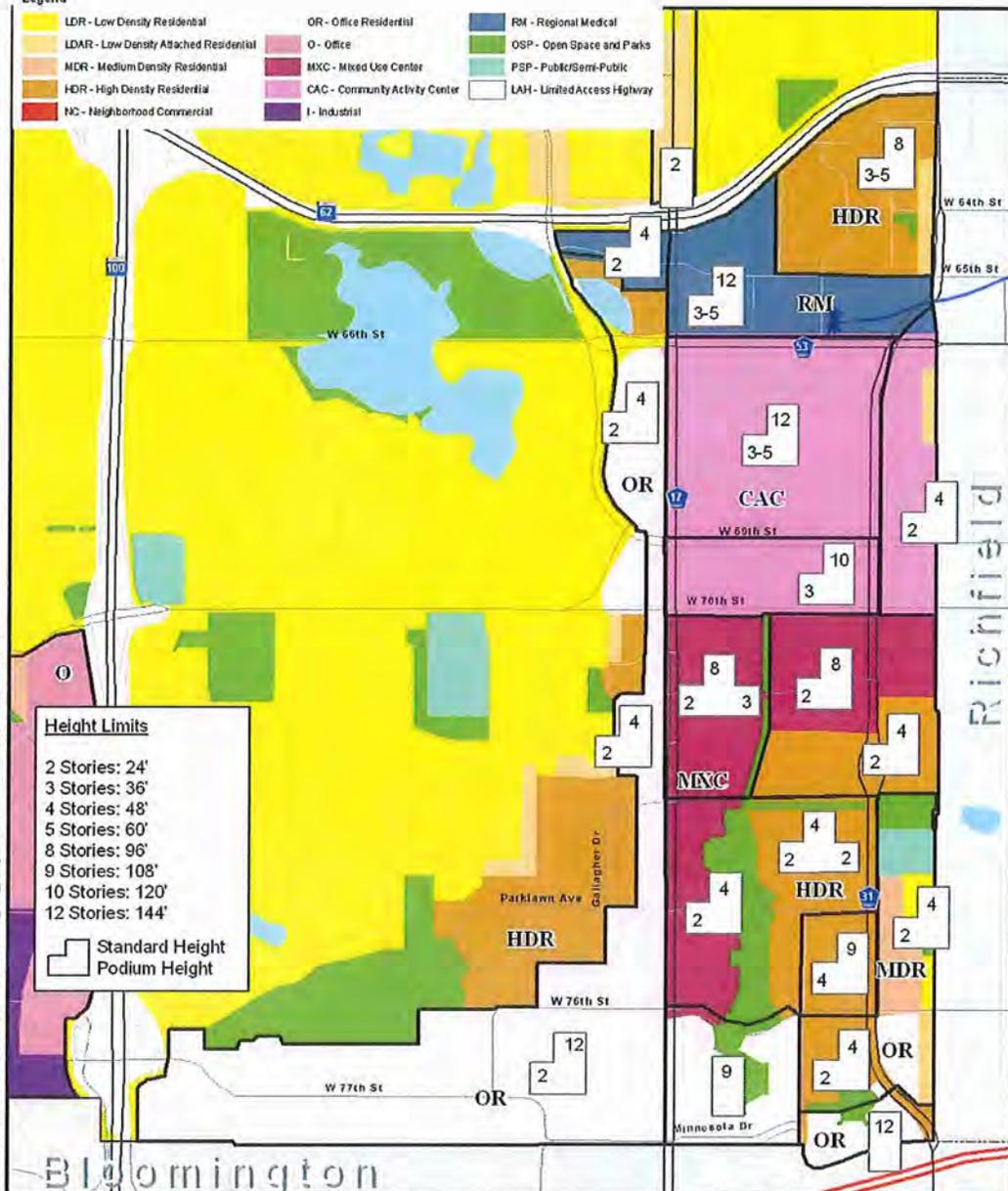
Date of Aerial Photography: August 2006



A4

Legend

- | | | |
|---|---------------------------------|------------------------------|
| LDR - Low Density Residential | OR - Office Residential | RM - Regional Medical |
| LDAR - Low Density Attached Residential | O - Office | OSP - Open Space and Parks |
| MDR - Medium Density Residential | MXC - Mixed Use Center | PSP - Public/Semi-Public |
| HDR - High Density Residential | CAC - Community Activity Center | LAH - Limited Access Highway |
| NC - Neighborhood Commercial | I - Industrial | |



Height Limits

2 Stories:	24'
3 Stories:	36'
4 Stories:	48'
5 Stories:	60'
8 Stories:	96'
9 Stories:	108'
10 Stories:	120'
12 Stories:	144'

Standard Height
 Podium Height

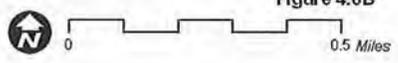
URS Corporation N:\318102300\Projects\LU_SE_heights.mxd Date: 10/25/2007 2:13:59 PM



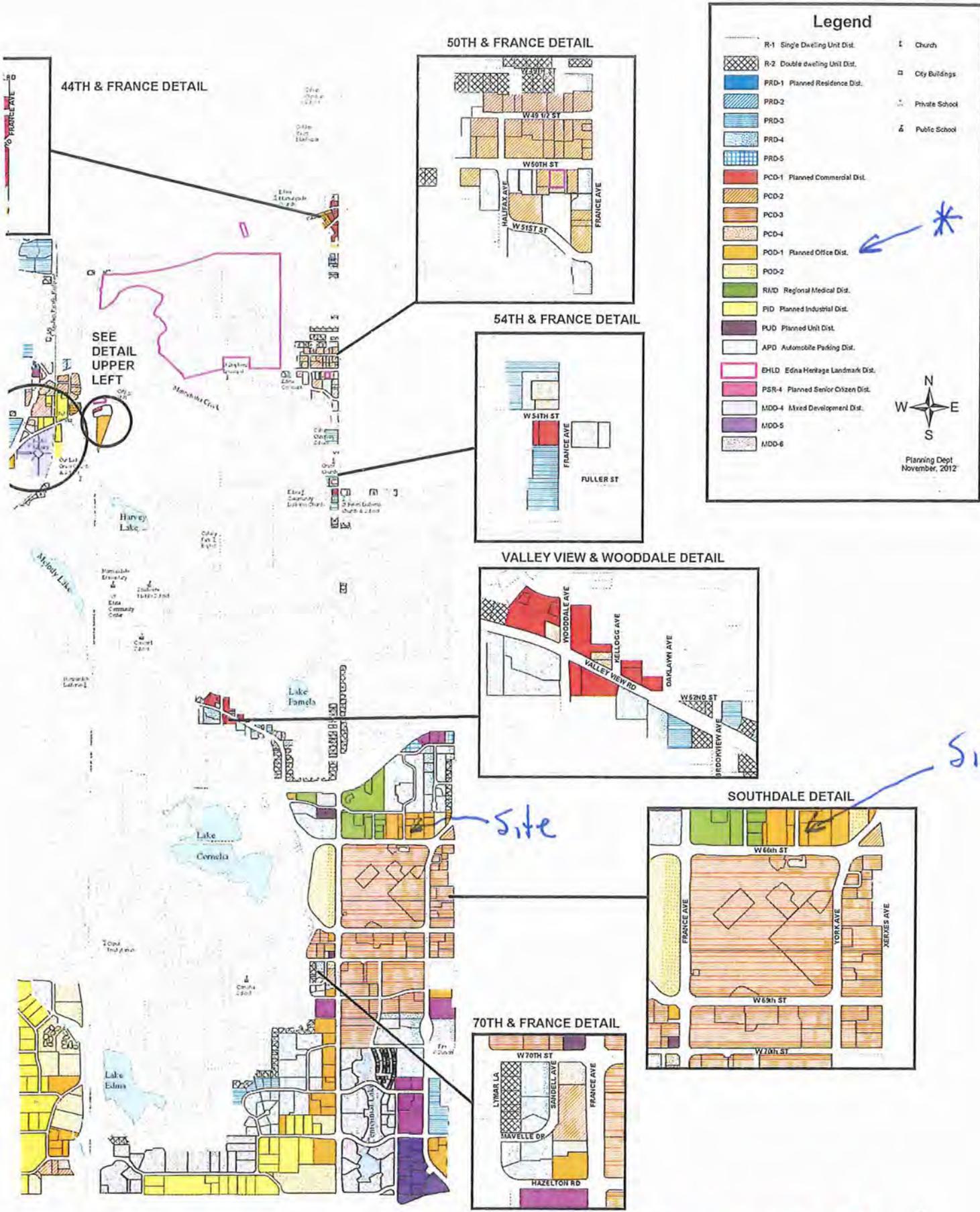
City of Edina
2008 Comprehensive Plan Update

Data Source: URS

Future Land Use Plan with Building Heights
Southeast Quadrant
Figure 4.6B



15



Site

site

ZONING

50TH & FRANCE DETAIL



54TH & FRANCE DETAIL

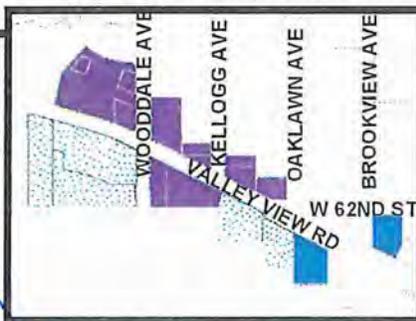


Legend

	HOD-2	Building height shall be determined by required setbacks, but shall not exceed 2 stories or 24 feet, whichever is less.
	HOD-3	Building height shall be determined by required setbacks, but shall not exceed 3 stories or 36 feet, whichever is less.
	HOD-4	Building height shall be determined by required setbacks, but shall not exceed 4 stories or 48 feet, whichever is less.
	HOD-8	Building height shall be determined by required setbacks, but shall not exceed 8 stories or 96 feet, whichever is less.
	HOD-9	Building height shall be determined by required setbacks, but shall not exceed 9 stories or 108 feet, whichever is less.
	HOD-10	Building height shall be determined by required setbacks, but shall not exceed 10 stories or 120 feet, whichever is less.
	HOD-12	Building height shall be determined by required setbacks, but shall not exceed 12 stories or 144 feet, whichever is less.
	Church	
	City Buildings	
	Private School	
	Public School	



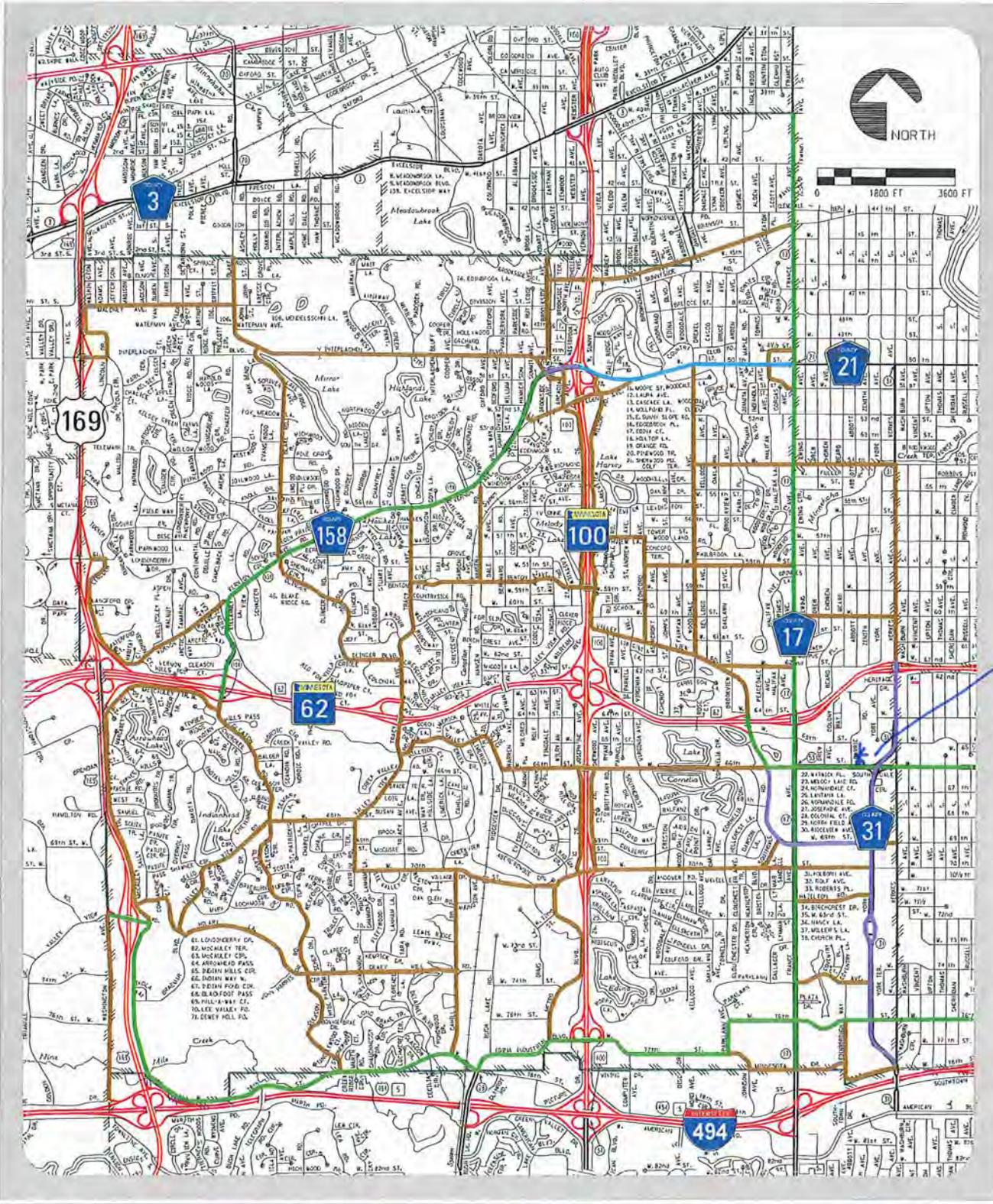
VALLEY VIEW & WOODDALE DETAIL



Site

HEIGHT

A7



LEGEND: — Principal Arterial — "A" Minor Arterial (Reliever) — "A" Minor Arterial (Augmenter) — "B" Minor Arterial — Collector



**City of Edina
2008 Comprehensive Plan Update**

Roadway Functional Classification

Figure 7.5

Date Printed: 10/27/2008
MSB Filename: K:\1668-03\Civil Plan\Fig 7-05.dgn

A8

3330 66th Street, Edina
Explanation of Request and Description of Project

Beacon Interfaith Housing Collaborative (Beacon) is seeking an amendment to the comprehensive plan, preliminary zoning approval to PUD and preliminary development approval at 3330 66th Street.

Beacon builds high quality, affordable housing for families and individuals. We believe housing is the foundation for people to create the stability and security we all seek. We believe that well-designed, professionally managed buildings create a win-win for communities and tenants. We believe everyone deserves to have choices in housing. We believe in home. Our development projects are usually undertaken with partners and collaborators in the faith communities. When a congregation has identified a desire to create affordable housing and the capacity to embark on a development project, our staff, with their specialized expertise, collaborate with congregational leaders to make the vision a reality. This model, over our 15 year history, has resulted in the creation of nearly 500 homes.

In partnership with Edina Community Lutheran Church, Beacon has entered into a purchase agreement with the owners at 3330 66 Street West and intends to convert the existing building into a residential apartment building, "66 West Apartments," that will provide 39 units of permanent housing for young adults who have experienced homelessness. We plan to seek funding from Minnesota Housing, Hennepin County and several private funding sources to make the units affordable to homeless young adults between the ages of 18 and 22. 66 West Apartment's units meet the Met Council definition of affordable rental housing according to the Livable Communities Act. Construction of these units will apply towards the City's Met Council goal of creating 212 additional affordable housing units in Edina between 2011 and 2020. See the separate attachment detailing the project funding structure.

66 West is supportive, affordable housing for young adults who have experienced homelessness. The goal is to support the tenants as they learn to live independently and develop their skills to be financially independent – thereby ending the cycle of homelessness. The building will be staffed by specialists serving homeless young adults. Our supportive service approach is also designed to create a sense of belonging and place for residents and to foster healthy relationships between youth and caring adults with professional training and skills. The outcome is that that community creates an engaging, safe "home" environment with ample opportunities for youth to access on-site or in the broader community.

Site and Development Description

The project site is bounded by West 66th Street to the South, Barrie Road to the west, and office buildings to the east and north. The site is currently zoned POD and contains a two-story building with a basement. The first floor is being used as a bank with a drive through facility. The remaining floors are not being used.

The building will contain housing units, as well as office and resident amenities. The building will feature 39 studio units, ranging in size from a net (paint to paint) 322 square feet to 451 square feet. Each apartment will contain a full kitchen and bathroom. The building will contain offices for on-site service providers and property management. There will be a community area for residents, as well as a fitness area, computer lab and laundry room.

The site is .9 acres. The project incorporates the entire existing 18,145 square foot building and also includes a 10,458 square foot addition. The remodeled building, with the addition, will have an 11,283 square foot building footprint and a total floor area of 28,603 square feet. The building will remain two stories, plus the existing basement. The design retains the brick on the existing building. Expected

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exterior materials on the addition will be brick and metal panels. The updated exterior will complement the surrounding buildings.

The site features two outdoor courtyards and a basketball court as resident amenities. The project retains most of the mature trees on the site. Additional garden areas and ornamental trees, including a rain garden, further enhance the existing landscaping. Plantings and a tree for shade will improve the existing bus stop area, currently just a bench. Additional pedestrian walkways to the public sidewalk are planned.

Sustainability. Currently, the building use is limited to one of the three floors due to parking requirements. This project maximizes the potential of the .9 acres by repurposing the entire existing building and adding an addition. The development reduces the impervious paving of the site by 6.9%. Beacon delivers buildings that are environmentally sustainable in design and operation. 66 West will incorporate many sustainable building elements, outlined in the attachment.

Parking, traffic and transit. The primary entrance is accessed off Barrie Road. The parking lot, as designed, has 19 parking stalls. Landscaping along Barrie Road could be removed and the parking lot expanded to accommodate an additional 12 spots, should a future user require additional parking. In addition, there is a paved, unstriped area, currently designed as a basketball court, in which 6 additional spots could be added with minimal site disruption. Thus, the site could contain a total of 37 parking stalls for a future use.

Beacon has provided funding so that the City of Edina could conduct a parking study to verify the parking needs and determine the impact on traffic. Beacon believes 19 parking spots would be sufficient to serve the residents, guests and staff. The independent parking study found that in other Beacon buildings that serve a similar population, the rate of parked cars to apartments range between 18% and 30%. In the most similar building serving young adults, the rate of parked cars, at its peak, was 18%. Thus, conservatively, Beacon would expect no more than 12 parking stalls will be required for residents. In addition, the building has offices for a maximum of 6 staff. There will be no facility vehicles requiring a parking stall. In addition, the project site is located across the street from the Southdale transit hub served by nine bus routes. Also, two high frequency bus routes stop immediately adjacent to the property – route 6 to Downtown Minneapolis and the University of Minnesota and route 515 to Richfield and Bloomington. Thus, it is expected all residents can utilize public transportation, reducing the dependency on cars. We would also expect that future uses would also take advantage of the proximity to public transportation. Finally, the project includes 26 bicycle parking spots and indoor bicycle storage.

The Requested Comprehensive Plan Amendment and PUD Rezoning are Consistency with Edina's Land Use and Housing Goals and Policies

The site is located in an area designated in the City of Edina's Comprehensive Plan (Comprehensive Plan) as Regional Medical. The Regional Medical category was recently amended to allow senior housing as an allowed land use. We are requesting that the Comprehensive Plan be similarly amended to add "affordable housing" as an allowed land use in the Regional Medical land use category. We are also applying to rezone the site to Planned Unit Development (PUD), which permits the establishment of uses allowed under the Comprehensive Plan. The proposed amendment and rezoning are consistent with the policies of the Comprehensive Plan, the Greater Southdale Area Final Land Use and Transportation Study Report (Report), and objectives of the City's Zoning Code.

Policies and Objectives for the Greater Southdale Area. The Report states in the vision that the "Greater Southdale Area is a place for people to live, work, play and shop." Thus, the area "is a vibrant mix of housing, jobs, commerce, health care, and entertainment." "Development in the Greater Southdale Area can meet the housing needs of Edina with options that may not fit in other locations within the city .

...” (Page 3). The Land Use Plan, as outlined in the Report, “advocates a strong component of mixed-use in the Southdale Area This approach has been taken in order to flexibly address future market opportunities and to balance traffic generation by mixing land use types and their resulting peak hour traffic characteristics.” (page 32). The parcel, 3330 66th Street West, is slated as “mixed use” in the final Land Use Plan Concept in the report. (page 33). Due to the importance of the Greater Southdale Area to the City’s vitality and welfare, the City Council adopted additional development objectives regarding the Area into the Zoning Code, including the following that specifically support the requested comprehensive plan amendment and PUD rezoning:

- (2) To encourage a range of housing types within the Greater Southdale Area.
- (3) To encourage a mixed use shopping, living and working environment that meets the needs of residents and visitors and helps mitigate the effects of traffic by reducing vehicle trips and miles by allowing residences in close proximity to employment and services.
- (8) To allow a density of development that improves the feasibility of mass transit services utilized by residents, shoppers and employees of the Greater Southdale Area.
- (9) To provide incentives to encourage affordable and life cycle housing.

Edina Zoning Code, Section 36-5.

Consistency with Comprehensive Plan Policies. The Comprehensive Plan, which postdates the Report, identifies the project site as Regional Medical. However, it also identifies the Southdale Area as a “potential area of change.” Thus, the plan acknowledges that Southdale appears to be an area “where change may occur during the life of this Plan.” (Page 4-30). The Potential Areas of Change were identified “as suitable areas to accommodate additional households and jobs... Because the City is fully developed, additional housing would have to occur through redevelopment.” (Page 4-31). In addition, the proposed Comprehensive Plan amendment and rezoning are supported by the following specific Land Use Policies within the Plan:

- 1. The City will endeavor to accommodate private redevelopment in the Greater Southdale Area
- 5. In reviewing development proposals, the City will examine how land use and transportation are integrated to ensure that new development and redevelopment expands non-motorized travel options.
- 7. The City will seek opportunities to increase the supply of affordable housing.
- 8. The City will grow and develop in a sustainable manner

Comprehensive Plan (Page 4-34).

Similarly, an amendment to allow affordable housing in the Regional Medical area is supported by the following goal from the Housing Chapter of the Comprehensive Plan:

- 3. Encourage the production of additional affordable housing units to meet the City’s housing needs and, at a minimum, its Metropolitan Council affordable housing need allocation (212 units).

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5. Promote a vision of community that is inclusive of a wide range of ages, incomes, and abilities and offers a wide range of housing options for Edina's residents.

7. Promote lifecycle housing to support a range of housing options that meet people's preferences and circumstances at all stages of life.

8. Acknowledge the interrelationship between land use and transportation, and support the expansion of existing transportation infrastructure-capacity through wise land use.

