



PLANNING COMMISSION STAFF REPORT

Originator Cary Teague Community Development Director	Meeting Date May 22, 2013	Agenda # VI.C.
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INFORMATION/BACKGROUND

Project Description

Fairview Southdale Hospital is proposing to build a two-level 88,974 square foot addition for a new Emergency Care and Urgent Care Center at their site at 6401 France Avenue. (See location on pages A1–A5.) The existing Emergency and Urgent Care facility would be remodeled for hospital use. (See project narrative on pages A6–A19; and the proposed plans on pages A20–A37.)

To accommodate the proposed addition, the following is requested:

1. Site Plan Review.
2. Drive-aisle setback from 20-feet to 6-10 feet for the new round-a-bout drop off and pick up area and drive aisle in front of the addition adjacent to Crosstown Highway. (See page A28.)
3. Parking Stall Variance from 2,728 to 2,051 spaces.

Surrounding Land Uses

- Northerly: Crosstown Highway 62 further north, single-family homes, zoned and guide low-density residential
- Easterly: The Colony Condominiums; zoned and guided for high-density residential
- Southerly: Southdale Medical Building; zoned and guided Regional Medical District
- Westerly: Fairview Hospital parking ramp

Existing Site Features

The subject property is 13.8 acres in size and contains the Fairview Southdale Hospital and parking ramp, and a freestanding medical office building. The site is

16.7 acres in size when adding the parking ramp site on the west side of France. (See pages A3–A4.)

Planning

Guide Plan designation: RM, Regional Medical (See page A5a.)
Zoning: RMD, Regional Medical District. (See page A2.)

Site Access

The primary access to the site would remain be off of 65th Street West. Users of the new facility would just continue north and west to the new Emergency and Urgent Care. The same parking ramp would be used. The site will continue to be two way all around the hospital. (See page A5 and A26.)

Parking & Traffic

As mentioned above, a parking space variance is requested to accommodate the required number of parking spaces for the site. Based on the square footage of the hospital, medical offices, expanded emergency room, number of doctors and beds, 2,728 parking spaces are required for the entire site; that includes the anticipated number of employees as a result of the expansion. There are 2,051 parking spaces on the site including both parking ramps.

WSB and Associates completed a traffic and parking study for the existing and proposed hospital operations. (See study on pages A37–A85.) The study concludes that the existing adjacent roadways can support the proposed addition, and there would be adequate parking provided. (See pages A49 –A50 of the study.) Although study concludes that no improvements are required at this time, in the future at France Avenue/65th Street intersection they are anticipated. Should delays and queuing become an issue in the future, minor intersection turn lane and phasing improvements may be necessary. Should these improvements be required in the future Fairview Southdale Hospital would be responsible for their share of those improvements.

Landscaping

The proposed landscape plan exceeds the City's requirements. Based on the perimeter of the site, 85 over story trees and a full complement of understory trees and shrubs are required. The site would contain 164 existing and proposed over story trees around the site & a full complement of understory trees and shrubs. (See landscape plan on pages A31–A32.)

Grading & Utilities

The city engineer has reviewed the plans and found them generally acceptable and offered comments and conditions for approval. (See city engineer memo on page A115.) These shall be made part of the conditions for approval of the project. Plans shall be subject to review and approval of the Minnehaha Creek Watershed District.

Building Materials

The building addition would be light in color to generally match the hospital; building materials would include architectural cast stone, stone, metal panel, curtain wall and glass. (See renderings on pages A21–A25.) The applicant will present a materials board at the Planning Commission meeting.

Variance – Drive Aisle Setback

Per the Zoning Ordinance, a variance should not be granted unless it is found that the enforcement of the ordinance would cause practical difficulties in complying with the zoning ordinance and that the use is reasonable. As demonstrated below, staff believes the proposal does meet the variance standards, when applying the three conditions:

Minnesota Statutes and Edina Ordinances require that the following conditions must be satisfied affirmatively. The Proposed Variance will:

1) Relieve practical difficulties that prevent a reasonable use from complying with ordinance requirements.

Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. "Practical difficulties" may include functional and aesthetic concerns.

Staff believes the proposed variance is reasonable. The access drive is located adjacent to Crosstown Highway 62. (See page A28.) There is no direct access to the highway from the hospital site. The hospital site is elevated from Crosstown; therefore, the paved surface would not be visible from the highway. The practical difficulty is caused by the right-of-way taken for the freeway entrance ramp. As a result there is excess green space (40 feet) between the highway and the round-a-bout turn around; which would appear that there is adequate separation from the highway.

- 2) ***There are circumstances that are unique to the property, not common to every similarly zoned property, and that are not self-created?***

Yes. The Hospital site is unique in the RMD zoning district. There are no other hospital sites within the district.

- 3) ***Will the variance alter the essential character of the neighborhood?***

No. There is adequate (40 feet) green space between the hospital drive aisle and the Crosstown Highway. There is also a 5-foot chain link fence separating the uses.

Variance – Parking Stalls

Per the Zoning Ordinance, a variance should not be granted unless it is found that the enforcement of the ordinance would cause practical difficulties in complying with the zoning ordinance and that the use is reasonable. As demonstrated below, staff believes the proposal does meet the variance standards, when applying the three conditions:

Minnesota Statutes and Edina Ordinances require that the following conditions must be satisfied affirmatively. The Proposed Variance will:

- 1) ***Relieve practical difficulties that prevent a reasonable use from complying with ordinance requirements.***

Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. "Practical difficulties" may include functional and aesthetic concerns.

Staff believes the proposed parking stall variance is reasonable. A parking study was conducted by WSB Associates that concludes that the City Code required parking is not necessary for the Hospital site. The study concludes that the hospital could function adequately with 1750 and 1850 parking spaces. There are an existing 2,051 spaces on the site. (See pages A49–A50 of the parking study.)

When the hospital was expanded back in 2000, the specific City Code calculation was not done, rather, a parking analysis was done which concluded that there would be adequate parking. (See minutes from Planning Commission and City Council meetings on pages A86–A114.) That study has proven to be accurate, as the parking has functioned adequately. The City of Edina does not want to require unnecessary paving or parking ramps for parking stalls that are not needed.

2) There are circumstances that are unique to the property, not common to every similarly zoned property, and that are not self-created?

Yes. Again, the hospital site is unique in the RMD zoning district. It is the only regional hospital in Edina. There are no other 16.5 acre sites held in common ownership and zoned the same within the City. Again, the City would rather see green space and development rather than large parking lots or ramps that are not needed.

3) Will the variance alter the essential character of the neighborhood?

No. The alternative to the variance would be to require the applicant to construct a larger and taller structured parking ramp, either on the west side of France or on hospital site. Based on the parking study done by WSB, this parking would not be needed.

Compliance Table

	City Standard (PCD-3)	Proposed
<u>Building Addition Setbacks</u>		
Front – Crosstown Highway	35 feet	65 feet
Front – West 65 th Street	35 feet	200+ feet
Front – France Avenue	35 feet	200+ feet
Side – East	100 feet	200+ feet
<u>Parking & Drive Aisle Setbacks</u>		
Front – Crosstown Highway	20 feet	6 & 10 feet*
Front – West 65 th Street	20 feet	200+ feet
Front – France Avenue	20 feet	200+ feet
Side – East	10 feet	200+ feet
Building Height	Twelve Stories or 144 feet whichever is less	Two Stories & 32 feet
Maximum Floor Area Ratio (FAR)	1.0 of the tract Tract size = 16.7 acres or 728,559 s.f.	.94 of the tract Gross s.f. = 687,257 s.f.
Parking Stalls (Site)	2,728** spaces based on the 1 space per bed; 1 space per 200 square feet of medical office and 1 space per employee on a major shift	2,051 spaces*
Parking Stall Size	8.5' x 18'	8.5 x 18'
Drive Aisle Width	24 feet	24 feet

*** Variance Required**

**** Assumes a 10% increase in staff with the addition**

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issue

- **Is the proposed emergency and urgent care addition and associated Variances reasonable for this site?**

Yes. Staff believes the proposal is reasonable for six reasons:

1. The proposed use is permitted in the RMD, Regional Medical District.
2. The proposed building setbacks are met.
3. The proposed addition could be supported by the existing roadway system, based on the traffic and parking study done by WSB and Associates. (See pages A37– A85.)
4. With the exception of the drive-aisle setback and parking stall variances, the proposal meets all minimum zoning ordinance requirements.
5. The variances are reasonable. The drive-aisle setback variance would have no impact on Crosstown Highway. There would still be a 40-foot green space separation between the freeway on ramp and the hospital drive-aisle. The parking study concludes that the proposed addition and existing uses on the site would be supported by the existing parking facilities. Traditionally, the City of Edina has not required parking stalls, when they are not needed. Additional parking could be provided by adding levels to the existing parking ramps if needed.
6. Fairview Hospital provides a convenient community asset that provides a vital service to the City of Edina and surrounding area.

Staff Recommendation

Recommend that the City Council approve the Site Plan with Variances for Fairview Southdale Hospital.

Approval is based on the following findings:

1. The proposal would meet the required standards and ordinances for a Site Plan with the exception of the parking space variance and drive aisle setback variance.

2. WSB conducted a parking and traffic impact study. The study concluded that the existing roadway system would support the proposed project; and the parking on the site would contain adequate parking to support the expansion and existing uses.
3. The variances requested are reasonable. The drive-aisle setback variance would have no impact on Crosstown Highway. There would still be a 40-foot green space separation between the freeway on ramp and the hospital drive-aisle. The parking study concludes that the site would be adequately parked.
4. The west parking ramp could be expanded should there ever be a need for additional parking for the site.

Approval of the Site Plan is subject to the following conditions:

1. Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, unless modified by the conditions below:
 - Site plan date stamped April 24, 2013.
 - Grading plan date stamped April 24, 2013.
 - Landscaping plan date stamped April 24, 2013.
 - Building elevations date stamped April 24, 2013.
 - Building materials board as presented at the Planning Commission and City Council meeting.
2. Prior the issuance of a building permit, a final landscape plan must be submitted, subject to staff approval. Additionally, a performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures.
3. The property owner is responsible for replacing any required landscaping that dies.
4. Submit a copy of the Minnehaha Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
5. Compliance with the conditions required by the city engineer in his memo dated May 17, 2013.
6. Should delays and queuing become an issue at the France Avenue/65 Street intersection in the future, minor intersection turn lane and phasing improvements may be necessary. Should these improvements be required

in the future Fairview Southdale Hospital will be responsible for their share of those improvements.

