



## PLANNING COMMISSION STAFF REPORT

Originator <b>Cary Teague</b> Community Development Director	Meeting Date <b>November 28, 2012</b>	Agenda # <b>VI.B</b>
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### INFORMATION/BACKGROUND

#### Project Description

Mount Properties is proposing to tear down the existing office buildings at 4005 West 65<sup>th</sup> Street and 6500 France Avenue to build a new five-story, 62-foot tall, 102,478 square foot medical office/retail building with an attached 4-5 level parking ramp. The first floor of the new building would contain 7,000 square feet of medical/retail space that may include a coffee shop. The next remaining 95,478 square feet would contain medical office use. (See site location, applicant narrative and plans on pages A1–A73.)

The applicant is request final approval of the project; therefore, the following is requested:

- Final Rezoning from POD-1, Planned Office District to PUD, Planned Unit Development;
- A Zoning Ordinance Amendment that establishes the PUD, Zoning District; and
- Final Development Plan.

The City Council approved the Comprehensive Guide Plan Amendment, Preliminary Rezoning to PUD, Planned Unit Development, and Preliminary Development plan for this project on November 5, 2012. (See approved preliminary plan on page A49a, and the minutes from the meeting on pages A113–A115.) The footprint of the building and the parking arrangement has not changed from the preliminary approval. The site plan is essentially the same as the approved preliminary plans with the exception that the chiller has been relocated to the top of the parking ramp, north of the stairwell. (See page A36.)

The Zoning Ordinance Amendment that would accommodate the requested rezoning to PUD, Planned Unit Development can be found on the attached pages A102–A104.)

### **Surrounding Land Uses**

- Northerly: Fairview Hospital parking ramp; zoned APD, Automobile Parking District
- Easterly: Fairview Hospital; zoned and guided Regional Medical District.
- Southerly: Point of France condos; Zoned PRD-4, Planned Residential District & High Density Residential
- Westerly: Cornelia Place Apartments; zoned PRD-4, High Density Residential; and guided High Density Residential

### **Existing Site Features**

The subject property is 2.34 acres in size, is relatively flat and contains two office buildings. (See pages A1–A3.)

### **Planning**

- Guide Plan designation: Regional Medical. (See pages A5–A14.)
- Zoning: POD-1, Planned Office District. (See page A4.)

### **Rezoning to PUD**

The Planning Commission and City Council found that the proposal meets criteria below to rezone this site to PUD; and granted Preliminary Rezoning Approval. The applicant's plans are generally the same as those submitted for the Preliminary Rezoning.

Per Section 850.04. Subd. 4 D provides the following regulations for a PUD:

- 1. Purpose and Intent. The purpose of the PUD District is to provide comprehensive procedures and standards intended to allow more creativity and flexibility in site plan design than would be possible under a conventional zoning district. The decision to zone property to PUD is a public policy decision for the City Council to make in its legislative capacity. The purpose and intent of a PUD is to include most or all of the following:**
  - a. provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan;**

- b. promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City;**
- c. provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian oriented design, and podium height at a street or transition to residential neighborhoods, parks or other sensitive uses;**
- d. ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;**
- e. maintain or improve the efficiency of public streets and utilities;**
- f. preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;**
- g. allow for mixing of land uses within a development;**
- h. encourage a variety of housing types including affordable housing; and**
- i. ensure the establishment of appropriate transitions between differing land uses.**

The applicant is proposing a development that would be consistent with the Comprehensive Plan. The Plan guides this site as Regional Medical; therefore, the proposed medical office with complementary retail uses would be consistent with the Comprehensive Plan. The proposal would create a more efficient and creative use of the property. The building would be pulled up close to the street, with sidewalks in front, and separated from the street by green space to promote a more walkable environment. Parking would be located in back in the ramp. Landscaping and patios are also proposed in front, with store fronts opening toward France Avenue. The applicant is also proposing to utilize sustainable design principals. Green building practices are suggested, and green roofs are proposed. (See pages A23–A25, of the applicant's narrative that explains how this project meets the above purpose and intent of the PUD Ordinance.)

## **2. Applicability/Criteria**

- a. Uses. All permitted uses, permitted accessory uses, conditional uses, and uses allowed by administrative permit contained in the various zoning districts defined in Section 850 of this Title shall be treated as potentially allowable uses within a PUD district, provided they would be allowable on the site under the Comprehensive Plan. Property currently zoned R-1, R-2 and PRD-1 shall not be eligible for a PUD.**

This site would be envisioned to allow uses permitted within the Regional Medical District, as this site is guided Regional Medical in the Comprehensive Plan. The Zoning Ordinance amendment on pages A102–A104 lists the uses that would be allowed on this site. WSB and Associates did a parking analysis that determined that the proposed parking would support the uses proposed. (See pages A74–A101.)

- b. Eligibility Standards. To be eligible for a PUD district, all development should be in compliance with the following:**
- i. where the site of a proposed PUD is designated for more than one (1) land use in the Comprehensive Plan, the City may require that the PUD include all the land uses so designated or such combination of the designated uses as the City Council shall deem appropriate to achieve the purposes of this ordinance and the Comprehensive Plan;**

The site is guided in the Comprehensive Plan for “Regional Medical,” which allows medical offices as a permitted use. The current site is zoned POD-1, and guided for Regional Medical. Within the Regional Medical District, 10% of an office building may contain retail uses allowed in the PCD-1 and PCD-2 Zoning District, as long as the primary function of the uses is to serve the office users. The proposed uses would be consistent with that standard. (See pages A105–A110 for a list of uses allowed within the PCD-1 and PCD-2 Districts.)

- ii. any PUD which involves a single land use type or housing type may be permitted provided that it is otherwise consistent with the objectives of this ordinance and the Comprehensive Plan;**

As mentioned above, the proposed uses would be medical office and limited retail, consistent with Comprehensive Plan.

**iii. permitted densities may be specifically stated in the appropriate planned development designation and shall be in general conformance with the Comprehensive Plan; and**

The proposed building density would have an FAR of 1.0 or 100%. The Floor Area Ratio contemplated in the Comprehensive Plan and Zoning Ordinance is 1.0 or 100%.

As a comparison, the TCO building has an FAR of 8.5 or 85%, and the Fairview Hospital campus has an FAR of 1.16 or 116%.

**iv. the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent described in #1 above.**

The proposed project does not closely relate to the conventional zoning district. Ten (10) variances would be required and a Comprehensive Plan Amendment necessary to accommodate the proposal. However, the request is similar to the request made by Twin City Orthopedic, and FAR now complies with the contemplated land use designation of Regional Medical in the Comprehensive Plan.

## Compliance Table

	City Standard (PCD-1)	Proposed
<b><u>Building Setbacks</u></b>		
Front – France Avenue	62 feet	<b>25 &amp; 35 feet*</b>
Front – 65 <sup>th</sup> Street	62 feet	<b>25 &amp; 35 feet*</b>
Side – West	62 feet	100+ feet
Rear – South	62 feet	<b>30 feet*</b>
<b><u>Parking Structure Setbacks</u></b>		
Front – France Avenue	52 feet	80+ feet
Front – North	52 feet	<b>25 feet*</b>
Side – West	58 feet	<b>15 feet*</b>
Rear – South	58 feet	<b>20 feet*</b>
<b>Chiller Setback</b>	20 feet	<b>6 feet*</b>
Building Height	Four Stories or 48 feet whichever is less	<b>Five Stories &amp; 62 feet*</b>
Parking Ramp Height	48 feet	4-5 levels & 40-52* <i>north side</i> 4-5 levels & 46-58 feet* <i>south side</i>
Building Coverage	30%	<b>59%*</b>
Maximum Floor Area Ratio (FAR)	50%	<b>100%*</b>
Parking Stalls	548 – Based on 95,478 s.f. medical (27 doctors) and 7,000 s.f. general retail	<b>423*</b> proof of parking to 548
Parking Stall Size	8.5' x 18'	8.5 x 18'
Drive Aisle Width	24 feet	24 feet

***\*Would require Variances if no PUD***

As a comparison, the following variances were required for Twin City Orthopedic:

1. Front building setback variance from 74 feet to 52 feet. (A 22-foot variance.)
2. Rear building setback variance from 74 feet to 20 feet. (A 54-foot variance.)
3. Side building setback variance from 74 feet to 20 feet. (A 54-foot variance.)
4. Front parking structure setback variance from 67 feet to 18 feet. (A 49-foot variance.)
5. Rear parking structure setback variance from 67 feet to 20 feet. (A 47-foot variance.)
6. Side parking structure setback variance from 57 feet to 10 feet. (A 47-foot variance.)
7. A side yard drive-aisle setback variance from 10 feet to 3 feet. (A 6.7-foot variance.)
8. Minimum tract area variance from 10 acres to 2 acres. (An 8 acre variance.)
9. A parking stall variance from 393 stalls to 373 stalls. (A 20-stall variance.)

**(The FAR for Twin City Orthopedic was .85 or 85%)**

**(The FAR for the Fairview Hospital Campus is 1.16 116%)**

## **Site Access**

The primary access to the site would remain off of 65<sup>th</sup> Street West. There would be a right-out only onto France. (See page A36.)

## **Parking & Traffic**

Per Section 850.08 Subd. 1, the following are the parking requirements: One space per 200 square feet for medical office and 1 space per doctor. The retail requirement is 8 spaces for the first 1,000 square feet and six spaces for each additional 1,000 square feet. The medical office would be 95,478 square feet, and would have 27 doctors (1 doctor for each 3,500 square feet of medical office.); the retail space would be 7,000 square feet. Based on the above, 548 parking stalls are required. The applicant is proposing 548 spaces with proof-of-parking. A parking study was done by WSB which concludes that the proposed parking would support the uses. (See pages A74–A101.) Should parking ever become a problem, the City of Edina could require the proof-of-parking stalls to be built.

A traffic study was also done by WSB, which concludes that the existing roadways support the proposed uses. (See traffic study on pages A74–A101.) However, as was conditioned in the approvals for Twin City Orthopedic, should signal improvements be deemed necessary at 65<sup>th</sup> Street and France Avenue, the property owner would be required to participate in appropriate cost sharing for signal improvements. This would be a requirement in the Developer's Agreement.

## **Bicycle Parking**

Per Section 850.07 Subd. 24.F. requires non-residential developments to provide bicycle parking spaces for a minimum of five percent of the automobile parking spaces required. Therefore, 27 bicycle parking spaces are required. A bike parking facility is proposed inside the parking ramp, adjacent to the entry into the parking ramp. This area must contain space for a minimum of 27 bikes.

## **Landscaping**

Based on the perimeter of the site, 34 overstory trees and a full complement of understory trees and shrubs are required. The applicant is proposing a combination of existing and proposed overstory trees that would total 61 around the perimeter of the site & 250+ understory trees and shrubs. (See landscape plan on page A62.)

## **Trash Enclosure**

The trash and recycling area would be within an enclosed area within the parking ramp. (See page A68.) Per section 850.10. Subd. 2. Of the Zoning Ordinance, the trash enclosure must be screened from all lot lines. Therefore, the proposed plan meets the ordinance.

## **Lighting**

As indicated in the applicant's narrative, there would be decorative wall mounted lighting along France Avenue and West 65<sup>th</sup> Street, and around the main entry. Metal halide pole lights would be located on top of the parking ramp with low height, sharp cutoff features to eliminate light spill off the property. The lights would be on timers. The lighting would be required to meet the city's lighting ordinance as follows:

"All exterior lighting and illuminating devices shall be provided with lenses, reflectors or shades so as to concentrate illumination on the property of the owner or operator of the lighting or illuminating devices. Rays of light or illumination shall not pass beyond the property lines of the premises utilizing the lights or illumination at an intensity greater than three foot-candles measured at property lines abutting property zoned residential and ten foot-candles measured at property lines abutting streets or property zoned non-residential. No light source, lamp or luminaire shall be directed beyond the boundaries of the lighted or illuminated premises."

## **Noise Study**

A noise study was conducted by Kvernstoen, Ronnholm & Associates. (See pages A26–A31.) The study concludes that by moving the chiller to the top of the parking ramp, north of the stairwell, would be in full compliance with the city noise ordinance.

## **Grading & Utilities**

The city engineer has reviewed the plans and found them acceptable and offered comments. (See page A117.) Review and approval is required by the Nine Mile Creek Watershed District.

## **Signage**

Signage would be allowed per the requirements of the Regional Medical District within the Zoning Ordinance Amendment for the PUD. (See draft on pages A102–A104.)

## PRIMARY ISSUES/STAFF RECOMMENDATION

### Primary Issues

- **Is the proposed rezoning to PUD appropriate for the site?**

Yes. As found by the Planning Commission and City Council with the Preliminary Approval of the PUD, the proposal to rezone the site to PUD is reasonable for the site for the following reasons:

1. The proposed uses would fit in to the neighborhood. As mentioned, this site is guided in the Comprehensive Plan for "Regional Medical," which allows medical uses, and limited retail. Through the PUD rezoning, the City has the ability to specifically limit the uses on the site to be consistent with limited retail uses per the PCD-1 Zoning District to ensure that the uses can be supported by the parking provided.
2. The proposal would create a more efficient and creative use of the property. The building would be pulled up close to the street with a podium height of two-stories, with sidewalks in front, and separated from the street by green space to promote a more walkable environment. Parking would be located in back. Landscaping and patios are also proposed in front, with store fronts opening toward France Avenue. The applicant is also proposing to utilize sustainable design principals, including a green roof. (See pages A23–A25, of the applicant's narrative that explains how this project meets the above purpose and intent of the PUD Ordinance.)
3. A floor area ratio of 1.0 or 100% would be consistent with the buildings and floor area ratios on the west side of France on 65<sup>th</sup> Street.
4. The existing roadways would support the project. WSB conducted a traffic impact study based on the proposed development, and concluded that the traffic generated from the project would not impact the adjacent driveways or intersections. No additional improvements other than those shown on the site plan would be required to accommodate the site redevelopment. (See traffic study on pages A74–A101.)
5. Ensure that the building proposed would be the only building built on the site, unless an amendment to the PUD is approved by City Council.
6. The proposed project would meet the following goals and policies of the Comprehensive Plan:
  - a. Building Placement and Design. Where appropriate, building facades should form a consistent street wall that helps to define the street and enhance the pedestrian environment. On existing auto-oriented

development sites, encourage placement of liner buildings close to the street to encourage pedestrian movement.

- Locate prominent buildings to visually define corners and screen parking lots.
- Locate building entries and storefronts to face the primary street, in addition to any entries oriented towards parking areas.
- Encourage storefront design of mixed-use buildings at ground floor level, with windows and doors along at least 50% of the front façade.
- Encourage or require placement of surface parking to the rear or side of buildings, rather than between buildings and the street.

b. Movement Patterns.

- Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
- Limit driveway access from primary streets while encouraging access from secondary streets.
- Provide pedestrian amenities, such as wide sidewalks, street trees, pedestrian-scale lighting, and street furnishings (benches, trash receptacles, etc.)

c. Encourage infill/redevelopment opportunities that optimize use of city infrastructure and that complement area, neighborhood, and/or corridor context and character.

7. The proposed plans are consistent with the Preliminary Plans that were recommended for approval by the Planning Commission and approved by the City Council.

**Staff Recommendation**

***Final Rezoning to Planned Unit Development  
& Final Development Plan***

Recommend that the City Council approve the Final Rezoning from POD-1, Planned Office District to PUD, Planned unit development, and Final Development Plan.