Comprehensive Plan (Pages 5-17 to 5-18).

The proposed plan amendment and PUD rezoning are highly consistent with the City's housing goals, as one of the purposes of the PUD is to encourage a variety of housing types, including affordable housing.

As described above, allowing affordable housing in the Regional Medical category is supported by multiple policies related to land use and housing in the City's plans and zoning code. Further, the proposed project will be an enhancement to the site and surrounding area and will not hinder other goals for development in the Regional Medical area. The site is located on the edge of the district.

Development opportunities exist at underutilized sites to the North and the East. The project repurposes an existing building, maximizes the potential of the .9 acre site as additional parking would be required for additional housing units or commercial space, and converts a auto-oriented drive-through commercial use into transit-oriented housing use. In addition, adding affordable housing as an allowed use is not dissimilar to the senior housing currently allowed in the Regional Medical category. This supportive housing project contains similar unit sizes, staff space, and common space to a typical senior housing or assisted living facility. The proposed 43 units per acre density is in the middle of the 12-80 unit density suggested for the Regional Medical area. The addition of residents will support the diversity of uses, employment base, and transit service in the Greater Southdale Area and the residents of this project will be well-served by those amenities and opportunities.

66 West *Sustainable Elements*

In order to create homes that are durable, healthy and efficient, Minnesota Housing require all funded projects to comply with the Minnesota Overlay and Guide to the 2011 Enterprise Green Communities Criteria. This guide outlines mandatory and optional sustainable building criteria to include in the project. Beacon Interfaith Housing Collaborative (Beacon) has committed to implement certain criteria and incorporated the elements or strategies into the building design. The document providing the written commitment is attached. In addition, at the financial closing, Beacon will reaffirm the commitment and certify the building criteria that were included in the construction documents. Finally, upon construction completion, Beacon, the architect, and the general contractor will certify that the elements and strategies were incorporated into the final building. Energy modeling and performance test results are also required to verify energy standard criteria.

Below are significant strategies or elements that will be included in 66 West through the Green Communities program. See the attached certification for a complete list.

- 66 West is a compact development, with 43.33 units per acre.
- 66 West is within walking distance of many services and facilities.
- The project site is adjacent to public transit stop that totaling nearly 160 stops per day.
- 66 West is an adaptive reuse of an existing building.
- At least 50% of the planting will be native species.
- The building will have a HERS (home energy rating system) index of at least 85, which is at least 15% more energy efficient than a HERS reference home and consistent with Energy Star compliant homes.
- 66 West will install only Energy Star rated appliances and light fixtures.
- All of the project's interior paints, primers, and adhesives will meet low VOC standards.
- 66 West will install Energy Star, continuous running bathroom fans exhausted directly to the outdoors.

In addition, 66 West will feature the following Green Building strategies.

- 66 West will reduce the impervious paving of the existing site by 6.9%.
- The building will feature hard surface flooring in the units.
- The project site will include a rain garden.

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AB

Project Name 66 West Apartments
 Project Address 3330 66th Street W
 Project Status Application

Organization Name Beacon Interfaith Housing Collaborative
 Organization Contact Sarah Larson
 Date 5/13/2014

INTENDED METHODS WORKSHEET: This worksheet identifies how the project team intends to incorporate all the Mandatory and adequate number of Optional Criteria into the development.

This worksheet must be filled out and submitted before the construction start date. For additional information on how to submit go to www.greencommunitiesonline.org/tools/certification/

INSTRUCTIONS:

- 1) Select an answer provided in the drop-down menu under Column D ("How Criterion will be implemented") for each criterion.
- 2) Explain special circumstances or request a waiver using Column E ("If necessary, describe deviations from Intended approach"). This may include information on an approach proposed by the project team that does not appear as an option in the drop-down menu.
- 3) Indicate where the Criterion references can be found within the project documents in Column F and G ("Criteria Documentation"). This is required for Criterion 1.1b
- 4) Indicate the project team member who is responsible for documenting and ensuring the completion of the Criterion under Column 'H' (Champion).
- 5) Indicate the number of optional points being pursued by completing Column H ("Intended Points").

*Gray text within the spreadsheet (under "Green Development Agreement" section at bottom) indicates the type of information that could occupy that cell. It is not intended to be left in your final submission documents.

Complete this document by signing the Green Development Agreement at the bottom of this worksheet. Without the mandatory signatures, this document remains incomplete and the project will not receive Step 1 Certification.

1 INTEGRATIVE DESIGN

Criteria Item	How Criterion will be implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points
			Location of Measure in Project Documents	Spec page number / plan type for locating measure		
1.1a Green Development Plan: Integrative Design Meeting(s)	The project team has conducted one or more integrative design meeting(s) and submitted a Green Development Plan or equivalent documentation			N/A	Architect	M
1.1b Green Development Plan: Criteria Documentation	The project team will create design and construction documentation (i.e. plans, details, and specifications) to include information on implementation of appropriate Enterprise Green Communities Criteria		Project Plans and Specifications	TBD	Architect	M
1.2a Universal Design (New Construction only)						0
1.2b Universal Design (Substantial & Moderate Rehab only)						0
						0
						Intended Points

2 EDUCATION + NEIGHBORHOOD FABRIC

Criteria Item	How Criterion will be implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points
			Location of Measure in Project Documents	Spec page number / plan type for locating measure		
2.1 Site Sensitive Selection (New Construction Only)	Site is previously developed and will not be on prime soils, on public parkland, on critical habitat, on the 100 year floodplain, or on a slope greater than 15%			N/A	Project Manager	M
2.2 Connections to Existing Development and Infrastructure (New Construction only, except for projects located on rural tribal lands, in colonias communities, or in communities of population less than 10,000)	The project is located on a site with access to existing roads, water, sewers, and other infrastructure within or contiguous (having at least 25% of the perimeter bordering) to existing development, connected to the pedestrian grid, and meeting the septic tank requirements			N/A	Project Manager	M
2.3 Compact Development (New Construction Only)	43.33 units/acre			N/A	Project Manager	M
2.4 Compact Development	43.33 units/acre					6
2.5 Proximity to Services (New Construction only)	Suburban/Mid-Site Town location: Project is 0.5-mile walk distance of at least three, or a 1-mile walk distance of at least six facilities			N/A	Project Manager	M
2.6 Preservation of and Access to Open Space	The project is classified as an Urban/Small City or a Suburban/Mid-Site Town			N/A	Project Manager	M
2.7 Preservation of and Access to Open Space						0
2.8 Access to Public Transportation	Route 6 has over 90 transit rides per weekday and Route 515 has nearly 70 transit rides per weekday. Both have nearly as many rides on the weekend. A stop that serves both routes is directly in front of the building.					5
2.9 Walkable Neighborhoods: Connections to Surrounding Neighborhood	Provide summary of the project's sidewalk and pathway connections to public spaces, open spaces or adjacent development					0
2.10 Smart Site Location: Passive Solar Heating / Cooling	Provide a brief narrative that describe passive solar heating/cooling tactics					0
2.11 Brownfield or Adaptive Reuse Site	The project is located on an adaptive reuse site					2

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2.12	Access to Fresh, Local Foods					0
2.13	LEED for Neighborhood Development certification					0
						13 Intended Points

3. SITE IMPROVEMENTS

Criteria Item	How Criterion will be Implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points
			Location of Measure in Project Documents	Spec page number / plan type for locating measure		
3.1	Environmental Remediation	Site has passed Phase I Environmental Site Assessment	Project Plans and Specifications	TBD	Project Manager	M
3.2	Erosion and Sedimentation Control (Except for Infill sites with buildable area smaller than one acre)	The project is an infill site with a buildable area smaller than one acre		N/A	Project Manager	M
3.3	Low Impact Development (New Construction only)	The project is substantial or moderate rehab		N/A	Project Manager	M
3.4	Landscaping	The Architect or Landscape Architect will provide certified tree or plant list showing at least 50% of the site area available for landscaping is planted with native or adaptive species	Project Plans and Specifications	TBD	Architect	M
3.5	Efficient Irrigation and Water Reuse	The Irrigation system will incorporate a drip system for at least 50% of the planting beds, separate zones for turf and beds, a timer that activates the valves for each zone timed to prevent water loss, and a rain delay sensor.	Project Plans and Specifications	TBD	Architect	M
3.6	Surface Stormwater Management	Provide a brief narrative of the design strategies and systems that will be implemented, and indicate the calculated volume of water being retained, infiltrated, or harvested on site				0
						0 Intended Points

4. WATER CONSERVATION

Criteria Item	How Criterion will be Implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points
			Location of Measure in Project Documents	Spec page number / plan type for locating measure		
4.1	Water-Conserving Fixtures	Project will specify toilets at 1.28 gpf or less, urinals at .5 gpf or less, bathroom faucets at 1.5 gpm or less, and showerheads and kitchen faucets at 2.0 gpm or less	Project Plans and Specifications	TBD	Architect	M
4.2	Advanced Water-Conserving Appliances and Fixtures					0
4.3	Water Reuse					0
						0 Intended Points

5. ENERGY EFFICIENCY

Criteria Item	How Criterion will be Implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points
			Location of Measure in Project Documents	Spec page number / plan type for locating measure		
5.1a	Building Performance Standard: Single family & Multifamily, 3 stories or fewer (New Construction only)	The project is not single-family or multi-family (three stories or fewer) new construction		N/A		M
5.1b	Building Performance Standard: Multifamily, 4 stories or more (New Construction Only)	The project is not multi-family (four stories or more) new construction		N/A		M
5.1c	Building Performance Standard: Single family & Multifamily, 3 stories or fewer (Substantial and Moderate Rehab)	The project will demonstrate energy performance of a HERS Index of 85 using an energy model and a Home Energy Rating certificate	Project Plans and Specifications	TBD	Mechanical Engineer	M
5.1d	Building Performance Standard: Multifamily, 4 stories or more (Substantial and Moderate Rehab)	The project is not a multifamily (four stories or more) rehabilitation		N/A		M
5.2	Additional Reductions in Energy Use					0
5.3	Sizing of Heating and Cooling Equipment and Ducts	Heating and cooling equipment will be sized in accordance with the ACCA manual, Parts J and S, or ASHRAE handbooks	Project Plans and Specifications	TBD	Mechanical Engineer	M
5.4	ENERGY STAR Appliances	The project will install Energy Star-rated clothes washers, dishwashers, and refrigerators	Project Plans and Specifications	TBD	Architect	M

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5.5a	Efficient Lighting: Interior Units	Project will install Energy Star Advanced Lighting Package		Project Plans and Specifications	TBD	Electrical Engineer	M
5.5b	Efficient Lighting: Common Areas and Emergency Lighting (all multifamily projects)	Project will use ENERGY STAR-labeled fixtures, LEDs, T8 fixtures with electronic ballasts or better, or any equivalent high-performance lighting fixtures and bulbs in all common areas		Project Plans and Specifications	TBD	Electrical Engineer	M
5.5c	Efficient Lighting: Exterior	Project will install ENERGY STAR qualified fixtures or LEDs (with a minimum efficacy of 45 lumens/watt) equipped with daylight sensors on all outdoor lighting		Project Plans and Specifications	TBD	Electrical Engineer	M
5.6a	Electricity Meter (New Construction and Substantial Rehab only)	Individual or sub-meters will be installed in all dwelling units		Project Plans and Specifications	TBD	Electrical Engineer	M
5.6b	Electricity Meter (Moderate Rehab only)	The project is new construction or substantial rehab					0
5.7a	Renewable Energy	Provide brief narrative describing the types of renewable energy system installed and the estimated percentage of energy it will provide for the overall energy demand of the project					0
5.7b	Photovoltaic / Solar Hot Water Ready						0
5.8	Advanced Metering Infrastructure						0
							0
							Intended Points

6. MATERIALS BENEFICIAL TO THE ENVIRONMENT

Criteria Item	How Criterion will be implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points	
			Location of Measure in Project Documents	Spec page number / plan type for locating measure			
6.1	Low / No VOC Paints and Primers	All interior paints and primers will meet the MPI and Green Seal standards for VOCs, based on the list provided in the Criteria	Project Plans and Specifications	TBD	Architect	M	
6.2	Low / No VOC Adhesives and Sealants	All adhesives will comply with Rule 1168 of the South Coast Air Quality Management District. All caulks and sealants will comply with Regulation 8, Rule 51 of the Bay Area Air Quality Management District (BAAQMD)	Project Plans and Specifications	TBD	Architect	M	
6.3	Construction Waste Management	The project scope will include a provision to follow a construction waste management plan that reduces waste by at least 25% by bringing debris to a mixed debris recycling facility.	Project Plans and Specifications	TBD	General Contractor	M	
6.4	Construction Waste Management: Optional	Provide a brief narrative that lists the materials in the Construction Waste Management Plan, the % recycled, salvaged, or diverted and the strategies to do so				0	
6.5	Recycling Storage for Multifamily Project	The project will provide a dedicated, permanent, and accessible area for the collection and storage of materials for recycling				5	
6.6	Recycled Content Material	Provide a brief narrative that summarizes the building materials made of recycled content material				0	
6.7	Regional Material Selection					0	
6.8	Certified, Salvaged and Engineered Wood Products					0	
6.9a	Reducing Heat-Island Effect: Roofing					0	
6.9b	Reducing Heat-Island Effect: Paving					0	
							5
							Intended Points

7. HEALTHY LIVING ENVIRONMENT

Criteria Item	How Criterion will be implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points
			Location of Measure in Project Documents	Spec page number / plan type for locating measure		
7.1	Composite Wood Products that Emit Low / No Formaldehyde	All composite wood products will be compliant with California 93120	Project Plans and Specifications	TBD	Architect	M
7.2	Environmentally Preferable Flooring	Any carpet, pad and adhesive will not be installed in entryways, laundry rooms, bathrooms, kitchens/kitchenettes, utility rooms, or any rooms of ground-connected floors. Any carpet products will meet Green Label or Green Label Plus certification. Any hard surface flooring products will be either ceramic tile, unfinished hardwood floors, or in compliance with the FloorScore program criteria	Project Plans and Specifications	TBD	Architect	M
7.3	Environmentally Preferable Flooring: Alternative Sources					0
7.4a	Exhaust Fans: Bathroom (New Construction and Substantial Rehab only)	The project will install ENERGY STAR-labeled continuous bathroom fans that exhaust to outdoors operate continuously at 20 cfm	Project Plans and Specifications	TBD	Mechanical Engineer	M
7.4b	Exhaust Fans: Bathroom (Moderate Rehab only)					0
7.5a	Exhaust Fans: Kitchen (New Construction and Substantial Rehab only)	The project will install power-vented fans or range hoods that exhaust to the outdoors at an intermittent rate of 100 cfm, per ASHRAE 62.2-2010	Project Plans and Specifications	TBD	Mechanical Engineer	M
7.5b	Exhaust Fans: Kitchen (Moderate Rehab only)					0

7.6a	Ventilation (New Construction and Substantial Rehab only)	The project will install a ventilation system that will satisfy the fresh air requirements of ASHRAE 62.2-2010		Project Plans and Specifications	TBD	Mechanical Engineer	M
7.6b	Ventilation (Moderate Rehab only)						0
7.7	Clothes Dryer Exhaust	All clothes dryers will exhaust directly to the outdoors using rigid-type duct work		Project Plans and Specifications	TBD	Mechanical Engineer	M
7.8	Combustion Equipment	All combustion equipment will be power vented or direct vented and hard-wired CO detectors will be installed in outside each sleeping area per criteria		Project Plans and Specifications	TBD	Mechanical Engineer	M
7.9a	Mold Prevention: Water Heaters	All water heaters will be installed with catch pans and drains piped to the exterior of the dwelling		Project Plans and Specifications	TBD	Mechanical Engineer	M
7.9b	Mold Prevention: Surfaces	All surfaces in bathrooms, kitchens, and laundry rooms will use materials that have durable and cleanable surfaces		Project Plans and Specifications	TBD	Architect	M
7.9c	Mold Prevention: Tub and Shower Enclosures	All bathrooms will have non-paper-faced backing materials such as cement board, fiber cement board, or equivalent		Project Plans and Specifications	TBD	Architect	M
7.10	Vapor Barrier Strategies (New Construction and Rehab projects with foundation work only)	The project will install a vapor barrier and capillary break under the slab	Provided at new slab on grade	Project Plans and Specifications	TBD	Architect	M
7.11	Radon Mitigation (New Construction and Substantial Rehab only)	The project is substantial or moderate rehab and will test for radon and install passive-radon features if necessary	Radon mitigation system will be installed in new slab-on-grade areas and added underneath basement units.	Project Plans and Specifications	TBD	Architect	M
7.12	Water Drainage (New Construction and Rehab projects replacing assemblies called out in Criterion only)	The project is moderate or substantial rehab that is not replacing assemblies called in this criterion	Addition will be compliant		N/A		M
7.13	Garage Isolation	The project does not have an attached garage			N/A		M
7.14	Integrated Pest Management	The project scope will include requirements to seal all wall, floor, and joint penetrations. Rodent proof screens will be provided at all openings.		Project Plans and Specifications	TBD	Architect	M
7.15	Lead-Safe Work Practices (Substantial and Moderate Rehab only)	The project was built after 1978			N/A		M
7.16	Smoke-Free Building	The project will enforce a no-smoking policy					9
							9
							Intended Points

8. OPERATION & MAINTENANCE

Criteria Item	How Criterion will be implemented	If necessary, provide additional information or explanation of alternative approach to meeting this measure	Criteria Documentation		Champion	Intended Points
			Location of Measure in Project Documents	Spec page number / plan type for locating measure		
8.1 Building Maintenance Manual (all multifamily projects)	The project scope will include a provision requiring the contractor to provide a manual to the owner that will address the operation and maintenance of the appliances, HVAC systems, water-systems, lighting, paving and landscaping. The owner will provide a copy of the manual to the management company.		Project Plans and Specifications	TBD	Project Manager	M
8.2 Resident's Manual	The owner will provide a manual to the property management company to provide to all residents at move-in. The manual will address green cleaning products and schedules, pest control, location of water shut offs, proper use of systems, and operations of all appliances and special plumbing fixtures.			N/A	Project Manager	M
8.3 Resident and Property Manager Orientation	The project scope will include a provision requiring the contractor to provide an orientation to the property manager on the building features' operation and maintenance, including the green features. The orientation will be video taped for future reference. The property manager will provide information on nearby transit facilities to promote a healthy lifestyle.		Project Plans and Specifications	TBD	Project Manager	M
8.4 Project Data Collection and Monitoring System						0
						0
						Intended Points

ENTERPRISE GREEN COMMUNITIES CRITERIA
TOTAL INTENDED POINTS

27

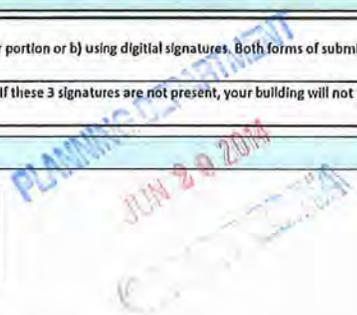
Green Development Agreement: The following signatures provides a written commitment demonstrating that all parties involved in the execution and delivery of this project agree to be accountable to measures and strategies as outlined in application documents.

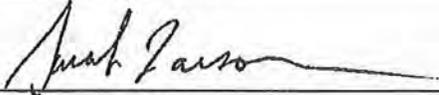
INSTRUCTIONS:
This signature portion is most easily satisfied by either: a) printing the Intended Methods tab and having the various team members fill out and sign their portion or b) using digital signatures. Both forms of submission will be accepted.
To proceed with Enterprise Green Communities Certification, signatures must be provided by at the project manager, architect, and general contractor. If these 3 signatures are not present, your building will not proceed through Step 1 of Certification.

MANDATORY SIGNATURES

6/5/2014

A16



	6/5/2014
---	----------

Project Manager: Sarah Larson

	6/5/2014
---	----------

Architect: David Hoiland, AIA

	<Insert Date>
--	---------------

General Contractor: General Contract T00

OPTIONAL SIGNATURES

<Provide Signature of Green Development Team Member>	<Insert Date>
--	---------------

Green Building Specialist: <PRUIT Green Development Team Member's Name>

<Provide Signature(s) of Green Development Team Member(s)>	<Insert Date>
--	---------------

Engineer (MEP): <PRUIT Green Development Team Member's Name(s)>

<Provide Signature(s) of Green Development Team Member(s)>	<Insert Date>
--	---------------

Energy and/or System Specialist: <PRUIT Green Development Team Member's Name(s)>

PLANNING DEPARTMENT
 JUN 20 2014
 CITY OF EDINA

66 West
Cost and Financing Information

66 West is an adaptive reuse from an existing bank to 39 residential studio apartments. At this point, the budgeted construction cost is \$5,300,000 and the total development cost is \$10,080,000. On a per unit basis, the construction cost is approximately \$136,000 and the development cost is approximately \$258,000.

Minnesota Housing Finance Agency urges development projects to contain costs. As a method to gauge appropriate costs, and as a way to give preference points to projects that contain costs, Minnesota Housing publishes historical averages of other affordable housing developments in Minnesota. As project cost varies based on location and housing type, costs are broken down by category. The average total development cost of new construction for adaptive reuse in the metro area for zero or one bedroom units is \$267,930 per unit. 66 West is nearly \$10,000 per unit more cost effective.

The bulk of the capital cost is expected to be covered through tax credit financing. Minnesota Housing awards tax credits on a competitive basis to developers of non-profit housing, who in turn, sell the credits to a private investor. Private funding and deferred loans from Minnesota Housing and local government units fill the remaining funding gap. The tax credit allocation and deferred funding restricts the rent and income levels.