7. Fire lanes must be signed subject to review and approval of the fire marshal.

Deadline for a city decision: August 20, 2013

City of Edina

- Legend**
-  Highlighted Feature
 -  Street Name Labels
 -  City Limits
 -  Creeks
 -  Lake Names
 -  Lakes
 -  Parks
 -  Parcels



PID: 2902824230169

6401 France Ave S
Edina, MN 55410



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City of Edina



- Legend**
- Surrounding House Number Labels
 - House Number Labels
 - Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Zoning
 - APD (Automobile Parking District)
 - MDD-4 (Mixed Development District)
 - MDD-5 (Mixed Development District)
 - MDD-6 (Mixed Development District)
 - PCD-1 (Planned Commercial District)
 - PCD-2 (Planned Commercial District)
 - PCD-3 (Planned Commercial District)
 - PCD-4 (Planned Commercial District)
 - PID (Planned Industrial District)
 - POD (Planned Office District)
 - POD-1 (Planned Office District)
 - POD-2 (Planned Office District)
 - PRD-1 (Planned Residential District)
 - PRD-2 (Planned Residential District)
 - PRD-3 (Planned Residential District)
 - PRD-4 (Planned Residential District)
 - PRD-5 (Planned Residential District)
 - PSR-4 (Planned Senior District)
 - R-1 (Single Dwelling Unit)
 - R-2 (Double Dwelling Unit)
 - RMD (Regional Medical District)
 - Parcels



PID: 2902824230169

6401 France Ave S
Edina, MN 55410



City of Edina



- Legend**
- Surrounding House Number Labels
 - House Number Labels
 - Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Parcels
 - 2012 Aerial Photo

Map created with ArcGIS. Copyright (C) 2008 GIS 2196



PID: 2902824230169

**6401 France Ave S
Edina, MN 55410**



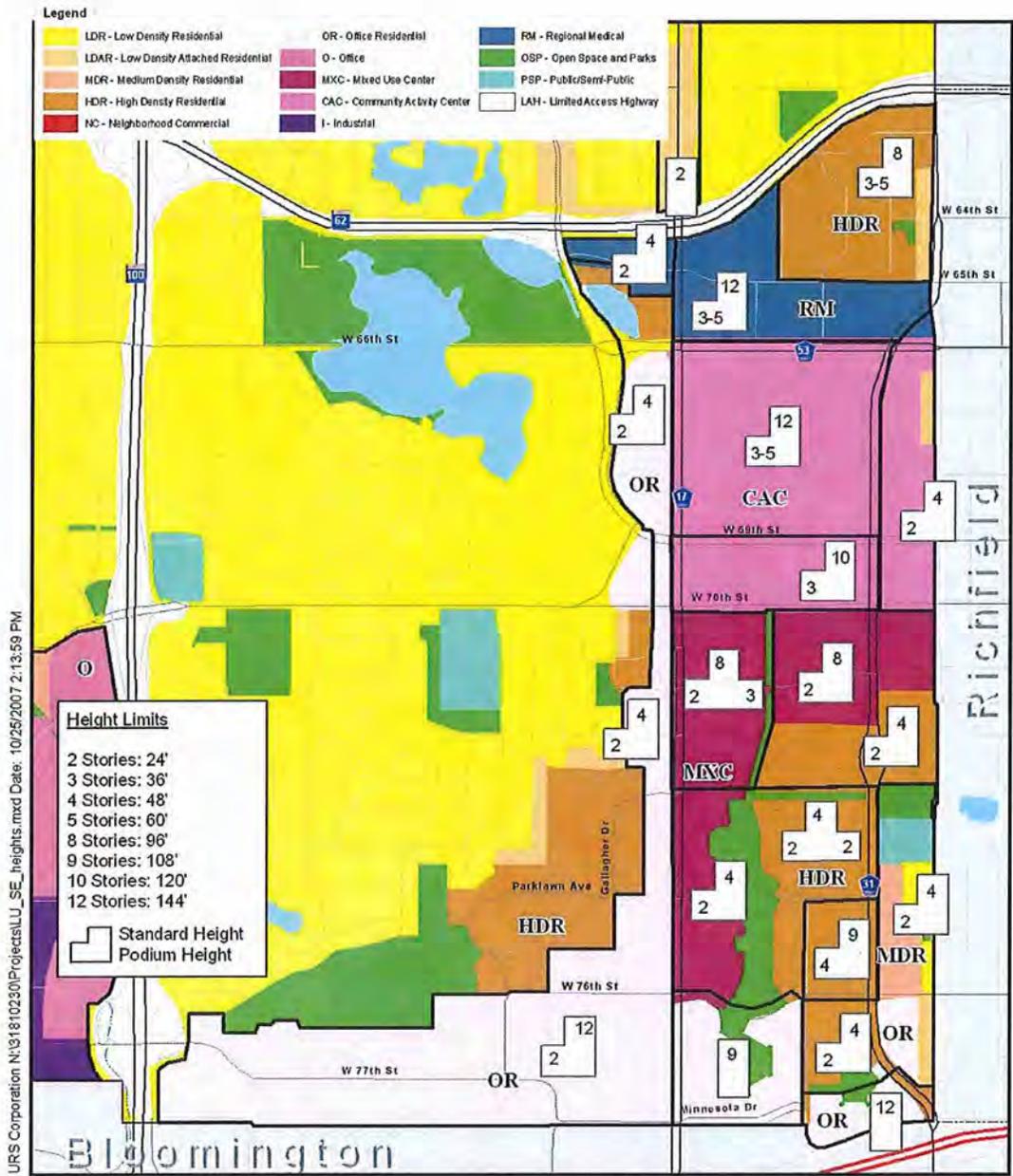


Map provided with AerialGIS. Copyright (C) LOCUS GIS 2015

A4



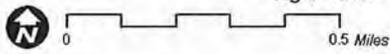
A5



City of Edina
2008 Comprehensive Plan Update

Data Source: URS

Future Land Use Plan with Building Heights
Southeast Quadrant
Figure 4.6B



ASg

Fairview Southdale Hospital – Emergency Center Expansion

22 April 2013, Walker & Associates

Site Plan Application – Written Statement

Introduction

For nearly fifty years Fairview Southdale Hospital has fulfilled its mission to provide high quality, low cost, exceptional healthcare for patients and families. Completion of this proposed addition in 2015 will mark a milestone in Fairview’s continued growth since completion of the original building in 1965.

Project Drivers

- Emergency room visits to this facility are expected to grow to approximately 50,000 per year
- Current Emergency Department occupies approximately 15,000 square feet and is at capacity
- Increase capacity and flexibility while improving flow and integration of services
- Optimize use of site and improve access to all functions
- Strengthen brand identify from Highway 62 and France Avenue

Project Components

- Combines Emergency Care with Urgent Care in one integrated and efficient space
- Improves site access to the Emergency Department / Urgent Care and other functions
- Adds 88,974 square feet of new built-out and shell space on three levels and 9,420 square feet of renovated space
- Adds an Observation unit to accommodate patient stays of less than 24 hours
- Relocates the existing helipad to the roof of the east wing
- Expands the existing Central Plant to serve the additional program area

PLANNING DEPARTMENT
APR 24 2013

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Design Considerations

- Over the past approximately eight months Fairview Southdale Hospital has worked closely with their Design Team to move this project forward. This highly skilled team of consultants provides professional services including masterplanning, site survey, geotechnical, environmental, architectural, civil engineering, landscape design, interior design, structural engineering, medical equipment planning, emergency department planning, lean planning, virtual reality, cost estimating, furniture, fixture, and equipment planning, mechanical engineering, electrical engineering, low voltage planning, helipad planning, traffic and parking planning, construction auditing, special inspections and testing, commissioning, signage design, elevator planning, and project management.
- The team has enrolled this project in Xcel Energy's Design Assistance Program and is working with Xcel's facilitator, the Weidt Group, to analyze the proposed project, create an energy model to predict energy use, develop strategies to conserve energy, implement the selected strategies, and then monitor the results.
- We looked closely at providing additional sustainable features in this building such as a green roof. There are limited opportunities to provide significant areas of green roof and many of these are interrupted by mechanical equipment. In the end we elected to focus our effort on the new building systems, mechanical systems, and electrical systems that will make this project as energy efficient as possible.

Key Team Members

Fairview Southdale Hospital has selected several key team members to assist them with this project. These team members are highly experienced in healthcare development and have worked with Fairview Southdale Hospital on past projects...

- HGA Architects
- Dunham
- The Weidt Group
- Knutson Construction
- Walker & Associates

READING DEPARTMENT
APR 24 2013
CITY OF MINNAPOLIS

A7

Fairview Southdale Hospital – Emergency Center Expansion

6 May 2013, Walker & Associates

Site Plan Application – Written Statement

Major Project Components

With improved access and a combined Emergency Care and Urgent Care, the completed addition provides 88,974 square feet of new built-out and shell space on three levels and 9,420 square feet of renovated space. Included in the project is an Observation Unit at the Second Level to accommodate patient stays of less than 24 hours, a relocated helipad at the Third Level roof of the east wing, and expansion of the existing Central Plant to serve the additional program area.

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Variance Application – Setback

To allow easy movement within the site for emergency and other large vehicles, we propose to reduce the required setback from the required ten feet to seven feet in a limited area adjacent to the outermost curve of the roundabout.

Variance Application – Parking

Actual average daily peak usage at the site is currently 1,495 parking spaces and the highest recorded parking usage is 1,690 parking spaces. We propose to reduce the required 2,575 spaces to 2,051 parking spaces. This amount of parking is still a full 37 percent more than the actual average daily peak and 21 percent more than the highest recorded parking usage.

Fairview Southdale Hospital – Emergency Center Expansion

22 April 2013, Walker & Associates

Variance Application – Parking

By the Zoning Ordinance, the Parking Requirement for this project is approximately 2,575 spaces. When this project is completed, the campus will provide 2,051 parking spaces. The actual average daily peak usage at the site is currently 1,495 parking spaces and the highest recorded parking usage is 1,690 parking spaces. We request a Parking Variance to accommodate construction of this new project.