Approval is based on the following findings:

1. The proposed land uses are consistent with the Comprehensive Plan.

2. The plans are consistent with the approved Preliminary Development Plan.
3. The site layout would be an improvement over a site layout required by standard zoning; the building is brought up to the street, provides podium height, and front door entries toward the street, includes sidewalks to encourage a more pedestrian friendly environment along the street.
4. The design of the building is of a high quality brick, glass and concrete. A five story building would be generally consistent with the four story buildings on West 65<sup>th</sup> Street.
5. Traffic would be improved in the area by eliminating the right-in and out access on France Avenue.
6. Based on the traffic study done by WSB, the existing roadways can support the proposed development.
7. The proposed project would meet the following goals and policies of the Comprehensive Plan:
  - a. Building Placement and Design. Where appropriate, building facades should form a consistent street wall that helps to define the street and enhance the pedestrian environment. On existing auto-oriented development sites, encourage placement of liner buildings close to the street to encourage pedestrian movement.
    - Locate prominent buildings to visually define corners and screen parking lots.
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    - Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
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    - Provide pedestrian amenities, such as wide sidewalks, street trees, pedestrian-scale lighting, and street furnishings (benches, trash receptacles, etc.)

- c. Encourage infill/redevelopment opportunities that optimize use of city infrastructure and that complement area, neighborhood, and/or corridor context and character.

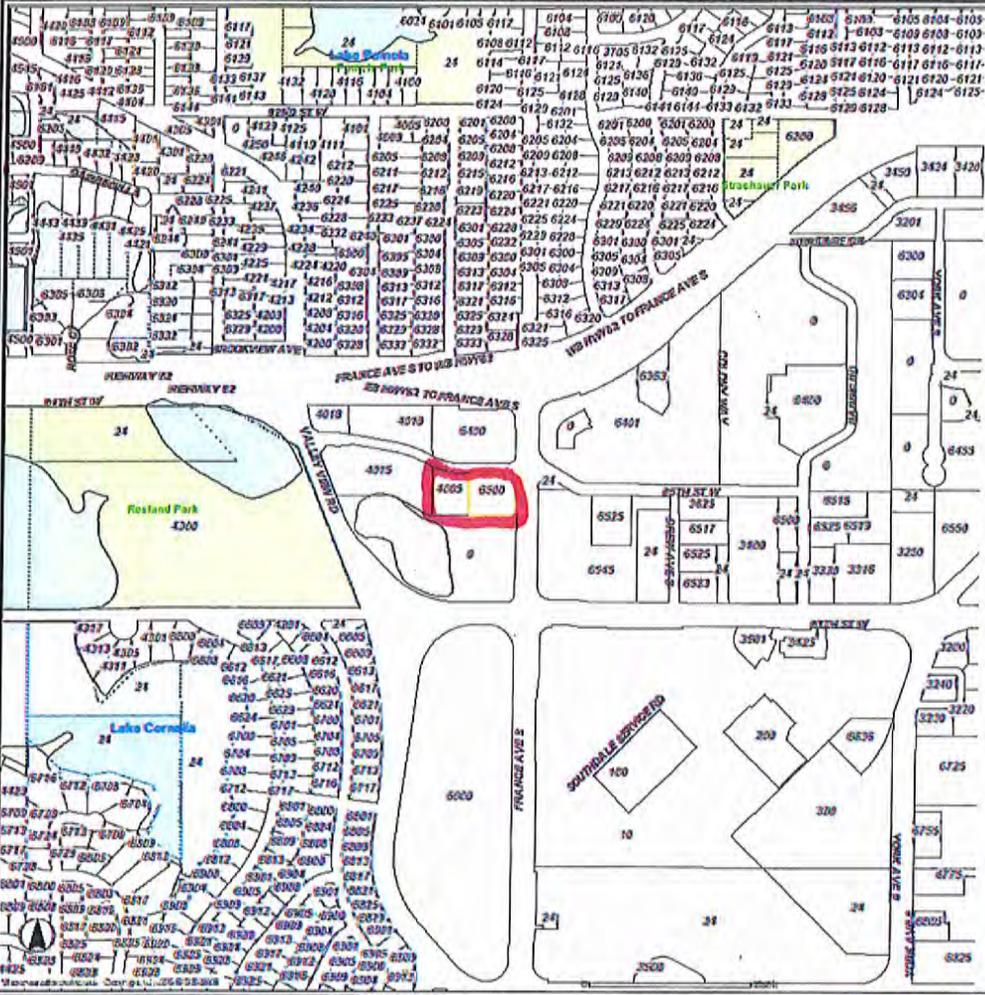
Approval is subject to the following conditions:

- 1) Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, unless modified by the conditions below:
  - Site plan date stamped November 6, 2012.
  - Grading plan date stamped November 6, 2012.
  - Landscaping plan date stamped November 6, 2012
  - Building elevations date stamped November 6, 2012.
  - Building materials board as presented to the Planning Commission and City Council.
- 2) Prior the issuance of a building permit, the following must be submitted:
  - a. A final landscape plan, subject to staff approval. Additionally, a performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures.
  - b. A construction management plan.
- 3) The property owner is responsible for replacing any required landscaping that dies.
- 4) Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
- 5) Compliance with the conditions required by the city engineer in his memo dated January November 20, 2012.
- 6) Site lighting shall conform to all minimum zoning ordinance standards including an intensity no greater than three foot-candles measured at property lines abutting property zoned residential and ten foot-candles measured at property lines abutting streets or property zoned non-residential.
- 7) Trash and recycling areas must be screened per Section 850.10 Subd. 2 of the City's Zoning Ordinance.

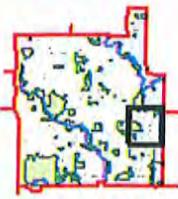
- 8) Sustainable design. The design and construction of the entire project must be done with the Sustainable Initiatives as outlined in the applicant's narrative within the Planning Commission staff report.
- 9) All buildings must be built with sprinkler systems, subject to review and approval of the fire marshal.
- 10) A Developers Agreement. As part of a Developers Agreement the property owner would be required to participate in appropriate cost sharing for signal improvements at 65th Street and France Avenue.
- 11) The applicant must enter into a Proof-of-Parking Agreement to add more parking if needed.
- 12) A minimum of 27 bicycle parking spaces shall be required. A bike parking plan must be submitted as part of the building permit application which demonstrates the City Code required bicycle parking requirements.
- 13) Adoption and compliance with the PUD Ordinance for the site.

**Deadline for a city decision:** February 1, 2013

# City of Edina



- Legend**
- Highlighted Feature
  - 1234 House Number Labels
  - STREET NAME Street Name Labels
  - City Limits
  - Creeks
  - Lake Names
  - Parks
  - Parcels



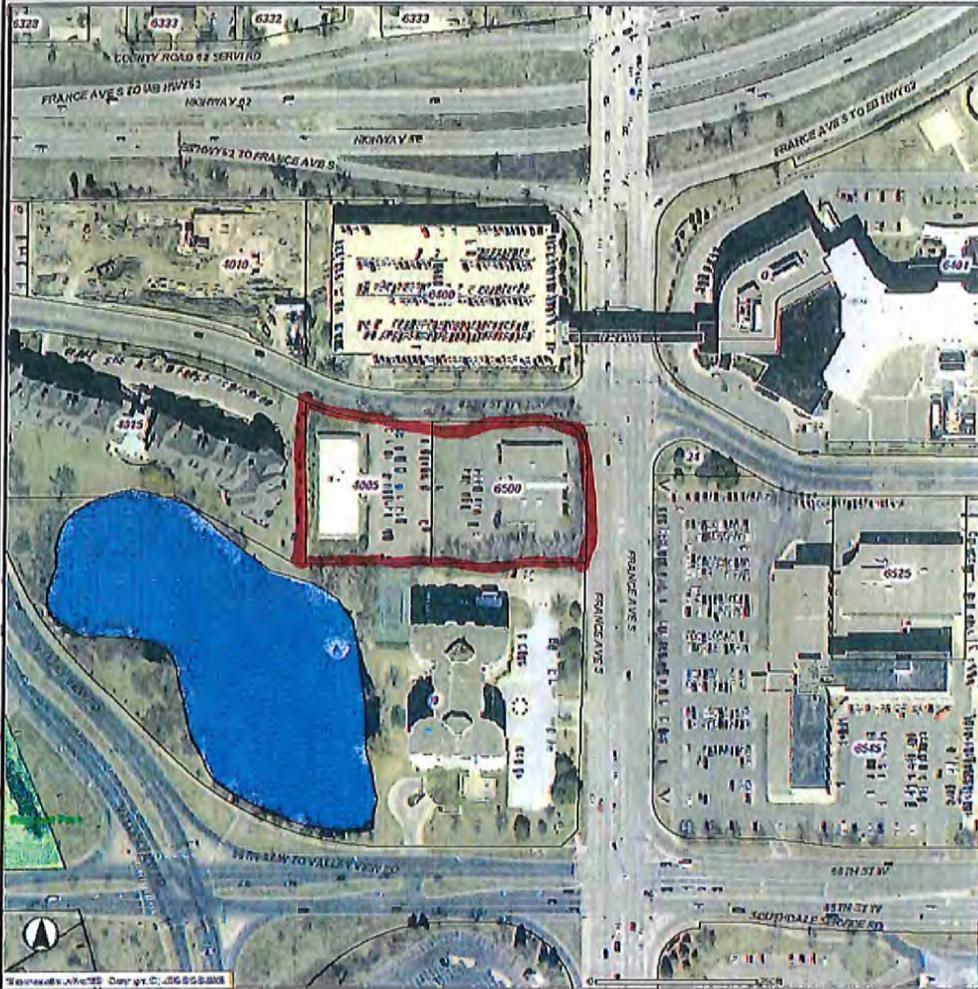
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**6500 France Ave S  
Edina, MN 55435**



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# City of Edina



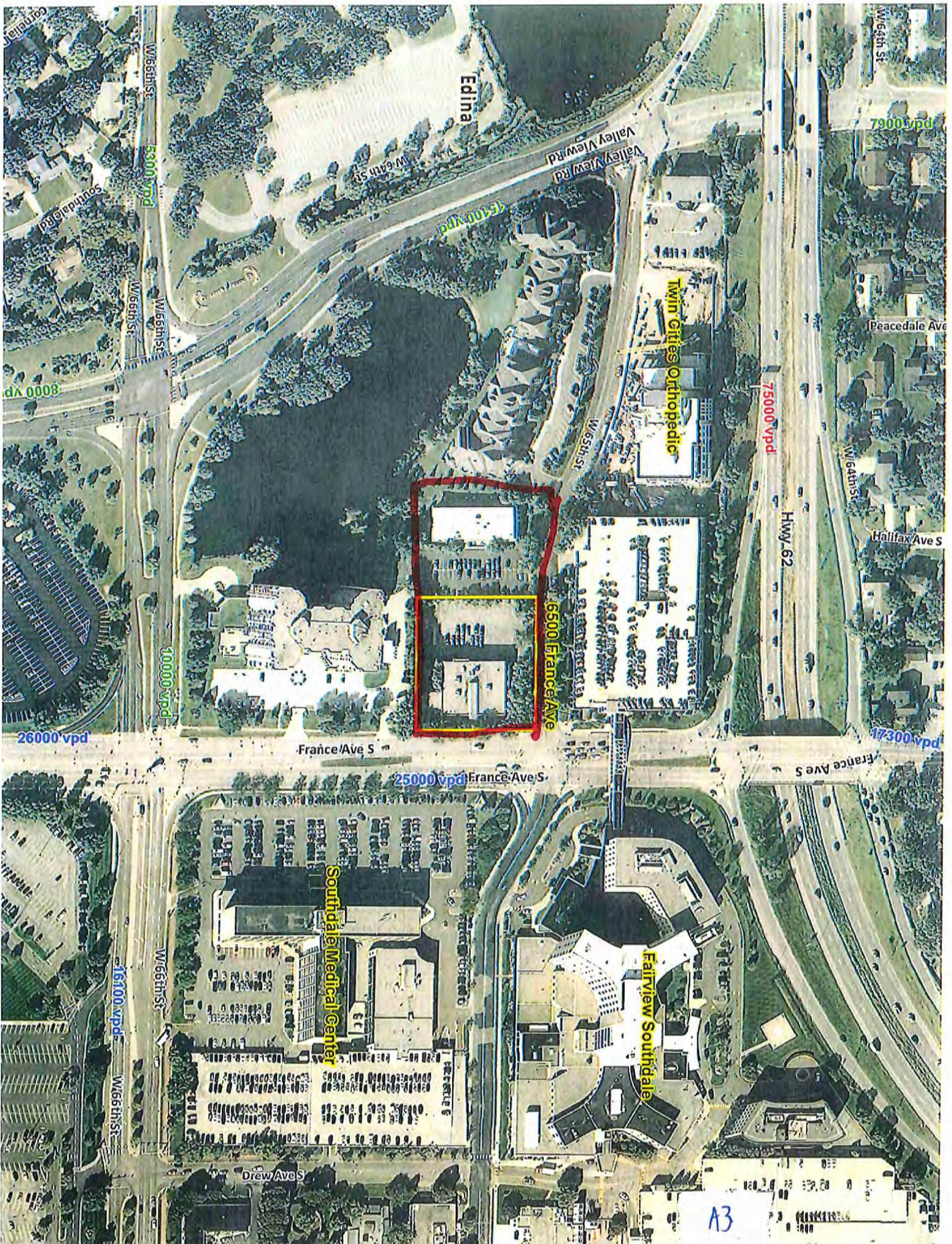
- Legend**
- House Number Labels
  - Street Name Labels
  - City Limits
  - Creeks
  - Lake Names
  - Lakes
  - Parks
  - Parcels
  - 2009 Aerial Photo



**PID: 3002824140008**

**6500 France Ave S  
Edina, MN 55435**





Edina

Twin Cities Orthopedic

Southdale Medical Center

Fairview Southdale

6500 France Ave

A3

# City of Edina



- Legend**
- House Number Labels
  - Street Name Labels
  - City Limits
  - Creeks
  - Lake Names
  - Lakes
  - Parks
  - Zoning
- APD (Automobile Parking District)
  - MDD-4 (Mixed Development District)
  - MDD-5 (Mixed Development District)
  - MDD-6 (Mixed Development District)
  - PCD-1 (Planned Commercial District)
  - PCD-2 (Planned Commercial District)
  - PCD-3 (Planned Commercial District)
  - PCD-4 (Planned Commercial District)
  - PID (Planned Industrial District)
  - POD-1 (Planned Office District)
  - POD-2 (Planned Office District)
  - PRD-1 (Planned Residential District)
  - PRD-2 (Planned Residential District)
  - PRD-3 (Planned Residential District)
  - PRD-4 (Planned Residential District)
  - PRD-5 (Planned Residential District)
  - PSR-4 (Planned Senior District)
  - R-1 (Single Dwelling Unit)
  - R-2 (Double Dwelling Unit)
  - RMD (Regional Medical District)
  - Parcels
  - 2009 Aerial Photo