PLANNING DEPARTMENT
AUG 20 2014
CITY OF EDINA



801 NORTH THIRD STREET, SUITE 100
MINNEAPOLIS, MN 55401
612.465.5100

CONSULTANT

PRELIMINARY
NOT FOR
CONSTRUCTION

REVISIONS
Date Description

ADD

DATE 06.19.21
PROJECT # 24-00
PHASE SITE PLAN REV 01

DRAWN BY
CHECKED BY

COLORED
SITE PLAN

LA20C



1 SITE PLAN
Scale: 1/8" = 1'-0"

728



PLANNING DEPARTMENT



66 West Apartments
 EDINA MN | 06.10.2014 | 14-0016

JUN 20 2014

CITY OF EDINA



66 West Apartments



Site Plan Review Submittal June 19, 2014

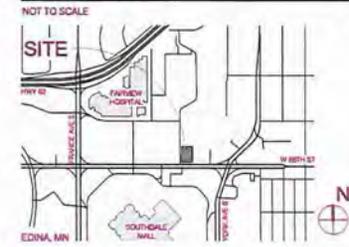
UNIT COUNT

UNIT TYPE	BASE LEVEL	LEVEL 1	LEVEL 2	UNIT TYPE TOTAL
TYPES A/B/C/D	0	0	1	1
TOTAL	0	16	19	35
Overall Total	0	16	20	36

SITE & BUILDING STATISTICS

A. SITE SQUARE FOOTAGE	-	36,358 SF	(0.8300 ACRES)
B. GROSS BUILDING SQUARE FOOTAGE:	LEVEL TWO	11,284 SF	
	LEVEL ONE	17,284 SF	
	SUBLEVEL 1	3,257 SF	
	TOTAL GROSS G.F.	31,825 SF	
	AVERAGE GROSS G.F.	22,669 SF	
C. PERCENT OF SITE COVERED & FLOOR AREA RATIO	% SITE COVERAGE F.A.R.	86.7%	53.7
D. DWELLING UNITS PER ACRE	36 UNITS / 0.83 ACRES	43 UNITS PER ACRE	
E. PERCENT PERVIOUS SITE AREA	16,288 SF	42.0% PERVIOUS	
F. ELEVATION ABOVE MEAN SEA LEVEL:	LEVEL 1	892' - 0"	
G. HEIGHT OF ROOF ABOVE GRADE		27' - 0"	
H. TOTAL NET LEASABLE AREA		13,072 SF	

VICINITY MAP



GENERAL NOTES

1. DO NOT SCALE DIMENSIONS.
2. SUBCONTRACTORS SHALL VISIT THE JOB SITE AND SHALL REVIEW THE CONTRACT DOCUMENTS TO FAMILIARIZE THEMSELVES WITH THE REQUIREMENTS AND INTENT OF THE SCOPE OF THE WORK. HIGH COORDINATION OF WORK. CLARIFICATIONS SHALL BE REPORTED TO GENERAL CONTRACTOR FOR CLARIFICATION.
3. BUILDING TO BE SPARKLE.

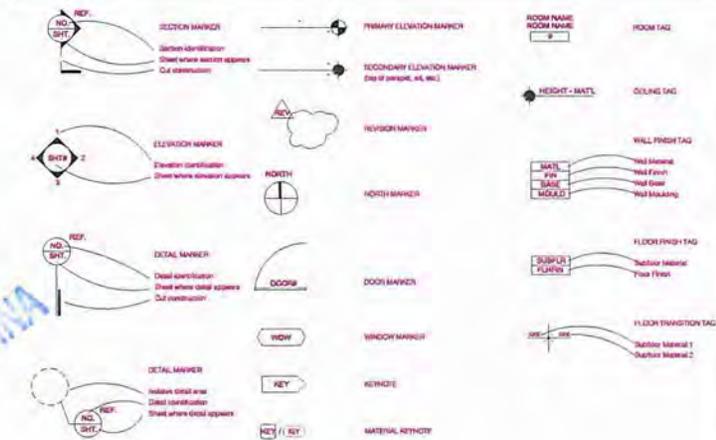
PARKING STALLS

STALL TYPE	CURRENT DESIGN PARKING	FUTURE PROOF OF PARKING
HC Single Parking Stall	1	1
Standard Parking Stall	15	30
Grand Total	16	31

BICYCLE PARKING

	BICYCLE PARKING
Total	20

SYMBOLS



CONTACTS

DEVELOPER/CLIENT

NAME: BEACON INTERIOR HOLDING
COLLADRATIVE
CONTACT: LEO BLOND
DANIEL LARSON
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SUITE 200 PAUL, MN 55114
PHONE NO: 651-284-8350
FAX NO: 651-284-8358
E-MAIL: BLANDON@BEACONINTERIORHOLDING.COM

CONTRACTOR - TRD

NAME: COMPANY / FIRM NAME HERE
CONTACT: CONTACT NAME HERE
ADDRESS: ADDRESS LINE 1
ADDRESS LINE 2
PHONE NO: PHONE NO. HERE
FAX NO: FAX NO. HERE
E-MAIL: E-MAIL HERE

ARCHITECT

NAME: URBANWORKS ARCHITECTURE LLC
CONTACT: TOD BLAIRS
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SUITE 145
PHONE NO: 612-455-3150
FAX NO: 612-455-3158
E-MAIL: CHWALANKO@URBANWORKS.COM

CIVIL ENGINEER

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CONTACT: KEVIN DANEBER
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BLANE, MN 55424
PHONE NO: 763-457-1111
FAX NO: 763-457-1234
E-MAIL: KEVIN@FRUCEPFA.COM

LANDSCAPE ARCHITECT

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CONTACT: BILL BOLANDY
ADDRESS: 1376 KELLY DRIVE
GOLDEN VALLEY, MN 55427
PHONE NO: 612-455-3181
FAX NO: 612-455-3158
E-MAIL: STUDIOGREEN@COMCAST.NET

STRUCTURAL ENGINEER

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CONTACT: JOHN TAMM
ADDRESS: 3482 DUNDAS VALLEY ROAD
MINNEAPOLIS, MN 55425
PHONE NO: 763-849-0022
FAX NO: 763-849-0021
E-MAIL: JTM@BKDM.COM

MEP ENGINEERS

NAME: STEEN ENGINEERING, INC.
CONTACT: MARK BERENSON
ADDRESS: 3400 DUNDAS VALLEY NORTH
CRYSTAL, MN 55409
PHONE NO: 763-580-4742
FAX NO: 763-580-4742
E-MAIL: MURTI@STEENENG.COM

INTERIOR DESIGNER

NAME: IQ GROUP
CONTACT: GURRY EWALD
ADDRESS: 811 N. 3RD. ST.
SUITE 145
PHONE NO: 612-455-3153
FAX NO: 612-455-3158
E-MAIL: GURRY@IQGROUP.MN.COM

PLANNING DEPARTMENT
JUN 20 2014
CITY OF EDINA

66 West Apartments
3330 66th St. West, Edina



801 NORTH THIRD STREET, SUITE 1
MINNEAPOLIS, MN 55401
612-455-3150
CONSULTANT

PRELIMINARY
NOT FOR CONSTRUCTION

REVISIONS
Date Description

A2

DATE: 06.19.14
PROJECT # 24-0
PHASE: SITE PLAN REV

DRAWN BY:
CHECKED BY:

KEY PLAN

TITLE SHEET

G001

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GENERAL NOTES

1. ALL COSTING INFORMATION TAKEN FROM SURVEY BY MEMBER AND ASSOCIATES, DATED APRIL 11, 2014.
2. A SUBSURFACE GEOTECHNICAL INVESTIGATION HAS NOT BEEN COMPLETED FOR THIS PROJECT TO DATE, BUT WILL BE COMPLETED PRIOR TO PROJECT CONSTRUCTION.
3. CONTRACTOR TO FIELD VERIFY ALL EXISTING CONDITIONS INCLUDING LOCATIONS OF EXISTING UTILITIES, AND NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING CONSTRUCTION.
4. CONTRACTOR TO PREVENT DIRT AND/OR DEBRIS FROM ENTERING STORM SEWER OR BEING TRANSPORTED OFF-SITE IN AN UNCONTROLLED MANNER. CONTRACTOR TO VERIFY AT PROJECT CLOSEOUT THAT STORM SEWER SYSTEM IS CLEAR OF SEDIMENT AND/OR DEBRIS AND IS FULLY FUNCTIONAL.
5. ALL AREAS DISTURBED BY CONSTRUCTION WHICH ARE OUTSIDE THE LIMITS OF PAVING ARE TO BE RESTORED AND REVEGETATED.
6. ALL UTILITY DEMOLITION AND/OR ABANDONMENT TO BE PERFORMED IN ACCORDANCE WITH CITY OF EDINA AND STATE OF MINNESOTA REGULATIONS AND STANDARDS.
7. EXISTING UTILITIES ARE SHOWN IN THEIR APPROXIMATE LOCATIONS. CONTRACTOR TO FIELD VERIFY THE LOCATION OF ALL EXISTING UTILITIES WHICH MAY INCLUDE BUT IS NOT LIMITED TO: ELECTRIC, TELEPHONE, GAS, CABLE TV, COMPUTER CABLE, FIBER OPTIC CABLE, SANITARY SEWER, STORM SEWER AND WATER MAIN. CONTRACTOR TO CONTACT THE OWNER STATE ONE CALL BEFORE EXCAVATING.
8. ALL EXISTING UTILITIES AND OTHER IMPROVEMENTS ARE TO REMAIN UNLESS NOTED OTHERWISE.
9. CONTRACTOR TO PREVENT FROM DAMAGE ALL EXISTING IMPROVEMENTS, LANDSCAPING, STRUCTURES AND UTILITIES THAT ARE TO REMAIN. CONTRACTOR TO REPAIR ANY DAMAGE AT OWN EXPENSE.
10. PROVIDE THE FOLLOWING MINIMUM COVER OVER THE TOP OF PIPE AS FOLLOWS:
 - a. 8" OVER WATER MAIN
 - b. 3" OVER SANITARY SEWER
 - c. 11" OVER STORM SEWER
12. ALL WORK TO CONFORM WITH CITY OF EDINA AND STATE OF MINNESOTA STANDARDS AND REGULATIONS.
13. ALL EXCAVATIONS MUST COMPLY WITH THE REQUIREMENTS OF DSHA 20 CFR, PART 1926, SUBPART P "EXCAVATIONS AND TRENCHES". THIS DOCUMENT STATES THAT EXCAVATION SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
14. SITE UTILITY CONTRACTOR TO FURNISH AND INSTALL ALL SANITARY SEWER AND STORM SEWER FACILITIES AND APPURTENANCES TO WITHIN THE FIVE FEET OUTSIDE THE BUILDING COORDINATE WITH MECHANICAL CONTRACTOR FOR EXACT LOCATION AND DEPTH OF CONNECTION WITH BUILDING.
15. CONTRACTOR SHALL COORDINATE WITH ARCHITECT'S DRAWINGS TO VERIFY LOCATION, SIZE AND QUANTITY OF ALL ROOF DRAINS AND UTILITY CONNECTIONS.
16. PROVIDE BARRICADES AT STREETS AND SIDEWALKS PER CITY OF EDINA AND MUTED REQUIREMENTS.
17. MAINTAIN 3 FEET VERTICAL AND 10 FEET HORIZONTAL SEPARATION BETWEEN WATER AND SEWER PIPES.
18. ALL PAVEMENT MARKINGS WITHIN ADJACENT STREETS TO BE RESTORED TO MATCH EXISTING.
19. DRAWINGS DO NOT INDICATE AREAS OF TEMPORARY SUPPORT SYSTEMS. THE CONTRACTOR IS RESPONSIBLE FOR ALL MEANS AND METHODS AND WILL HAVE TOTAL CONTROL OVER THE TYPES AND DESIGN OF ALL SHORING, SHEETING, BRACING, ANCHORAGES, EXCAVATION SUPPORT WALLS, DIRECTIONAL BORING, ASBEST JACOBS, SOIL STABILIZATION AND OTHER METHODS OF PROTECTING EXISTING IMPROVEMENTS. SEE SPECIFICATIONS FOR SUBMITTAL REQUIREMENTS.
20. STORAGE AND PROTECTION OF EXISTING SITE FEATURES WHICH NEED TO BE REMOVED AND REPLACED FOR CONSTRUCTION OF PROJECT ARE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR TO PREVENT DAMAGE OR THEFT OF THESE ITEMS AND TO REPLACE AT OWN EXPENSE.
21. CONTRACTOR TO RECORD EXISTING CONDITIONS AS NEEDED (PHOTOGRAPHS, VIDEO PHOTOGRAPHS, FIELD SURVEYING, ETC.) TO ENABLE RECONSTRUCTION TO MATCH EXISTING CONDITIONS. CONTRACTOR TO DOCUMENT EXISTING CONDITIONS SO THAT RECONSTRUCTED AREAS WILL HAVE POSITIVE DRAINAGE SIMILAR TO EXISTING. SEE SPECIFICATIONS.
22. ANY REDESIGN OF DEMOLISHED STRUCTURAL ELEMENTS WILL NEED TO BE DESIGNED AND CERTIFIED BY A REGISTERED ENGINEER.
23. WHERE DEMOLITION, EXCAVATION, UNDOORSPINNING, PILE DRIVING OR SIMILAR WORK IS TO BE PERFORMED ADJACENT TO OR IN THE IMMEDIATE VICINITY OF EXISTING STRUCTURES, THE CONTRACTOR WILL PROVIDE BUILDING SURVEYS AND SEISMIC MONITORING.
24. CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS PRIOR TO STARTING CONSTRUCTION.
25. ALL MATERIALS FOR PROPOSED CONSTRUCTION OR REPAIR OF EXISTING FACILITIES SHALL BE NEW PRODUCTS DIRECT FROM THE FACTORY AND FREE FROM DEFECTS.
26. WHEN WORKING AROUND EXISTING TELEPHONE OR ELECTRICAL POLES, THE CONTRACTOR SHALL BRACE THE POLE FOR SUPPORT.
27. WHEN WORKING AROUND EXISTING UTILITIES, LIGHT POLES, TRAFFIC SIGNALS, TELEPHONE OR POWER POLES, THE CONTRACTOR SHALL PROVIDE SUFFICIENT SUPPORT OR BRACING TO PREVENT EXCESSIVE STRESS ON THE PIPING. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY DAMAGES CAUSED BY FAILURE TO EXACTLY LOCATE AND PROTECT THESE UNDERGROUND FACILITIES.
28. CADD FILES OF THE SITE LAYOUT PLAN WILL BE PROVIDED TO THE CONTRACTOR AT THE TIME OF CONSTRUCTION FOR STAKING PURPOSES. DIMENSIONS SHOWN HEREIN ARE FOR GENERAL REFERENCES TO TYPICAL LAYOUT OF THE SITE. NOT ALL SITE ELEMENTS ARE DIMENSIONED.
29. CARE MUST BE TAKEN DURING CONSTRUCTION AND EXCAVATION TO PROTECT ANY SURVEY MONUMENTS AND/OR PROPERTY MARKS.
30. ALL STRAPS FROM TREES REMOVED WITHIN PROJECT LIMITS SHALL BE GROUND AND REMOVED IN THEIR ENTIRETY.
31. WASTE MATERIALS INCLUDING PAVEMENT REMOVED DURING CONSTRUCTION, WASTE PIPING AND SUPPLIES, CONSTRUCTION DEBRIS AND EXCESS EXCAVATED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE PROJECT SITE AND DISPOSED OF PROPERLY BY THE CONTRACTOR.
32. MAXIMUM CROSS-SLOPES FOR SIDEWALKS AND ADA ACCESS ROUTES SHALL NOT EXCEED 2.0%. ADA SIDEWALKS SHALL NOT EXCEED 5.00% SLOPE ALONG THE PATH OF TRAVEL.
33. CONTRACTOR SHALL NOT BLOCK DRAINAGE FROM OR DIRECT EXCESS DRAINAGE ONTO ADJACENT PROPERTY.
34. ADEQUATE DRAINAGE SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION AND ANY DRAINAGE STRUCTURE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO THE SATISFACTION OF THE OWNING AUTHORITY. ALL CONSTRUCTION STORM RUNOFF SHALL COMPLY WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) REQUIREMENTS.
35. THE CONTRACTOR SHALL PROVIDE A CONTINUOUS, ACCESSIBLE AND SAFE PEDESTRIAN WALKWAY THAT MEETS ADA AND MINNESOTA MUTED STANDARDS IF WORKING IN A SIDEWALK AREA, AND TRAFFIC CONTROL FOR MINNESOTA MUTED REQUIREMENTS FOR WORK IN THE PUBLIC RIGHT-OF-WAY.

EROSION CONTROL NOTES

1. INSTALL PERIMETER EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES. (MAY BALES ARE NOT AN ACCEPTABLE PERIMETER CONTROL).
2. BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY ROCK CONSTRUCTION ENTRANCE AT EACH POINT WHERE VEHICLES EXIT THE CONSTRUCTION SITE. USE 2 INCH OR GREATER DIAMETER ROCK IN A LAYER AT LEAST 12 INCHES THICK ACROSS THE ENTIRE WIDTH OF THE ENTRANCE. EXTEND THE ROCK ENTRANCE AT LEAST 50 FEET INTO THE CONSTRUCTION ZONE USING A GEOTEXTILE FABRIC BENEATH THE AGGREGATE TO PREVENT MIGRATION OF SOIL INTO THE ROCK FROM BELOW.
3. REMOVE ALL SOLLS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE ORDERED BY CITY INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. DUMPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.
4. INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. CATCH BASIN INLETS OR OTHER APPROVED DEVICES ARE REQUIRED IN UNDISTURBED AREAS THAT MAY RECEIVE RUNOFF FROM THE PROJECT AREA. MAY BALES OR FILTER FABRIC WRAPPED GRATES ARE NOT ALLOWED FOR INLET PROTECTION.
5. LOCATE SOIL OR DIRT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, COVER THE STOCKPILES BY MULCHING, VEGETATIVE COVER, TARP, OR OTHER MEANS. CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TEN FEET FROM THE DRAINAGE BUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
6. MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. RESPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED, OR POTTED EROSION CONTROL DEVICES IMMEDIATELY.
7. TEMPORARILY OR PERMANENTLY ESTABLISH ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERTAKEN. ACHIEVE EROSION RESISTANCE TO RAIN AND RUNNING WATER WITHIN 7-14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATING, AND/OR GEOTEXTILE AND STAKING IN GREEN SPACE AREAS. AN EARLY APPLICATION OF GRAVEL BASE ON AREAS TO BE PAVED IS RECOMMENDED TO MINIMIZE EROSION POTENTIAL.
8. REMOVE ALL TEMPORARY, SYNTHETIC, STRUCTURAL, NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS MANUAL IS THE ESTABLISHED COVER OVER EXPOSED AREA.
9. READY MIXED CONCRETE AND CONCRETE BATCH PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND MIXING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE WASHOUT LOCATIONS. UNDER NO CIRCUMSTANCE MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO ANY PUBLIC OR PRIVATE STORM DRAIN CONNECTION. CONTRACTOR SHALL SET UP A CONCRETE WASHOUT AREA ON-SITE THAT COMPLES WITH MPCA REQUIREMENTS.
10. CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
11. ALL EROSION CONTROL ELEMENTS ARE TEMPORARY. CONTRACTOR TO INSTALL EROSION CONTROL ELEMENTS PRIOR TO START OF LAND DISTURBING ACTIVITIES. MAINTAIN IN GOOD CONDITION DURING CONSTRUCTION AND REMOVE FROM THE SITE UPON COMPLETION OF FINAL PAVING AND TURF ESTABLISHMENT.
12. EROSION CONTROL SHALL BE PLACED ALONG THE PERIMETER OF THE SITE. EXCAVATION: EROSION CONTROL SHALL BE PLACED SO IT DOES NOT COVER THE EXISTING PAVEMENT OR DRIVE LANE THAT ARE TO REMAIN. MANY METHODS OF EROSION CONTROL WILL WORK AND IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSTALL THE MOST APPROPRIATE TO THE SITE CONDITIONS AND THAT WHICH MEETS THE CITY OF EDINA STANDARDS. GRAPHICALLY SHOWN ON THE PLANS FOR CLARITY BUT SHALL BE PLACED IN THE MOST APPROPRIATE LOCATIONS IN THE FIELD AND BEHIND THE BACK OF CURB. EXISTING CURB AND SIDEWALKS WILL BE REPAIRED BY THE CONTRACTOR AND PAVED OR SOLIDLY BY THE CONTRACTOR. SEE DETAILS AND SPECIFICATIONS.
13. CONTRACTOR TO PREVENT DIRT AND/OR DEBRIS FROM ENTERING STORM SEWER OR BEING TRANSPORTED OFF-SITE IN AN UNCONTROLLED MANNER. CONTRACTOR TO VERIFY AT PROJECT CLOSEOUT THAT STORM SEWER SYSTEM IS CLEAR OF SEDIMENT AND/OR DEBRIS AND IS FULLY FUNCTIONAL.
14. CONTRACTOR TO PROVIDE TEMPORARY SEED AND MULCH ON ALL NON-PAVED AREAS WITHIN 7 DAYS AFTER MOUD GRADING IS COMPLETED. SEED WITH ANNUAL RYE SEED AT 80 LBS PER ACRE AND WOOD MULCH FIBER AT 40 LBS PER 1,000 SQ.
15. STRAWBALES ARE NOT ALLOWED ON SITE IN ANY CAPACITY.