Condition	Answer	Explanation
<i>Relieve practical difficulties in complying with the zoning ordinance and that the use is reasonable</i>	Yes	Long term monitoring of parking use by the Owner confirms that the current parking provided on this campus easily meets the current parking need. Projections indicate that the existing parking arrangement will serve the parking needs of the expanded facility as well. This approach to serving the parking need is proven and reasonable.
<i>Correct extraordinary circumstances applicable to this property but not applicable to other property in the vicinity or zoning district</i>	Yes	This facility is landlocked on all sides and does not have the option to construct additional areas of surface parking. Most of the parking needs for the facility are served by parking ramps at the east and west sides of the campus. Current and future parking needs are well met by the current parking arrangement.
<i>Be in harmony with the general purposes and intent of the zoning ordinance</i>	Yes	The current parking arrangement provides ample, convenient, and safe parking in accordance with the general purpose and intent of the zoning ordinance.
<i>Not alter the essential character of a neighborhood</i>	Yes	The current parking arrangement has been in place since 2002 and fits well with the surrounding neighborhood.

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PLANNING DEPARTMENT
 APR 24 2013
 CITY OF EDINA

Fairview Southdale Hospital – Emergency Center Expansion

22 April 2013, Walker & Associates

Existing Condition

<i>PID, Property Address</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Site Area in Square Feet, Acres</i>	<i>Floor Area in Square Feet*</i>	<i>Gross Floor Area in Square Feet**</i>	<i>Parking Spaces</i>
3002824140001, 6400 France Avenue South	West Parking Ramp, IRET Properties	125,484 2.88	0	0	1,018 structured
2902824230169, 6401 France Avenue South	Hospital, East Parking Ramp, and Central Plant, Fairview Southdale Hospital	531,671 12.21	561,562 Hospital, 12,868 Central Plant	514,783	74 surface, 975 structured
2902824230170, 2902824230171, 6405 France Avenue South	Medical Office Building, IRET Properties	21,021 0.48	60,275	Estimated 54,248	0
2902824230166, 6363 France Avenue South	Medical Office Building, IRET Properties	44,402 1.02	77,136	Estimated 69,442	0
2902824230167, Address Unassigned	Unbuilt, Fairview Southdale Hospital	5,981 0.14	0	0	0
Totals		728,559 16.73	711,841	638,473	2,067

* Excludes parking ramps only

** Excludes parking ramps, mechanical rooms, restrooms, elevator shafts, stairwells, and similar spaces according to the Zoning Ordinance

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APR 24 2013

Completed Condition

<i>PID, Property Address</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Site Area in Square Feet, Acres</i>	<i>Floor Area In Square Feet*</i>	<i>Gross Floor Area in Square Feet**</i>	<i>Parking Spaces</i>
3002824140001, 6400 France Avenue South	West Parking Ramp, IRET Properties	125,484 2.88	0	0	1,018 structured
2902824230169, 6401 France Avenue South	Hospital, East Parking Ramp, and Central Plant, Fairview Southdale Hospital	531,671 12.21	650,536 Hospital, 13,200 Central Plant	Estimated 594,860	58 surface, 975 structured
2902824230170, 2902824230171, 6405 France Avenue South	Medical Office Building, IRET Properties	21,021 0.48	60,275	Estimated 54,248	0
2902824230166, 6363 France Avenue South	Medical Office Building, IRET Properties	44,402 1.02	77,136	Estimated 69,442	0
2902824230167, Address Unassigned	Unbuilt, Fairview Southdale Hospital	5,981 0.14	0	0	0
Totals		728,559 16.73	801,147	Estimated 718,550	2,051

* Excludes parking ramps only

** Excludes parking ramps, mechanical rooms, restrooms, elevator shafts, stairwells, and similar spaces according to the Zoning Ordinance

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Parking Required

<i>Property Address</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Licensed Patient Beds</i>	<i>Peak Shift</i>	<i>Peak Employees Per Shift</i>	<i>Required Parking*</i>
6401 France Avenue South	Hospital, Fairview Southdale Hospital	326	Day	1,525	1,851
6405 France Avenue South	Medical Office Building, IRET Properties	0	Day	180	Estimated 320
6363 France Avenue South	Medical Office Building, IRET Properties	0	Day	175	Estimated 404
Totals		326		1,880	2,575

*According to the Zoning Ordinance, Required Parking is calculated as follows; Medical or Dental Offices and Clinics - One space for each 200 square feet of Gross Floor Area, plus one space per physician or dentist; Hospitals - One space per bed, plus one space per employee or volunteer on the major shift

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Parking Provided and Used

<i>Parking Structures</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Parking Users</i>	<i>Peak Days</i>	<i>Parking Spaces Provided</i>	<i>Average Peak Occupied Parking Spaces</i>
East Parking Ramp, West Parking Ramp,	Parking Structures, Fairview Southdale Hospital	Patients, Visitors, and Staff	Tuesday and Wednesday	1,993	1,455
Surface Parking	Surface Parking, Fairview Southdale Hospital	Patients, Visitors, and Staff	Tuesday and Wednesday	58	Estimated 40
Totals				2,051	1,495

Fairview Southdale Hospital – Emergency Center Expansion

22 April 2013, Walker & Associates

Variance Application – Setback

By Zoning Ordinance, the setback to the roundabout within our site is ten feet. To allow easy movement within the site for emergency and other large vehicles we propose to reduce the setback to four feet in the limited area adjacent to the outermost curve of the roundabout. We request a Parking Variance to accommodate construction of this new project.

<i>Condition</i>	<i>Answer</i>	<i>Explanation</i>
<i>Relieve practical difficulties in complying with the zoning ordinance and that the use is reasonable</i>	Yes	The need to expand this facility is potentially compromised by the tightly constricted site. Expansion of the roundabout allows for better flow of emergency vehicles, fire department equipment, and other larger vehicles. This is a minor accommodation and is reasonable.
<i>Correct extraordinary circumstances applicable to this property but not applicable to other property in the vicinity or zoning district</i>	Yes	This facility is landlocked on all sides and has limited area available for expansion to fulfill its public mission. The proposed driveway configuration allows some flexibility in using the available site area to its best use.
<i>Be in harmony with the general purposes and intent of the zoning ordinance</i>	Yes	The proposed driveway configuration requires this minor variance and accomplishes the general purpose and intent of the zoning ordinance.
<i>Not alter the essential character of a neighborhood</i>	Yes	The proposed driveway configuration is internal to this building site and fits well with the surrounding neighborhood.

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APR 24 2013
 Planning Department
 City of Southdale

Fairview Southdale Hospital – Emergency Center Expansion

9 May 2013, Walker & Associates

Existing Condition

<i>PID, Property Address</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Site Area in Square Feet, Acres</i>	<i>Floor Area in Square Feet¹</i>	<i>Gross Floor Area in Square Feet²</i>	<i>Parking Spaces</i>
3002824140001, 6400 France Avenue South	West Parking Ramp, IRET Properties	125,484 2.88	0	0	1,018 structured
2902824230169, 6401 France Avenue South	Hospital, East Parking Ramp, and Central Plant, Fairview Southdale Hospital	531,671 12.21	561,562 Hospital, 12,868 Central Plant	484,250 ³	74 surface, 975 structured
2902824230170, 2902824230171, 6405 France Avenue South	Medical Office Building, IRET Properties	21,021 0.48	64,128	57,659	0
2902824230166, 6363 France Avenue South	Medical Office Building, IRET Properties	44,402 1.02	77,958	68,920	0
2902824230167, Address Unassigned	Unbuilt, Fairview Southdale Hospital	5,981 0.14	0	0	0
Totals		728,559 16.73	716,516	610,829	2,067

AH

¹Excludes parking ramps only

²Excludes parking ramps, mechanical rooms, restrooms, elevator shafts, stairwells, and similar spaces according to the Zoning Ordinance

³Excludes 57,752 square feet along with 326 patient toilets at 60 square feet average per space

Completed Condition

<i>PID, Property Address</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Site Area in Square Feet, Acres</i>	<i>Floor Area In Square Feet¹</i>	<i>Gross Floor Area in Square Feet²</i>	<i>Parking Spaces</i>
3002824140001, 6400 France Avenue South	West Parking Ramp, IRET Properties	125,484 2.88	0	0	1,018 structured
2902824230169, 6401 France Avenue South	Hospital, East Parking Ramp, and Central Plant, Fairview Southdale Hospital	531,671 12.21	650,536 Hospital, 13,200 Central Plant	560,678 ³	58 surface, 975 structured
2902824230170, 2902824230171, 6405 France Avenue South	Medical Office Building, IRET Properties	21,021 0.48	64,128	57,659	0
2902824230166, 6363 France Avenue South	Medical Office Building, IRET Properties	44,402 1.02	77,958	68,920	0
2902824230167, Address Unassigned	Unbuilt, Fairview Southdale Hospital	5,981 0.14	0	0	0
Totals		728,559 16.73	805,822	687,257	2,051

MS

¹Excludes parking ramps only

²Excludes parking ramps, mechanical rooms, restrooms, elevator shafts, stairwells, and similar spaces according to the Zoning Ordinance

³Excludes 57,752 square feet along with 326 patient toilets at 60 square feet average per space

Parking Required

<i>Property Address</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Licensed Patient Beds</i>	<i>Peak Shift</i>	<i>Peak Employees Per Shift</i>	<i>Required Parking¹</i>
6401 France Avenue South	Hospital, Fairview Southdale Hospital	326	Day	1,525	1,851
6405 France Avenue South	Medical Office Building, IRET Properties	0	Day	180	320
6363 France Avenue South	Medical Office Building, IRET Properties	0	Day	175	404
Totals		326		1,880	2,575

¹According to the Zoning Ordinance, Required Parking is calculated as follows; Medical or Dental Offices and Clinics - One space for each 200 square feet of Gross Floor Area, plus one space per physician or dentist; Hospitals - One space per bed, plus one space per employee or volunteer on the major shift

At 6

Parking Provided and Used

<i>Parking Structures</i>	<i>Building Use, Owner/Taxpayer</i>	<i>Parking Users</i>	<i>Peak Days</i>	<i>Parking Spaces Provided</i>	<i>Average Peak Occupied Parking Spaces</i>
East Parking Ramp, West Parking Ramp,	Parking Structures, Fairview Southdale Hospital	Patients, Visitors, and Staff	Tuesday and Wednesday	1,993	1,455
Surface Parking	Surface Parking, Fairview Southdale Hospital	Patients, Visitors, and Staff	Tuesday and Wednesday	58	40
Totals				2,051	1,495¹