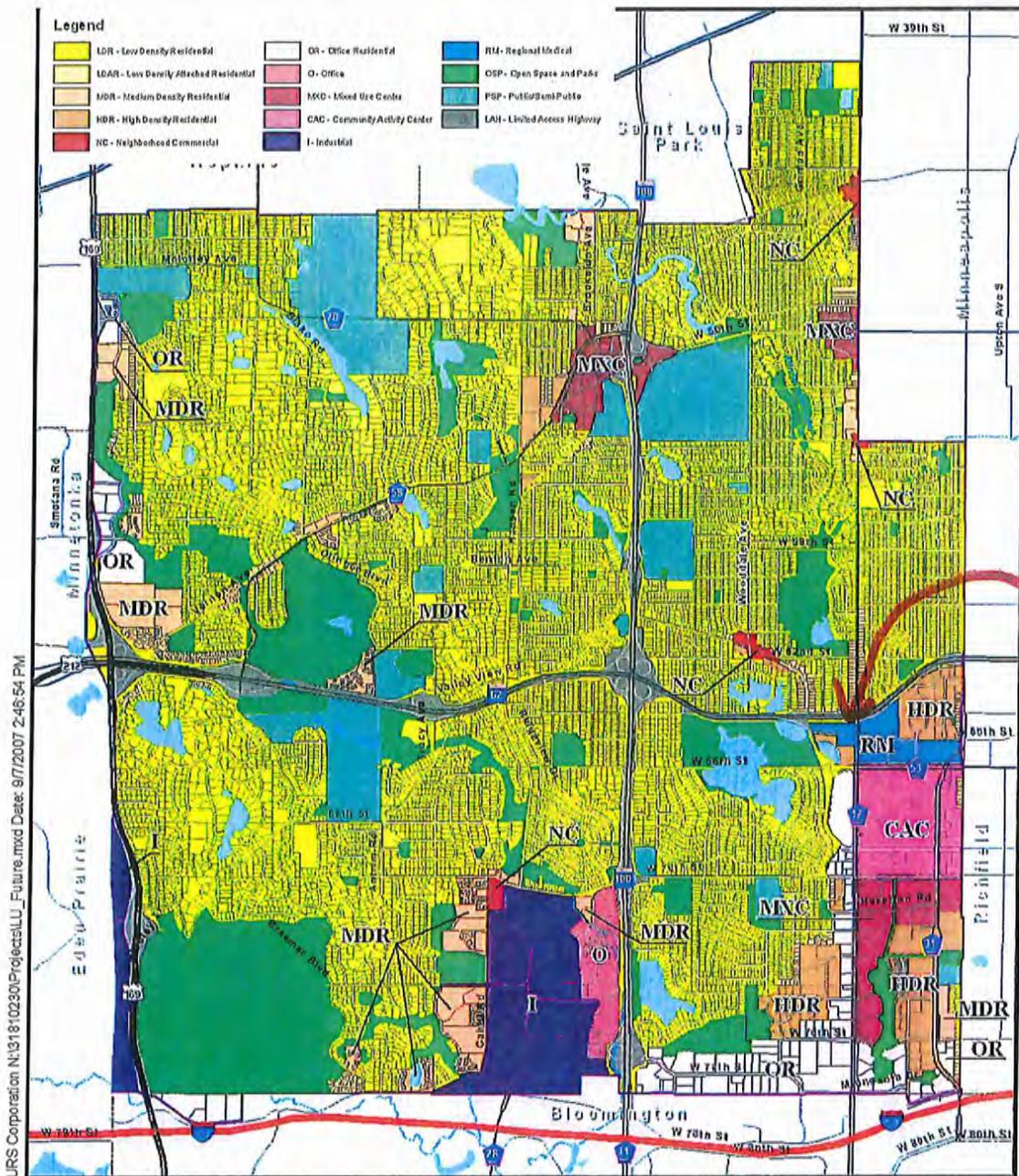


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**6500 France Ave S  
Edina, MN 55435**



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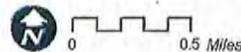
Figure 4.3

Future Land Use Plan



**City of Edina**  
2008 Comprehensive Plan Update

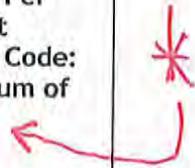
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Nonresidential and Mixed Use Categories	Description, Land Uses	Development Guidelines	Density Guidelines
RM Regional Medical	Hospitals, medical and dental offices and clinics, and laboratories for performing medical or dental research, diagnostic testing, analytical or clinical work, having a direct relationship to the providing of health services. General office uses are permitted.	Form-based design standards for building placement, massing and street-level treatment. Pedestrian circulation and open space amenities should be provided for larger sites.	Floor to Area Ratio - Per current Zoning Code: maximum of 1.0
OSP Open Space and Parks	Applies to major parks and protected open space that is publicly owned. May not include all small parks, since some are included in residential land use districts.	Performance and buffering standards for intensive outdoor recreation, parking.	N/A
PSP Public/Semi-Public	Applies to schools, large institutional uses (churches, cemeteries) and semi-public uses such as country clubs. Some small uses of these types may be integrated into other land use districts.	Performance and buffering standards for intensive outdoor recreation, parking.	To be determined - may require review of large-scale development or institutional expansion
LAH Limited Access Highway	Expressways and access ramps for two regional arterial highways (TH 62 and TH 100) occupy land within the City to serve local and regional travel needs.	NA	NA



\*Floor-to-area ratio, or FAR, refers to the ratio of a building's floor area to the size of its lot. Thus, an FAR of 1.0 could mean a two-story building covering 50% of the lot; a 3-story building on one-third of the lot, etc.

**Potential Areas of Change**

Among its many purposes, the Comprehensive Plan functions as a long range tool that attempts to anticipate where change and growth will occur in the City. Identifying those potential areas of change is an initial stage in the process of guiding new construction and redevelopment when it is proposed by private property owners. It is not an attempt to stimulate change, but to acknowledge that it may occur and be proactive in shaping it. Locations identified in this section appear to be areas where change may occur during the life of this Plan. Many of these areas were identified in a group exercise at Public Meeting #2 as



between offices and residential buildings. Edinborough is linked with the Centennial Lakes project by a network of pedestrian greenways.

The following guidelines are directed toward creating successful mixed use environments. (Also see the citywide standards for movement patterns, public spaces and built form earlier in this section.)

**Building Placement and Design.** Where appropriate, building facades should form a consistent street wall that helps to define the street and enhance the pedestrian environment. On existing auto-oriented development sites, encourage placement of liner buildings close to the street to encourage pedestrian movement.

- Locate prominent buildings to visually define corners and screen parking lots.
- Locate building entries and storefronts to face the primary street, in addition to any entries oriented towards parking areas.
- Encourage storefront design of mixed-use buildings at ground floor level, with windows and doors along at least 50% of the front façade.



### Movement Patterns.

- Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
- Limit driveway access from primary streets while encouraging access from secondary streets.
- Encourage enhanced transit stops, including shelters, shade and seating where feasible.
- Provide pedestrian amenities, such as wide sidewalks, street trees, pedestrian-scale lighting, and street furnishings (benches, trash receptacles, etc).



**Appropriate Parking Standards.** Mixed use developments often produce an internal capture rate. This refers to residents and workers who obtain goods and services from within the development without making additional vehicle trips. Parking ratios for mixed use development should reflect the internal capture rate and the shared parking opportunities this type of development offers.



### 3. Improve Connectivity in Large-scale Superblock Development.

**Internal and external connectivity.** As part of redevelopment or expansion of large-scale sites, reintroduce an internal local street and pathway network that connects through the site and to suitable entry points at the perimeter. The goal is to encourage pedestrians to reach the site and drivers upon arriving to continue all further movement by foot. As a result, the capacity of internal roads can be reduced and more area devoted to amenities, providing still more incentives to walk. Bicycle facilities should allow residents in surrounding neighborhoods to bike safely to the site. Transit stops should be provided in visible and central locations.

**“Edge” or transitional uses.** Moderately sized liner buildings should be encouraged to soften the edge of large-scale superblock development. Medium-density housing types such as townhouses combined with structured parking may also be an appropriate transitional use.



### 4. Provide appropriate transitions between land uses.

Rather than discouraging movement between adjacent land uses with berms and fences, focus on creating elegant and attractive transitions between adjacent uses. Transitional areas include well-landscaped pedestrian walkways, seating areas, arcades, and other spaces that encourage public use, rather than separation.



**5. Buildings Frame the Street.** Building placement and heights can serve to define the streetscape and visually reduce the apparent width of the street. Generally speaking, wider streets can accommodate taller buildings subject to the height limitations described elsewhere in this Chapter.

**6. Façade Articulation.** Primary facades should be designed with a well-defined base, middle and top, providing visual interest at ground level. Building entries and access points should be clearly visible from the primary street. Long building facades should be divided into smaller increments using contrasting materials, textures, detailing, setbacks or similar techniques.





**7. Transparency and Natural Surveillance.** Building forms and facades should provide an awareness of the activity within the buildings through frequent doors and windows oriented toward public streets and open space.

**8. Variety of Building Forms.** Encourage an integrated mix of building types, heights and footprints within blocks, rather than single buildings or building groups.

**9. Building Height Transitions.** Taller buildings (generally four stories or higher) should step down to provide a height transition to surrounding residential buildings, including buildings across a street or pathway, and to avoid excessive shadowing of sidewalks, parks and public spaces.



**10. Building Heights.** The question of building height is particularly significant in a largely developed community, where any new building has the potential to block views or cast shadows on established neighborhoods and land uses. The design guidelines above, as well as the recommended building heights in Figures 4.6A, 4.6B, and 4.6C, provide general guidance for buildings that exceed the typical residential height of 2.5 stories:

Edina already has many tall buildings, but not all of them provide the kind of transition outlined in Principle 9 above. Building height should be considered within the larger urban design context as illustrated in the photos below.





## Building Height Issues

1. **Visual Impacts**  
*City Image / Landmarks / Views*
2. **Aesthetic Scale / Proportions**
3. **Light Access**
4. **Shadow Impacts**
5. **Density**
6. **Traffic Capacity**
7. **Utility Capacity**
8. **Cost of Land**
9. **Market Conditions**
10. **Developer Investment / Building Technology**

Visual / Aesthetics

Capacity

Financial



Because of the limited number of locations where tall buildings can be sited, and the need for sensitivity to surrounding uses, the following Height Maps have been established for those parts of the city with potential for higher density development. The maps were developed to specify the following height measurements:



**Podium Height:** The "podium" is that part of the building that abuts the street, or that provides the required transition to residential neighborhoods, parks, and other sensitive uses.

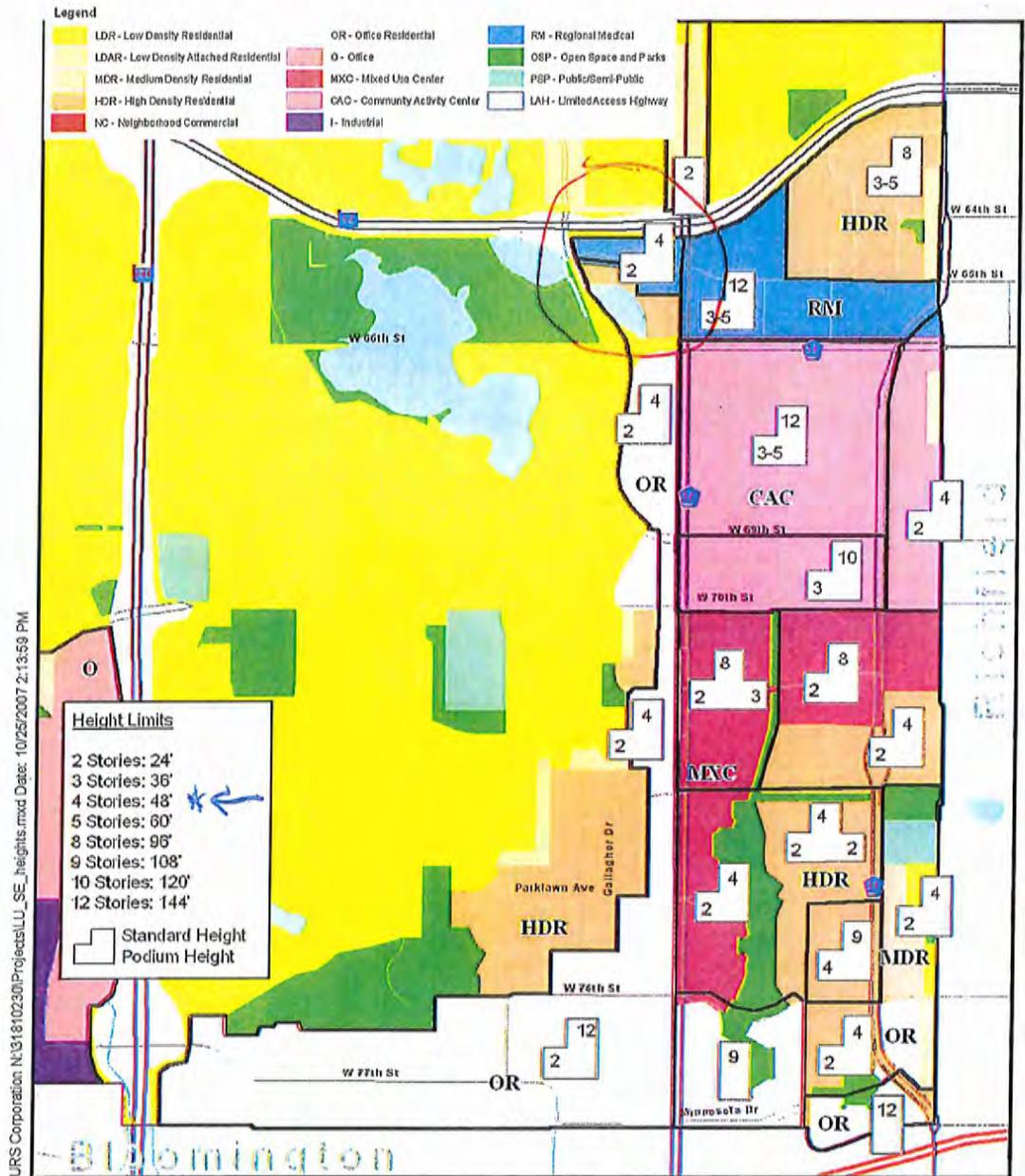




**Standard Height:** This height measurement extends to the top of the building (building height is measured as specified in the Zoning Ordinance).



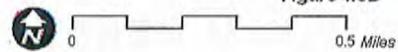
*"Podium height" building examples*



**City of Edina**  
2008 Comprehensive Plan Update

Data Source: URS

**Future Land Use Plan with Building Heights**  
Southeast Quadrant  
Figure 4.6B



A13



A14



415



416



17



View from Northwest showing 4005 W 65th St. Bldg.



View from Southwest showing 4005 W 65th St. Bldg.

Existing Conditions



View from Northeast showing 6500 France Building



View from Southeast showing 6500 France Building

November 6, 2012

EDWARD FARR  
ARCHITECTS INC

# EDINA MEDICAL PLAZA



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

A17a

**EDINA MEDICAL PLAZA**  
6500 France Avenue South

**PROJECT SUMMARY**

November 6, 2012

Our development team is looking forward to working with the City of Edina to obtain the necessary approvals for the **Edina Medical Plaza**. Everyone will strive to make this building an outstanding addition to this regional medical area.

**Development Team**

These professionals are local business owners that live and work in the community.

**Architect:** Edward Farr Architects, Ed Farr  
Long established firm that is strong on design concepts.  
Past projects include Normandale Lakes Office Park and  
Two Twelve Medical Center

**Civil Engineering:** Alliant Engineering, Mark Rausch  
Multi-disciplinary consulting firm focusing on transportation  
and land development services representing the public and  
private sectors.

**Developer:** Mount Development Co., Stephen Michals  
Five medical buildings have been completed in the past 6  
years for Ridgeview Medical Center.