LEGEND

DNH	EXISTING SANITARY MANHOLE
STMH	EXISTING STORM MANHOLE
EMH	EXISTING ELECTRIC MANHOLE
CB	EXISTING CATCH BASIN
CB	EXISTING CATCH BASIN
FB	EXISTING FIRE HYDRANT
VV	EXISTING WATER VALVE
ET	EXISTING ELECTRIC TRANSFORMER
LP	EXISTING LIGHT POLE
BL	EXISTING BOLLARD LIGHT
GM	EXISTING GAS METER
FR	EXISTING TELEPHONE RESIN
—	EXISTING SIGN
—UE	EXISTING UNDERGROUND ELECTRIC
—OH	EXISTING OVERHEAD UTILITY LINE
—	EXISTING SANITARY SEWER LINE
—	EXISTING STORM SEWER LINE
—W	EXISTING WATER MAIN OR SERVICE
—FD	EXISTING UNDERGROUND FIBEROPTIC
—G	EXISTING GAS MAIN OR SERVICE
---	PROPERTY LINE
25'	PROPOSED CONTOUR
+SE3	PROPOSED SPOT ELEVATION
+	IDENTIFIES SURFACE DRAINAGE
---	PROPOSED SAWDUST LINE
---	PROPOSED STORM SEWER
---	PROPOSED SANITARY SEWER
---	PROPOSED WATERMAIN
⊗	PROPOSED CATCH BASIN
⊙	PROPOSED MANHOLE
⊖	PROPOSED DOWNSPOUT
⊕	PROPOSED VALVE
<	PROPOSED FIRE DEPARTMENT CONNECTION
---	PROPOSED RETAINING WALL
---	SILT FENCE
⊗	EROSION CONTROL AT CURB/WH
---	PROPOSED BITUMINOUS PAVING
---	PROPOSED CONCRETE PAVING
---	PROPOSED CONCRETE SIDEWALK
---	PROPOSED CONSTRUCTION ENTRANCE



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PRELIMINARY
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REVISIONS
Date Description

A23

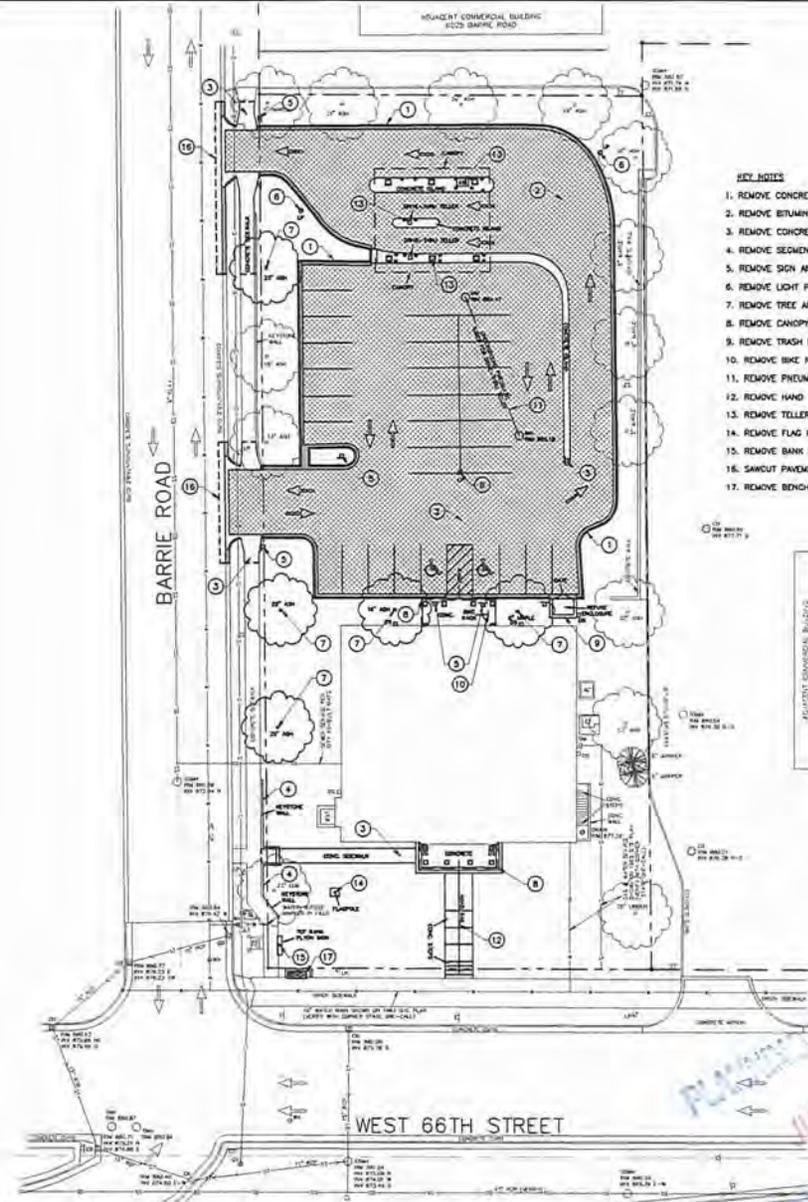
DATE: 06.24.14
PROJECT # 14-0000
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NOTES AND LEGEND

PRELIMINARY
NOT FOR CONSTRUCTION

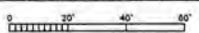
C100

JUN 24 2014
CITY OF EDINA



- KEY NOTES**
1. REMOVE CONCRETE CURB AND CUTTER
 2. REMOVE BITUMINOUS PAVEMENT
 3. REMOVE CONCRETE PAVEMENT
 4. REMOVE SEGMENTAL RETAINING WALL
 5. REMOVE SIGN AND BASE
 6. REMOVE LIGHT POLE AND BASE
 7. REMOVE TREE AND STUMP
 8. REMOVE CANOPY, SUPPORT POSTS, AND FOOTINGS
 9. REMOVE TRASH ENCLOSURE AND FOOTINGS
 10. REMOVE BIKE RACK
 11. REMOVE PNEUMATIC TUBE INFRASTRUCTURE
 12. REMOVE HAND RAILING
 13. REMOVE TELLER MACHINES AND BOLLARDS
 14. REMOVE FLAG POLE AND BASE
 15. REMOVE BANK SIGN AND FOOTINGS
 16. SAWCUT PAVEMENT TO FULL DEPTH
 17. REMOVE BENCH AND CONCRETE PAD

1 SITE DEMOLITION PLAN
 NORTH
 C200



1"=20'

PRELIMINARY
 NOT FOR CONSTRUCTION

66 West
 Apartments
 3330 66th St. West, Edina



161 NORTH 114TH STREET, SUITE 1
 MINNEAPOLIS, MN 55411
 612.436.3100

CONSULTANT
**PIERCE PINI &
 ASSOCIATES**
 200 CHRYSLER AVENUE, SUITE 200
 MINNEAPOLIS, MN 55415
 612.338.8800

PRELIMINARY
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REVISIONS

Date Description

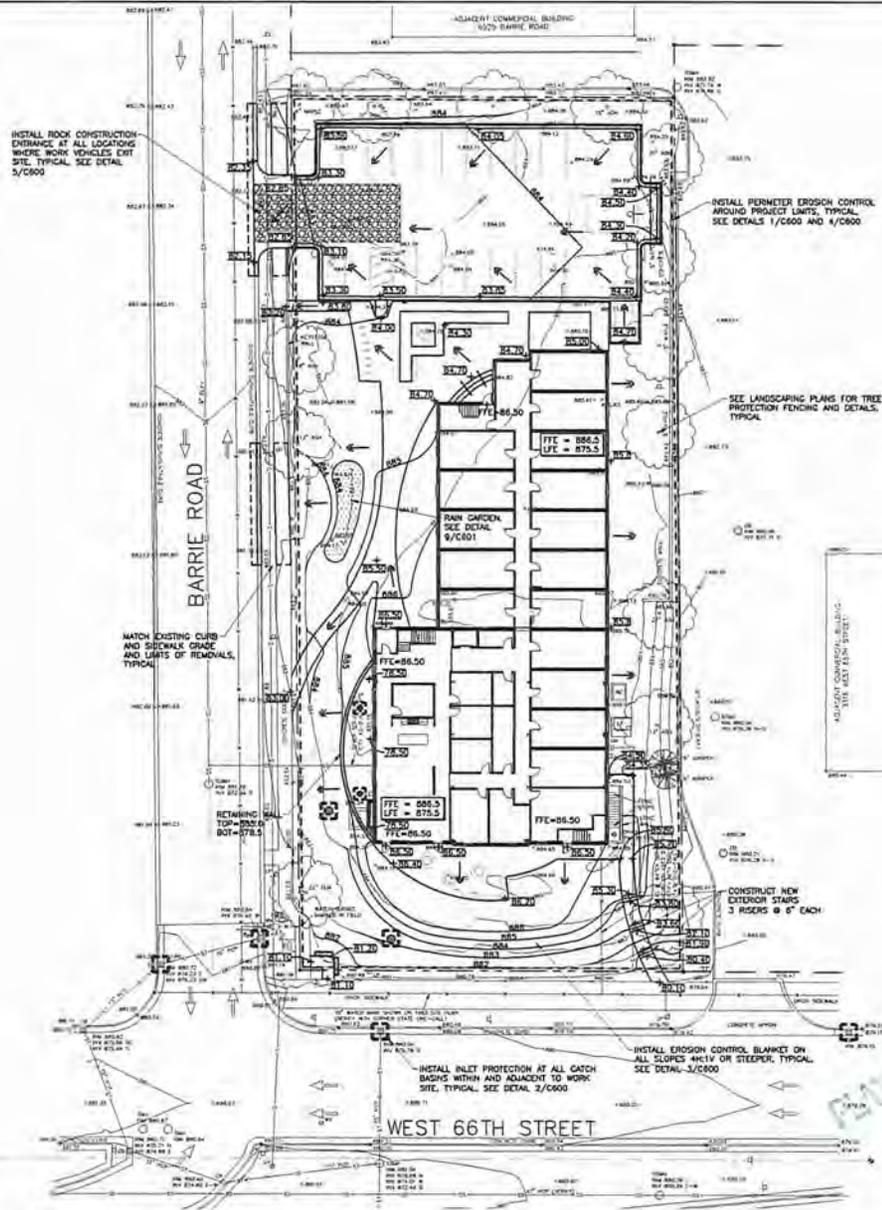
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DATE 06.11.14
 PROJECT # 14-
 PHASE 1
 DRAWN BY
 CHECKED BY

SITE DEMOLITION PLAN

C200

PLANNING DEPARTMENT
 JUN 2 8 2014
 CITY OF EDINA



1 GRADING, DRAINAGE AND EROSION CONTROL PLAN



1"=30'

RECEIVED
PLANNING DEPARTMENT
JUN 20 2014
CITY OF EDINA

PRELIMINARY
NOT FOR CONSTRUCTION

66 West
Apartments
3330 66th St. West, Edina



301 NORTH THIRD STREET, SUITE 4
MINNEAPOLIS, MN 55401
912.454.2100

CONSULTANT
PIERCE PINI &
ASSOCIATES

PRELIMINARY
NOT FOR
CONSTRUCTION

REVISIONS
Date Description

ARS

DATE 06.16.14
PROJECT # 14-
PHASE 1
DRAWN BY
CHECKED BY

GRADING,
DRAINAGE AND
EROSION
CONTROL PLA

C300

PERMIT REQUIREMENTS:

A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED AS PART OF THE GENERAL PERMIT. MODIFICATION TO DISCHARGE STORMWATER ASSOCIATED WITH CONSTRUCTION ACTIVITY UNDER THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES)/STATE POLLUTION CONTROL SYSTEM (SPDES) CONSTRUCTION STORMWATER PERMIT FOR THIS PROJECT. THIS PERMIT IS ISSUED BY THE MINNESOTA POLLUTION CONTROL AGENCY (MPCA).

THE GOAL OF POLLUTION PREVENTION EFFORTS DURING PROJECT CONSTRUCTION IS TO CONTROL SOIL AND POLLUTANTS ON THE SITE AND PREVENT THEM FROM LEAVING THE PROJECT SITE AND FLOWING TO SURFACE WATERS. THE PURPOSE OF THIS PERMIT IS TO PROVIDE GUIDELINES FOR ACHIEVING THAT GOAL. THE SWPPP MUST BE KEPT UPGRADE AND UPDATED AS NECESSARY DURING THE COURSE OF CONSTRUCTION TO KEEP IT CURRENT WITH ANY MODIFICATIONS TO THE POLLUTION CONTROL MEASURES BEING UTILIZED.

THIS PROJECT CONCEPTS OF THE CONSTRUCTION OF A NEW MULTI-UNIT LIVING COMPLEX, PARKING LOT, AND SIDEWALKS ON AN EXISTING SITE WITH A BANK IN EDINA, MINNESOTA.

PROPOSED STORMWATER MANAGEMENT DESIGN IS NOT REQUIRED TO MEET THE REQUIREMENTS OF THE CITY OF EDINA AND THE MINNESOTA CREEK WATERSHED DISTRICT BECAUSE THERE WILL BE A REDUCTION IN IMPERVIOUS AREA TO THE SITE AS A RESULT OF THE CONSTRUCTION. THIS PROJECT IS NOT REQUIRED TO MEET THE MPCA PERMANENT STORMWATER MANAGEMENT REQUIREMENTS BECAUSE THE DISTURBED AREA IS LESS THAN ONE ACRE AND THE INCREASE IN IMPERVIOUS AREA IS LESS THAN ONE ACRE.

SEMI-ANNUAL FLOOD OR IMPROVED WATERS WITHIN ONE MILE OF THE SITE:
-THIS PROJECT DISCHARGES TO CORNELIA LAKE - THIS LAKE IS IDENTIFIED AS AN IMPAIRED WATER BY THE MPCA'S 303(d) IMPAIRED WATERS LIST FOR NUTRIENT/EUTROPHICATION BIOLOGICAL DEFICIENCY. THESE IMPAIRED AREAS ARE REQUIRED TO BE TREATED AS PART OF THE NPDES PERMIT.

STATEMENT OF FILL IN WATERS OF THE STATE:
-N/A
DRINKING WATER SUPPLY MANAGEMENT AREA:
-N/A

THE PROJECT STORMWATER DISCHARGE IS NOT ANTICIPATED TO IMPACT ANY OF THE FOLLOWING:
-OUTSTANDING RESOURCE VALUE WATERS, TROUT WATERS, WETLANDS, CALCAREOUS FISH, PROPERTIES LISTED BY THE NATIONAL REGISTER OF HISTORIC PLACES OR ARCHAEOLOGICAL SITES
THE PROJECT STORMWATER DISCHARGE IS NOT SUBJECT TO ADDITIONAL REGULATION DUE TO ANY OF THE FOLLOWING:
-OTHER FORMAL ENVIRONMENTAL REVIEWS, ENDANGERED OR THREATENED SPECIES

STORMWATER POLLUTION PREVENTION PLAN (SWPPP) SUPERVISION RESPONSIBILITIES:
1. THE OWNER AND CONTRACTOR ARE PERMITTED(S) AS IDENTIFIED BY THE NPDES PERMIT.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ON-SITE IMPLEMENTATION OF THE SWPPP, INCLUDING THE ACTIVITIES OF ALL OF THE CONTRACTOR'S SUBCONTRACTORS.
3. CONTRACTOR SHALL PROVIDE A PERSON(S) KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMPs TO OVERSEE ALL INSTALLATION AND MAINTENANCE OF BMPs AND IMPLEMENTATION OF THE SWPPP.

4. CONTRACTOR SHALL PROVIDE PERSON(S) MEETING THE TRAINING REQUIREMENTS OF THE NPDES PERMIT TO CONDUCT INSPECTION AND MAINTENANCE OF ALL EROSION PREVENTION AND SEDIMENT CONTROL BMPs IN ACCORDANCE WITH THE REQUIREMENTS OF THE PERMIT. ONE OF THESE INDIVIDUALS MUST BE AVAILABLE FOR AN ON-SITE INSPECTION WITHIN 72 HOURS UPON REQUEST BY MPCA. CONTRACTOR SHALL PROVIDE TRAINING DOCUMENTATION FOR THESE INDIVIDUALS AS REQUIRED BY THE NPDES PERMIT. THE TRAINING DOCUMENTATION SHALL BE RECORDED IN OR WITH THE SWPPP BEFORE THE START OF CONSTRUCTION OR AS SOON AS THE PERSONNEL FOR THE PROJECT HAVE BEEN DETERMINED. DOCUMENTATION SHALL INCLUDE:
4.1. NAMES OF THE PERSONNEL ASSOCIATED WITH THE PROJECT THAT ARE REQUIRED TO BE TRAINED PER PART 102.1 OF THE PERMIT
4.2. DATES OF TRAINING AND NAME OF INSTRUCTOR AND ENTITY PROVIDING TRAINING
4.3. CONTENT OF TRAINING COURSE OR WORKSHOP INCLUDING THE NUMBER OF HOURS OF TRAINING

4.4. FOLLOWING FINAL TRAINING AND THE TERMINATION OF COVERAGE FOR THE NPDES PERMIT, THE OWNER IS EXPECTED TO FURNISH LONG TERM OPERATION AND MAINTENANCE (O & M) OF THE PERMANENT STORM WATER MANAGEMENT SYSTEM.

STORMWATER MANAGEMENT DESIGN REQUIREMENTS:
THE FOLLOWING DESIGN CRITERIA APPLY TO THE DESIGN OF STORMWATER TREATMENT FACILITIES. N/A INDICATES NOT APPLICABLE OR NOT CONSTRUCTED AS PART OF THIS PROJECT.
1. TEMPORARY SEDIMENT BASINS: N/A
2. PERMANENT NOT SEDIMENTATION BASINS: N/A
3. PERMANENT INFILTRATION/FILTRATION: MINIMUM OF 3.226 CUBIC FEET OF INFILTRATION VOLUME
4. PERMANENT REGIONAL Ponds: N/A
5. ALTERNATIVE METHODS: N/A

SEQUENCE OF CONSTRUCTION:
THE FOLLOWING SEQUENCE DESCRIBES, IN GENERAL, THE WORK ON THE SITE.
1. CONTRACTOR SHALL VERIFY THAT ALL PERMITS HAVE BEEN OBTAINED AND/OR OBTAIN THE NECESSARY PERMITS.
2. CONTRACTOR SHALL PERFORM SITE INSPECTION, RECORD KEEPING AND RECORD RETENTION IN ACCORDANCE WITH ALL PERMITS.
3. CONTRACTOR SHALL INSTALL ALL PERMITTER AND DOWN-DRAINAGE EROSION CONTROL AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), CONSTRUCTION ENTRANCES AND BEST PRACTICE PROTECTION DEVICES PRIOR TO SITE GRADING, EXCAVATION, STOCKPILING OR DISTURBING EXISTING VEGETATIVE COVER.
4. CONTRACTOR SHALL PERFORM SITE GRADING, EXCAVATION, STOCKPILING WORK IN ACCORDANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP).
5. CONTRACTOR SHALL INSTALL, INSPECT, MONITOR AND MAINTAIN TEMPORARY AND PERMANENT EROSION CONTROL BMPs AS SHOWN ON PLANS & IN ACCORDANCE WITH NPDES PERMIT, CONTINUOUSLY DURING THE WORK. CONTRACTOR SHALL STABILIZE ALL EXPOSED SOILS NO LATER THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
6. CONTRACTOR SHALL REPLACE OR REPAIR EROSION CONTROL AND SEDIMENT CONTROL BMPs THAT ARE NOT FUNCTIONING PROPERLY.
7. CONTRACTOR SHALL PERFORM SITE RESTORATION ACTIVITIES FOR PERMANENT VEGETATIVE ESTABLISHMENT.
8. CONTRACTOR SHALL REPAIR SEDIMENT CONTROL DEVICES PRIOR TO SUBMITTING NOTICE OF TERMINATION (NOT).

9. SUBMIT NOTICE OF TERMINATION TO MPCA WITHIN 30 DAYS OF FINAL STABILIZATION.
CONSTRUCTION ACTIVITY FIELD REQUIREMENTS:
1. FIELD REQUIREMENTS SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE NPDES PERMIT AND STORMWATER POLLUTION PREVENTION PLAN (SWPPP).
A. THE CONTRACTOR MUST IMPLEMENT SWPPP AND PROVIDE BMPs IDENTIFIED IN THE SWPPP IN AN APPROPRIATE AND FUNCTIONAL MANNER.
B. THE CONTRACTOR SHALL RESPOND TO CHANGING SITE CONDITIONS AND IMPLEMENT/SUPPLEMENT EROSION PREVENTION AND SEDIMENT CONTROL MEASURES UTILIZED TO PROVIDE ADEQUATE PROTECTION OF DISTURBED SOILS AND ADEQUATE PREVENTION OF SEDIMENT TRANSPORT OFF-SITE.
C. WITHIN 72 HOURS OF THE WATER POLLUTION PREVENTION CONSTRUCTION ACTIVITY FIELD REQUIREMENTS SHALL BE FURNISHED BY THE CONTRACTOR.

EROSION PREVENTION PRACTICES:
CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING EROSION PREVENTION PRACTICES:
A. THE CONTRACTOR SHALL ATTEMPT TO PHASE ALL WORK TO MINIMIZE EROSION AND MAINTAIN NEGATIVE COVER TO THE EXTENT FEASIBLE. THE LOCATION OF AREAS NOT TO BE DISTURBED MUST BE IDENTIFIED ON THE SITE BEFORE CONSTRUCTION BEGINS.
B. STABILIZATION ON ALL EXPOSED SOILS MUST BE INITIATED IMMEDIATELY WHENEVER CONSTRUCTION ACTIVITY HAS TEMPORARILY CEASED OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 7 CALENDAR DAYS INCLUDING STOCKPILES WITH SIGNIFICANT SALT, CLAY OR ORGANIC COMPONENTS. STABILIZATION MUST BE COMPLETED WITHIN 7 CALENDAR DAYS.
C. THE NORMAL WETTED PERIPHERY OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH THAT DRAINS WATER FROM A CONSTRUCTION SITE OR DIVERTS WATER AROUND A SITE MUST BE STABILIZED BY CONTRACTOR WITHIN THE PERIOD OF DISCOVERY OF DISCHARGE TO ANY SURFACE WATER WITHIN 24 HOURS OF DISCOVERY TO A SURFACE WATER. TEMPORARY OR PERMANENT DITCH SHALL BE BUILT USING A SEDIMENT CONTAINMENT SYSTEM DO NOT USED TO BE STABILIZED UNTIL THEY ARE NO LONGER USED AS A SEDIMENT CONTAINMENT SYSTEM. AFTER WHICH THEY MUST BE STABILIZED WITHIN 24 HOURS.

D. TEMPORARY OR PERMANENT EROSION OCCUPATION AT PIPE OUTLETS MUST BE PROVIDED WITHIN 24 HOURS OF COVERING TO A SURFACE WATER.
E. THE CONTRACTOR MUST DIRECT DISCHARGES FROM BMPs TO VEGETATED AREAS OF THE SITE IN ORDER TO INCREASE SEDIMENT REMOVAL AND MAXIMIZE INFILTRATION UNLESS INFEASIBLE. THE CONTRACTOR MUST UTILIZE BEST PRACTICE DEVICES IF NECESSARY TO PREVENT EROSION WHICH DIVERTS STORMWATER TO VEGETATED AREAS.

SEDIMENT CONTROL PRACTICES:
CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING SEDIMENT CONTROL PRACTICES:
A. CONTRACTOR MUST INSTALL ALL DOWN GRADIENT PERMITTER CONTROLS BEFORE ANY UP GRADIENT DISTURBANCE BEGINS. CONTRACTOR SHALL MAINTAIN PERMITTER CONTROLS UNTIL FINAL STABILIZATION HAS BEEN ESTABLISHED.
B. CONTRACTOR SHALL PROVIDE GRADING AND BMP INSTALLATION TO LIMIT ALL SLOPES OF 2H:1V OR STEEPER TO AN SLOPED LENGTH OF 75 FEET OR LESS.
C. IF DOWN GRADIENT SEDIMENT CONTROLS ARE OVERLOOKED, THE CONTRACTOR MUST INSTALL ADDITIONAL UPGRADIENT SEDIMENT CONTROL PRACTICES OR REDUNDANT BMPs TO ELIMINATE OVERLOOKING. THE SWPPP MUST BE AMENDED TO IDENTIFY THESE ADDITIONAL PRACTICES.
D. TRAIL AND INSTALLATION OF SEDIMENT CONTROL DEVICES CAN BE ADJUSTED BY CONTRACTOR TO ACCOMMODATE SHORT-TERM ACTIVITIES SUCH AS CLEARING AND GRUBBING OR VEHICLE PASSAGE. ANY SHORT-TERM ACTIVITY MUST BE COMPLETED AS QUICKLY AS POSSIBLE AND THE SEDIMENT CONTROL PRACTICES MUST BE INSTALLED IMMEDIATELY AFTER THE ACTIVITY IS COMPLETED AND IN ALL CASES PRIOR TO THE NEXT CONSTRUCTION EVENT.