¹Highest recorded parking usage is 1,690 parking spaces

Existing Hospital – Excluded Floor Areas

24 April 2013, Fairview Southdale Hospital

<i>Floor</i>	<i>Description</i>	<i>Area</i>	<i>Extended Area</i>
Lower Level	Mechanical Rooms	varies	18,170
Lower Level	Janitor/Electrical/ Linen	varies	1,513
Lower Level	Shafts	varies	1,011
1st Floor	Mechanical Rooms	211	211
1st Floor	Janitor/Electrical/ Linen	varies	736
1st Floor	Shafts	varies	1,666
1st Floor	Ambulance Garage	1,948	1,948
2 thru 8	Shafts	varies	1,764
2 thru 8	Janitor/Electrical/ Linen	varies	3,598
2nd Floor	Mechanical Space	2,317	2,317
LL thru 8	Elevator shafts	600	5400
LL thru 3	Elevator shaft #7	180	720
LL thru 8	Stair A thru D	155	5580
2, 3	Stair E	168	336
LL- 3	Stair F	180	720
LL -1	Stair G	224	448
LL- 1	Stair H	200	400
LL- 1	Stair M	160	320
1st Floor	Atrium Stairs	1057	1057
LL thru 8	Dumbwaiters	60	540
LL	Public/Staff Restrooms	varies	1265
1st Floor	Public/Staff Restrooms	varies	1112
2, 3	Public/Staff Restrooms	220	440
4 thru 8	Public/Staff Restrooms	46 (23 ea)	230
9	Penthouse		6250
Total			57,752

A17

Emergency Center Expansion – Excluded Floor Areas

24 April 2013, HGA

<i>Description</i>	<i>Area</i>
Basement Level	
Elevators/Stairwells	725
Mechanical Room	4,272
Electrical / Communication	2,090
Area Well	419
Elevator Equipment	345
Subtotal	7,851
First Level	
Elevators/Stairwells	962
Mechanical Shafts	471
Electrical / Communication	420
Public Restrooms	484
Subtotal	2,337
Second Level	
Elevators/Stairwells	854
Mechanical Shafts	117
Electrical / Communication	307
Patient Toilets (18 at 60 square feet)	1,080
Subtotal	2,358
Total	12,546

A18

IRET Buildings – Excluded and Gross Floor Areas

8 May 2013, Todd Mulvihill, BDH+Young

6405 France Building	
Third Floor (Second Floor Similar to Third Floor)	
Gross Building	21,376
Verticals	1,716
Service Areas	287
Extrapolated Toilets (4X56.25)	225
Gross Floor Area	19,148
Fourth Floor	
Gross Building	21,376
Verticals	1,501
Service Areas	287
Extrapolated Toilets (4X56.25)	225
Gross Floor Area	19,363
6363 France Building	
First and Second Floors (Each)	
Gross Building	12,993
Verticals	1,071
Service Areas	277
Extrapolated Toilets (4X56.25)	225
Gross Floor Area	11,420
Third through Sixth Floors (Each)	
Gross Building	12,933
Verticals	911
Service Areas	277
Extrapolated Toilets (4X56.25)	225
Gross Floor Area	11,520

AM



T.O. CURTAIN WALL
36'-10"
 T.O. PARAPET
32'-0"
 T.O. PARAPET
17'-0"

T.O. PARAPET
32'-0"
 T.O. PARAPET
17'-0"

① NORTH OVERALL ELEVATION
100' x 110'



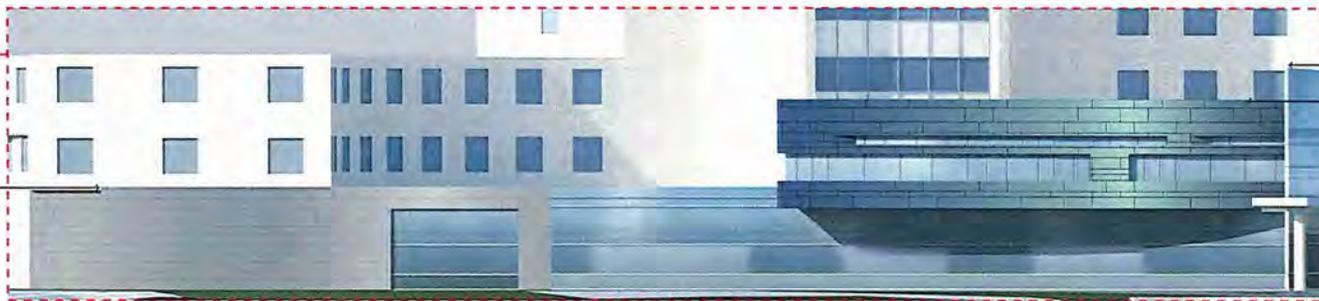
T.O. CANOPY
15'-6"

② NORTH ELEVATION - DROPOFF AREA
100' x 110'



T.O. CURTAIN WALL
17'-0"
 T.O. CURTAIN WALL
15'-8"
 T.O. CURTAIN WALL
14'-2"

③ NORTH ELEVATION - NEW AMBULANCE GARAGE AREA
100' x 110'



T.O. PARAPET
17'-0"

T.O. CURTAIN WALL
36'-10"
 T.O. PARAPET
32'-0"

④ NORTH ELEVATION - WAITING LOBBY AREA
100' x 110'

A20

4/23/2013 10:00 AM SITE PLAN APPLICATION A400.00



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 Architectural & Mechanical Consulting Engineers



① SOUTH ELEVATION - SECOND LEVEL OBSERVATION UNIT
REV: 11/13



② EAST ELEVATION - EAST ENTRY AT EXISTING GARAGE AREA
REV: 11/13



Fairview
 Fairview
 Southdale
 Emergency
 Center
 Expansion

6401 FRANCE AVENUE SOUTH
 EDINA, MINNESOTA

DATE: 04/24/13
 DRAWN BY: J. J. JENSEN

HGA NO: 2505-219-00
 ISSUE DATE: APRIL 24, 2013

EXTERIOR
 ELEVATION
 RENDERINGS

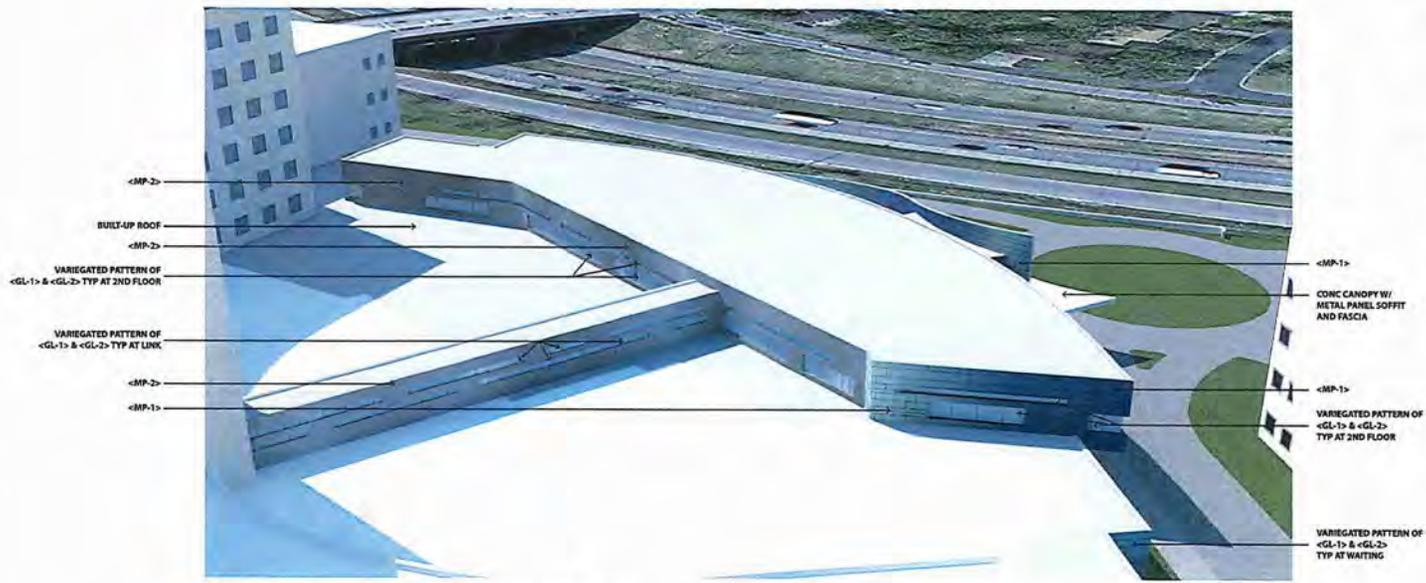
SITE PLAN APPLICATION
A401.

A21

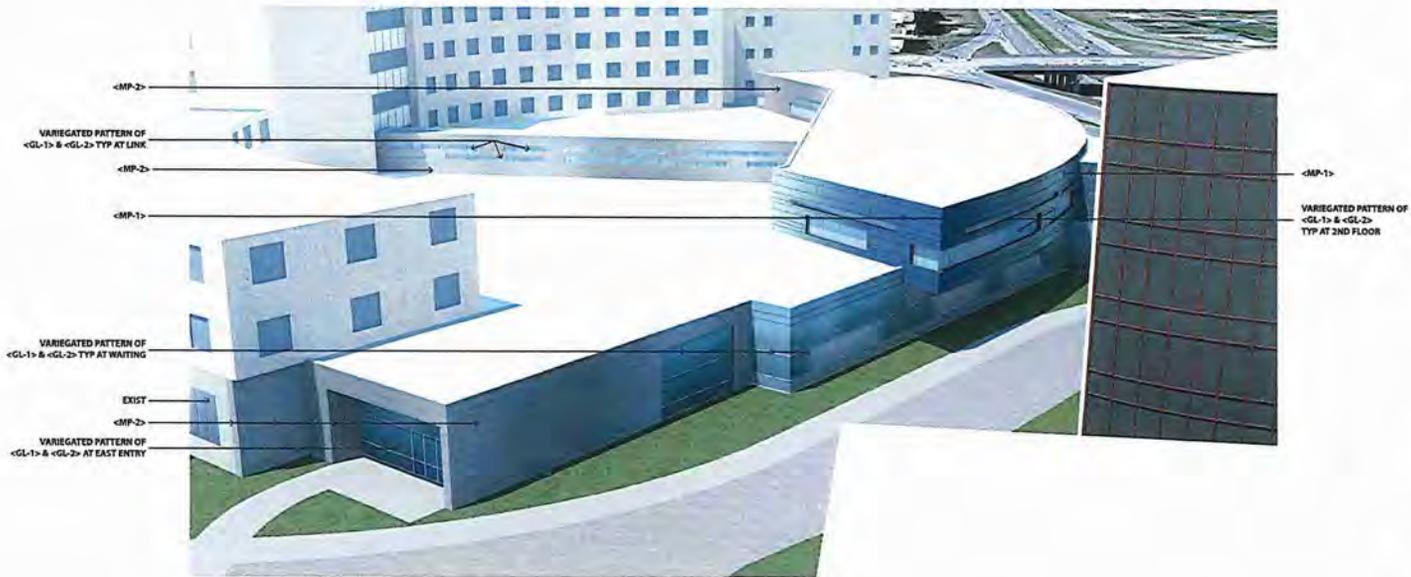
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A34

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1 SOUTHEAST PERSPECTIVE @ SECOND LEVEL OBSERVATION UNIT



2 NORTHEAST PERSPECTIVE @ NEW EAST ENTRY



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Fairview
 Fairview
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 Emergency
 Center
 Expansion

3401 FRANCE AVENUE SOUTH
 EDINA, MINNESOTA

DATE: 04/24/13
 SCALE: 1/8"=1'-0"

HGA NO: 2500-213-00
 ISSUE DATE: APRIL 24, 2013

EXTERIOR
 ELEVATION
 RENDERINGS

SITE PLAN APPLICATION

A402.