**Owner:** Aurora Investments, LLC, Luigi Bernardi  
Current medical building ownership;  
All "class A" buildings  
7373 France Avenue, Edina  
212 Medical Center, Chaska  
Multiple Buildings in Wisconsin

Please contact Stephen Michals with any questions on this new building - 952-941-1383.

PLUMBING DEPARTMENT  
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Site Area = 102,965 sq ft / 2.34 acres

Property Guided per Comp. Guide Plan: RM Regional Medical, no change proposed

Property Zoned: POD-1, proposed to P.U.D.

Proposed Redevelopment: New Medical Office Building and Parking Ramp

5 stories, 102,478 gross sq ft / 96,500 rentable sq ft\* with 4 level parking ramp.

Proof of Parking, if needed – add 1 additional level (5 total) on parking ramp.

\*Not including partial basement mechanical equipment room.

Proposed Occupancy:

Medical Offices - 95,478 gross sq ft to 102,478 gross sq ft (93% - 100%)

Medical Retail - 7,000 gross sq ft to 0 gross sq ft (7% - 0%)

Total - 102,478 gross sq ft (100%)

Proposed Parking:

Levels 1 - 4 = 423 stalls 4.13/1000 gsf

Proof of Pkg: 1 add'l level = 125 stalls 1.22/1000 gsf

Total possible parking = 548 stalls 5.35/1000 gsf

Building Setbacks:

	Med Office Bldg	Parking Ramp
North	25'-0"/35'-0"***	varies: 25'-0" min / 50'-0" max.
South	30'-0"	20'-0"
East	25'-0"/35'-0"***	225'-0"
West	226'-0"	15'-0"

\*\* 35'-0" setback is above podium level

**Overview:** Our revised submittal reduces the height of the medical building down from 89 ft (six stories bldg plus rooftop HVAC equipment) down to 62 ft (five story building with no rooftop HVAC). Note that our new five story / 62 ft bldg height is comparable in height to the previous submittal's Phase I / four story building height (including HVAC equipment); since the City Council recommended that we shouldn't build taller than that Phase 1 / four story proposal. Another difference in this submittal is that the project will no longer be 'phased'; we intend to construct the entire five story building. The building area remains similar, with our F.A.R. slightly below the 1.0 maximum identified for this Regional Medical District. To reduce the number of stories from six to five, we increased the size of each floor plan westward, towards the parking ramp, leaving room underneath for our service drive on grade.

We have eliminated the basement level of our parking ramp, and raised that level up above grade. The parking ramp will be constructed at four levels / 423 stalls to meet our anticipated parking demand. However, the ramp is expandable by one more level (+ 125 cars) for a total of five levels / 548 cars for proof-of-parking compliance.

**Planning Concept:** As per our earlier application, joining the two parcels (6500 France & 4005 W 65<sup>th</sup> St) offer many advantages to the site layout. We are still adopting an urban, pedestrian-friendly, streetscape along France Ave by bringing the building forward to the street; as well as incorporating a 'podium' design effect by setting the building back above 2<sup>nd</sup> floor to maintain a comfortably-scaled pedestrian experience. The streetscape includes over-story trees along the right-of-way, a 10 ft wide sidewalk and decorative planters that contain colorful plantings (annuals, perennials and low evergreen shrubs). Vehicular ingress is available at two driveways along 65<sup>th</sup> St W.; and vehicular egress is available at three points from the site, dispersing the traffic more effectively. The main public vehicular entrance off of W 65<sup>th</sup> St leads to a visitor entry drop-off circle at the front door, as well as a dedicated delivery

area / loading zone for the building.

The City is currently in process to update several intersections along France Avenue at 66<sup>th</sup> St, 69<sup>th</sup> St and 70<sup>th</sup> St. The City's consultant, WSB Associates, has provided the preliminary designs at those intersections for reference. Our proposed project has been refined to incorporate a consistent design in the southwest quadrant of the 65<sup>th</sup> St and France Avenue intersection including the addition of an oval shaped raised planter between two new relocated pedestrian ramps. The new ramp locations will be set at the end radius points of the curb and be perpendicular to either 65<sup>th</sup> St W or France Avenue. There are final traffic signal design details to be done by others that will need to be coordinated with the City Engineer and County regarding the existing traffic signal base, pedestrian crossing buttons and hand hole relocations, as needed.

**Building Design:** The building design will be an attractive façade using multiple colors of face brick, warm-toned architectural precast concrete wall panels with a variety of surface finishes, reflective bronze-tone Low-e glass in champagne painted aluminum frames and a small amount of EIFS decorative cornice trim. The main entrance located off of W 65<sup>th</sup> St serves as a visitor drop-off area for patients, and features a distinctive glazed crown with backlighting at night. The NE building corner at France Ave and W 65<sup>th</sup> St is still an important point of identity for the building, and has been redesigned with the upgraded corner design treatment as suggested in earlier city reviews. The corner plaza area will offer outdoor seating, and also coordinates with the City's initiative to upgrade the pedestrian experience at these corners. The plaza will have decorative surfacing treatment, planting areas and bollards behind the curb.

**Visual Screening for the Adjacent Properties:** We will supplement the already mature landscape buffer between Point of France and our site with new coniferous trees and over-story trees along our south yard. The ash trees along this borderline with Point of France are approx. 35' – 50' tall, providing excellent screening for most months of the year. On the west, we will be improving the visual screening buffer for the apartment occupants by adding more evergreen trees. The existing outdoor trash dumpsters will be placed inside a dedicated trash & recycling room, screened from public view.

**Parking:** The ramp has been re-designed to better complement the office façade with a compatible multi-colored architectural precast concrete façade. We are proposing to build four levels / 423 cars initially, which we feel will serve the building based on our experience. Later, if demand requires, we could build one more ramp level (Level 5 / 125 more cars), for a total possible parking count of 548. However, we are promoting many forms of alternative transportation in order to reduce conventional car parking demand; refer to our list at the end of this narrative. This will be a Pay-Park facility, similar to the Fairview Southdale Hospital ramp. Up to 20% of the parking may be compact car size; but we have not yet shown striping for these smaller stalls on our plan. New to our application is the alignment of the parking ramp levels to the medical building floors, in order to offer convenient pedestrian / wheelchair access from most levels of the ramp directly into the building.

**Required Parking - w/ Retail option:**

Retail, 1st Floor - 7,000 GSF: $8/1000 + (6/1000 \times 6000 \text{ GSF}) =$	44 Stalls
Medical Office - 95,478 GSF: $1/200 + 1/3500 \text{ (Doctors)} =$	504 Stalls
<b>Total Required Parking =</b>	<b>548 Stalls</b>

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**Required Parking - no Retail:**

Medical Office - 102,478 GSF:  $1/200 + 1/3500$  (Doctors) = 542 Stalls  
Total Required Parking = **542 Stalls**

**Provided Parking:**

Levels 1 - 4 Ramp	423
Level 5 Ramp (proof of pkg)	125
<hr/> Total Possible	<hr/> <b>548</b>

**Sanitary Sewer and Watermain:** The existing properties located at 4005 65<sup>th</sup> St. W. and 6500 France Ave So. are currently encumbered by two public utilities - an existing 12" ductile iron water main and 12" ductile iron sanitary sewer in the south quarter of the property. The existing water main and sanitary sewer referenced serve other properties and thus the continuity of those mains is required. A second 8" sanitary sewer line is located within the boundary of both properties but that line is in use only by the two existing buildings and thus does not need to remain if the proposed project is constructed.

The existing 12" sanitary sewer along the southern property line of the 6500 France Ave property will remain as is. The proposed development does not require any modification to that line. The proposed plan is to construct a new sanitary service wye in the existing line to provide an 8" service to the new building. The existing 8" sanitary service line to the 4005 building will be 'cut off' at the west side of the proposed parking ramp and will then be connected to the parking ramp for servicing the floor drains in the lowest level of the ramp.

The existing 12" water main loop cutting through the site will be relocated and re-routed to the north side of the properties. A new 12" main will be installed northerly along France Ave., then westerly just to the north of the proposed building and ramp to the western edge of the property, then southerly back to the existing water main completed the new loop. It is currently proposed to directional bore install the water main on the east end of the site to avoid removal of the existing trees in that location. The east to west water main will be installed via direct bury as its proposed location can be installed without removal of the existing trees along W 65<sup>th</sup> St. An 8" water service is proposed from a tee off the re-routed 12" water main just north of the proposed building entrance. The proposed building service will enter the building in the corner west of the main building entrance. A new hydrant is proposed in the central entrance area of the new building off the proposed 8" service line.

**Stormwater Management:** The proposed stormwater management storage facilities proposed for the Edina Medial Plaza project meets the requirements of Nine Mile Creek Watershed and the City of Edina. The three primary requirements that have been met are:

1. Volume retention onsite equivalent to 1" of runoff over the entire proposed site impervious surface. (Accomplished via underground storage and infiltration via perforated piping and rock bedding).
2. Water quality volume from entire site equivalent to runoff from the 2" type II storm event and 25 year sediment storage. (Retention volume counts towards WQ volume and remainder accomplished via additional underground storage and filtration via perforated piping and rock bedding).
3. Discharge rate control shall be provided so the proposed conditions do not exceed existing conditions for the 2, 10, and 100 yr storm events. The proposed site plan reduces by 8.5% the amount of impervious surface and thus proposed discharge rates are decreased automatically.

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The proposed plan is to maintain the same point of stormwater discharge from the properties as is currently present. A private storm sewer collection system will be routed from north to south through the central portion of the site collecting the majority of site runoff. The storm sewer will drain through a pretreatment device to the proposed underground storage system at the southern end of the site which will ultimately outlet via the existing 15" corrugated metal pipe (CMP) culvert that drains southerly to the City pond. An agreement to maintain the current storm sewer discharge point to the City pond will be coordinated with the Point of France property owners. The condition of the existing 15" CMP will be verified; the pipe will be removed and replaced if the existing conditions are found to be in poor condition.

**Landscape Design Strategies:** The landscape design provides a mix of over-story, coniferous and ornamental trees, shrubs and perennials to create a vibrant display of color and foliage. We will attempt to preserve the four existing Honeylocust trees along France Avenue and eight of the boulevard Ash trees along W 65<sup>th</sup> St. Raised curbed planters are to be provided along France Avenue that will be planted with colorful, annual flowers. Coordination will occur with City staff to provide landscaping along France Avenue that will be consistent with the work proposed by the City at other intersections. The perimeter of the Medical Office Building will be planted with a mixture of plant types to soften and compliment the building architecture. The parking ramp will be screened by existing and proposed conifers on the west and south sides. A trail and a gazebo will overlook the Point of France Pond to the south of the parking garage. The diversity of plantings will provide color variety and year round interest. In addition, the project is proposing to provide a green roof system on top, covering approximately 1/3 of our roof. This sustainable initiative will help reduce the heat island effect, reduce stormwater runoff quantity, as well as providing a nicer view down on our roof from the residents of Point of France building.

**Noise Ordinance Compliance:** Upon relocation of our Rooftop HVAC units down inside the basement of the office building, we still need to have an outside air-cooled chiller for heat rejection. During Preliminary Plan Review we proposed the chiller to be ground mounted along our south property line; however, that location could not meet the city's noise ordinance. So this Final Development Plan submittal shows our new chiller location on the upper level of the parking ramp. An acoustical analysis by Kvernstoen, Ronnholm & Assoc, enclosed, concluded that this new chiller location results in full compliance with the city noise ordinance.

**Site Lighting:** Decorative wall mounted lighting along France Av and W 65<sup>th</sup> St frontages, and around the main entry area. Metal halide pole lights on top of the parking ramp with low-height, sharp cutoff features to eliminate light spill off our property; these are on timers.

**Site Signage:** Building name / address at corner of France & W 65<sup>th</sup> St., and address above front entry. Possible tenant names on façade based on City code. Directional signs at both entries along W 65<sup>th</sup> St.

**Hours of Use:** Normal Business Hours anticipated. Possible retail tenants at street level with extended and/or weekend hours.

**Zoning:** Planned Office District 1 (POD-1), proposing to PUD, due to variances requested for building setbacks, building height and Floor Area Ratio (F.A.R.).

**Green Building Practices:** Throughout all phases of the project - *Design, Construction and Operation*, we will use best practices of environmental awareness. All clinics and vendors will be educated on *Reduce-Recycle-Reuse* operations. We have a Green Building Practices

Narrative with our submission. New to our proposal is the inclusion of a green roof area on top of our building, which can be enjoyed by residents on the north side of Point of France.

## Community Benefits from the New Project

1. Our application is for a new five level medical office building. The 6500 France building and 4005 W. 65th Street buildings will be removed. Both law firms will relocate in the community and retain the current jobs.
2. This site provides outstanding visibility for medical clinics offering service to our community and support to the regional medical presence of the SW suburbs. This is consistent with the City Guide Plan of medical buildings along France Avenue which is an address easily recognized for patients.
3. The building is sized to have a critical mass of providers to allow cross referrals among the clinics. This is a key factor for the success of each clinic.
4. Edward Farr Architects is known for creating special building design features. This building is more than a traditional medical building. The focal point will be the glass and brick detail of the main entry corner. Numerous surface changes occur on each side of the building to create interest and shadow elements. City code specifies brick and glass.
5. We are striving to create a pedestrian environment along France Avenue with extensive flower gardens. TangleTown Gardens is the designer of these features, and their passion for unique colorful plants is shown in the enclosed photos. Colored concrete walks and accent lighting will make this area a pleasant visual experience. The improvements will be coordinated with the City to be consistent with improvements being proposed by the City at other intersections along France Avenue.
6. Green Building Practices will be implemented for the three phases: *building design, construction phase and long term operation* of the clinics. The General Contractor has compiled a summary of the *Green Practices* we will pursue for each phase of the project.
7. The adjacent 4005 W. 65th land parcel will be included for a consistent regional medical master plan. This took substantial effort and negotiation with the building owner.
8. A portion of the building roof will have a vegetated green roof, with native wild flowers and sedum, to enhance the view down onto the roof from the neighboring Point of France building, as well as reducing our heat island effect and improving our stormwater runoff quantity.
9. There will be ongoing seminars with the clinics to evaluate methods and new technology for their clinics – *Reduce* consumption, *Re-use* materials, and *Recycle* waste. The benefit to the building will be reduced operating expenses and good stewardship of our business resources.

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10. Alternative transportation is a key element of any project. A tangible goal will be to reduce the number of cars coming to the building which will reduce the parking stalls required to service the clinics. We have provided several successful alternate transportation systems:

Designated premium parking spaces for "smart cars"

Scooter, motorcycle, bike parking will be an enclosed space that is secure, well lit, and air tempered. We want these employees or patients to know they are recognized for their efforts.

Employees may participate in the ZAP chip monitor system which will provide a monthly printout of number of rider days. There will be a calculation of carbon footprint poundage saved to each participant.

Hour Car business has been evaluated for rental cars parked in the ramp. The established bus system will support the need around the hospital and shopping center for the hourly car rental.