E. ALL STORM SEWER INLETS AND OUTLETS SHALL BE PROTECTED BY CONTRACTOR WITH APPROPRIATE BMPs DURING THE WORK. THESE PRACTICES SHALL REMAIN IN PLACE UNTIL THE POTENTIAL SOURCES FOR DISCHARGING SEDIMENT TO PLETS HAVE BEEN STABILIZED BY CONTRACTOR.
F. TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROL. SOIL STOCKPILES SHALL NOT BE PLACED IN SURFACE WATERS OR STORMWATER CONVEYANCES. ALL SOIL STOCKPILES THAT REMAIN UNCOVERED FOR A PERIOD GREATER THAN 7 DAYS SHALL BE PROTECTED BY CONTRACTOR WITH COVER OF MULCH, EROSION CONTROL MATS, OR PLASTIC SHEETING.
G. CONTRACTOR SHALL IMPLEMENT MEASURES TO CONTROL VEHICLE TRACKING OFF SITE. ROAD CONSTRUCTION ENTRANCES OR EQUIPMENT SYSTEM MUST BE INSTALLED BY CONTRACTOR TO MINIMIZE TRACKING FROM SITE. CONTRACTOR SHALL PROVIDE STREET SHEEPING AS NECESSARY IF BMPs ARE NOT ADEQUATE TO PREVENT TRACKING FROM BEING TRACKED ONTO THE STREET.
H. CONTRACTOR SHALL SHOW TEMPORARY SEDIMENTATION BASINS AS REQUIRED BY THE PERMIT.
I. CONTRACTOR MUST MINIMIZE SOIL COMPACTION AND PRESERVE TOPSOIL, UNLESS INFEASIBLE.
J. TRACKING SOIL IS NOT REQUIRED TO BE REMOVED FROM A SPECIFIC AREA OF THE SITE UNLESS IT IS COMPACTED.
K. CONTRACTOR MUST PRESERVE A 50 FT NATURAL BUFFER OR PROVIDE REDUNDANT SEDIMENT CONTROL DEVICES LOCATED WITHIN 25 FEET OF THE PROJECT DISTURBANCE AREAS AND STORMWATER FLEETS TO THE SURFACE WATER.

L. IF PONDERS, FLOCCULATORS OR OTHER TREATMENT CHEMICALS ARE USED ON SITE, THE CONTRACTOR MUST COMPLY WITH THE FOLLOWING REQUIREMENTS:
a. THE CONTRACTOR MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITIVE TREATMENT. EROSION AND SEDIMENT CONTROLS MUST BE USED THROUGHOUT THE TREATMENT STORMWATER IS DIRECTED TO A SEDIMENT CONTROL SYSTEM WHICH ALLOWS FOR FLOC SETTLEMENT AND REMOVAL.
b. CHEMICALS MUST BE SELECTED THAT ARE APPROPRIATELY SIZED TO THE TYPES OF SOILS LIKELY TO BE EXPOSED DURING CONSTRUCTION. CHEMICALS MUST BE USED IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES AND WITH THE MANUFACTURER'S RECOMMENDATIONS AND REMOVAL DESIGN SPECIFICATION PROVIDED BY THE MANUFACTURER.

DOWNGRADING AND BASH COVERING:
CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING DOWNGRADING REQUIREMENTS:
A. CONTRACTOR'S DOWNGRADING ACTIVITIES THAT HAVE SEDIMENT-LOADED DISCHARGE WATER MUST BE DISCONTINUED AND A TEMPORARY OR PERMANENT SEDIMENTATION BASIN WHICHEVER POSSIBLE, OTHERWISE IT MUST BE DISCONTINUED THROUGH SOME FORM OF BEST MANAGEMENT PRACTICE (BMP) BY CONTRACTOR TO LIMIT SEDIMENT FROM LEAVING THE SITE. PRIOR TO DISCONTINUING, THE CONTRACTOR SHALL PERFORM VISUAL TEST TO ENSURE ADEQUATE TREATMENT IS OBTAINED IN THE BASIN OR BMP AND APPLY ADDITIONAL TREATMENT AS REQUIRED TO ENSURE ADEQUATE TREATMENT.
B. THE CONTRACTOR SHALL DISCHARGE WATER FROM DOWNGRADING IN A MANNER THAT DOES NOT CAUSE EROSION, CONTAMINATION OF SURFACE WATER, OR OTHER ADVERSE IMPACTS TO RECEIVING DISCHARGE MEASURE AND NOT ADVERSELY AFFECT THE RECEIVING WATER OR DOWNSTREAM DRAINAGES OR WETLANDS.
C. IF CONTRACTOR IS USING FILTERS WITH BACKWASH BATH, THE CONTRACTOR SHALL HAUL THE BACKWASH WATER AWAY FOR DISPOSAL, RETURN THE BACKWASH WATER TO THE BEGINNING OF THE TREATMENT PROCESS, OR DISCHARGE THE BACKWASH WATER INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION.

INSPECTIONS AND MAINTENANCE:
CONTRACTOR SHALL BE RESPONSIBLE FOR PERFORMING THE FOLLOWING INSPECTIONS AND MAINTENANCE:
A. WHEN INSPECTING TWO EROSION PREVENTION AND SEDIMENT CONTROL BMPs THAT ARE NONFUNCTIONAL, ALL NONFUNCTIONAL BMPs MUST BE REPAIRED, REPLACED, OR SUPPLEMENTED WITH FUNCTIONAL BMPs WITHIN 24 HOURS AFTER DISCOVERY OR OTHERWISE IN ACCORDANCE WITH THE NPDES PERMIT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MPCA OF ANY EROSION CONTROL MEASURES DEEMED NECESSARY BY MPCA WITHIN 24 HOURS OF NOTICE FROM MPCA.
B. THE CONTRACTOR MUST ROUTINELY INSPECT THE SITE ONCE EVERY 7 DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS.
C. ALL INSPECTIONS AND MAINTENANCE CONDUCTED DURING CONSTRUCTION MUST BE RECORDED IN WRITING BY CONTRACTOR AND RETAINED WITH THE SWPPP BY CONTRACTOR. MAINTENANCE MUST BE COMPLETED BY CONTRACTOR IN CONFORMANCE WITH NPDES PERMIT. CONTRACTOR'S RECORDS MUST INCLUDE:
a. DATE AND TIME OF INSPECTION.
b. NAME OF PERSON CONDUCTING INSPECTION.
c. FINDING OF INSPECTION INCLUDING RECOMMENDATIONS FOR CORRECTIVE ACTION.
d. DETAILS OF CORRECTIVE ACTION TAKEN (DATE, TIME, PARTY CONDUCTING MAINTENANCE ACTIVITIES).
e. DATE AND AMOUNT OF RAINFALL GREATER THAN 0.5 INCHES IN 24 HOURS.
f. IF ANY DISCHARGE IS OBSERVED TO BE OCCURRING DURING THE INSPECTION, A RECORD OF DISCHARGE, ALL POINTS OF THE PROPERTY FROM WHICH THERE IS A DISCHARGE MUST BE MADE, AND THE DISCHARGE SHALL BE RECORDED (COLOR, IDOR, FLOATING, SETTLED, OR SUSPENDED SOLIDS, FOAM, OIL SHEEN, AND OTHER INDICATORS) AND PHOTOGRAPHED.
g. DOCUMENTATION OF CHANGES MADE TO SWPPP.

D. IN AREAS OF PROJECT WHERE FINAL STABILIZATION IS COMPLETE, INSPECTIONS CAN BE REDUCED TO ONCE A MONTH. THESE AREAS SHALL BE INSPECTED BY CONTRACTOR FOR MINIMUM PERIOD OF 12 MONTHS-WHICH MEETING AND MEETING WITHIN 72 HOURS OF EACH SPIKE OF EROSION OR CONSTRUCTION FOLLOWING ANY WATER STOPPAGE, WHICHEVER COMES FIRST.
E. THE CONTRACTOR SHALL MAINTAIN RECORDS OF INSPECTIONS AND MAINTENANCE OF BMPs UNTIL AVOIDER PERMITS HAS OBTAINED COVER, OR THE PROJECT HAS UNDERGONE FINAL STABILIZATION AND AN NPDES PERMIT HAS BEEN SUBMITTED TO THE MPCA.
F. ALL EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED BY CONTRACTOR ACCORDING TO THE DETAILS INCLUDED IN THE CONSTRUCTION DOCUMENTS AND IN ACCORDANCE WITH THE PRODUCER MANUFACTURER'S RECOMMENDATIONS. IN CASES OF NONFUNCTIONAL BMPs MUST BE REPAIRED OR REPLACED WITHIN 24 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW.
G. ALL PERMITTER CONTROL DEVICES MUST BE REPAIRED, REPLACED OR SUPPLEMENTED BY THE CONTRACTOR WITHIN 24 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW. THE HEIGHT OF THE DEVICE, CONTRACTOR SHALL REPAIR OR REPLACE DEVICE THAT IS NONFUNCTIONAL AT THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY, OR THEREAFTER AS SOON AS FIELD CONDITIONS ALLOW.
H. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND SEDIMENT REMOVED BY CONTRACTOR ONCE DISCOVERY OF DISCHARGE FROM THE STORAGE VOLUME WITHIN 72 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW.
I. ALL SEDIMENT DEPOSITS WITHIN SURFACE WATERS OR STORMWATER CONVEYANCES MUST BE REMOVED AND RESTABILIZED BY CONTRACTOR WITHIN 72 HOURS OF DISCOVERY, INCLUDING DEADENDS AND STORM SEWER SEDIMENT DEPOSITS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS NECESSARY TO CONDUCT NECESSARY RESTORATION WORK.
J. CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING EXISTING PAVED SURFACES CLEAN OF SEDIMENT. CONSTRUCTION ENTRANCES SHALL BE CHECKED DAILY BY CONTRACTOR. IF THE ENTRANCE BECOMES IMBURGED WITH SEDIMENT, BY THE ENTRANCE, IT IS TO BE REPAIRED AS APPROPRIATE BY CONTRACTOR. STREETS LEADING TO AND FROM THE CONSTRUCTION ENTRANCE SHALL BE CHECKED DAILY BY CONTRACTOR. ALL SURFACES LEADING TO AND FROM THE CONSTRUCTION ENTRANCE SHALL BE SWEEP CLEAN OF ANY TRACKED MATERIALS BY CONTRACTOR AS SOON AS POSSIBLE AND WITHIN 24 HOURS OF DISCOVERY. CONTRACTOR SHALL EXTEND SWEEPING TO THE EXTREMITY OF ANY SEDIMENT TRACKING THAT OCCURS OFF-SITE.
K. CONTRACTOR SHALL BE RESPONSIBLE TO REMOVE ANY OFF-SITE SEDIMENT ACCUMULATING IN A MANNER AND AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS.

L. ALL INFILTRATION/FILTRATION AREAS MUST BE INSPECTED BY CONTRACTOR TO ENSURE THAT NO SEDIMENT FROM ONGOING CONSTRUCTION IS ACCUMULATING OVER THE INFILTRATION/FILTRATION AREA. PREVENTION AND MAINTENANCE OF OVER INFILTRATION/FILTRATION MUST BE PROVIDED BY CONTRACTOR.
M. CONTRACTOR SHALL PROTECT INFILTRATION/FILTRATION AREAS FROM SEDIMENTATION AND OVER-COMPACTION. BURIAL, EXCAVATION, SEDIMENT AND EROSION CONTROL DEVICES MUST BE PROVIDED BY CONTRACTOR TO PREVENT SEDIMENTATION AND THE AREA MUST BE GRADED OFF AND MARKED SO THAT HEAVY CONSTRUCTION EQUIPMENT WILL NOT COMPACT THE SOIL.
N. INSPECTORS CAN BE SUSPENDED DUE TO FREQUENT GRADING CONDITIONS UNTIL FIRST RUNOFF OCCURS OF CONSTRUCTION ACTIVITIES RESUME.

EQUALITY PREVENTION MEASURES:
CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING THE FOLLOWING POLLUTION PREVENTION MANAGEMENT MEASURES ON THE SITE:
A. THE CONTRACTOR SHALL MINIMIZE THE EXPOSURE OF ALL PRODUCTS, MATERIALS, AND WASTES FROM STORMWATER WHICH MAY BE A SOURCE OF CONTAMINATION TO STORMWATER OR ARE NOT DESIGNED TO BE EXPOSED TO STORMWATER.
B. BUILDING PRODUCTS THAT MAY LEACH POLLUTANTS MUST BE UNDER COVER (PLASTIC SHEETING, TEMPORARY ROOFS, ETC.) TO PREVENT THE DISCHARGE OF POLLUTANTS OR PROTECTED BY A SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
C. PESTICIDES, HERBICIDES, INSECTICIDES, FERTILIZERS, TREATMENT CHEMICALS, AND LANDSCAPE MATERIALS MUST BE UNDER COVER (PLASTIC SHEETING, TEMPORARY ROOFS, ETC.) TO PREVENT THE DISCHARGE OF POLLUTANTS OR PROTECTED BY A SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
D. HAZARDOUS MATERIALS, TOXIC WASTE, INCLUDING OIL, DIESEL FUEL, GASOLINE, HYDRAULIC FLUIDS, PAINT SOLVENTS, PETROLEUM-BASED PRODUCTS, WOOD PRESERVATIVES, ACIDITIES, CURING COMPOUNDS, AND ADHESIVES MUST BE STORED IN SEALED CONTAINERS TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. RESTRICTED ACCESS STORAGE AREAS MUST BE PROVIDED TO PREVENT UNAUTHORIZED STORAGE AND DISPOSAL OF HAZARDOUS MATERIALS MUST COMPLY WITH ALL STATE REQUIREMENTS.
E. SOLID WASTE MUST BE STORED, COLLECTED, AND DISPOSED IN COMPLIANCE WITH ALL STATE REQUIREMENTS.
F. PORTABLE TOILETS MUST BE POSITIONED SO THAT THEY ARE SECURE AND WILL NOT BE SHIPPED OVER. CONTRACTOR WASTE MUST BE DISPOSED IN ACCORDANCE WITH ALL STATE REQUIREMENTS.
G. THE CONTRACTOR SHALL TAKE REASONABLE STEPS TO PREVENT THE DISCHARGE OF SPILLED OR LEAKED CHEMICALS, INCLUDING FUEL, FROM ALL AREAS WHERE CHEMICALS OR FUEL WILL BE LOADED OR UNLOADED. THE CONTRACTOR MUST PROVIDE CLEANING IN A CONTAINED AREA UNLESS INFEASIBLE. THE CONTRACTOR MUST ENSURE ADEQUATE SUPPLIES ARE AVAILABLE AT ALL TIMES TO CLEAN UP SPILLED MATERIALS AND THAT AN APPROPRIATE DISPOSAL METHOD IS AVAILABLE FOR RECOVERED SPILLED MATERIALS. ALL SPILLS MUST BE CLEANED UP AND REPORTED IN ACCORDANCE WITH STATE REQUIREMENTS. SPILL CLEAN UP MATERIALS MUST BE USED WHERE POSSIBLE.
H. THE CONTRACTOR MUST LIMIT VEHICLE AND EQUIPMENT WASHING TO A DEFINED AREA WHEN COMPLETED ON THE PROJECT SITE. RINSEOFF FROM THE WASHING AREA MUST BE CONTAINED IN A SEDIMENT BASIN OR OTHER SIMILARLY EFFECTIVE CONTAINMENT. WASTE FROM THE WASHING ACTIVITY MUST BE PROPERLY DISPOSED OF. THE CONTRACTOR MUST PROPERLY USE AND STORE SOAPS.
I. THE CONTRACTOR MUST PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OPERATIONS (CONCRETE, STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS, AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE PROJECT CONSTRUCTION. CONTAMINATED MATERIALS AND OTHER CONSTRUCTION MATERIALS AND THE CONTAINMENT MUST BE DISPOSED SO THAT IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH ALL MPCA RULES. A SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY THAT REQUIRES SITE PERSONNEL TO UTILIZE PROPER FACILITIES FOR DISPOSAL OF CONCRETE AND OTHER WASHOUT WASTES.

FINAL STABILIZATION:
THE CONTRACTOR SHALL ENSURE FINAL STABILIZATION OF THE SITE. FINAL STABILIZATION REQUIRES THE FOLLOWING:
A. SOIL DISTURBING ACTIVITIES ARE COMPLETE AND A UNIFORM PERMANENT VEGETATIVE COVER WITH A DENSITY OF 75% OVER THE ENTIRE PERVIOUS SURFACE HAS BEEN ACHIEVED, INCLUDING STABILIZATION OF ALL DITCHES AND SHOALS.
B. CONTRACTOR SHALL ENSURE THAT ALL PERMANENT STORMWATER TREATMENT SYSTEMS ARE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE NPDES PERMIT.
C. CONTRACTOR SHALL REMOVE ALL TEMPORARY SYNTHETIC AND STRUCTURAL BMPs.
D. CONTRACTOR SHALL REMOVE ALL SEDIMENTS FROM STORM WATER CONVEYANCES AND PERMANENT WATER QUALITY BASINS.

RECORD AVAILABILITY AND RETENTION:
-THE CONTRACTOR SHALL MAKE THE SWPPP, INCLUDING ALL CERTIFICATES, REPORTS, RECORDS, OR OTHER INFORMATION OF THE PERMIT, AVAILABLE TO FEDERAL, STATE, OR LOCAL OFFICIALS WITHIN 72 HOURS UPON REQUEST FOR THE DURATION OF THE PERMIT AND FOR THREE YEARS FOLLOWING THE SUBMITTAL OF THE NOTICE OF TERMINATION.
-THE CONTRACTOR SHALL MAKE THE RESPONSIBLE PERSON, TRAINED AS REQUIRED BY THIS PERMIT, AVAILABLE ON SITE WITHIN 72 HOURS WHEN REQUESTED BY THE MPCA FOR AN ON-SITE INSPECTION.

NOTIFICATION AND ENTRY:
-THE CONTRACTOR MUST ALLOW ACCESS AS REQUIRED BY STATE REGULATIONS FOR REPRESENTATIVES OF THE MPCA OR ANY MEMBER THEREOF WHEN AUTHORIZED BY IT, TO ENTER UPON THE PROJECT SITE FOR THE PURPOSE OF OBTAINING INFORMATION, EXAMINATION OF RECORDS, OR CONDUCTING SURVEYS OR INVESTIGATIONS.
NOTICE OF TERMINATION:
-PERMITS MUST BE SUBMITTED A NOTICE OF TERMINATION (NOT) WITHIN 30 DAYS IF ONE OR MORE OF THE FOLLOWING CONDITIONS HAVE BEEN MET:
1. FINAL STABILIZATION HAS BEEN ACHIEVED ON ALL PORTIONS OF THE SITE FOR WHICH PERMITS IS REQUIRED INCLUDING THE REMOVAL OF ALL TEMPORARY MEASURES SUCH AS SILT FENCE.
2. ANOTHER OWNER HAS ASSUMED CONTROL OVER ALL PORTIONS OF THE SITE THAT HAVE NOT ACHIEVED FINAL STABILIZATION.
CHANGE TO SWPPP:
-THE PERMITTEE MUST AMEND THE SWPPP AS NECESSARY TO INCLUDE ADDITIONAL REQUIREMENTS, SUCH AS ADDITION, OR MODIFIED BMPs, DESIGNED TO CORRECT PROBLEMS IDENTIFIED OF ADDRESS SITUATIONS WHENEVER:
1. THERE IS A CHANGE IN DESIGN, CONSTRUCTION, OPERATION OR MAINTENANCE, REPAIRS OR SEASONAL CONDITIONS THAT HAVE SIGNIFICANT EFFECT ON DISCHARGE. INSPECTION IS REQUIRED WITHIN 24 HOURS OF A RAINFALL EVENT GREATER THAN ONE-HALF INCH.
2. THE DESIGN OR OPERATION BY SITE OPERATORS, LOCAL, STATE OR FEDERAL OFFICIALS INDICATE THE SWPPP IS NOT EFFECTIVE.
3. THE SWPPP IS NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS ON THE SWPPP IS NOT CONSISTENT WITH THE TERMS AND CONDITIONS OF THIS PERMIT.
4. THE MPCA DETERMINES THAT DISCHARGE MAY CAUSE OR CONTRIBUTE TO NON-ATTAINMENT OF ANY APPLICABLE WATER QUALITY STANDARDS ON THE SWPPP DOES NOT INCORPORATE THE REQUIREMENTS RELATED TO AN APPROVED TOWN MAXIMUM DAILY LOAD (TMDL).
SOURCE ACKNOWLEDGMENT:
-THIS STORMWATER POLLUTION PREVENTION PLAN WAS PREPARED BY INDIVIDUAL(S) TRAINED IN ACCORDANCE WITH THE PERMIT'S TRAINING REQUIREMENTS FOR PREPARATION OF SWPPPS. INDIVIDUAL(S) PREPARING THIS SWPPP:
PREPARED BY: KIVON GARDNER, P.E.
KIVON GARDNER, P.E.
KIVON GARDNER AND ASSOCIATES
KIVONGARDNER@PDR.COM
7623-337-1211
TRAINING/CERTIFICATION:
DATE OF TRAINING/CERTIFICATION: SEPTEMBER 16-17, 2013
PROGRAM: UNIVERSITY OF WISCONSINA
DESIGN OF CONSTRUCTION SWPPP
AREAS HILLS, MN
INSTRUCTOR: JOHN CHAPMAN
CERTIFICATION EXPIRATION: 2017

NOTE: QUANTITIES ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL DETERMINE FOR THEMSELVES THE EXACT QUANTITIES FOR BIDDING AND CONSTRUCTION. CONTRACTOR SHALL NOT RELY ON THESE QUANTITIES FOR THEIR BID AND CIVIL ENGINEER IS NOT RESPONSIBLE FOR COST ESTIMATES OR ACTUAL CONSTRUCTION COSTS.

CONTACT INFORMATION	
OWNER & LONG TERM MAINTENANCE	
CONTACT PERSON:	
ADDRESS:	
TELEPHONE:	
EMAIL:	
CONTRACTOR	
BUSINESS:	X
CONTACT PERSON:	X
ADDRESS:	X
TELEPHONE:	X
DIAL:	X
CONTRACTOR'S ONSITE REPRESENTATIVE	
BUSINESS:	
CONTACT PERSON:	
ADDRESS:	
TELEPHONE:	
DIAL:	

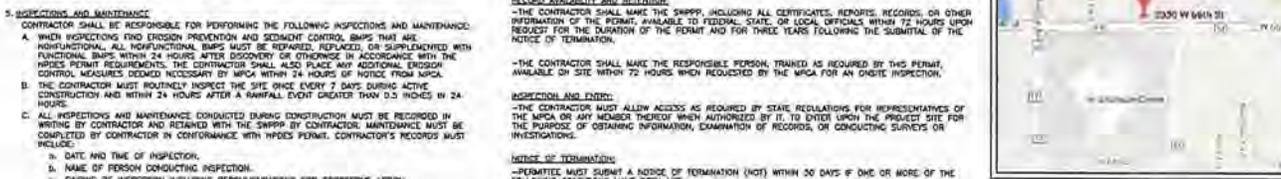
NOTE: CONTRACTOR'S ONSITE REPRESENTATIVE SHALL BE COMPLETED ON THE PLAN SET KEPT IN THE CONSTRUCTION TRAILER. THIS INFORMATION SHALL ALSO BE PROVIDED TO THE OWNER AND CIVIL ENGINEER.