- <GL-2>
- <GL-1>
- <MP-1>
- VARIEGATED PATTERN OF <GL-1> & <GL-2> TYP AT 2ND FLOOR
- VARIEGATED PATTERN OF <GL-1> & <GL-2> TYP AT WAITING
- CONC CANOPY & COLLARING W/ METAL PANEL SOFFIT & FASCIA
- CAST-IN-PLACE SITEWALL W/ REVEAL PATTERN

- VARIEGATED PATTERN OF <GL-1> & <GL-2> TYP AT 2ND FLOOR
- <GL-2>
- <GL-1>
- <STN-1>

1 NORTH PERSPECTIVE FROM HWY 62

A23



- BUILT-UP ROOF
- <MP-2>
- VARIEGATED PATTERN OF <GL-1> & <GL-2> TYP AT LINK
- <MP-1>
- CONC CANOPY
- <MP-1>
- VARIEGATED PATTERN OF <GL-1> & <GL-2> TYP 2ND FLOOR
- <MP-1>
- <GL-2>
- <GL-1>
- <CSTN-1>
- GARAGE DOOR PAINT TO MATCH <CSTN-1>
- <GL-2>
- CAST-IN-PLACE CONC SITE WALL W/ REVEAL PATTERN

- <MP-2>
- VARIEGATED PATTERN OF <GL-1> & <GL-2> TYP 2ND FLOOR
- BUILT-UP ROOF
- <CSTN-1>
- <GL-1>
- <STN-1>

2 NORTHWEST PERSPECTIVE

FAIRVIEW

Fairview Southdale Emergency Center Expansion

901 FRANCE AVENUE SOUTH
EDINA, MINNESOTA

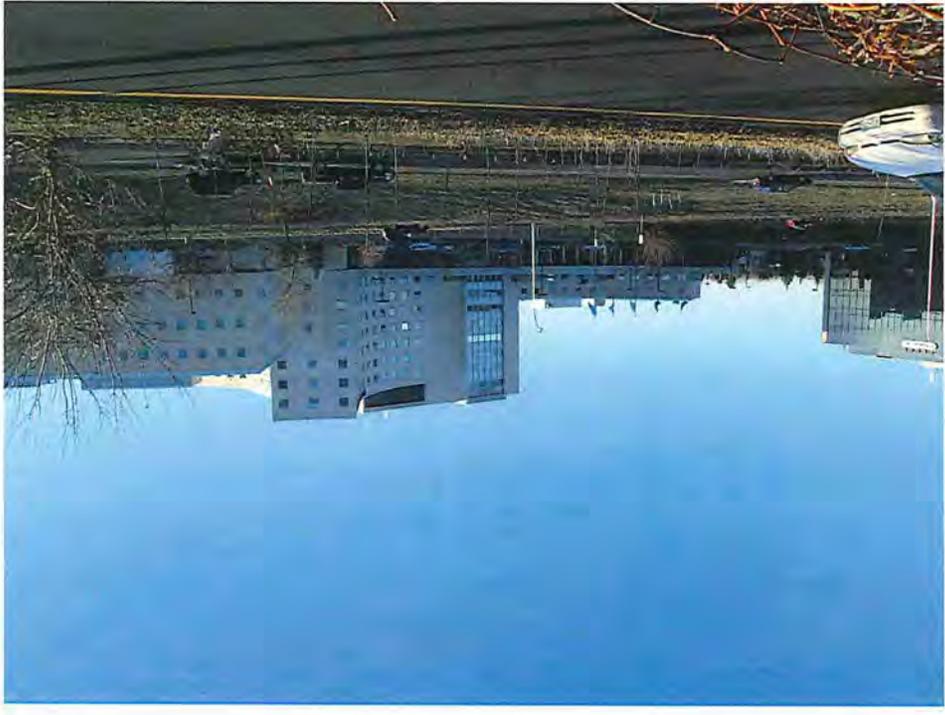
HGA NO. 2500-213-00
ISSUE DATE: APRIL 24, 2013

EXTERIOR ELEVATION RENDERINGS

SITE PLAN APPLICATION

A403.

A9900



1 EXISTING HOSPITAL



2 PROPOSED EXPANSION



HATCH MOTT MACDONALD ARCHITECTS
 1000 BROADWAY, SUITE 2000
 NEW YORK, NY 10018
 TEL: 212 512 2000 FAX: 212 512 2001
 WWW.HGM.A9900.COM



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 6401 FRANKLIN AVENUE SOUTH
 EDINA, MINNESOTA
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ARCHITECT
 HATCH MOTT MACDONALD ARCHITECTS
 1000 BROADWAY, SUITE 2000
 NEW YORK, NY 10018
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 WWW.HGM.A9900.COM

DATE: 04/23/13
 DRAWN BY: JACOB
 CHECKED BY: JACOB

HGA NO. 2000.01.00
 ISSUE DATE: APRIL 23, 2013
 EXISTING SITE
 PHOTO AND
 NEW
 RENDERING

SITE PLAN APPLICATION
 A9900

STORM-WATER POLLUTION CONTROL PLAN:

1. SITE AREA = 15.80 ACRES
DISTURBED AREA = 2.4 ACRES
NEW IMPERVIOUS AREA = 0.57 ACRES
2. SITE WORK ACTIVITY INCLUDES SITE GRADING, BUILDING EXCAVATION, UTILITY TRENCHING AND BACKFILLING, STORM DRAINAGE SYSTEM, PAVING AND LANDSCAPE.
3. EXISTING NEAR SURFACE SOIL IS FILL CONSISTING OF SANDY LEAN CLAY, CLAYEY SAND, AND SAND WITH SILT. RUNOFF COEFFICIENT VARIES DEPENDING ON COVER CONDITION.
4. DISTURBED AREA OF SITE DISCHARGES NORTH TO AN EXISTING STORM DRAINAGE SYSTEM UNDER HIGHWAY 62 IN THE MINNEAPOLIS CREEK WATERSHED DISTRICT.
5. SEE SHEET C200 FOR DETAILS

SEQUENCING:

1. CONSTRUCT STABILIZED EXITS
2. INSTALL SILT FENCE (MACHINE SLICED)
3. INSTALL INLET PROTECTION
4. REMOVE EXISTING BUILDING
5. STRIP TOPSOIL
6. MAINTAIN ALL TEMPORARY EROSION CONTROLS
7. COMPLETE BUILDING EXCAVATION
8. INSTALL UTILITY STRUCTURES & PIPES
9. COMPLETE SITE GRADING
10. CONSTRUCT PAVEMENT AND WALKS
11. TOP DRESS & SDD TOP AREAS
12. REMOVE ACCUMULATED SEDIMENT FROM INLET PROTECTION & EROSION CONTROLS
13. REMOVE TEMPORARY EROSION CONTROLS



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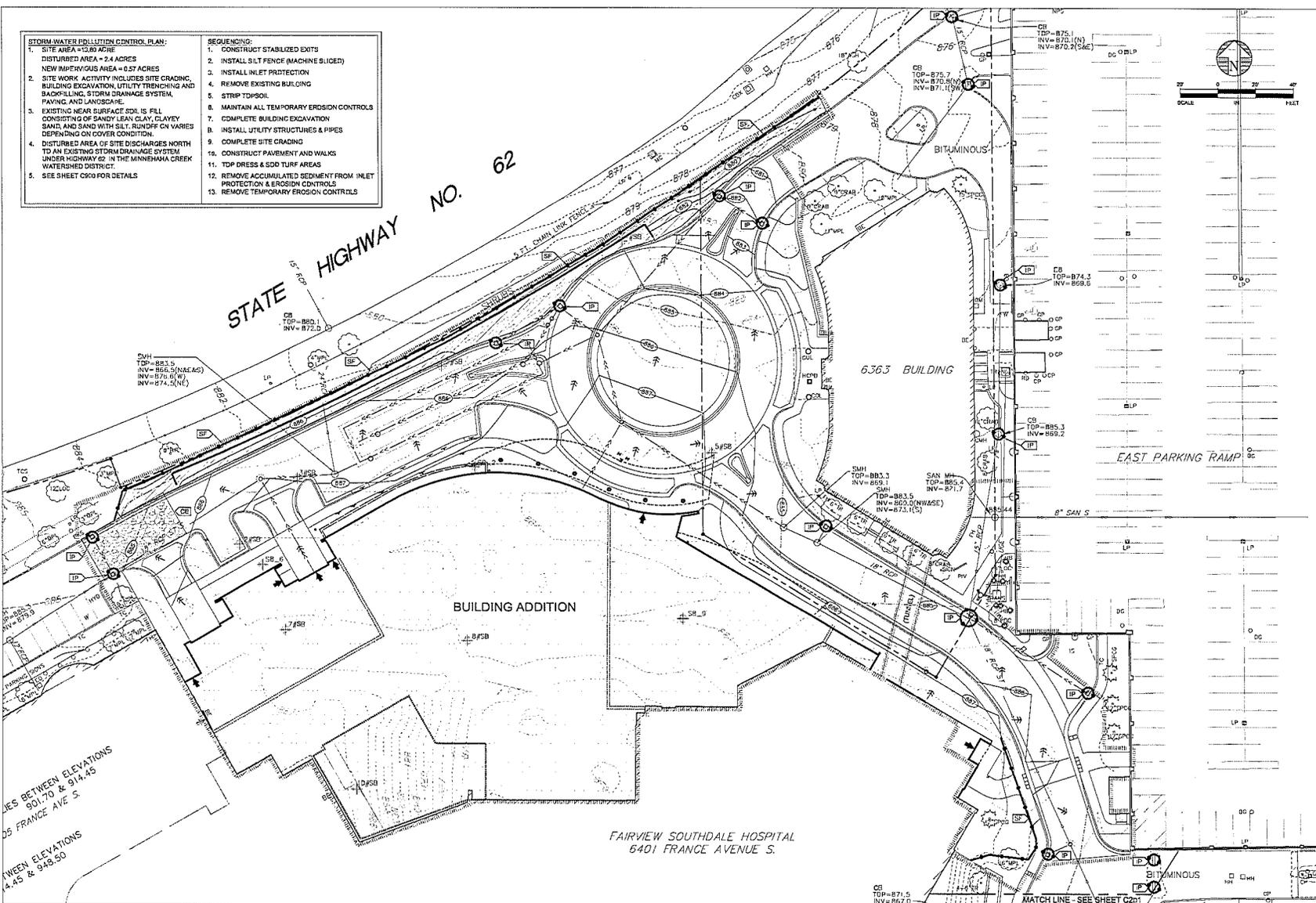
FAIRVIEW