The MTC #6 bus line services multiple stops within 1 block of the building. Additional routes around Southdale are 515, 538, 539, 578, 579, 684. See the attached graphic

11. The estimated real estate taxes will be over two times the current revenue. The two existing buildings pay \$144,000 and the fully assessed medical building will likely pay in excess of \$380,000.
12. There will be over 100 new medical staff positions in the building. In addition, there will be 40-60 construction jobs over 12 months.
13. Storm water management will be improved through *rate control* and *water quality*, including our green roof. The current site has direct, unrestricted runoff. There is almost 8% less hard surface area in the new plan than the current buildings, which mean more green space for the community.
14. The current buildings have substantial deferred maintenance. The building exterior, landscaping and parking lot are ready for redevelopment.
15. The new development is using existing utilities and roads in the community. Urban planning considers this good stewardship to reuse existing sites with current infrastructure.
16. The new building exterior has been enhanced over code to create a significant *Gateway* image on this corner.
- A focal point is the W 65th Street pedestrian corner
  - Detailed brick design on all four sides
  - Accent lighting along public street frontages
  - Special landscaping from *Tangletown Gardens*

17. Every business has a carbon footprint. We will provide information to each clinic to establish a "Carbon Impact Analysis." For the first two years we plan to have seminars for clinic managers discussing ways to reduce their impact and manage more efficiently. *PayitGreen Company* has set the model to challenge companies to purchase local products to reduce paper correspondence and conserve operational materials.
18. Ramp Enhancements
  - All columns are clad in architectural precast panels
  - Architectural features add interest and shadow lines
  - Three colors and finishes to the precast panels create a variety of textures
19. The entrance traffic circle will have an area for a significant piece of art on the center area. We are planning a special commissioning for this feature.

### **Final comment for the approval of the project**

The building size at 102,478 sq ft over five stories is very important to create a successful project.

Market rents set the overall budget. We are projecting rents to be 20% higher than other buildings in area to absorb the land cost. Redevelopment is difficult when you are purchasing two older buildings. The value for the Developer on the acquisition of the two older buildings is the land. The viability of the development is based on the approval of the 102,478 sq ft project.

Thank you for your review and consideration of this project.

We are ready to move forward on this outstanding new medical building which will be an enhancement to your Regional Medical Area.

NOV 06 2012

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Triangle Drive  
Eden Prairie  
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www.edfarrarch.com

November 6, 2012

Cary Teague, Community Development Director  
**City of Edina**  
4801 W. 50<sup>th</sup> Street  
Edina, MN 55424

Re: **Acoustical Report Summary**  
Edina Medical Plaza  
6500 France Av S. redevelopment

Cary,

Enclosed are two acoustical reports prepared by our Acoustical Consultant, Kvernstoen, Ronnholm & Associates, Inc. The first report, dated October 31, 2012, identifies the existing ambient noise levels along our shared property line with the Point of France. The second report, dated November 1, 2012, identifies the noise levels that will be emitted from our proposed chiller equipment, now located on the top level of the parking ramp.

#### Executive Summary

- Existing Noise Levels were measured in two positions along our south property line: Position 1 was located at our SE corner, along France Av; and Position 2 was located approximately 185 ft west of that SE corner, along our shared property line with the Point of France. Measurements were taken at both locations for two hour durations each at four different times of day / days of the week. The existing ambient noise level at Position 1 was approximately 65 dB, and the ambient noise level at Position 2 was approximately 55 dB.
- Acoustical data published by the chiller manufacturer was analyzed with relation to the proposed chiller location on the top level of our parking ramp. It was concluded that the noise levels from our chiller comply with the city noise ordinance. Further, the noise levels from the chiller were more than 10 dB less than the ambient noise levels already present on site, thereby resulting in no additional noise impact at the property line.

Please refer to both technical reports, enclosed, for further detail.

Sincerely,

**Edward Farr Architects, Inc.**



Edward A. Farr, AIA  
President

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encl: Kvernstoen, Ronnholm & Assoc. report dated October 31, 2012 (Existing Ambient Noise)  
Kvernstoen, Ronnholm & Assoc. report dated November 1, 2012 (Chiller Noise Compliance)



31 October 2012

Stephen Michals  
Mount Development Co.  
3600 Minnesota Dr. Suite 525  
Edina, MN. 55435

**NOISE MONITORING REPORT: 6500 FRANCE AVENUE SOUTH**

Kvernstoen, Rönholm & Associates, Inc. was retained by Aurora Investments, LLC to measure noise levels at the property lines at the 6500 France location, at the following times, at two locations:

- 7:00 AM – 9:00 AM on a weekday
- 11:00 AM – 1:00 PM on a weekday
- 6:00 PM – 8:00 PM on a weekday
- 11:00 AM – 1:00 PM on a Saturday

The two locations were as follows, and as shown on Fig 1 and a site map, enclosed:

- On the property line between 6500 France Avenue and the Point of France Condominium, at the approximate middle of the E/W façade of POF
- 20' west of France Avenue on the property line between 6500 France and the Point of France Condominium

To that end I traveled to the site on 10/27/12, 10/29/12, and 10/30/12 to perform statistical noise measurements.

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**Procedure**

Each measurement period was 2 hours. The microphone was set on a tripod at the locations shown on Fig 1. The meter used was a Larson-Davis precision noise level meter (SN #150) equipped with logging functions and a wind screen for the microphone. The meter was field-calibrated directly before and after the measurements. The temperatures and wind conditions were as noted with the data.

**Results**

Measurement results are shown in Table 1 on the next page. I have included several statistical measurements in order to show the variability of the traffic noise, although the only important number is the L<sub>50</sub> metric (underlined in Bold). The L<sub>xx</sub> numbers refer to the percentage of time that the noise level was exceeded for that percentage of time.

**Table 1 (All measurements A-weighted)**

**(7:00 AM – 9:00 AM, Wed, 10/30/12, Temp: 31degrees F, no wind)**

Pos.	L <sub>1</sub>	L <sub>5</sub>	L <sub>10</sub>	L <sub>50</sub>	L <sub>90</sub>	L <sub>95</sub>	L <sub>99</sub>
1.	75.5	73.9	73.0	<b><u>66.7</u></b>	57.0	55.0	52.5 dBA
2.	68.8	64.8	61.2	<b><u>55.5</u></b>	51.9	51.5	50.8 dBA

Only the noise level at location #2 is within the MPCA rules as expressed in Minnesota Rule 7030. The rules are as follows:

Daytime (7:00 AM – 10:00 PM): 60 dBA  
 Nighttime (10:00 PM – 7:00 AM): 50 dBA

**Table 2 (All measurements A-weighted)**

**(11:15AM – 1:15 PM, Wed, 10/30/12, Temp: 45 degrees F, light winds)**

Pos.	L <sub>1</sub>	L <sub>5</sub>	L <sub>10</sub>	L <sub>50</sub>	L <sub>90</sub>	L <sub>95</sub>	L <sub>99</sub>
1.	75.5	73.8	72.8	<b><u>66.0</u></b>	57.8	56.4	54.7 dBA
2.	76.3	70.5	65.5	<b><u>56.0</u></b>	52.9	52.3	51.2 dBA

Only the noise level at location #2 is within the MPCA rules as expressed in Minnesota Rule 7030.

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**Table 3 (All measurements A-weighted)**

**(6:00 PM – 8:00 PM, Mon, 10/ 29/12 Temp: 44 degrees F, moderate winds)**

Pos.	L <sub>1</sub>	L <sub>5</sub>	L <sub>10</sub>	L <sub>50</sub>	L <sub>90</sub>	L <sub>95</sub>	L <sub>99</sub>
1.	74.0	72.2	71.2	<u>64.0</u>	55.4	53.7	52.0 dBA
2.	63.0	60.1	59.5	<u>54.4</u>	51.2	50.8	50.0 dBA

Only the noise level at location #2 is within the MPCA rules as expressed in Minnesota Rule 7030.

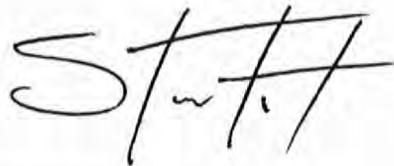
**Table 4 (All measurements A-weighted)**

**(11:00 AM – 1:00 PM, Sat, 10/ 27/12 Temp: 38 degrees F, very light winds)**

Pos.	L <sub>1</sub>	L <sub>5</sub>	L <sub>10</sub>	L <sub>50</sub>	L <sub>90</sub>	L <sub>95</sub>	L <sub>99</sub>
1.	75.5	73.9	73.0	<u>66.7</u>	57.0	55.0	52.5 dBA
2.	68.8	64.8	61.2	<u>55.5</u>	51.9	51.5	50.8 dBA

Only the noise level at location #2 is within the MPCA rules as expressed in Minnesota Rule 7030.

Please contact me with any comments or questions.



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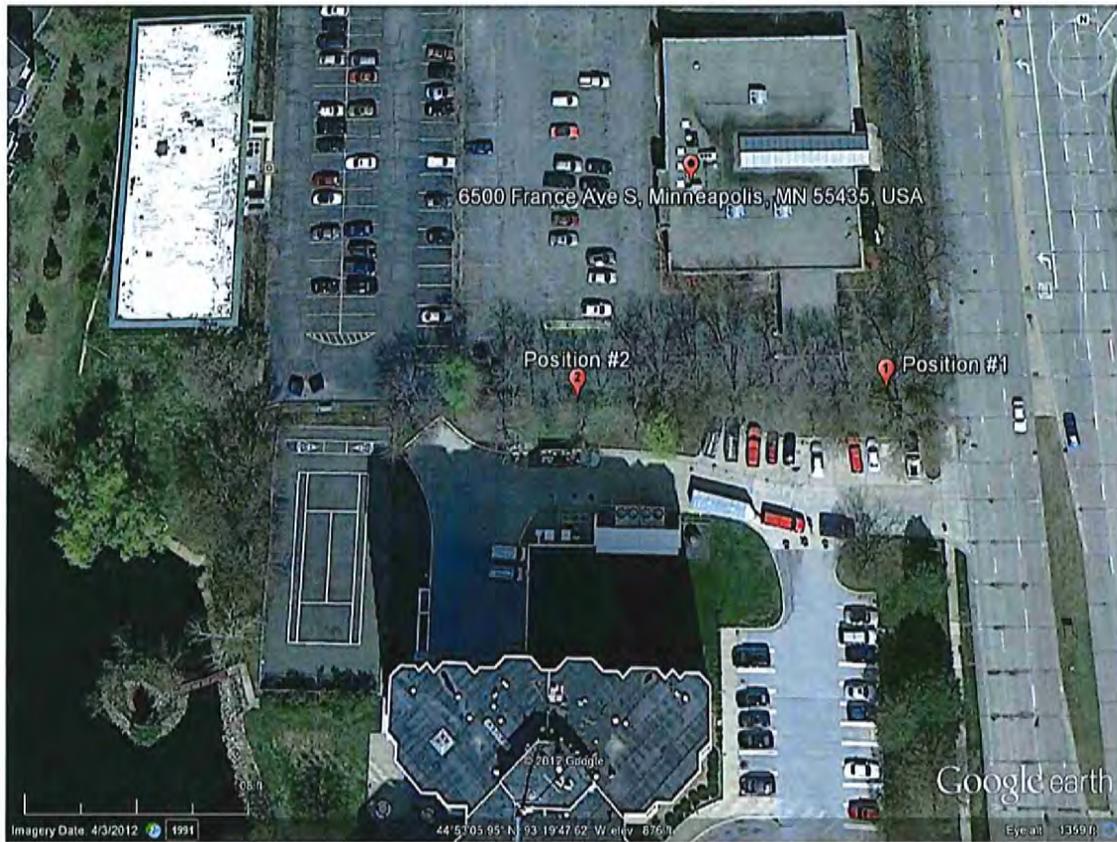
Steven Kvernstoen

cc: Luigi Bernardi  
Ed Farr

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NOV 06 2012  
CITY OF EDINA

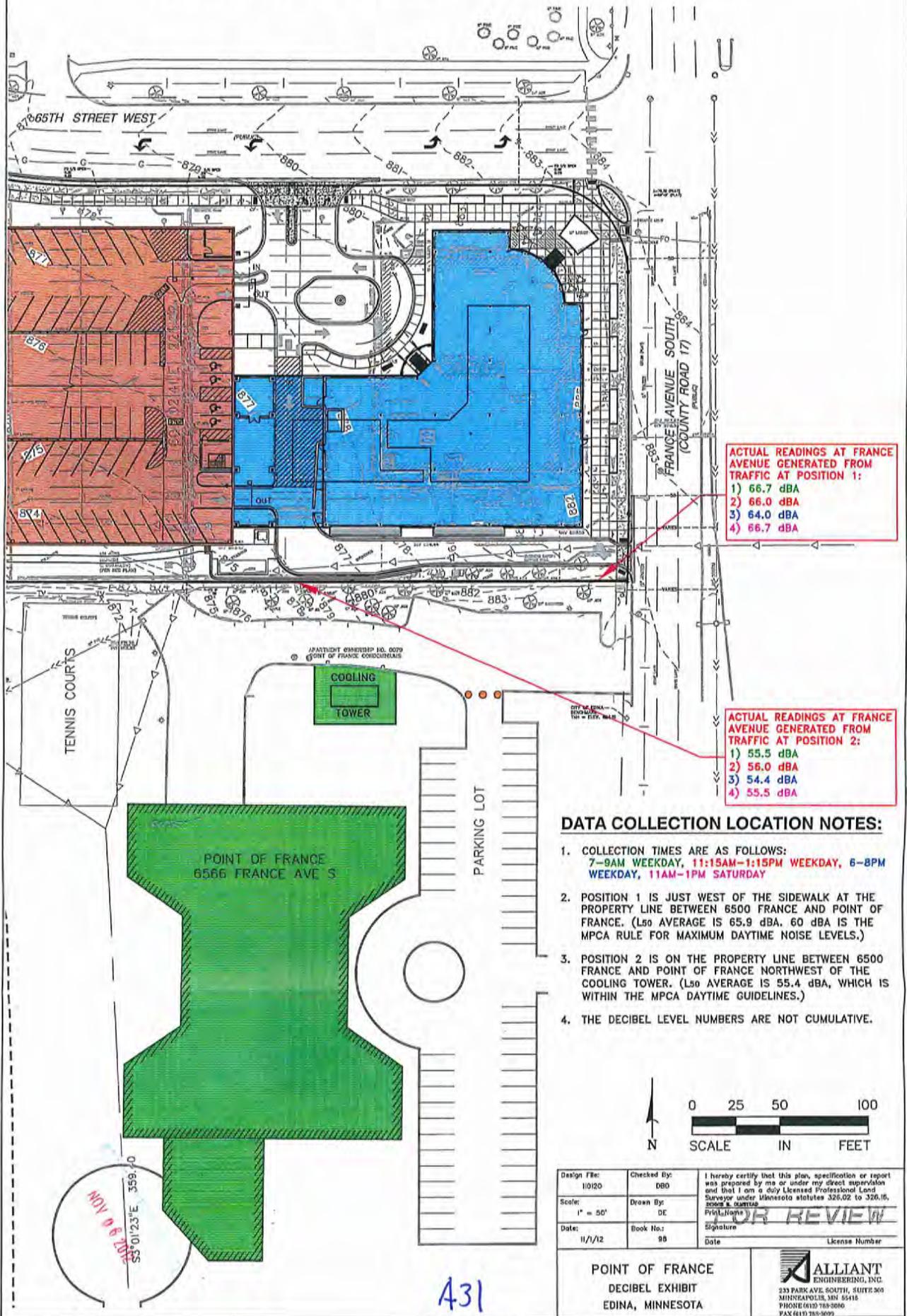
**FIGURE 1.**



PLANNING DEPARTMENT  
NOV 06 2012  
CITY OF EDINA

A3

# DECIBEL LEVEL GRAPHIC





1 November 2012

Stephen Michals  
Mount Development Co.  
3600 Minnesota Dr. Suite 525  
Edina, MN. 55435

**RE: CHILLER NOISE FROM 6500 FRANCE AVENUE SOUTH**

Stephen,

I have reviewed the site plan and chiller noise data that you provided, with the chiller now relocated to the top level of the parking ramp as shown in Fig 1. I have combined the noise levels from the chiller measured at the property line and at the building itself, with the ambient noise levels already existing on the site that were identified in my October 31<sup>st</sup> report (mainly from traffic noise).