URBAN AND RURAL
3330 60th St, West, Edina
910 NORTH DOTT STREET, GATE 1
MINNEAPOLIS, MN 55412
612-645-2828

CONSULTANT
PIERCE PINI &
ASSOCIATES
1000 WASHINGTON AVENUE, SUITE 100
MINNEAPOLIS, MN 55415
612-338-7300

PRELIMINARY
NOT FOR CONSTRUCTION

REVISIONS
7 Date Description



ACREAGE SUMMARY	
TOTAL SITE AREA	0.90 ACRES
EXISTING IMPERVIOUS AREA	0.87 ACRES
EXISTING PERVIOUS AREA	0.03 ACRES
PROPOSED IMPERVIOUS AREA	0.52 ACRES
PROPOSED PERVIOUS AREA	0.38 ACRES

SOIL ASSOCIATION HYDROLOGIC SOIL GROUP	
SILT SAND	B/C

ESTIMATED BMP QUANTITIES	
SILT FENCE	833 LF
TREE PROTECTION FENCING	100 LF
TIRED CONSTRUCTION ENTRANCE	75 FT
EROSION CONTROL BLANKET	400 SF
CONCRETE WASHOUT AREA	1 EACH
INLET PROTECTOR	1 EACH
SEED/SMULCH	0.4 ACRES

966 West Apartments
3330 60th St, West, Edina

STORMWATER POLLUTION PREVENTION PLAN

C30

DATE: 10/14/2013
PROJECT #:
PHASE:
DRAWN BY:
CHECKED BY:



801 NORTH THIRD STREET, SUITE 1
MINNEAPOLIS, MN 55417
912.465.3100

CONSULTANT
PIERCE PINI &
ASSOCIATES
3800 CENTRAL AVENUE, SUITE 200
MINNEAPOLIS, MN 55412
612.338.2200

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CONSTRUCTION

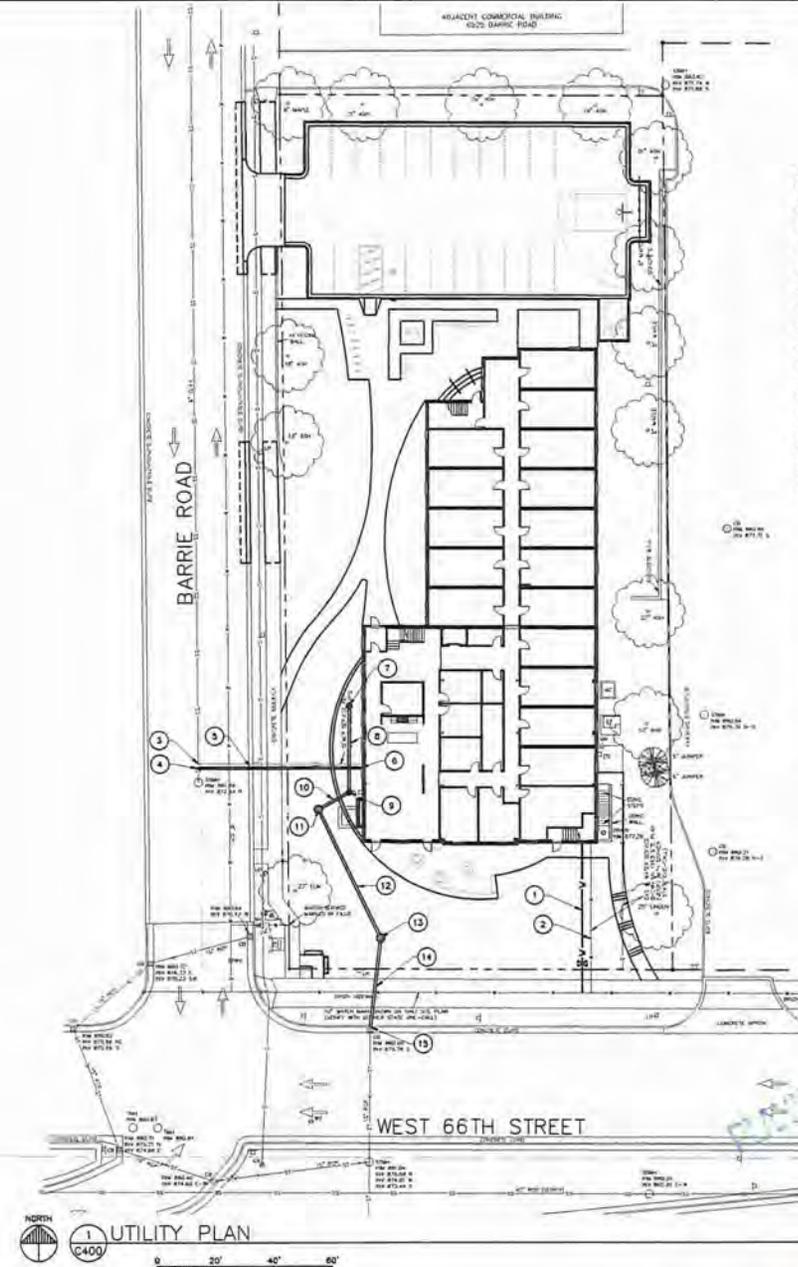
REVISIONS
Date Description

DATE 06.18.14
PROJECT # 14-
PHASE
DRAWN BY
CHECKED BY

UTILITY PLAN

C400

1. KEY TAP EXISTING 10" DIP WATERMAIN AND INSTALL NEW 6" DIP FIRE SERVICE AND POST INDICATOR VALVE, SEE DETAIL 6/C600.
2. EXISTING 2" COPPER WATER SERVICE PER ORIGINAL BUILDING PLANS, FIELD VERIFY EXACT LOCATION AND SIZE. EXISTING 2" SERVICE TO BE USED FOR DOMESTIC WATER SUPPLY. SEE MECHANICAL.
3. REMOVE EXISTING 4" SANITARY SERVICE AND INSTALL NEW 6" PVC SANITARY SERVICE
4. EXISTING 6" PVC INV = 872.72, NEW 6" PVC INV = 872.88
5. INSTALL 35 LF 6" PVC SDR-26 SANITARY SEWER AT 1.208
6. NEW 6" PVC SANITARY INV = 873.52, COORDINATE EXACT LOCATION AND INVERT WITH MECHANICAL
7. INSTALL ATRIUM DRAIN IN WINDOW WELL. RM = 878.5, INV = 877.00, SEE DETAIL 8/C600
8. INSTALL 29 LF 6" PVC STORM SEWER AT 1.008
9. INSTALL ATRIUM DRAIN IN WINDOW WELL. RM = 878.5, INV = 876.71, SEE DETAIL 9/C600
10. INSTALL 12 LF 6" PVC STORM SEWER AT 1.008
11. INSTALL 24" DIAMETER NYLOPLAST DRAIN BASIN WITH SOLID LID. RM = 884.2, INV = 878.59, SLUMP = 873.10. SEE SHEET C602.
12. INSTALL 49 LF 6" PVC STORM SEWER AT 1.008
13. INSTALL 24" DIAMETER NYLOPLAST DRAIN BASIN WITH SOLID LID. RM = 883.3, INV = 878.10, SLUMP = 873.10. SEE SHEET C602.
14. INSTALL 30 LF 6" PVC STORM SEWER AT 1.008. BORE DR JACK PIPE UNDER BRICK SIDEWALK. EXISTING SIDEWALK TO REMAIN OPEN DURING CONSTRUCTION.
15. CORE DRILL NEW NORTH INVERT INTO EXISTING CATCH BASIN. INV = 875.80.



NORTH
1
C400
UTILITY PLAN
0 20' 40' 60'
1"=20'

PRELIMINARY
CITY OF EDINA
JUN 20 2014

PRELIMINARY
NOT FOR CONSTRUCTION



501 NORTH THIRD STREET, SUITE 1
MINNEAPOLIS, MN 55401
612.468.3100

CONSULTANT
PIERCE PINI &
ASSOCIATES
ARCHITECTS AND PLANNERS
1100 CHRYSLER AVENUE, SUITE 200
MINNEAPOLIS, MN 55401
612.468.3100

PRELIMINARY
NOT FOR
CONSTRUCTION

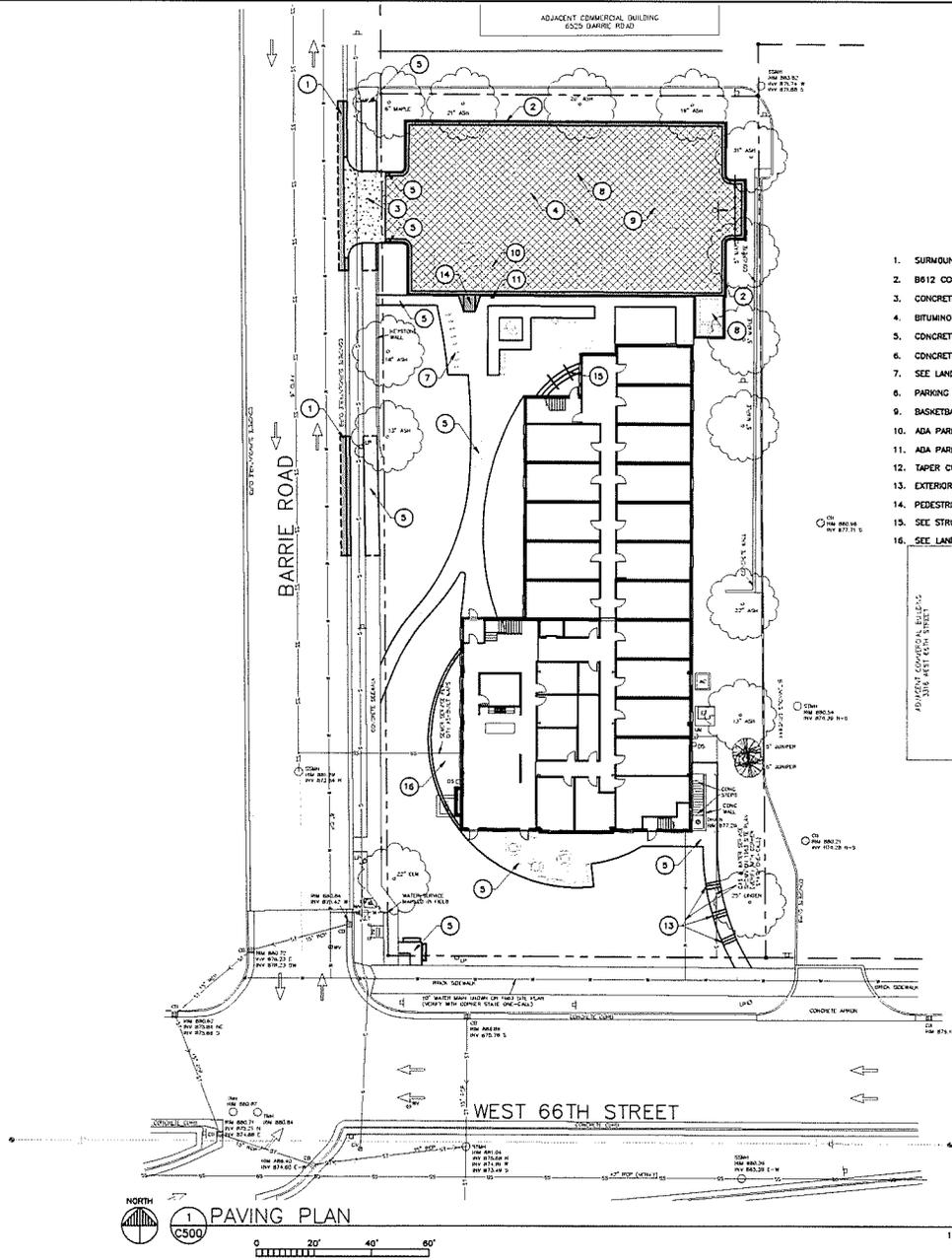
REVISIONS
Date Description

DATE 06.16.2
PROJECT # 14-
PHASE 1

DRAWN BY
CHECKED BY

PAVING PLAN

C500

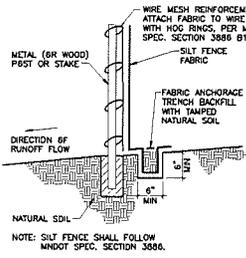


1. SURMOUNTABLE CURB, MATCH EXISTING STYLE AND ELEVATION WITHIN RIGHT-OF-WAY
2. B612 CONCRETE CURB AND GUTTER, SEE DETAIL 3/C601
3. CONCRETE DRIVEWAY APRON, SEE DETAIL 5/C600
4. BITUMINOUS PAVEMENT, SEE DETAIL 1/C600
5. CONCRETE SIDEWALK, SEE DETAIL 4/C600
6. CONCRETE PAVEMENT, SEE DETAIL 2/C600
7. SEE LANDSCAPING PLANS FOR BIKE RACK SLEEVE LOCATIONS
8. PARKING LOT STRIPING, 4" YELLOW EPOXY PAINT, TYPICAL
9. BASKETBALL COURT STRIPING, 4" WHITE EPOXY PAINT
10. ADA PARKING SYMBOL, SEE DETAIL 7/C600
11. ADA PARKING SIGN, SEE DETAIL 8/C600
12. TAPER CURB FLUSH WITH DRIVEWAY APRON
13. EXTERIOR STAIRS, SEE LANDSCAPING
14. PEDESTRIAN RAMP, SEE DETAIL 6/C600
15. SEE STRUCTURAL FOR STAIR DETAILS
16. SEE LANDSCAPING FOR WINDOW WELL MATERIAL

ADJACENT COMMERCIAL BUILDING
3330 WEST 66TH STREET

REVISIONS
JUN 20 2014
CITY OF EDINA

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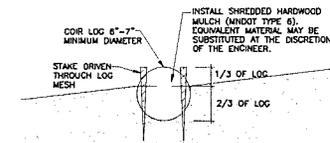
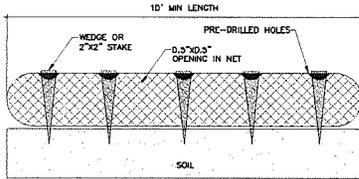
- DESIGN RECOMMENDATIONS**
1. SILT FENCES SHOULD BE INSTALLED ON THE CONTOUR (AS OPPOSED TO UP AND DOWN A HILL) AND CONSTRUCTED SO THAT FLOW CANNOT BYPASS THE ENDS.
 2. ENSURE THAT THE DRAINAGE AREA IS NOT GREATER THAN 1/4 ACRE PER 100 FT OF FENCE.
 3. MAKE THE FENCE STABLE FOR THE 10-YEAR PEAK STORM RUNOFF.
 4. WHERE ALL RUNOFF IS TO BE STORED BEHIND THE SILT FENCE, ENSURE THAT THE MAXIMUM SLOPE LENGTH BEHIND THE FENCE DOES NOT EXCEED THE SPECIFICATIONS SHOWN IN TABLE 1.

FIGURE 1 TYPICAL INSTALLATION FOR SILT FENCE

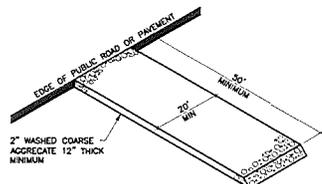
TABLE 1 MAXIMUM SLOPE LENGTH AND SLOPE FOR WHICH SILT FENCE IS APPLICABLE

SLOPE H:V	PERCENT	BY CALCULATION		BY ACCEPTED DESIGN PRACTICES
		SILT FENCE STORAGE EQUALS 2 FT FOR A 100-YEAR EVENT	SILT FENCE STORAGE EQUALS 3 FT FOR A 2-YEAR EVENT OR 3 FT FOR A 160-YEAR EVENT	
100:1	1%	400 FT	920 FT	160 FT
50:1	2%	200 FT	450 FT	75 FT
25:1	4%	106 FT	225 FT	75 FT
20:1	5%	80 FT	160 FT	75-50 FT
17:1	6%	67 FT	150 FT	50 FT
12.5:1	8%	50 FT	112 FT	50 FT
10:1	10%	40 FT	90 FT	50-25 FT
5:1	20%	26 FT	45 FT	25-15 FT
4:1	25%	16 FT	36 FT	15 FT
3:1	33%	12 FT	27 FT	15 FT
2:1	50%	8 FT	18 FT	15 FT

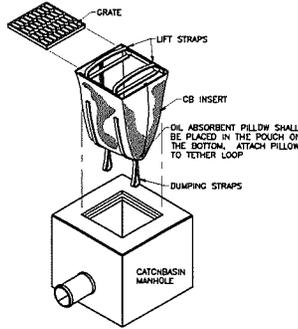
1 HEAVY DUTY SILT FENCE DETAIL NO SCALE



4 COIR LOG NO SCALE

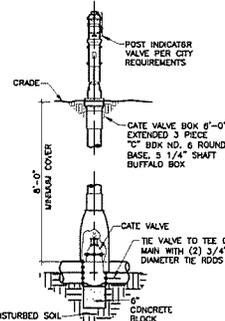


5 GRAVEL CONSTRUCTION ENTRANCE NO SCALE

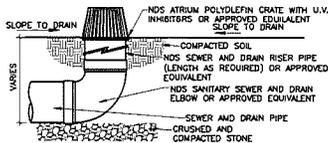


1. OIL ABSORBENT PILLOW SHALL BE REMOVED AND REPLACED WHEN NEAR SATURATION.
2. USE DANDY BAG, JR. AS MANUFACTURED BY DANDY PRODUCTS, INC.
3. AN EQUIVALENT CATCH-BASIN EROSION CONTROL INSERT METHOD OR PRODUCT MAY BE USED WITH PRIOR APPROVAL FROM ENGINEER.

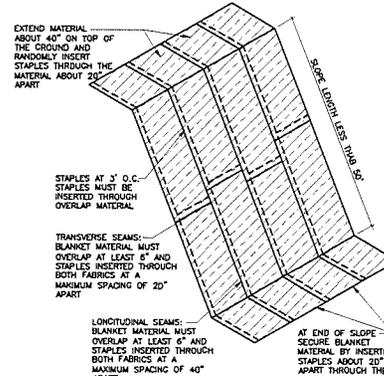
2 CB INSERT EROSION CONTROL NO SCALE



6 POST INDICATOR VALVE NO SCALE

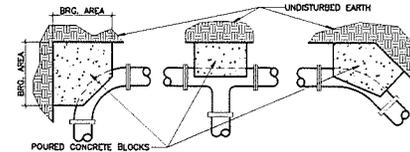


8 ATRIUM GRATE WITH ELBOW NO SCALE



1. EROSION CONTROL BLANKET TO BE CATEGORY 4-COCONUT 2S FOR SLOPES GREATER THAN 5:1 AND SIDES AND BOTTOM OF ALL DRAINAGE SWALES AND PONDING AREAS AND CATEGORY 2-STRAW 2S FOR ALL SLOPES LESS THAN 5:1 PER MNDOT SPEC. SECTION 3845.
2. INSTALL PER MNDOT SPEC. SECTION 2575

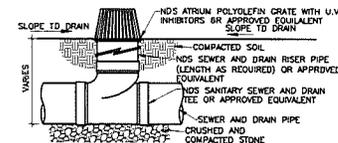
3 EROSION CONTROL BLANKET NO SCALE



BEARING AREA OF BLOCK IN SQ. FT.			
FITTING SIZE	TEE & END	90° BEND	45° BEND
3"	0.6	0.9	0.5
4"	1.1	1.5	0.8
6"	2.4	3.4	1.8
8"	4.3	6.1	3.3
10"	6.7	9.5	5.2
12"	9.7	13.7	7.4

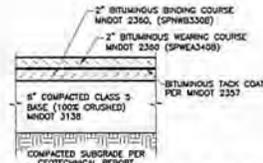
- NOTE: 1. CONTRACTOR MAY PROVIDE "MEGALUG" JOINT RESTRAINT AS AN ALTERNATIVE TO CONCRETE THRUST BLOCKS. SEE MINIMUM JOINTS EACH SIDE OF FITTING.

7 TYPICAL THRUST BLOCK DETAIL NO SCALE

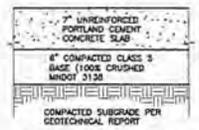


9 ATRIUM GRATE WITH TEE NO SCALE

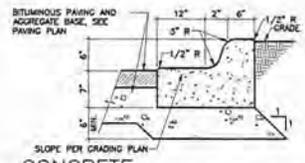
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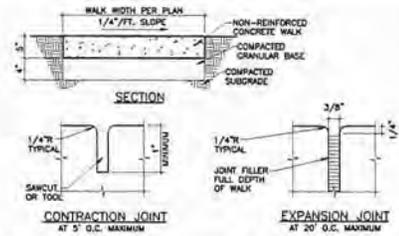
1 BITUMINOUS PAVEMENT DETAIL
C601 NO SCALE



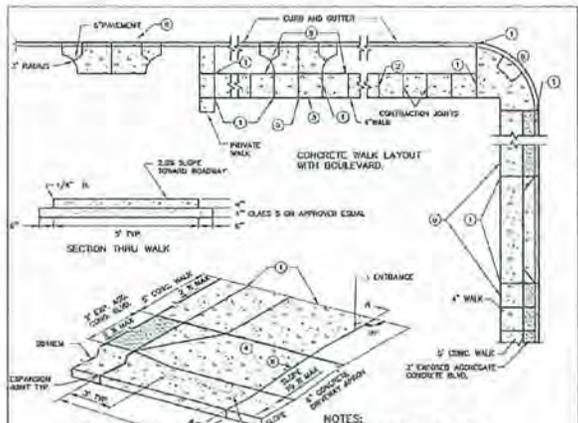
2 CONCRETE PAVEMENT
C601 NO SCALE



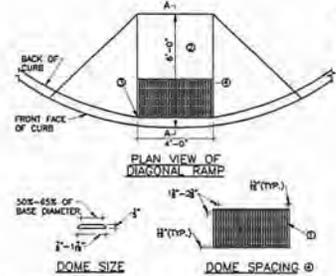
3 CONCRETE CURB & GUTTER (B612)
C601 NO SCALE



4 CONCRETE WALK & JOINT DETAILS
C601 NO SCALE



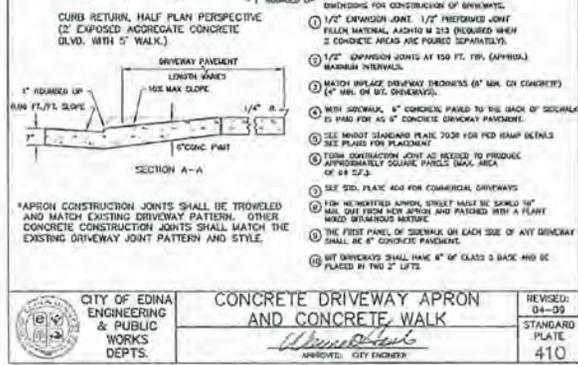
6 PEDESTRIAN CURB RAMP
C601 NO SCALE



- NOTE:
TO COMPLY WITH THE AMERICANS DISABILITIES ACT (ADA), ALL STATE AND LOCAL AGENCIES ARE REQUIRED TO COMPLY WITH THIS STANDARD PLATE.
- 1/4" INCH PERFORMED JOINT FILLER MATERIAL, MANDOT M213.
 - WHEN POSSIBLE, PROVIDE A CLEAR PATH OF TRAVEL 4'-2" WIDE BEHIND THE PEDESTRIAN RAMP. A RELATIVELY FLAT 4' x 8' WILL ALLOW WHEELCHAIRS TO NAVIGATE AROUND THE PEDESTRIAN RAMP. NO SIGNALS, SIGNS, CABINETS, OR OTHER OBSTRUCTIONS ARE ALLOWED IN THE PATH OF TRAVEL.
 - PLACE THE DETECTABLE WARNING (TRUNCATED DOME) AT THE BACK OF CURB. WHEN THE DETECTABLE SHALL BE HAND FORMED TO FILL THE GAP.
 - ADA REQUIRED TRUNCATED DOME AREA SHALL BE 2'-0" MIN. IN DIRECTION OF TRAVEL, AND SHALL CONTRAST VISUALLY WITH THE ADJACENT WALKING SURFACE. THE ENTIRE TRUNCATED DOME AREA SHALL BE A LIGHT (GRAY OR BUFF) COLOR WHEN THE ADJACENT SIDEWALK IS A DARK COLOR (RED OR DARK GRAY) WHEN THE ADJACENT SIDEWALK IS A LIGHT GRAY/CREAM COLOR.