Fairview
Southdale
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Center
Expansion

8401 FRANCE AVENUE
SOUTH EDINA, MINNESOTA

HGA NO: 2560-213-00
ISSUE DATE: APRIL 24, 2015
SITE EROSION AND SEDIMENT CONTROL PLAN

SITE PLAN APPLICATION
C200



STORM WATER POLLUTION PREVENTION NOTES:

1. A MPCA GENERAL STORM-WATER PERMIT FOR CONSTRUCTION ACTIVITY MUST BE OBTAINED BY THE CONTRACTOR. A STORM-WATER POLLUTION PREVENTION PLAN (SWPPP) MUST BE PREPARED, APPROVED, AND IMPLEMENTED AS PART OF THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR PROPER CONSTRUCTION AND MAINTENANCE OF ALL TEMPORARY EROSION AND SEDIMENTATION CONTROLS.
2. THE CONTRACTOR MUST AMEND THE SWPPP AND PREPARE A SUPPLEMENTAL BEST MANAGEMENT PRACTICES (BMP) ACTION PLAN AS NECESSARY TO ADDRESS CHANGES IN CONSTRUCTION, OPERATION, MAINTENANCE, WEATHER, OR SEASONAL CONDITIONS THAT AFFECT THE DISCHARGE OF POLLUTANTS TO SURFACE WATERS AND COMPLY WITH REGULATORY AGENCY REQUIREMENTS.
3. THE CONTRACTOR MUST PLAN AND IMPLEMENT SUITABLE CONSTRUCTION PHASING, VEGETATIVE BUFFER STRIPS, AND OTHER PRACTICES THAT MINIMIZE EROSION AND COMPLY WITH THE INSPECTION AND MAINTENANCE REQUIREMENTS OF THE MPCA GENERAL PERMIT (MN R18000).
4. SEDIMENT CONTROL PRACTICES MUST BE USED TO MINIMIZE SEDIMENT ENTERING SURFACE WATERS OR ADJACENT PROPERTY. ESTABLISH SEDIMENT CONTROL PRACTICES AT ALL DOWN GRADIENT PERIMETERS BEFORE LAND DISTURBING ACTIVITIES BEGIN. ALL STORM INLETS MUST BE PROTECTED WITH SUITABLE BMPs DURING CONSTRUCTION UNTIL FINAL STABILIZATION IS ESTABLISHED.
5. TEMPORARY SOIL STOCKPILES MUST HAVE PERIMETER SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS.

6. SEDIMENT FROM PROJECT SITE TRACKED BY VEHICLES ON ADJACENT ROADWAY MUST BE CLEANED DAILY WITH A STREET SWEEPER.
7. CONTROL DUST BY APPLYING WATER AS OFTEN AS REQUIRED BY SOIL AND WEATHER CONDITIONS.
8. CONTRACTOR MUST ROUTINELY INSPECT THE PROJECT SITE EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER EACH RAINFALL EVENT THAT EXCEEDS 0.5 INCHES. ALL INSPECTIONS MUST BE PROPERLY DOCUMENTED.
9. ALL NONFUNCTIONAL BMPs MUST BE REPAIRED, REPLACED, OR SUPPLEMENTED WITH FUNCTIONAL BMPs.
10. ALL CONSTRUCTION WASTE MATERIALS MUST BE PROPERLY DISPOSED OF OFF-SITE IN A PERMITTED DISPOSAL FACILITY.
11. CONTRACTOR MUST ENSURE FINAL STABILIZATION OF THE PROJECT SITE. IF PERMANENT STABILIZATION CANNOT OCCUR WITHIN 30 DAYS OF COMPLETING GRADING ACTIVITY, ESTABLISH TEMPORARY VEGETATIVE COVER WITH GRASS AND TYPE 3 MULCH.
12. ALL SLOPES STEEPER THAN 4:1 MUST BE STABILIZED WITH CATEGORY 2 EROSION CONTROL BLANKET (MVDOT 2885).
13. EROSION CONTROL FIBER LOGS MUST BE PRODUCT LISTED ON MVDOT'S APPROVED PRODUCTS LIST.
14. REMOVE TEMPORARY EROSION & SEDIMENT CONTROL MEASURES AFTER SITE HAS BEEN PERMANENTLY STABILIZED. AT LEAST 75% OF SITE MUST HAVE EMERGENT VEGETATION, SDD OR EROSION BLANKETS.

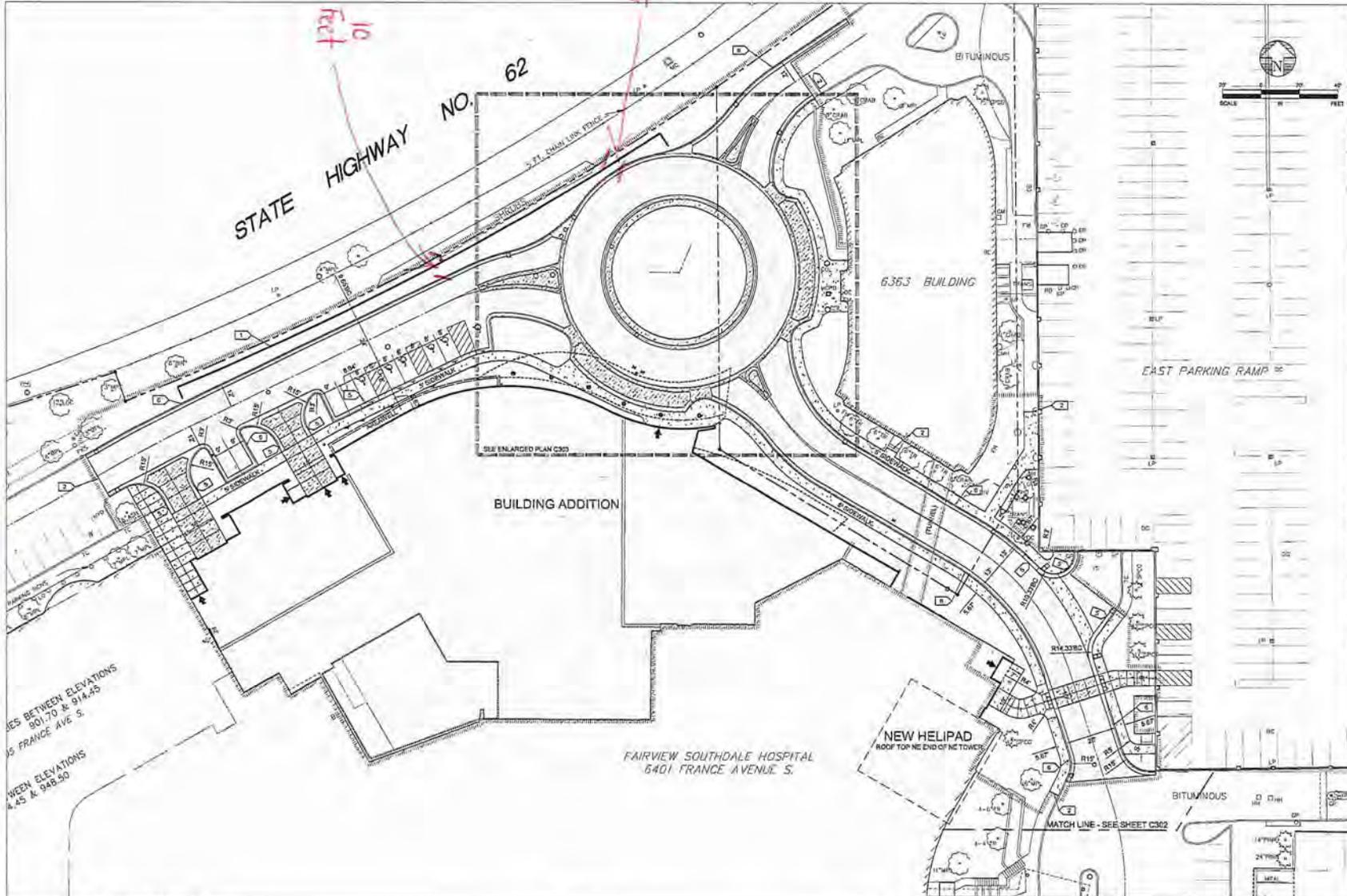
KEY NOTES

- SI SILT FENCE
- IP INLET PROTECTION
- ECB EROSION CONTROL BLANKET
- CEB ROCK CONSTRUCTION EXIT

LEGEND

- EXISTING INDEX CONTOUR
- EXISTING CONTOUR
- EXISTING CONTOUR (REMOVED)
- SPOT ELEVATION
- PROPOSED CONTOURS (1' INTERVAL)
- DRAINAGE ARROW
- DRAINAGE DIVIDE
- PROJECT LIMITS
- INLET PROTECTION
- SILT FENCE (MACHINE SLICED)
- FIBER LOG CHECK
- EROSION CONTROL BLANKET

A736



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FAIRVIEW
 Fairview Southdale
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GENERAL NOTES FOR LAYOUT:

1. ALL DIMENSIONS TO CURBS AND CURB/GUTTER ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED. BACK OF CURB IS TYPICALLY 8 INCHES BEHIND THE FACE OF CURB.
2. GUIDELINES FOR CONSTRUCTION OF NEW CONCRETE SURFACING:
3. MAXIMUM SPACING OF CONTROL JOINTS IN WALKS TO BE 8 FEET, BUT GENERALLY MATCH THE WALK WIDTH.
4. MAXIMUM SPACING OF CONTROL JOINTS IN CONCRETE TRAFFIC SLABS TO BE 12 FEET.
5. MAXIMUM SPACING OF 1/2" HIGH EXPANSION JOINTS TO BE 40 FEET.
6. ALIGN JOINTS WITH WALK CORNERS, HORIZONTAL REFLECTIONS, BUILDING CORNERS, ETC. AND USE EQUAL PAVE SIZES.
7. SEE XXXXXX FOR JOINTING DETAILS.