I have used the chiller noise data with the chiller operating at 100%, since that is the worst-case scenario. A more likely scenario for the chiller would probably be 50% - 75% operation, which would lower the noise levels by 2 – 4 dB's.

The Sound Power Levels (PWLS) for the chiller at 100% operation are as follows:

Table 1

63	125	250	500	1k	2k	4k	8k	dBA
93	95	98	97	91	88	83	78	<b>98 dBA</b>

I have calculated the sound Pressure Levels (SPLS) to the balcony of the top floor of the POF building (worst-case), and to the ground level at the south property line. These calculations and results are shown on the next page.

**Noise Levels at the top balcony at POF:**

		63	125	250	500	1000	2000	4000	8000	dBA
PWLS		93	95	98	97	91	88	83	78	98
A-Weighting		-26.2	-16.1	-8.6	-3.2	0	1.2	1	-1.1	
Dist Atten (200')	240'	-48	-48	-48	-48	-48	-48	-48	-48	
Sub Sum		18.8	30.9	41.4	45.8	43	41.2	36	28.9	
Barrier		6	7	8	9	12	13	15	19	
		12.8	23.9	33.4	36.8	31	28.2	21	9.9	
										<b>39.4 dBA</b>

The noise level at the balconies of the top floor of POF will be approximately 39.4 dBs. This is well below the City and State noise requirements of 50 dBA. Since the noise levels from the chiller are more than 10 dB's below the ambient noise levels in this location, they will not add to the existing ambient noise levels.

**Noise Levels at south property line, ground level:**

I also calculated the noise levels from the chiller at the ground level property line of POF, as follows:

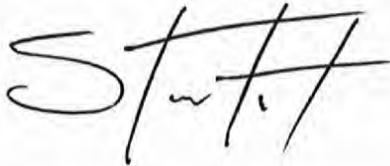
		63	125	250	500	1000	2000	4000	8000	dBA
PWLS		93	95	98	97	91	88	83	78	98
A-Weighting		-26.2	-16.1	-8.6	-3.2	0	1.2	1	-1.1	
Dist Atten	115'	-42	-42	-42	-42	-42	-42	-42	-42	
Sub Sum		24.8	36.9	47.4	51.8	49	47.2	42	34.9	
Barrier		8	9	10	11	14	15	17	21	
		16.8	27.9	37.4	40.8	35	32.2	25	13.9	
										<b>43.4 dBA</b>

NOV 06 2012  
 CITY OF...

A33

The noise level at the north property line of POF will be approximately 43.4 dBs. This is well below the City and State noise requirements of 50 dBA, and well below the ambient levels recorded at this location. Since the noise levels from the chiller are more than 10 dB's below the ambient noise levels in this location, they will not add to the existing ambient noise levels.

Please contact me with any comments or questions.

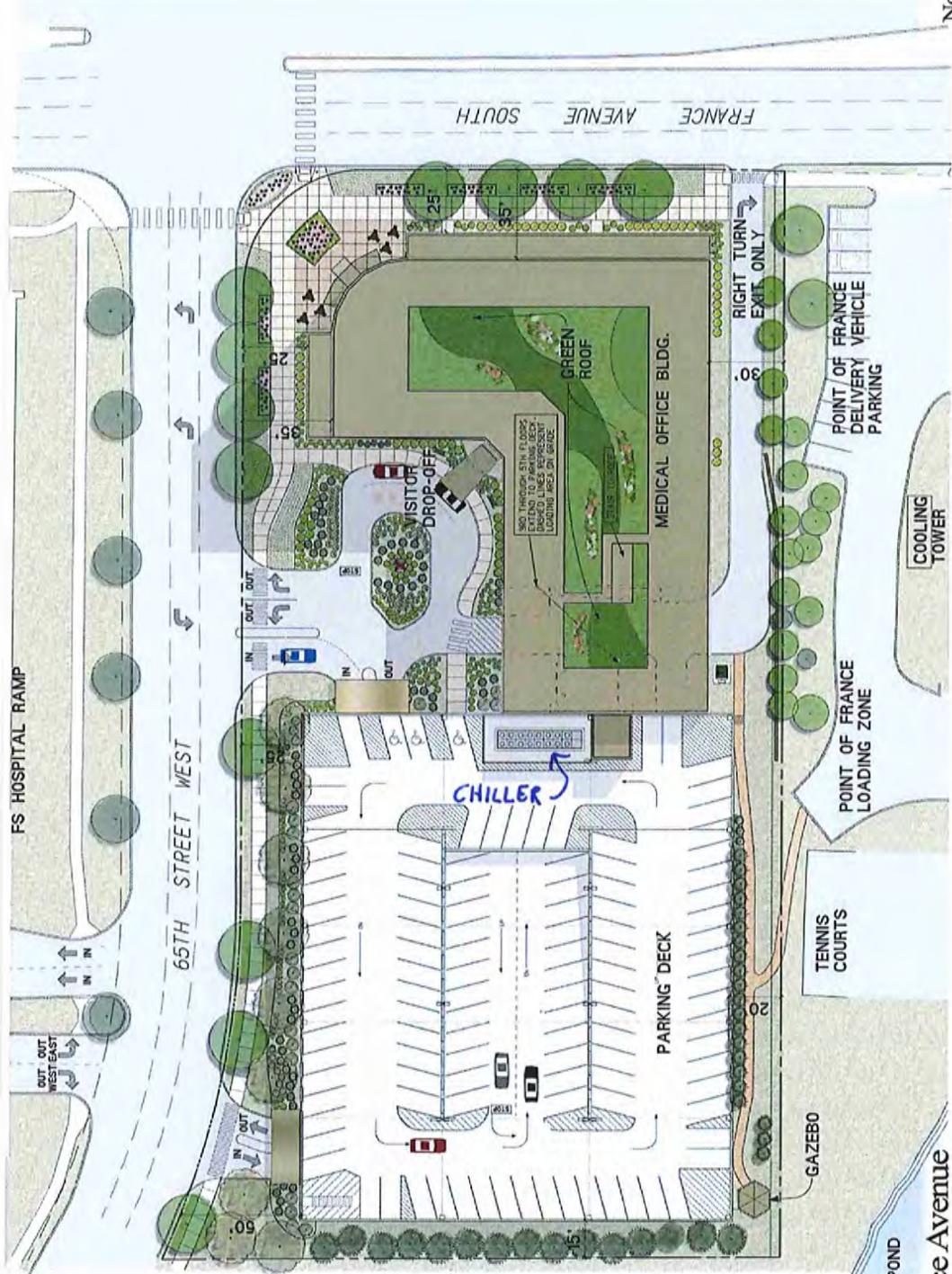


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Steven Kvernstoen

PLANNING DEPARTMENT  
NOV 06 2012





November 6, 2012

Site Plan  
6500 France Avenue

EDWARD FARR  
ARCHITECTS INC

# EDINA MEDICAL PLAZA





Main Entry

November 6, 2012

EDWARD FARR  
ARCHITECTS INC



# EDINA MEDICAL PLAZA



Parking Ramp Entry

November 6, 2012

EDWARD FARR  
ARCHITECTS INC



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

# EDINA MEDICAL PLAZA



View from Northeast Corner

November 6, 2012

EDWARD FARR  
ARCHITECTS INC



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.



View from Southeast Corner

November 6, 2012

EDWARD FARR  
ARCHITECTS INC



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Investments, LLC



MOUNT DEVELOPMENT CO.



AFD

View from Southwest Corner

November 6, 2012

EDWARD FARR  
ARCHITECTS INC



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Investments, LLC



MOUNT DEVELOPMENT CO.

# EDINA MEDICAL PLAZA



View from Northwest Corner

November 6, 2012

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AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

# EDINA MEDICAL PLAZA



Northwest Aerial

November 6, 2012

EDWARD FARR  
ARCHITECTS INC



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

# EDINA MEDICAL PLAZA

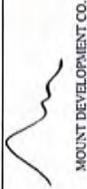


Northeast Aerial

November 6, 2012

EDWARD FARR  
ARCHITECTS INC

# EDINA MEDICAL PLAZA





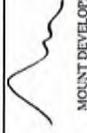
Main Entry Aerial

November 6, 2012

EDWARD FARR  
ARCHITECTS INC



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

# EDINA MEDICAL PLAZA

A44

Professional Engineer No. 15262  
 State of Minnesota  
 License No. 15262  
 Date: 12/14/2017

PROJECT NAME: EDINA MEDICAL PLAZA  
 PROJECT NUMBER: 17-1-002P

**MATERIALS LEGEND**

- 1 FACE BRICK #1 - FIELD
- 2 FACE BRICK #2 - BASE
- 3 FACE BRICK #3 - ACCENT
- 4 EIFS CORNICE
- 5 PRECAST (PC) CORNICE
- 6 BRASS MOUNTING PLATES IN CHANGING ALUM FRAMES
- 7 ARCHYL PC CONC - LIMESTONE
- 8 STRUCT PC CONC #1 - EXP AGG TO MATCH BRICK #1
- 9 STRUCT PC CONC #2 - ACID ETCH TO MATCH BRICK #2
- 10 STRUCT PC CONC #3 - ACID ETCH TO MATCH BRICK #3
- 11 PC CONC #4 TO MATCH PRECAST CORNICE
- 12 PREFINISHED LOUVER
- 13 DECORATIVE WALL LIGHT
- 14 TRANSLUCENT CORNICE BY STAINLESS STEEL SUPPORT
- 15 BUILDING ADDRESS NUMBERS
- 16 LIGHT PALES, METAL HANDLE
- 17 PREFINISHED METAL PANELS
- 18 ORNAMENTAL GUARDRAIL

**EDWARD FARR ARCHITECTS INC**  
 1000 Park Avenue, Suite 100  
 Edina, MN 55425  
 Phone: 763.933.1100  
 Fax: 763.933.1101  
 www.edwardfarr.com

**AURORA Investments, LLC**  
 1000 Park Avenue, Suite 100  
 Edina, MN 55425  
 Phone: 763.933.1100  
 Fax: 763.933.1101  
 www.edwardfarr.com

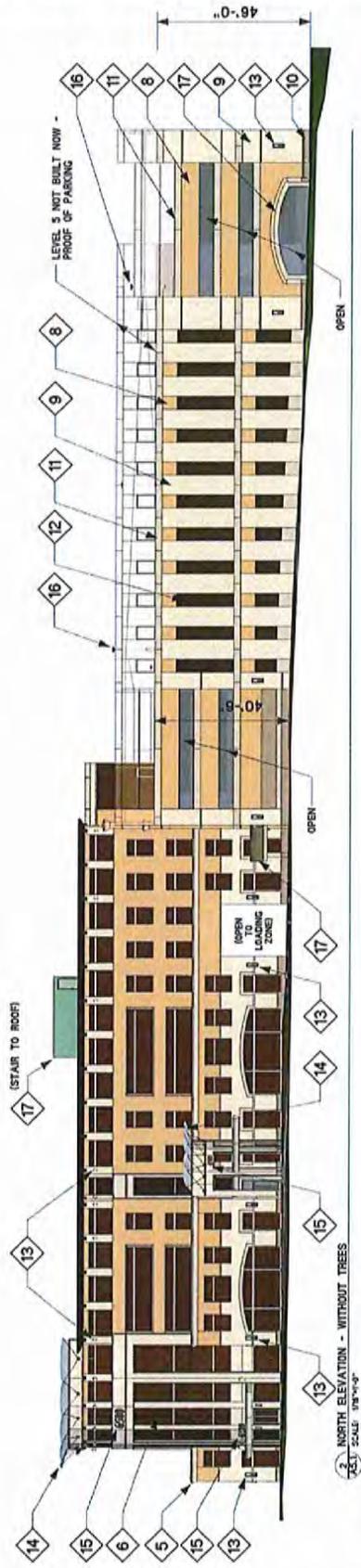
PROJECT: EDINA MEDICAL PLAZA  
 RE-ZONING SUBMITTAL  
 8500 FRANCE AVENUE  
 EDINA, MINNESOTA

DATE: 12/14/2017  
 DRAWN BY: J. HARRIS  
 CHECKED BY: J. HARRIS  
 PROJECT NUMBER: 17-1-002P

SHEET TITLE: NORTH ELEVATIONS  
 SHEET NUMBER: T1.064  
 SCALE: A5.1



1 NORTH ELEVATION - WITH TREES  
 SCALE: 1/8" = 1'-0"



2 NORTH ELEVATION - WITHOUT TREES  
 SCALE: 1/8" = 1'-0"

A45

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Edward A. Farr  
 Project Manager  
 Reg. No. 103302

Architectural Firm

**MATERIALS LEGEND**

- 1 FACE BRICK #1 - FIELD
- 2 FACE BRICK #2 - BASE
- 3 FACE BRICK #3 - ACCENT
- 4 EPS CORNICE
- 5 PRECAST PCI CORNICE
- 6 BRASS MEDICINE CLASS IN CHAMPAGNE ALUM FRAMES
- 7 ARCH'D. PC CONC - LIMESTONE
- 8 STAINL. PC CONC #1 - EXP
- 9 FACE TO MATCH BRICK #1
- 10 BRICK TO MATCH BRICK #2
- 11 PC CONC #4 - ACCENT TO MATCH BRICK #3
- 12 PRECAST CORNICE
- 13 FINISHED LOUVER
- 14 DECORATIVE WALL LIGHT
- 15 TRANSLUCENT CROWN W/ STAINLESS STEEL SUPPORT
- 16 BUILDING ADDRESS NUMBERS
- 17 LIGHT POLES, METAL WALDSE
- 18 PREFINISHED METAL PANELS
- 19 ORNAMENTAL GUARDRAIL