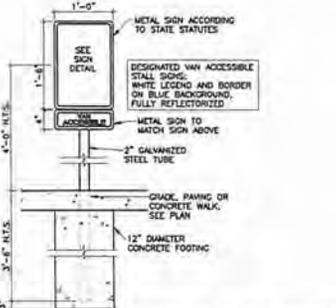


7 HC PARKING SYMBOL
C601 NO SCALE

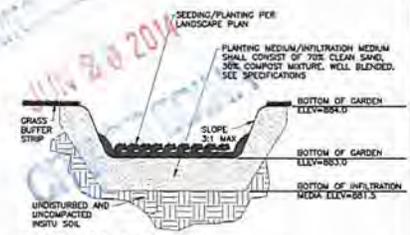
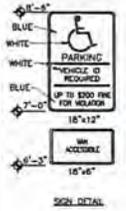


5 CONCRETE DRIVEWAY APRON
C601 NO SCALE

CITY OF EDINA
ENGINEERING & PUBLIC WORKS DEPTS.
REVISION: 04-99
STANDARD PLATE 410
APPROVED: CITY ENGINEER

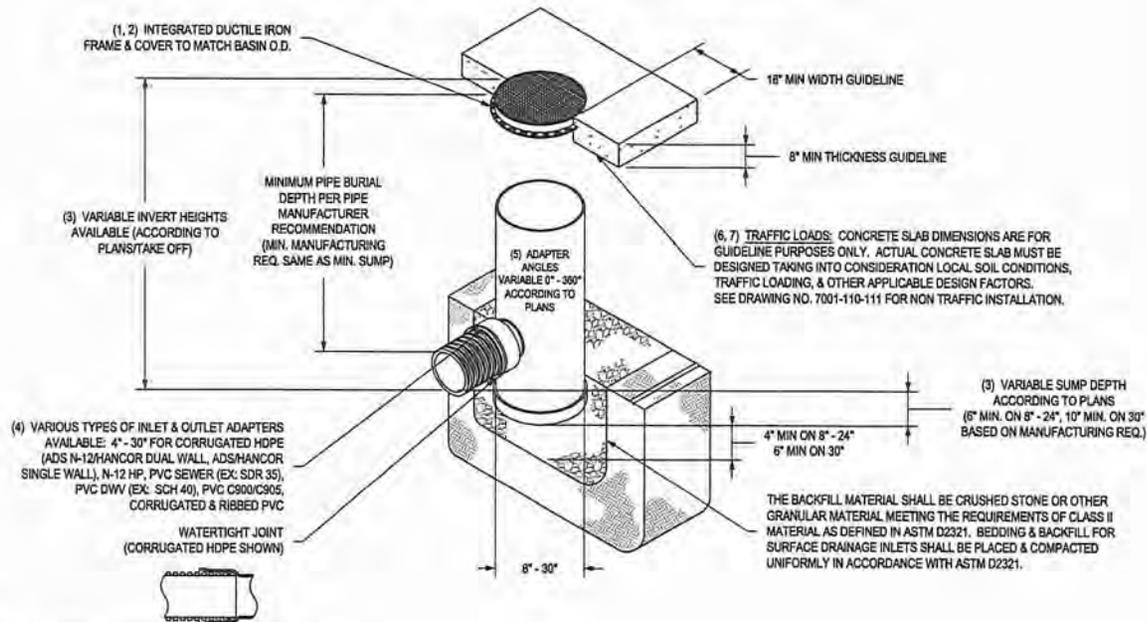


8 ACCESSIBLE PARKING SIGN DETAIL
C601 NO SCALE



9 RAIN GARDEN
C601 NO SCALE

NYLOPLAST DRAIN BASIN WITH SOLID COVER



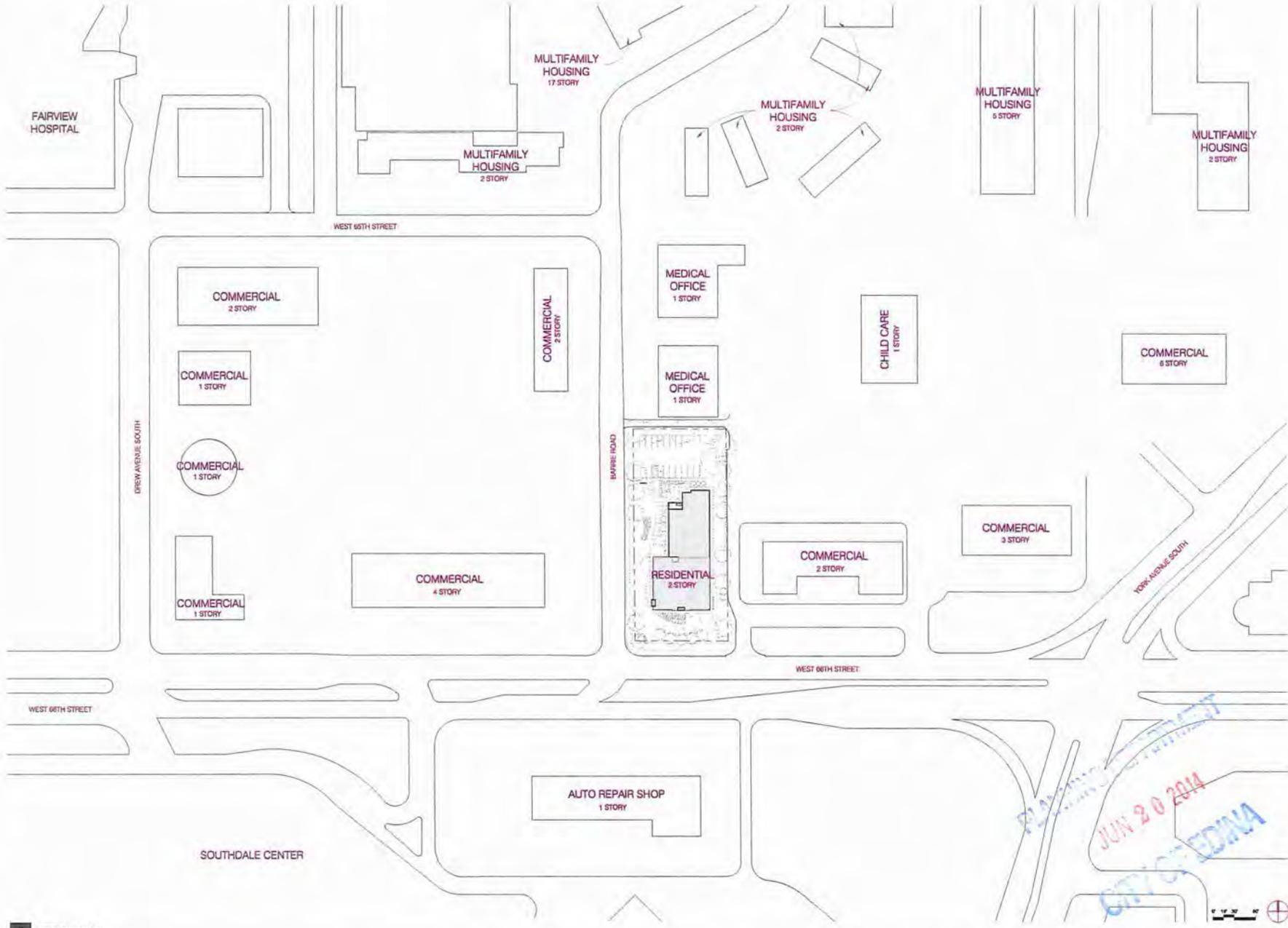
- 1 - 8" - 30" SOLID COVERS SHALL BE DUCTILE IRON PER ASTM A538 GRADE 70-50-05.
- 2 - 12" - 30" FRAMES SHALL BE DUCTILE IRON PER ASTM A538 GRADE 70-50-05. 8" & 10" SOLID COVERS FIT DIRECTLY ONTO DRAIN BASINS WITH THE USE OF A PVC BODY TOP. SEE DRAWING NO. 7001-110-045.
- 3 - DRAIN BASIN TO BE CUSTOM MANUFACTURED ACCORDING TO PLAN DETAILS. RISERS ARE NEEDED FOR BASINS OVER 84" DUE TO SHIPPING RESTRICTIONS. SEE DRAWING NO. 7001-110-085.
- 4 - DRAINAGE CONNECTION STUB JOINT TIGHTNESS SHALL CONFORM TO ASTM D3212 FOR CORRUGATED HDPE (ADS N-12/HANCOR DUAL WALL, N-12 HP, & PVC SEWER (4" - 24").
- 5 - ADAPTERS CAN BE MOUNTED ON ANY ANGLE 0° TO 360°. TO DETERMINE MINIMUM ANGLE BETWEEN ADAPTERS SEE DRAWING NO. 7001-110-012.
- 6 - 12" - 30" SOLID COVERS SHALL MEET H-20 LOAD RATING.
- 7 - 8" & 10" SOLID COVERS ARE RATED FOR LIGHT DUTY APPLICATIONS ONLY; NO CONCRETE COLLAR NEEDED FOR LIGHT DUTY RATING.

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DRAWN BY	EDC	MATERIAL	
DATE	8-14-07		
REVISED BY	EDC	PROJECT NO./NAME	
DATE	3-14-10		
DWG SIZE	A	SCALE	1:40 SHEET 1 OF 1

Nyloplast		3130 VERONA AVE BUNFORD, GA 30018 PHN (770) 832-2443 FAX (770) 832-2480 www.nyloplast-us.com
TITLE		DRAIN BASIN WITH SOLID COVER QUICK SPEC INSTALLATION DETAIL
DWG NO.	7001-110-298	REV B

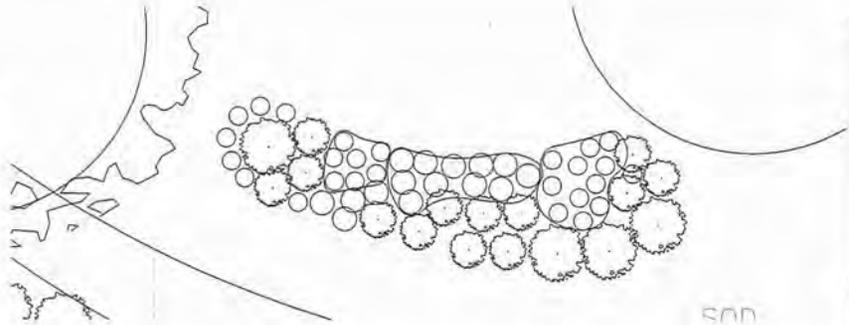
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NOT FOR CONSTRUCTION



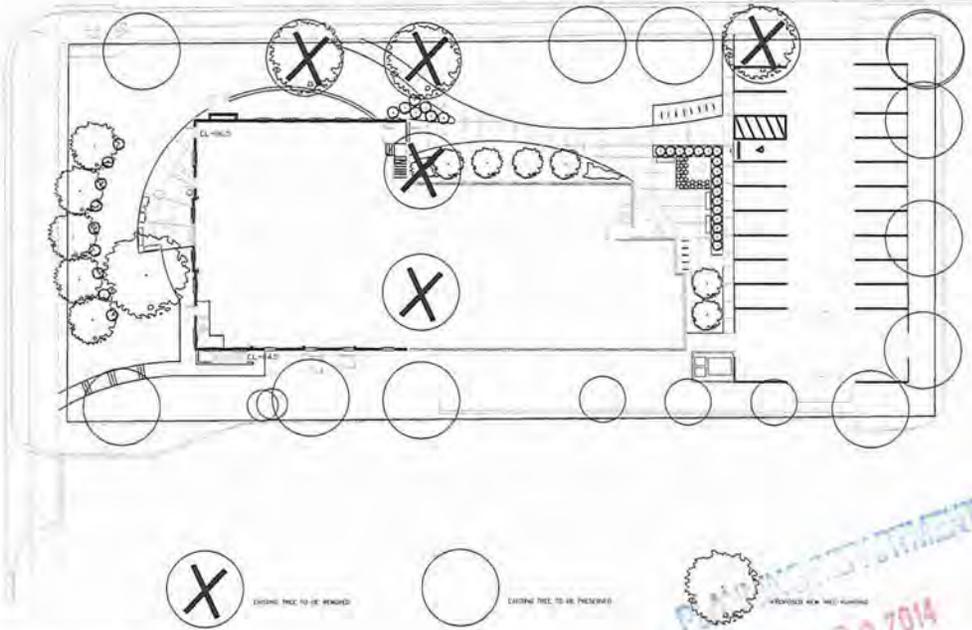
A32

L-301
RAIN GARDEN (SHADE) PLANTING SCHEDULE

NO.	PLANT	QTY	REMARKS
1	ARBOREAL (SHADE)	1	
2	SHRUB	1	
3	PERENNIAL	1	
4	ANNUAL	1	
5	GRASS	1	
6	GROUND COVER	1	
7	WATER PLANT	1	
8	ROCK	1	
9	WOOD CHIP	1	
10	COMPOST	1	
11	SOIL	1	



1 RAIN GARDEN PLANTING DETAIL
1" = 10'-0"



2 REMOVALS/PRESERVATION PLAN
1" = 20'-0"

66 West
Apartments
3330 66th St. West, Edina



801 NORTH THIRD STREET, SUITE 145
MINNEAPOLIS, MN 55401
612.463.3100



A35

REVISIONS
Date Description

DATE: 08.16.2014
PROJECT #: 24-0218
PHASE: SITE PLAN REVIEW
DRAWN BY: JG
CHECKED BY: SP

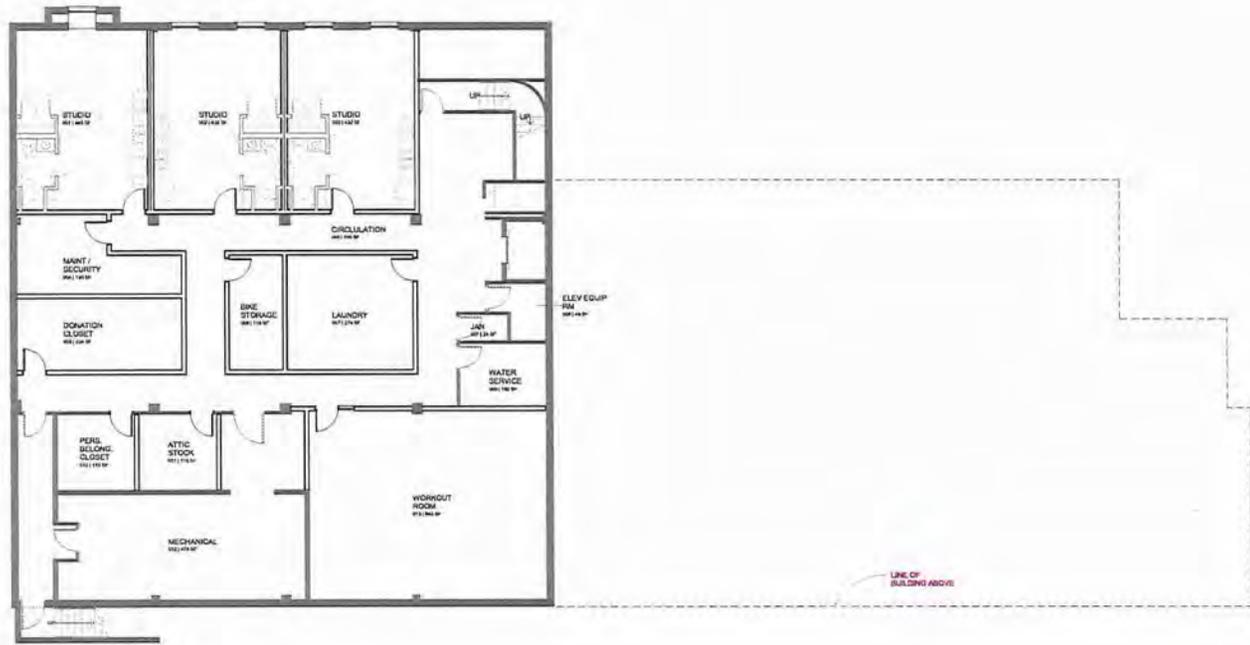
PLANT REMOVAL PRESERVATION PLAN
AUG 20 2014

PLANT REMOVAL
PRESERVATION
PLAN

L301

GENERAL NOTES:

1. DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
2. DIMENSIONS AT INTERIOR WALLS ARE TO CENTERLINE OF WALL
3. DIMENSIONS AT UNIT DECKING ARE TO CENTERLINE OF WALLS
4. EXTEND ALL SWAITS TO UNDERSIDE OF ADJAC SHEATHING ON TOP OF FLOOR FINISH FACING WHERE APPLICABLE
5. ALL RESIDENTIAL COMMON WALLS ARE TYPE I/II VOA AND 18" W/1" FINE RAFTERS WITH ACUSTIC RATING
6. ALL RESIDENTIAL COMMON WALLS ARE TYPE III 1 1/2" FINE RAFTERS WITH ACUSTIC RATING UNLESS OTHERWISE NOTED
7. ALL UNITS ARE TYPE II ACCESSIBLE UNLESS OTHERWISE NOTED
8. HANDICAP 8' 0" MIN. AND 8' 0" MIN. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLANS. ALL OTHER PARKING AREAS SHALL PROVIDE 7' 0" MIN. CLEAR HEIGHT
9. DIMENSIONS 6' 0" ARE TO FACE OF GRA



66 NORTH THIRD STREET, SUITE
MINNEAPOLIS, MN 55401
612.461.2100

CONSULTANT

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REVISIONS
Date Description

DATE 06.19.21
PROJECT # 3448
PHASE SITE PLAN REV

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PLANNING DEPARTMENT
JUN 20 2014
CITY OF EDINA

SUBLEVEL
PLAN

A001

137



801 NORTH THIRD STREET, SUITE
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612.468.3100

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Date Description

DATE 06.19.21
PROJECT # 14-01
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LEVEL 1 PLAN

A101

GENERAL NOTES:

1. DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
2. DIMENSIONS AT INTERIOR WALLS ARE TO CENTERLINE OF WALL
3. DIMENSIONS AT UNIT DIMENSIONS ARE TO CENTERLINE OF WALLS
4. EXTEND ALL SWAPTS TO UNDERSIDE OF HOOD SHEATHING ON TOP OF ROOF PENETRATIONS WHERE APPLICABLE
5. ALL RESIDENTIAL EXTERIOR WALLS ARE TYPE IV, V, VI, AND VI R/F FIVE RATING WITH ACOUSTIC RATING
6. ALL RESIDENTIAL CORRIDOR WALLS ARE TYPE VII, I R/F FIVE RATING WITH ACOUSTIC RATING - UNLESS OTHERWISE NOTED
7. ALL UNITS ARE TYPE II-A RESIDENTIAL UNLESS OTHERWISE NOTED
8. MAINTAIN 6" MIN. AND 8" MAX. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLAN. ALL OTHER PARKING AREAS SHALL PROVIDE 7' MIN. CLEAR HEIGHT
9. DIMENSIONS @ DMU ARE TO FACE OF DMU



PLANNING DEPARTMENT
JUN 28 2014
CITY OF EDINA

AS2



80 NORTH THIRD STREET, SUITE 1
INDIANAPOLIS, IN 46201
812.463.3320

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REVISIONS
Date Description

DATE 9/15/12
PROJECT # 14-6
PHASE SITE PLAN/REV
DRAWN BY
CHECKED BY

LEVEL 2 PLAN

A102

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GENERAL NOTES:

1. DIMENSIONS AT EXTENSION WALLS ARE TO OUTSIDE FACE OF BRICKWORK
2. DIMENSIONS AT INTERIOR WALLS ARE TO CENTERLINE OF WALL
3. DIMENSIONS AT UNIT CORNERS ARE TO CENTERLINE OF WALLS
4. EXCEED ALL SPACINGS TO JOISTS, OR JOIST SPACING ON TOP OF FLOOR FINISHES WHERE APPLICABLE
5. ALL FINISHES TO BE ACCORDING TO THE 2012 U.S. GBCI GREEN SOURCE GUIDE
6. ALL INTERIOR COMMON WALLS ARE TYPE WA, 1 1/2" PRC
7. ALL INTERIOR TYPE II ACCESSIBLE WALLS OTHER THAN TYPE WA, 1 1/2" PRC
8. ALL INTERIOR TYPE II ACCESSIBLE WALLS OTHER THAN TYPE WA, 1 1/2" PRC
9. ALL INTERIOR TYPE II ACCESSIBLE WALLS OTHER THAN TYPE WA, 1 1/2" PRC
10. DIMENSIONS IN ON-LINE TO FACE OF DIM.