KEYNOTES:

- 1. NEW RETAINING WALL
- 2. MATCH EXISTING WIDTH
- 3. BOLLARDS
- 4. CHAIN LINK FENCE WITH MAN GATE
- 5. NOSEDOWN CURB & CUTTER
- 6. BE18 CONCRETE CURB & GUTTER
- 7. D412 CONCRETE CURB & GUTTER
- 8. 7'-2" WIDE CONCRETE BAND

SURFACING LEGEND:

- BITUMINOUS PAVEMENT
 - 2" BITUMINOUS WEAR COURSE (MDDOT 2380)
 - 3.5" BITUMINOUS NON-WEAR COURSE (MDDOT 2380)
 - 12" AGGREGATE BASE (MDDOT 3138)
 - T.A. APPROVED GRANULAR SUBGRADE
- CONCRETE WALK
 - 4" CONCRETE WALK
 - 4" AGGREGATE BASE (MDDOT 3138)
 - REINFORCING - NONE
 - T.A. APPROVED GRANULAR SUBGRADE
- HEAVY DUTY CONCRETE
 - 7" CONCRETE PAVEMENT
 - 4" AGGREGATE BASE (MDDOT 3138)
 - REINFORCING - NONE
 - T.A. APPROVED GRANULAR SUBGRADE

LEGEND:

- DOOR ACCESS
- SINGLE SIGN
- EXPANSION JOINT
- CONTROL JOINT
- NEW MANHOLE
- NEW DRAINAGE STRUCTURE
- NEW HYDRANT
- NEW GATE VALVE
- PROJECT LIMITS

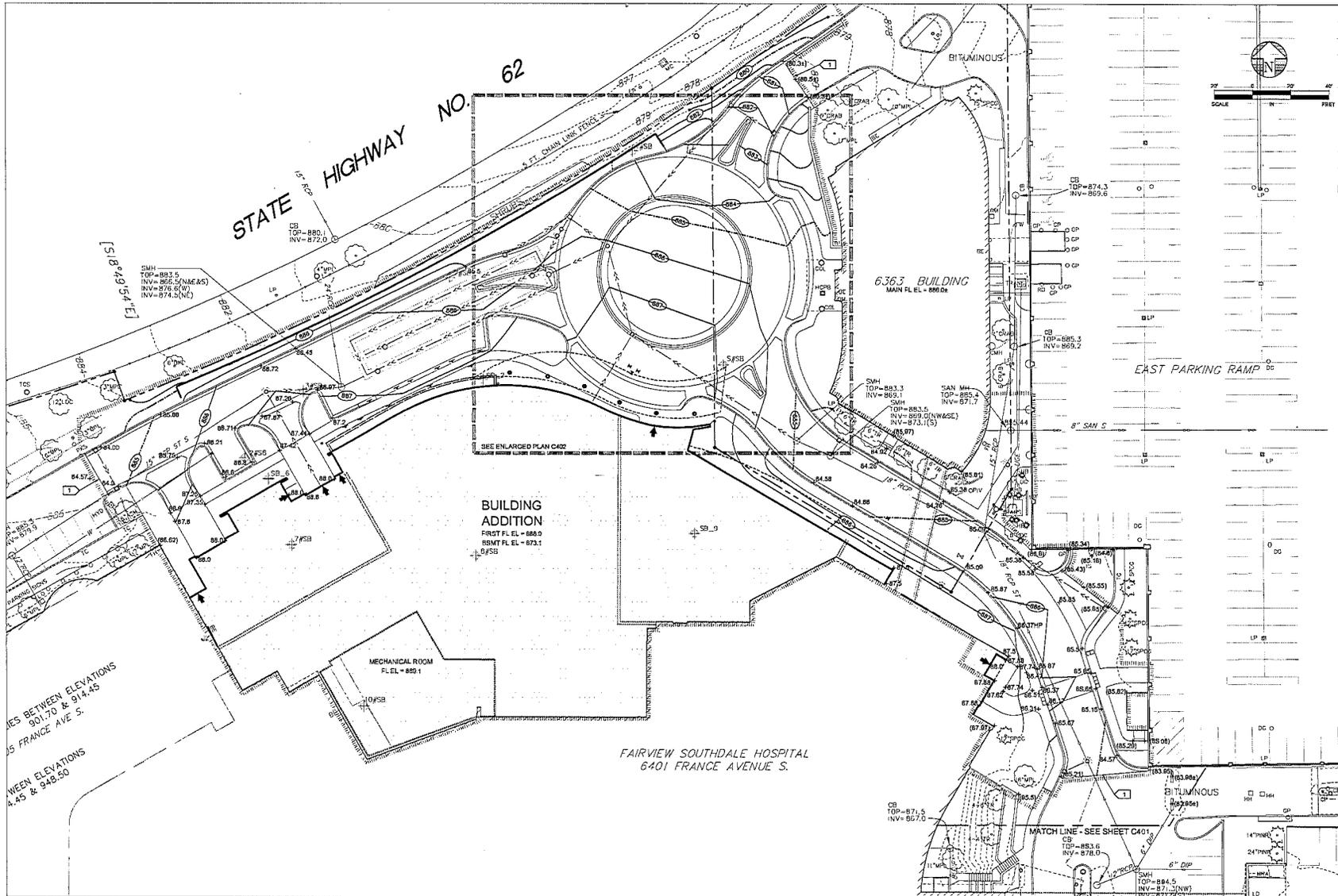
HGA NO: 2203-213-00
 ISSUE DATE: APRIL 24, 2013

SITE LAYOUT AND SURFACING PLAN

SITE PLAN APPLICATION

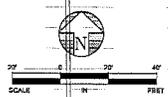
C301

A-24



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 Minneapolis, Minnesota 55408
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BUILDING ADDITION
 FIRST F.L. EL. = 888.0
 BSMT F.L. EL. = 873.1
 6/7 SB

FAIRVIEW SOUTHDALE HOSPITAL
 6401 FRANCE AVENUE S.

EL. BETWEEN ELEVATIONS
 901.70 & 914.45
 65 FRANCE AVE S.

EL. BETWEEN ELEVATIONS
 84.45 & 848.50

- EARTHWORK NOTES:**
1. SEE SPECIFICATION SECTION 310000 EARTHWORK.
 2. FIELD VERIFY UTILITY LOCATIONS PRIOR TO CONSTRUCTION. UNDERGROUND UTILITIES ARE SHOWN AT APPROXIMATE LOCATIONS.
 3. UTILIZE THE GOPHER STATE ONE CALL SYSTEM AT 1-888-222-1186, 48 HOURS PRIOR TO PERFORMING ANY EXCAVATION.
 4. SEE SHEET C100 SITE SURVEY FOR BENCHMARKS.
 5. SPOT ELEVATIONS ARE TYPICALLY GIVEN AT FINISHED SURFACE. AT T.O.P OF CURB, ELEVATIONS ARE NOTED WITH A "C".
 6. PRIOR TO CONSTRUCTION FIELD VERIFY EXISTING BUILDING FLOOR ELEVATIONS TO CONFIRM MATCH WITH NEW FLOOR ELEVATIONS.

KEYNOTE
 [Symbol] MATCH EXISTING GRADE

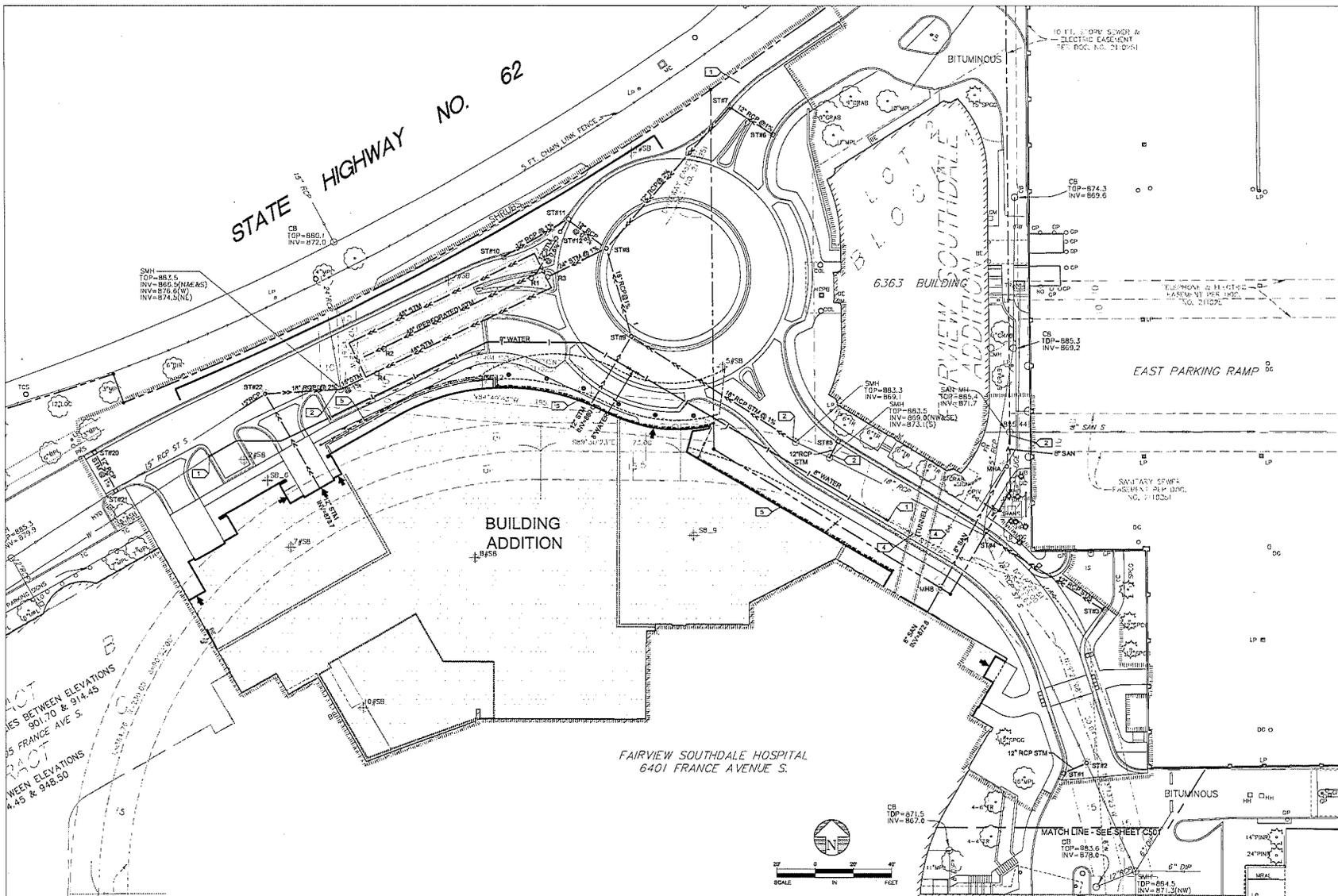
- LEGEND**
- SOIL BORING LOCATION W/ GROUND ELEVATION [Symbol]
 - DRAINAGE ARROW [Symbol]
 - DRAINAGE DIVIDE [Symbol]
 - CONSTRUCTION LIMITS [Symbol]
 - EXISTING INDEX CONTOUR [Symbol]
 - EXISTING CONTOUR [Symbol]
 - EXISTING CONTOUR (REMOVED) [Symbol]
 - SPOT ELEVATION [Symbol]
 - PROPOSED CONTOURS (2' INTERVAL) [Symbol]