**SOUTH ELEVATION - WITH TREES**  
 SCALE: 1/8"=1'-0"



**AURORA**  
 Investments, LLC

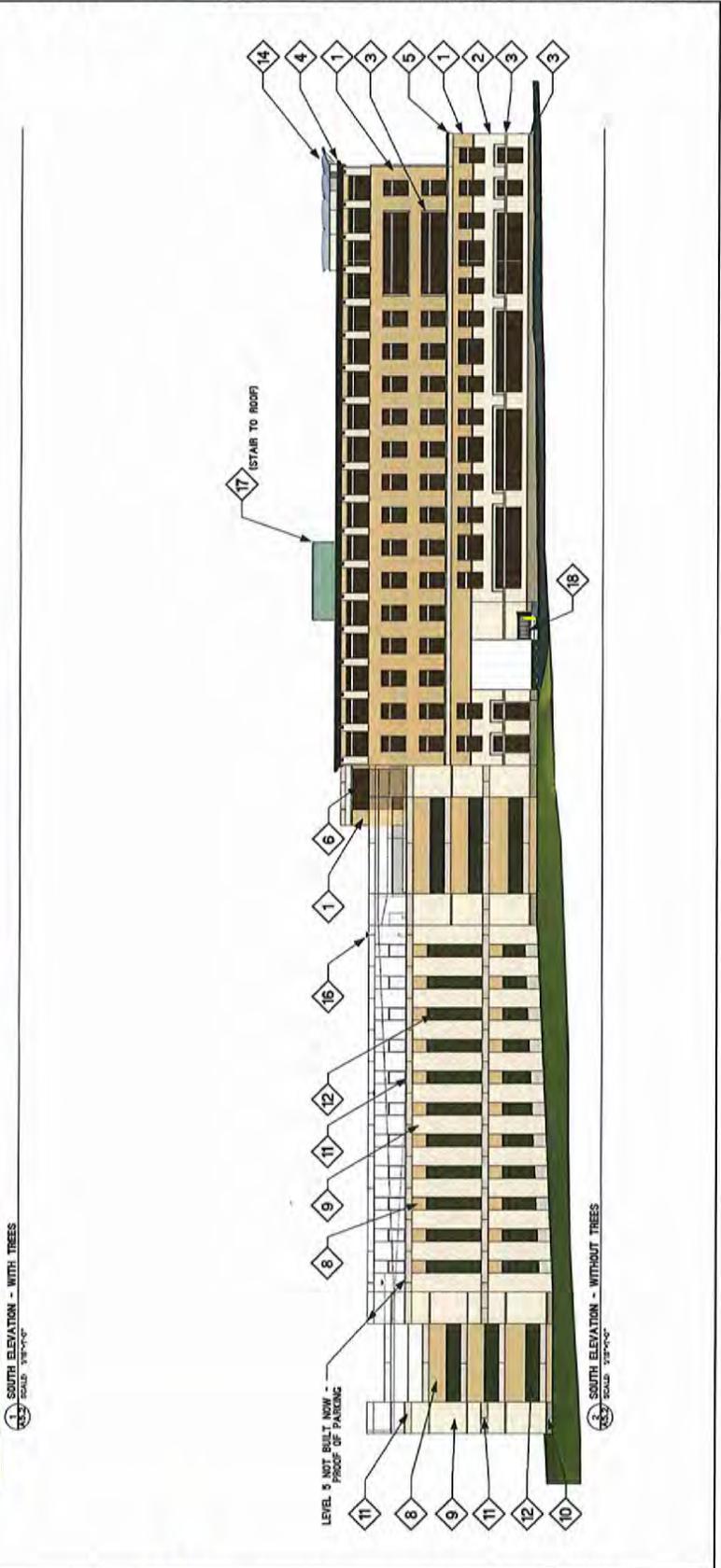
CLIENT

PROJECT  
 AMENT DEVELOPMENT CO.  
 EDINA MEDICAL  
 PLAZA  
 RE-ZONING  
 SUBMITTAL

8600 FRANCE AVENUE  
 EDINA, MINNESOTA

DATE  
 CITY, STATE, ZIP

Sheet Title	SOUTH ELEVATIONS
Project Number	11.064 A5.2



**SOUTH ELEVATION - WITHOUT TREES**  
 SCALE: 1/8"=1'-0"

AKG

EDWARD FARR ARCHITECTS INC. HAS PREPARED THIS SPECIFICATION OF FINISHES FOR THE PROJECT AND HAS NOT CONDUCTED VISUAL VERIFICATION OF THE FINISHES. VISUAL VERIFICATION OF THE FINISHES SHALL BE THE RESPONSIBILITY OF THE ARCHITECT. THE ARCHITECT SHALL BE RESPONSIBLE FOR OBTAINING THE NECESSARY SAMPLES AND APPROVALS FROM THE MANUFACTURERS OF THE FINISHES. THE ARCHITECT SHALL BE RESPONSIBLE FOR OBTAINING THE NECESSARY APPROVALS FROM THE CITY OF EDINA. THE ARCHITECT SHALL BE RESPONSIBLE FOR OBTAINING THE NECESSARY APPROVALS FROM THE CITY OF EDINA. THE ARCHITECT SHALL BE RESPONSIBLE FOR OBTAINING THE NECESSARY APPROVALS FROM THE CITY OF EDINA.

DATE: 11/15/2012  
 PROJECT: EDINA MEDICAL PLAZA  
 DRAWING: EAST AND WEST ELEVATIONS

- MATERIALS LEGEND**
- 1 FACE BRICK #1 - FIELD
  - 2 FACE BRICK #2 - BASE
  - 3 FACE BRICK #3 - ACCENT
  - 4 EPS CORNICE
  - 5 PRECAST EPIC CORNICE
  - 6 BRONZE REFLECTIVE GLASS IN CHAMPAGNE ALUM FRAMES
  - 7 ARCHYL PC CONC - UNSTYRENE
  - 8 STRUCT PC CONC #1 - EXP AGG TO MATCH BRICK #1
  - 9 STRUCT PC CONC #2 - ACD STK TO MATCH BRICK #2
  - 10 STRUCT PC CONC #3 - ACD STK TO MATCH BRICK #3
  - 11 PRECAST #4 TO MATCH PC CONC #4 TO MATCH PRECAST CORNICE
  - 12 PREFINISHED LOUVER
  - 13 DECORATIVE WALL LIGHT
  - 14 TRANSLUCENT CHOWN W/ STAINLESS STEEL SUPPORT
  - 15 BUILDING ADDRESS NUMBERS
  - 16 LIGHT POLES, METAL HALIDE
  - 17 PREFINISHED METAL PANELS
  - 18 ORNAMENTAL GUARDRAIL

**EDWARD FARR ARCHITECTS INC.**  
 2211 Hennepin Avenue, Suite 200, Minneapolis, MN 55425  
 Phone: 612.338.1100  
 Fax: 612.338.1101  
 www.edwardfarr.com

**AURORA INVESTMENTS, LLC**  
 PROJECT: EDINA MEDICAL PLAZA  
 RE-ZONING SUBMITTAL  
 6500 FRANCE AVENUE  
 EDINA, MINNESOTA

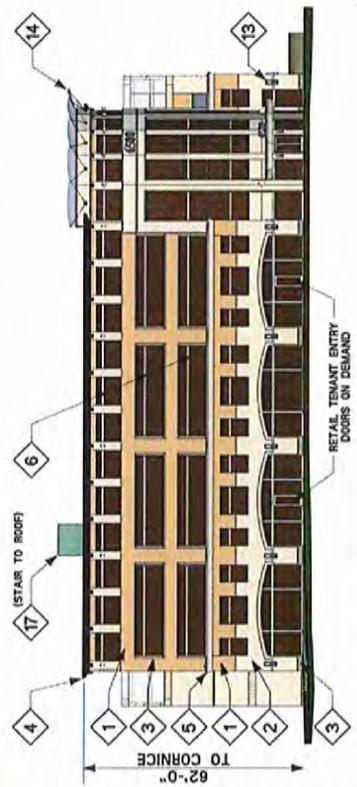
PROJECT: EDINA MEDICAL PLAZA  
 RE-ZONING SUBMITTAL  
 6500 FRANCE AVENUE  
 EDINA, MINNESOTA

DATE: 11/15/2012  
 PROJECT: EDINA MEDICAL PLAZA  
 DRAWING: EAST AND WEST ELEVATIONS

PROJECT NUMBER: 11.064  
 SHEET NUMBER: A5.3



1 EAST ELEVATION - WITH TREES  
 SCALE: 1/8"=1'-0"



3 EAST ELEVATION - WITHOUT TREES  
 SCALE: 1/8"=1'-0"



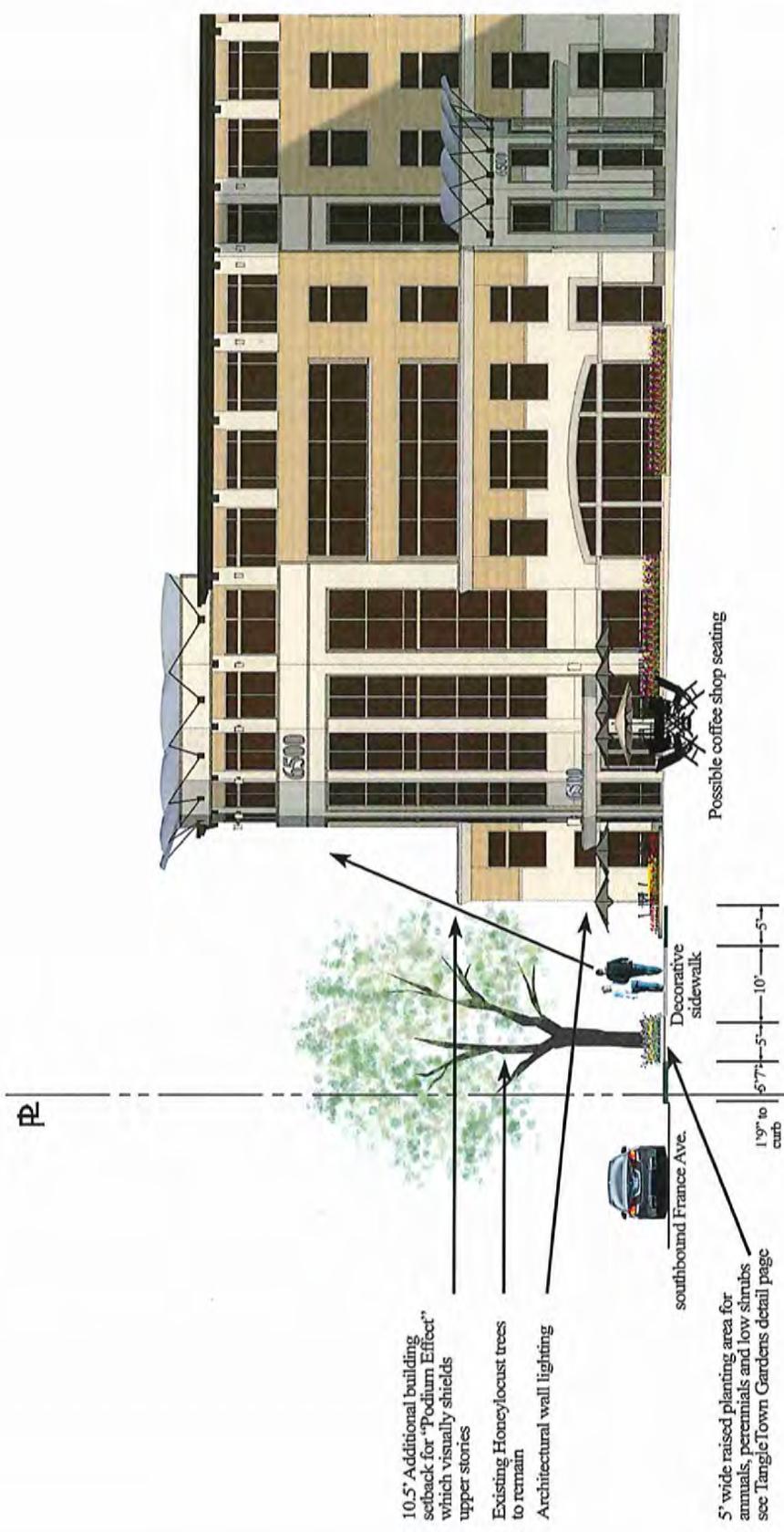
2 WEST ELEVATION - WITH TREES  
 SCALE: 1/8"=1'-0"



4 WEST ELEVATION - WITHOUT TREES  
 SCALE: 1/8"=1'-0"

A47

448



Pedestrian sidewalk at France Avenue looking south

November 6, 2012

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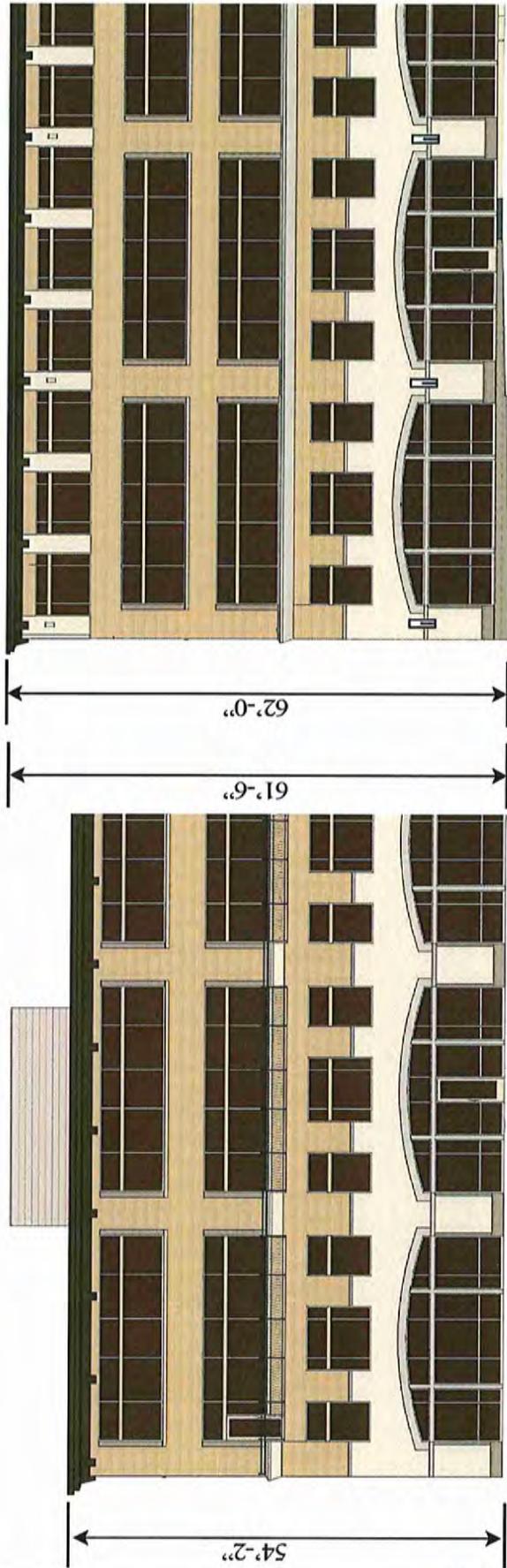
# EDINA MEDICAL PLAZA



AURORA Investments, LLC



MOUNT DEVELOPMENT CO.