APPROVED FOR PERMIT
 JULY 20 2014
 METROPOLITAN ENGINEERING CONSULTANTS

A39



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MINNEAPOLIS, MN 55411
612.464.3100

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# Date Description	

DATE: 06.19.13
PROJECT #: 24-0
PHASE: SITE PLAN 100
DRAWN BY:
CHECKED BY:

ENLARGED
UNIT PLANS

A151

GENERAL NOTES:

1. REFER TO IRISH FOR TYPICAL ACCESSIBLE CODES AND CLEARANCES REQUIRED
2. INSTALL OVER-SHOWER BLOORING AT ALL UNITS REFER TO SHEET IRISH FOR DETAILS
3. WITHIN UNITS, ALL WALLS ARE 1/2" MIN UNLESS NOTED OTHERWISE
4. HVAC RETURN LOCATED IN WALL (REFER TO MECHANICAL FOR DETAIL)
5. NO PENETRATIONS OR OPENINGS ALLOWED IN SHIRT WALLS
6. PROVIDE FLOORING AND BASE AT REFRIGERATOR AND PANIC LOCATIONS
7. CRYSTALLINE BOARD SOFT AT 8" UNLESS NOTED OTHERWISE
8. PROVIDE NON-COMBUSTIBLE MATERIALS IN RETURN PLUMBING (WALLS AND SOFFIT)
9. REFER TO IRISH FOR ALL MOUNTING HEIGHTS (LIGHT SWITCHES, OUTLETS, THERMOSTATS, DIMENSIONAL CONTROLS, SENSITIVE ITEMS, ETC.). NOTE REQUIRED VARIATIONS FOR ACCESSIBLE AND TYPE 'X' UNITS
10. NOTE THAT DIM. ELEVATIONS MAY BE REQUIRED (SCHEDULED FROM ENLARGED UNIT ELEVATIONS SHEET). REFER TO ACTUAL UNIT PLANS FOR LOCATIONS
11. ADDITIONAL INSULATION IN WALLS AT BATHROOM, LAUNDRY, BEDROOMS AND IN ANY FLOORING WALL THAT IS ADJACENT TO ADJUT FLOOR OR MECHANICAL UNIT
12. DIMENSIONS AT PARTY WALLS ARE TO CENTERLINE, DIMENSIONS WITHIN UNITS ARE TO FACE OF WALLS UNLESS OTHERWISE NOTED

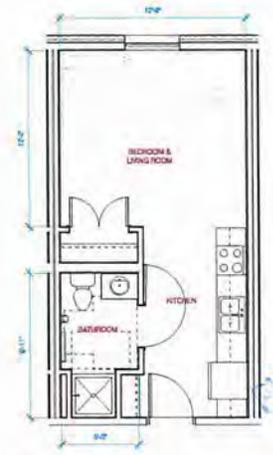


2 TYPE 'A' UNIT
ASS1 1/4" = 1'-0"

TYPE 'A' UNIT NOTES:

1. PROVIDE BLOORING FOR OVER-SHOWER IN ALL TYPE 'A' UNITS
2. PROVIDE AND INSTALL SHOWER DRIP FLOOR-UNDER SEALS WALL TYPE 'X' UNITS
3. PROVIDE TOILET TANK FRESH COILING, ON SIDE AWAY FROM WALL, TYP
4. AT ROLL UNDER SINKS WITH FRONT APPROACH, INSULATE PIPES. OPTION TO RELOCATE SINK TRAP IN AREA BEYOND REQUIRED MIN. CLEARANCES
5. PROVIDE FLOORING AND BASE AT ROLL-UNDER SINKS
6. PROVIDE SWITCH FOR KITCHEN EMERGENCY IN FACE OF CABINET

KEYNOTES:



1 TYPICAL UNIT
ASS1 1/4" = 1'-0"

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JUN 20 2014
CITY OF EDINA

A40



801 NORTH THIRD STREET, SUITE
MINNEAPOLIS, MN 55401
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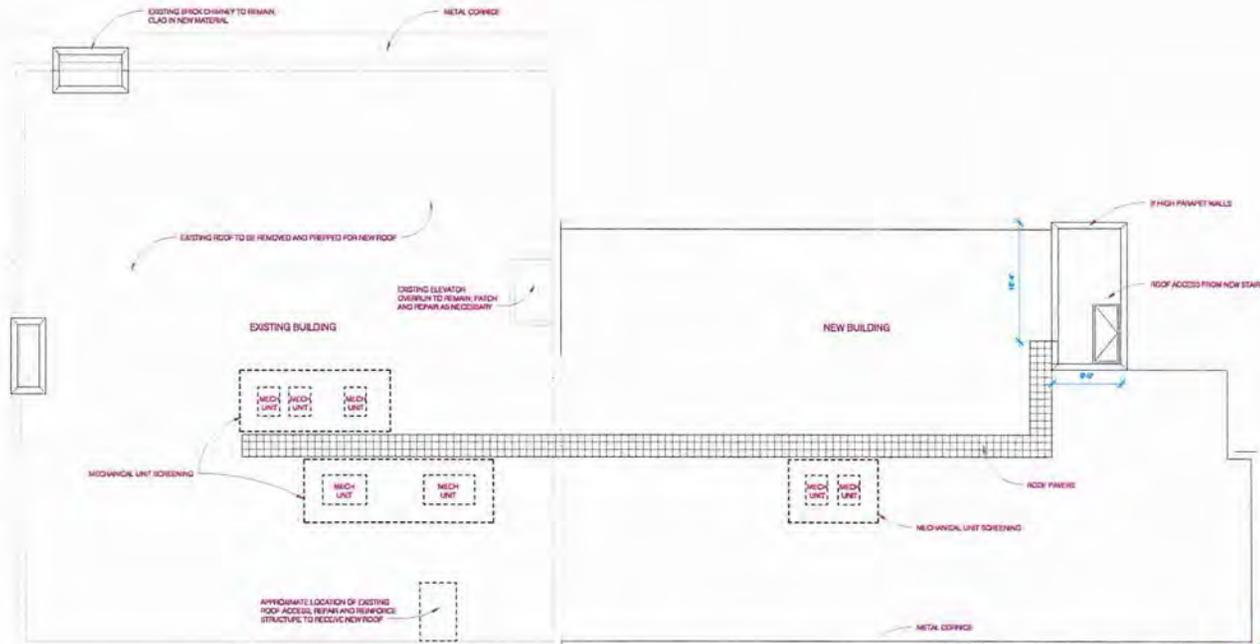
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DATE 06.19.21
PROJECT # 14-01
PHASE SITE PLAN RDR

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ROOF PLAN

A201



PLANNING DEPARTMENT
JUN 20 2014
CITY OF EDINA



ALUMINUM TRELLIS



VINYL DOUBLEHUNG WINDOW



EXISTING BRICK



DARK BRICK



METAL PANEL - VERTICAL



METAL PANEL - HORIZONTAL



66 West
Apartments
3330 66th St. West, Edina

A-41

URBANWORKS
ARCHITECTURE

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612.466.5100

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Date Description

DATE 06.18.21
PROJECT # 14-01
PHASE SITE PLAN ADD

DRAWN BY
CHECKED BY

EXTERIOR
BUILDING
MATERIALS

A300

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2 SOUTH ELEVATION
A301 12" = 1'-0"



1 WEST ELEVATION
A301 12" = 1'-0"

66 West Apartments

3330 66th St. West, Edina

A42



801 NORTH THIRD STREET, SUITE 100
MINNEAPOLIS, MN 55401
612.486.3100

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DATE 06.19.21
PROJECT # 1408
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BUILDING ELEVATIONS

A301

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2 NORTH ELEVATION
ASR 1/2" = 1/8"



1 EAST ELEVATION
ASR 1/2" = 1/8"

66 West Apartments
 3330 66th St. West, Edina

A43



801 NORTH THIRD STREET, SUITE 100
 MINNEAPOLIS, MN 55401
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 PHASE SITE PLAN REV

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BUILDING ELEVATIONS

A302

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PLANNING
 JUN 20 2014
 ARCHITECTURE

66 West
Apartments
3330 66th St. West, Edina

A351



201 NORTH THIRD STREET, SUITE 1
MINNEAPOLIS, MN 55401
912.456.3100

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Date Description

DATE 06.19.21
PROJECT # 14-01
PHASE SITE PLAN REV 01
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BUILDING
PERSPECTIVE

A351

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1 BUILDING PERSPECTIVE FROM NORTH
A351 | 12" = 1'-0"

PRELIMINARY
JUN 23 2014



DATE: July 15, 2014
TO: Cary Teague – Planning Director
CC: Chad Millner – City Engineer
FROM: Ross Bintner P.E. - Environmental Engineer
RE: **3330 66th Street West – Development Review**

The Engineering Department has reviewed the subject property for street and utility connections, grading, storm water, erosion and sediment control.

1. City Standard Plates available here: http://edinamn.gov/index.php?section=construction_standards
2. A separate permit is required from Nine Mile Creek Watershed District: www.ninemilecreek.org

Survey

3. No comments.

Soils

4. Submit soils, soil boring and geotechnical report.

Details

5. No comments

Traffic and Street

6. A traffic study has been reviewed and shows no undue burden on the transportation network.
7. Show replacement of brick sidewalk with salvaged or like for utility service crossing location.
8. Show replacement of concrete sidewalk with like for utility service crossing location.
9. Commercial entrance should follow standard plate 400 and 410.

Sanitary and Water Utilities

10. Verify location, and remove moribund water service on southwest property corner to main if it exists.

Storm Water Utility

11. Provide hydraulic and hydrology calculations that meet Nine Mile Creek Watershed District standards.
Capacity is available in public stormwater system from NC_III subwatershed, downstream of project.
12. Provide copies of maintenance agreement for private stormwater systems.
13. A revised SAC unit determination will be required at building permit application.
14. Provide drainage outlet from raingarden.

Grading, Erosion and Sediment Control

15. No comments.



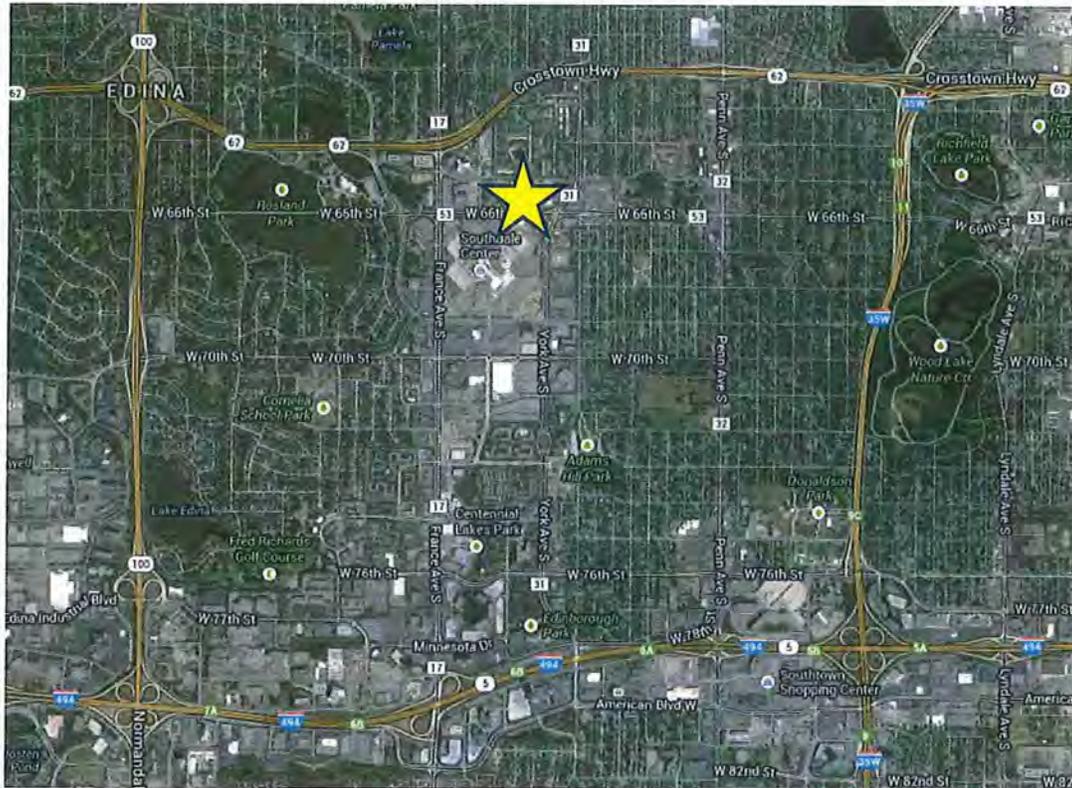
Other Agency Coordination

16. Nine Mile Creek Watershed permit is required. MDH, MPCA and MCES permits may be required.

ENGINEERING DEPARTMENT

7450 Metro Boulevard • Edina, Minnesota 55439
www.EdinaMN.gov • 952-826-0371 • Fax 952-826-0392

147



Traffic Impact Study

66 West Apartments Edina, MN

I hereby certify that this report was prepared by me or under my direct supervision, and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

By:

Michael P. Spack

Michael P. Spack, P.E., P.T.O.E.

License No. 40936

Date: July 11, 2014

Executive Summary

Background: Beacon Interfaith Housing Collaborative (Beacon) is proposing to develop a 39 unit apartment building at the site of an existing TCF Bank located northeast of the 66th Street/Barrie Road intersection in Edina, MN. The purpose of this study is to determine if transportation improvements are needed to accommodate the proposed building and to ensure there will be adequate parking available on site after the apartment building is fully occupied.

Results: The traffic impacts of the proposed apartment building on the study intersections were analyzed in the 2015 build-out conditions. The principal findings are:

- i. The forecast traffic from the proposed development will have little impact on the operations of the study intersections.
- ii. All study intersections will operate acceptably through the 2015 build-out scenarios.
- iii. The proposed 19 unit parking lot is forecast to be adequate for the 66 West Apartment building.

Recommendations: Other than the proposed changes of closing the east leg of the existing site southern driveway and converting the northern driveway to a full access intersection, no modifications are needed to be made by the developer to the study intersections.

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1. Introduction

a. Purpose of Study

Beacon Interfaith Housing Collaborative (Beacon) is proposing to develop a 39 unit apartment building at the site of an existing TCF Bank located northeast of the 66th Street/Barrie Road intersection in Edina, MN. The purpose of this study is to determine if transportation improvements are needed to accommodate the proposed building and to ensure there will be adequate parking available on site after the apartment building is fully occupied.

b. Study Objectives

The objectives of this study are:

- i. Document how the study intersections currently operate.
- ii. Forecast the amount of traffic expected to be generated by the proposed development.
- iii. Determine how the study intersections will operate in the year 2015 with development traffic.
- iv. Determine if there will be adequate parking on site when the apartment is fully occupied.
- v. Recommend improvements, if needed.

The study intersections are:

- i. 66th Street/Barrie Road
- ii. Barrie Road/Existing Southern Site Access
- iii. Barrie Road/Existing Northern Site Access

2. Proposed Development

a. Site Location

The site is located north of the Southdale Shopping Center on the northeast corner of the 66th Street/Barrie Road intersection in Edina, MN (see Figure 1 in the Appendix).

b. Land Use Intensity and Development Timing

The proposed site will have 39 apartment units. The site is currently being used as a TCF Bank with a drive through. The existing bank building will be remodeled and an addition will be constructed to make up the apartment building. Site access will be via the Barrie Road/Existing Northern Site Access intersection. The Existing Southern Site Access east leg will be removed as part of the development.

The site is proposed to have a 19 stall parking lot. A conceptual site plan is shown in Figure 2 in the Appendix.

For the purposes of this study, the development is anticipated to be built and fully occupied by the year 2015.

3. Analysis of Existing Traffic Conditions

a. Transportation Network Characteristics

66th Street West is also Hennepin County State Aid Highway 53. It is a divided road with five lanes (three westbound and two eastbound) and a 35 mph speed limit near the site. According to MnDOT it has an average of 16,000 vehicles per day using it near the site.

Barrie Road is a local Edina road. It is a two lane, undivided road with a 30 mph speed limit near the site.

All of the study intersections are two-way stop controlled with stop signs on the minor approaches. The 66th Street/Barrie Road intersection is a $\frac{3}{4}$ intersection restricting vehicles from making left turns or through movements from Barrie Road. Existing traffic control and travel lanes are shown in Figure 3 in the Appendix for each study intersection.

b. Traffic Volumes

Intersection video was collected at each of the study intersections under normal weekday conditions in June 2014 when there was clear weather. Using these videos, turning movement counts were collected from 6:30 to 9:30 a.m. and from 3:30 to 6:30 p.m. at the three existing study intersections. The peak hours for each intersection were found to be:

- 66th Street/Barrie Road: 7:30 to 8:30 a.m. and 4:15 to 5:15 p.m.
- Barrie Road/Existing Southern Site Access: 8:30 to 9:30 a.m. and 4:30 to 5:30 p.m.
- Barrie Road/Existing Northern Site Access: 8:30 to 9:30 a.m. and 4:30 to 5:30 p.m.

The turning movement count data from the counts are contained in fifteen minute intervals in the Appendix.

c. Level of Service



Source: City of San Jose, CA

An intersection capacity analysis was conducted for the existing intersections per the *Highway Capacity Manual, 2010*. Intersections are assigned a "Level of Service" letter grade for the peak hour of traffic based on the number of lanes at the intersection, traffic volumes, and traffic control. Level of Service A (LOS A) represents light traffic flow (free flow conditions) while Level of Service F (LOS F) represents heavy traffic flow (over capacity conditions). LOS D at intersections is typically considered acceptable in the Twin Cities region. Individual movements are also assigned LOS grades. One or more individual movements typically operate at LOS F when the overall intersection is operating acceptably at LOS D. The pictures on the left represent some of the LOS grades (from a signal controlled intersection in San Jose, CA). These LOS grades represent the overall intersection operation, not individual movements.

The LOS results for the existing study hours are shown in Table 1. These are based on the existing traffic control and lane configurations as shown in Figure 3 in the Appendix. The existing turning movement volumes from the Appendix were used in the LOS calculations. The LOS calculations were done in accordance with the *Highway Capacity Manual 2010* using VISTRO™ software. The complete LOS calculations, which include grades for individual movements, are included in the Appendix. The study intersections currently operate acceptably at LOS A or better with all movements operating at LOS C or better.

Table 1 – Existing Peak Hour Level of Service (LOS)¹

Intersection	A.M. Peak	P.M. Peak
66 th St/Barrie Rd	A (c)	A (b)
Barrie Rd/Existing Southern Site Access	A (b)	A (b)
Barrie Rd/Existing Northern Site Access	A (a)	A (b)

¹The first letter is the Level of Service for the intersection. The second letter (in parentheses) is the Level of Service for the worst operating movement.

4. Projected Traffic

a. Site Traffic Forecasting

A trip generation analysis was performed for the development site based on the methods and rates published in the *ITE Trip Generation Manual, 9th*

Edition. Based on Land Use Code 220, the 39 unit apartment building will generate:

- 130 vehicles entering and 130 vehicles exiting the subdivision per day
- 4 vehicles entering and 16 vehicles exiting the subdivision in the a.m. peak hour
- 16 vehicles entering and 8 vehicles exiting the subdivision in the p.m. peak hour

A trip distribution pattern was developed for the generated traffic to and from the site. This pattern is based on existing traffic counts as well as taking into account site access and access to the regional transportation system. The trip distribution pattern is:

- 35% to the west on 66th Street
- 25% from the west on 66th Street
- 65% to the north on Barrie Road
- 35% from the north on Barrie Road
- 40% from the east on 66th Street

The traffic generated by the site development was assigned to the area roadways per this distribution pattern.

Since the 66 West Apartments will be taking over the site of the existing TCF Bank, the existing traffic to and from the bank was deleted from the network for the future Build scenarios.

b. Non-site Traffic Forecasting

Since the site is expected to be built and fully occupied in 2015, a background growth rate was applied to the existing traffic volumes to represent future traffic. The MnDOT State Aid office has a 20 year growth rate projection for Hennepin County of 10%. This means that 20 years from now, MnDOT projects traffic in the area will be 10% higher than current volumes. This leads to an annual growth rate of 0.5%. This growth rate of 0.5% was applied to existing traffic on the network.

c. Total Traffic

Traffic forecasts were developed for the year 2015 Build Scenarios by adding the traffic generated by the proposed development to the existing traffic with the 0.5% growth rate applied and subtracting out the existing TCF Bank traffic. The resultant 2015 Build peak hour forecasts are shown in the Appendix under the capacity analysis section for each scenario.

5. Traffic and Improvement Analysis for 2034 Scenarios

a. Level of Service Analysis

The LOS results for the 2015 Scenario study hours are shown in Table 2. These are based on the existing traffic control and lane configurations at the study intersections with the deletion of the east leg of the Barrie Road/Existing Southern Site Access (named Barrie Road/Southern Driveway in Table 2) and the conversion of the northern site access to a full access intersection (named Barrie Road/66 West Site Access in Table 2). The lane configurations used can be seen in the capacity analysis section of the Appendix for the Build scenarios. The forecast turning movement volumes for the 2015 peak hour scenarios as shown in the Appendix were used in the LOS calculations. The LOS calculations were done in accordance with the 2010 *Highway Capacity Manual* using VISTRO™ software. The complete LOS calculations, which include queue lengths and grades for individual movements, are included in the Appendix.

Table 2 – 2015 Build Level of Service (LOS)¹

Intersection	A.M. Peak Hour	P.M. Peak Hour
66 th St/Barrie Rd	A (c)	A (b)
Barrie Rd/Southern Driveway	A (b)	A (b)
Barrie Rd/66 West Site Access	A (a)	A (a)

¹The first letter is the Level of Service for the intersection. The second letter (in parentheses) is the Level of Service for the worst operating movement.

Comparing the results from Table 2 to Table 1, the LOS results are forecast to not get any worse with the conversion of the existing bank site to the 66 West Apartments. The northern driveway intersection actually improves with the conversion to the 66 West Apartments because the existing bank is generating more traffic than the apartment is forecast to. No additional improvements or modifications are needed to accommodate traffic from the 66 West Apartments.

6. Parking Analysis

a. Existing Parking Counts

The 66 West Apartment building is proposing a 19 stall parking lot for its 39 unit building. Typically this would be considered not enough parking for a building of its size, but the 66 West Apartments is housing for young adults who have experienced homelessness. Because of this, the parking demand is likely to be lower than most apartment buildings since vehicle ownership rates are expected to be lower at 66 West than a typical suburban apartment building.

In order to determine how much parking can be expected at the 66 West Apartments, parking lot counts were conducted at three similar sites in Minneapolis that are also managed by Beacon. The three sites were:

- Nicollet Square: 3700 Nicollet Avenue South, Minneapolis, MN
- Lydia Apartments: 1920 LaSalle Avenue South, Minneapolis, MN
- Cedar View: 3146 Cedar Avenue South, Minneapolis, MN

The number of vehicles in each of these parking lots was counted after 10:00 p.m. every day for a week in June of 2014. The highest number of parked vehicles at each lot can be seen in Table 3. Full parking counts can be seen in Figure 4 in the Appendix.

Table 3 – Existing Parking Lot Counts

Apartment Building	Number of Apartment Units	Highest Parking Lot Count	Rate of Maximum Parked Vehicles to Apartment Units
Nicollet Square	42	10	0.18
Lydia Apartments	40	7	0.24
Cedar View	10	3	0.30

b. Projected 66 West Apartments Parking

Looking at Table 3, it can be seen that the maximum parking space to apartment unit demand is 0.3. For the 39 units at the 66 West Apartments, that leads to 12 parking spaces needed. Since the proposed parking lot includes 19 parking spaces, there will be adequate parking on site.



7. Conclusions and Recommendations

The traffic and parking impacts of the proposed apartment building on the study intersections were analyzed in the 2015 build-out conditions. The principal findings are:

- The forecast traffic from the proposed development will have little impact on the operations of the study intersections.
- All study intersections will operate acceptably through the 2015 build-out scenarios.
- The proposed 19 unit parking lot is forecast to be adequate for the 66 West Apartment building.



Other than the proposed changes of closing the east leg of the existing site southern driveway and converting the northern driveway to a full access intersection, no modifications are needed to be made by the developer to the study intersections.