Phase 1: 4 Story with RTU's  
(Previous Submittal)

Proposed: 5 Story w/o RTU's

Building Height Comparison

November 6, 2012

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ARCHITECTS INC

EDINA MEDICAL PLAZA

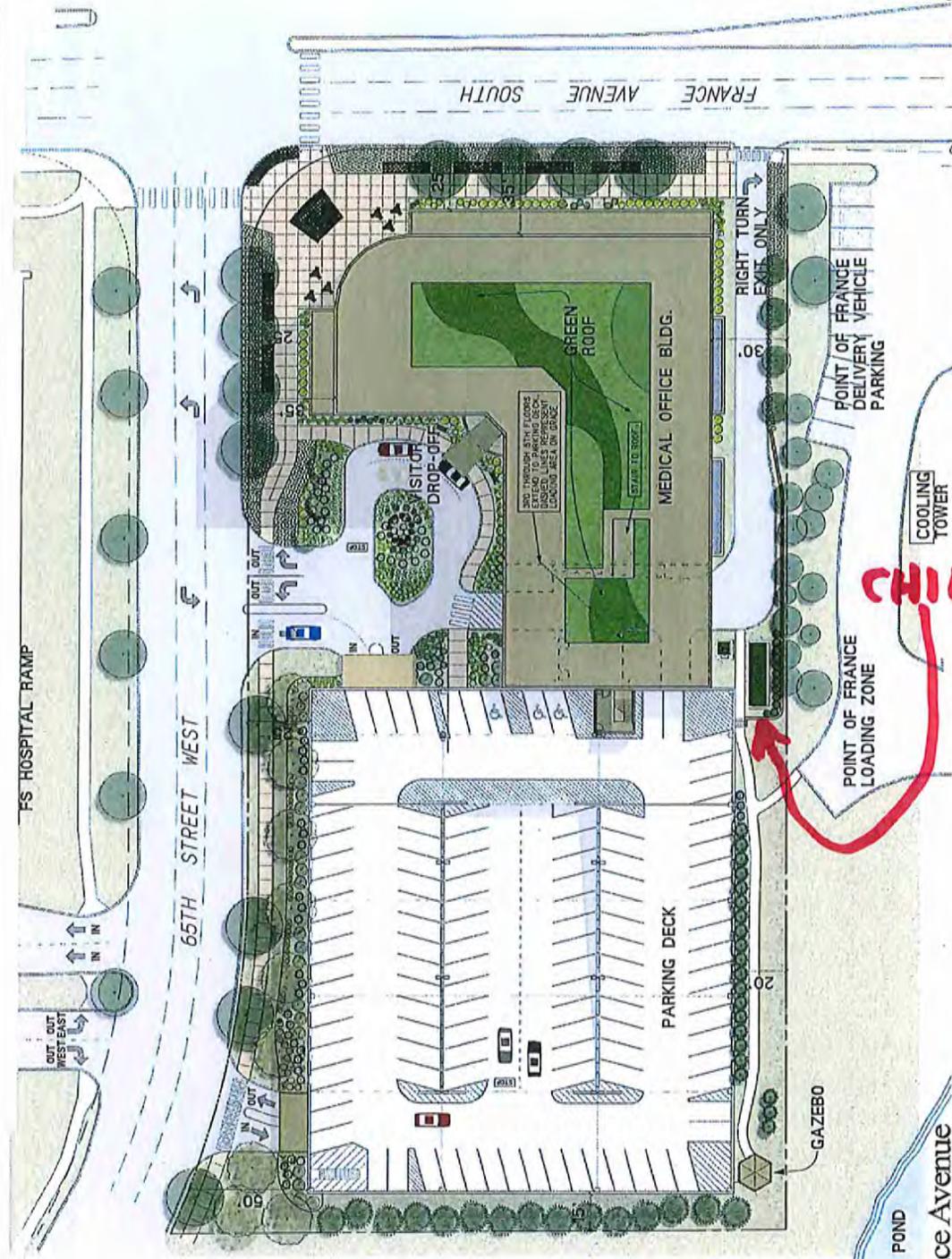


AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

ATA



September 10, 2012

A49a

**PRELIMINARY  
PLAN**

Site Plan  
6500 France Avenue

EDWARD FARR  
ARCHITECTS INC

# EDINA MEDICAL PLAZA



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.



**6500 France Ave. Medical Plaza**  
 Dean Engelmann & Scott Endre, Principals  
 Olivia Spyra, Landscape Designer



**6500 France Ave.**  
 Dean Engelmann & Scott Endre, Principals  
 Olivia Spyra, Landscape Designer

**Planting Concepts  
 for public sidewalk  
 planter areas**

ASD

Edina Medical Plaza



6500 France Avenue South  
5 Stories - 62'

Cornelia Place Apartments



4105 W. 65th Street  
4 Stories - 79'  
(4 level building including gable roof)

Building measurements to the highest point

November 6, 2012

EDWARD FARR  
ARCHITECTS INC

EDINA MEDICAL PLAZA



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

Edina Medical Plaza



6500 France Avenue South  
5 Stories - 62'

Fairview Southdale Hospital



6401 - 6405 France Avenue South  
4 & 8 Stories - 124'

Building measurements to the highest point

November 6, 2012

EDWARD FARR  
ARCHITECTS INC

EDINA MEDICAL PLAZA



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

Edina Medical Plaza



6500 France Avenue South  
5 Stories - 62'

Point of France



6566 France Avenue South  
13 Stories - 123'

Building measurements to the highest point

November 6, 2012

EDWARD FARR  
ARCHITECTS INC

EDINA MEDICAL PLAZA



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

Edina Medical Plaza



6500 France Avenue South  
5 Stories - 62'

Southdale Medical Center



6545 France Avenue South  
6 Stories - 92'

Building measurements to the highest point

November 6, 2012

EDWARD FARR  
ARCHITECTS INC

EDINA MEDICAL PLAZA



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

AS4

Edina Medical Plaza



6500 France Avenue South  
5 Stories - 62'

Southdale Office Centre



6600 & 6800 France Avenue South  
6 & 7 Stories - 99'

Building measurements to the highest point

November 6, 2012

EDWARD FARR  
ARCHITECTS INC

EDINA MEDICAL PLAZA



AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

A55

Edina Medical Plaza



6500 France Avenue South  
5 Stories - 62'

— 87.0' Twin Cities Orthopedic

— 62.0' Edina Medical Plaza

Twin City Orthopedics



4010 West 65th Street  
4 Stories - 82' & 85'



Building Overlay

Building measurements to the highest point

November 6, 2012

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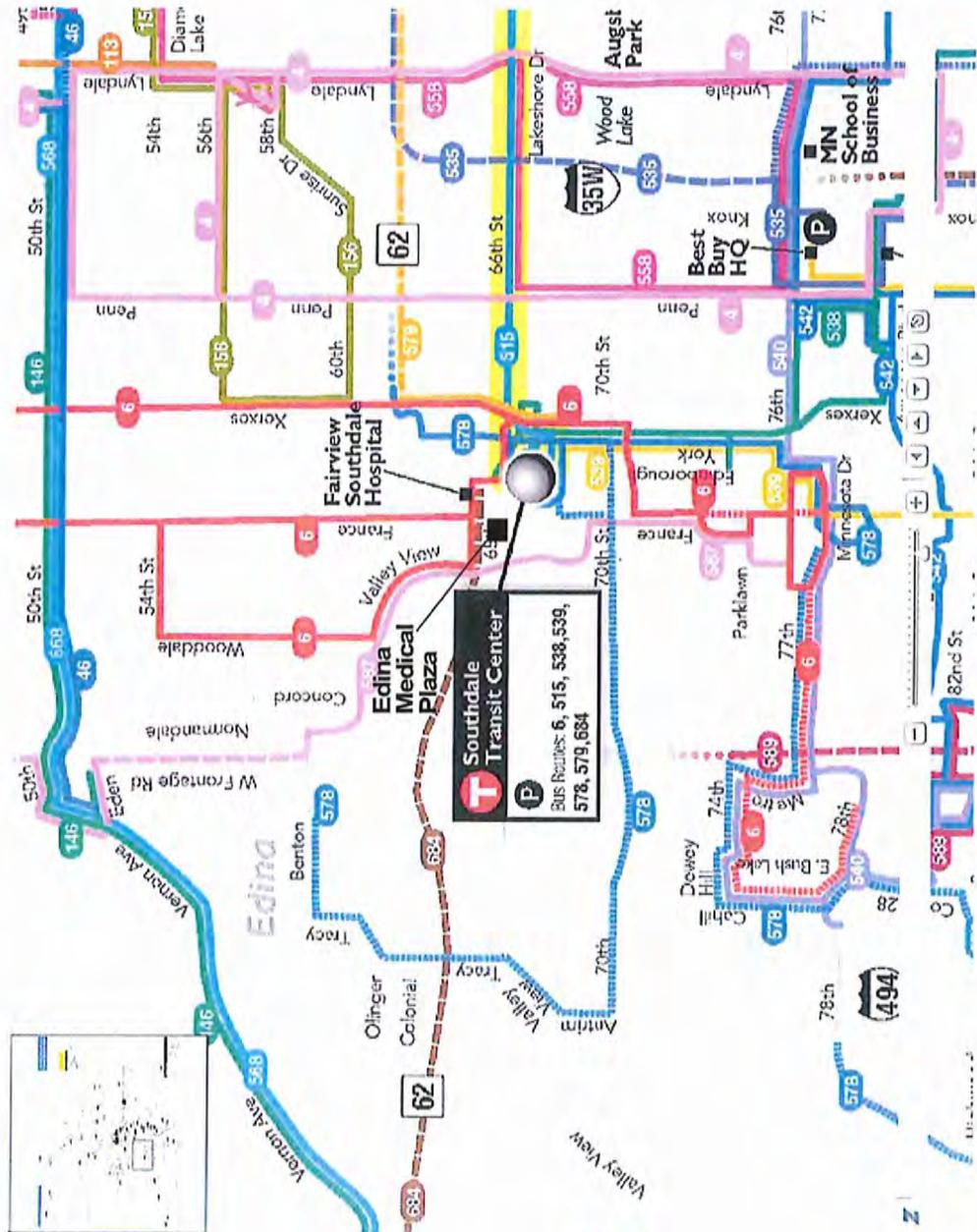
AURORA  
Investments, LLC



MOUNT DEVELOPMENT CO.

EDINA MEDICAL PLAZA

A56



## Bus Routes

- 1 - 65 St W & Southdale Medical Bldg  
 Stop ID: 1156
  - Route 6 - South
- 2 - 65 St W & Forewer Hospital  
 Stop ID: 4269
  - Route 6 - North
- 3 - 65 St W & Drew Av S  
 Stop ID: 1163
  - Route 6 - North
- 4 - Francis Av S & Hwy 62  
 Stop ID: 47760
  - Route 6 - North
- 5 - Drew Av S & 65 St W  
 Stop ID: 49110
  - Route 6 - South
- 6 - Francis Av S & 80314 / 40316  
 Stop ID: 44196
  - Route 6 - South
- 7 - 65 St W & Valley View Rd  
 Stop ID: 62864
  - Route 6 - South
- 8 - 65 St W & Valley View Rd  
 Stop ID: 52653
  - Route 6 - North
- 9 - Valley View Rd & 65 St W  
 Stop ID: 1176
  - Route 577 (Express) - North
- 10 - Francis Av S & Hwy 62 / 62 St W  
 Stop ID: 47791
  - Route 6 - North
- 11 - Francis Av S & 62226  
 Stop ID: 44197
  - Route 6 - South
- 12 - 65 St W & Drew Av S  
 Stop ID: 4370
  - Route 6 - North
- 13 - Valley View Rd & 65 St W  
 Stop ID: 1261
  - Route 577 (Express) - South
- 14 - Drew Rd & 65 St W  
 Stop ID: 4367
  - Route 578 (Express) - North
- 15 - Valley View Rd & 66 St W  
 Stop ID: 4026
  - Route 577 (Express) - North
- 16 - Valley View Rd & Brookview Av  
 Stop ID: 1127
  - Route 6 - North
- 17 - Francis Av S & Hollox Av  
 Stop ID: 44156
  - Route 577 (Express) - South
- 18 - Francis Av S & Hollox Av  
 Stop ID: 1127
  - Route 6 - North
- 19 - Francis Av S & 62 St W  
 Stop ID: 1153
  - Route 6 - South
- 20 - Baine Rd & 85417-6447  
 Stop ID: 4946
  - Route 578 (Express) - South
- 21 - Baine Rd & 85417-6447  
 Stop ID: 4245
  - Route 578 (Express) - North
- 22 - 65 St W & Baine Rd  
 Stop ID: 4366
  - Route 6 - North
- 23 - Valley View Rd & 66 St W  
 Stop ID: 6136
  - Route 577 (Express) - North
- 24 - Valley View Rd & 66 St W  
 Stop ID: 6135
  - Route 577 (Express) - South
- 25 - Francis Av S & Hollox Av  
 Stop ID: 44156
  - Route 6 - North
- 26 - Francis Av S & Hollox Av  
 Stop ID: 1127
  - Route 6 - South
- 27 - Francis Av S & 62 St W  
 Stop ID: 1153
  - Route 6 - North



New Station

A57



## General Information

### Why ZAP?



#### Easy to Use

DERO ZAP makes it easy to track and register bike registrations and instructor RFID capability at this year's conference. In one interactive and powerful web application for a variety of event planning tasks.

#### An Ounce of Prevention

A simple amount of exercise every day helps to prevent physical and cognitive mental problems. Get your employees in and productive early by using ZAP to help with your health and wellness goals.

#### Save the Planet

Cycling is a zero-emission mode of transportation. Encouraging employees to commute to work by bicycle reduces the most significant greenhouse gas emissions from the transportation sector.

#### Car Parking is Expensive

The average car parking space costs \$1,000 - \$2,000 per month. Encouraging employees to commute to work by bicycle reduces the most significant greenhouse gas emissions from the transportation sector.



### How Does It Work?



An RFID tag like the one pictured above is attached to the handle of each commuter's bike wheel. The tag's unique ID number will be detected if anyone...



DERO ZAP stations are placed at your facility. The stations are solar powered and wireless, requiring no maintenance.

When a commuter like the station, an RFID tag will be detected by the station...

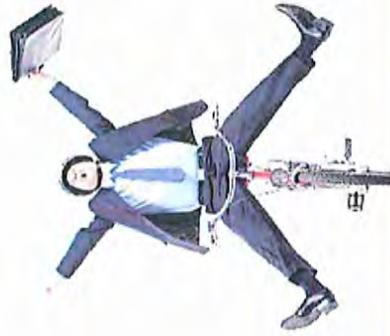


Users log into the application to view their ZAP. Commuters may view their bike status, and administrators may view statistics for their entire organization. Data will be available within 24 hours of the upload. Images are displayed to a computer for further investigation.

DERO ZAP stations are solar powered and wireless, requiring no maintenance. When a commuter like the station, an RFID tag will be detected by the station...



www.dero.com 1.800.337.6729



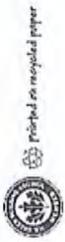
### ZAP

RFID Technology meets Bike Commuting Realities

Many organizations in the private, public and non-profit sectors want to promote commuting by bike as an environmentally friendly and healthful transportation alternative. To get bike commuting rolling they need a practical way to tally and manage bike trip data. Dero has the solution — Dero ZAP.

DERO ZAP is a solar-powered, automated RFID (Radio Frequency Identification) system with integrated hardware and software. Via a web-based interface to the Dero ZAP system software, participants are enrolled and registered with a unique RFID tag. The tamper-proof tags are attached to the spokes of each participant's bike's front wheel. ZAP units mounted on steel poles around your campus identify registered bikes from up to 30 feet away.

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### BIKE FILE

u-lock compatible  
slightly offset for easy access  
Modular design for easy installation

The Bike File is our most space efficient u-lock compatible product. Sturdy sliding hangers allow nine bikes to be securely stored in an eight-foot section while allowing for easy loading and unloading of bikes.

Ceiling mount, wall mount and floor mount options are available.

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A54



smart Expressions



open your mind.



Car ownership is so yesterday. With HOURCAR you can reserve a car when you need it, drive it where you want to, let someone else worry about car payments, parking, insurance, gas, and maintenance, and be an environmental superstar. At the same time!

**HOURCAR is the smart way to drive.**

HOURCAR offers 35 cars (locations) in Minneapolis and Saint Paul. Find out where your nearest HOURCAR lives, and sign up (join) to drive today.



**Latest News**

**REVENGE OF THE ELECTRIC CAR**

HOURCAR Co-Sponsors "Revenge of the Electric Car" Screening

<http://www.hourcar.com/press-releases/revge-of-the-electric-car-screening>

November 2, 2011

On Monday, November 14, HOURCAR is co-sponsoring a special screening (see <http://www.hourcar.com/press-releases/revge-of-the-electric-car-screening>) of the new film *Revenge of the Electric Car*. Sponsored by Fresh Energy (<http://www.freshenergy.com>), this special event includes a pre-screening reception and a panel discussion after the film with director Chris Paine and Fresh Energy Executive Director Michael Noble.



New Plug-In HOURCARs Join Saint Paul Fleet

<http://www.hourcar.com/press-releases/join-saint-paul-fleet>

October 4, 2011



smart for two electric drive

>> the next generation of smart

AS1