HGA NO: 2560-213-00
 ISSUE DATE: APRIL 24, 2013

SITE GRADING PLAN

SITE PLAN APPLICATION

C400

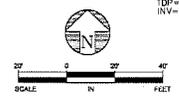


A30

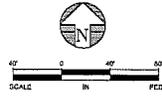
- UTILITY NOTES:**
1. UTILIZE THE COPPER STATE ONE CALL SYSTEM @ (888) 282-1166, 48 HOURS PRIOR TO PERFORMING ANY EXCAVATION.
 2. EXISTING UNDERGROUND UTILITIES ARE SHOWN AT APPROXIMATE LOCATIONS. ALL EXISTING UTILITIES MAY NOT BE SHOWN. VERIFY LOCATION AND ELEVATION OF UTILITIES IMPACTED BY CONSTRUCTION, PRIOR TO PLACING NEW PIPE OR UTILITY STRUCTURES.
 3. VERIFY BUILDING STORM, SANITARY, AND WATER SERVICE LOCATIONS AND ELEVATIONS WITH PLUMBING CONTRACTOR, PRIOR TO CONSTRUCTION OF BERVADE LINES.
 4. PROVIDE MINIMUM 1.5' COVER ON ALL WATER MAINS. ALL WATER MAIN CONSTRUCTION MUST COMPLY WITH CITY REQUIREMENTS.
 5. SANITARY AND STORM SEWER CONSTRUCTION MUST COMPLY WITH CITY REQUIREMENTS.
 6. COORDINATE CONNECTIONS TO EXISTING UTILITY SYSTEMS WITH UTILITY OWNER.
 7. SEE TABLE FOR UTILITY STRUCTURE INFORMATION.
 8. ADJUST ALL CASTINGS TO ELEVATION 14-INCH BELOW FINISHED SURFACE.

- KEYNOTE**
- 1 CONNECT TO EXISTING WATERMAIN
 - 2 CONNECT TO EXISTING STRUCTURE
 - 3 NEW STORMWATER RETENTION PIPE, 48" PERFORATED CSP OR 24" W/ 24" CRUSHED STONE ON SIDES AND 6" CRUSHED STONE ON TOP AND BOTTOM.
 - 4 FIELD VERIFY LOCATION OF EXISTING WATERMAIN AT TUPREL AND 1360 BUILDING SERVICE
 - 5 0' FOUNDATION DRAIN PIPE

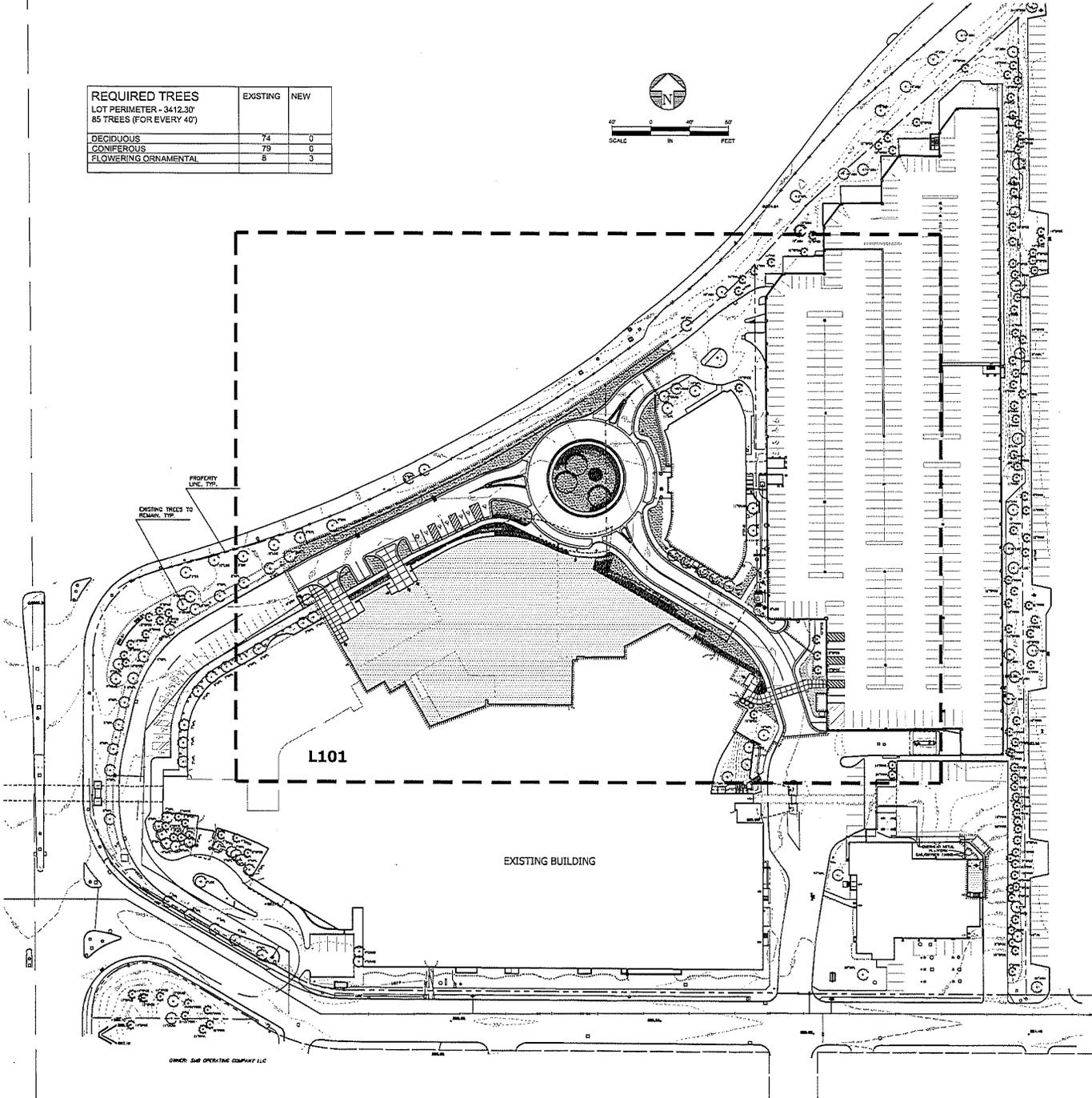
- LEGEND**
- EXISTING SANITARY SEWER
 - EXISTING STORM SEWER
 - EXISTING WATER
 - NEW SANITARY SEWER
 - NEW STORM SEWER
 - NEW WATER
 - NEW MANHOLE
 - NEW DRAINAGE STRUCTURE
 - NEW HYDRANT
 - NEW GATE VALVE
 - PROJECT LIMITS



REQUIRED TREES	EXISTING	NEW
LOT PERIMETER - 3412.30'		
85 TREES (FOR EVERY 40')		
DECIDUOUS	74	0
CONIFEROUS	79	0
FLOWERING ORNAMENTAL	8	3



A31



PROPERTY LINE, TYP.
EXISTING TREES TO REMAIN, TYP.

L101

EXISTING BUILDING

OWNER: S&B OPERATING COMPANY LLC



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Fairview
Southdale
Emergency
Center
Expansion

6401 FRANCE AVENUE
SOUTH EDINA, MINNESOTA

HGA NO: 2500-215-00
ISSUE DATE: APRIL 24, 2013

OVERALL PLANTING
PLAN

SITE PLAN APPLICATION

L100

PLANT MATERIALS SCHEDULE

REF. QTY	COMMENT/BOTANICAL NAME	SIZE	ROOT	SPACING	REMARKS
TREES (Continued from Schedule)					
187	SMALLER HYDRANGEA	2.5' DIA.	30"	AS SHOWN	
SPRINGS (Continued from Schedule)					
188	SMALLER JUNIPER	#5	CTNR	AS SHOWN	
189	JUNIPER CHRYSAE 30yr	#5	CTNR	AS SHOWN	
190	REDFLOW GLOVE ARDISCHVITE	7" W.	BS	AS SHOWN	
PERENNIALS, GRASSES/GRASS ORNAMENTAL GRASSES					
191	KANE FUCHSIA FEATHER REED GRASS	#1	CTNR	30" O.C.	
192	Calliopsis 1 and 2 yr	#1	AS SHOWN		
193	MAGNOLIA ONE FLOWER	#1	CTNR	30" O.C.	
194	Calliopsis 1 and 2 yr	#1	AS SHOWN		
195	LITTLE SNAPPETT DAILY	#1	CTNR	24" O.C.	
196	Artemisia 1 and 2 yr	#1	CTNR	24" O.C.	
197	STELLA D'ORO DAY LILY	#1	CTNR	24" O.C.	
198	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
199	MDUNA SKYRACER	#1	CTNR	30" O.C.	
200	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
201	FLAME GRASS	#1	CTNR	30" O.C.	
202	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
203	COASTAL BLUE SEDGE	#1	CTNR	30" O.C.	
204	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
205	GREEN CARPET JAPANESE SPURGE	4" W.	CTNR	18" O.C.	
206	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
207	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
208	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
209	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	
210	Artemisia 1 and 2 yr	#1	CTNR	30" O.C.	

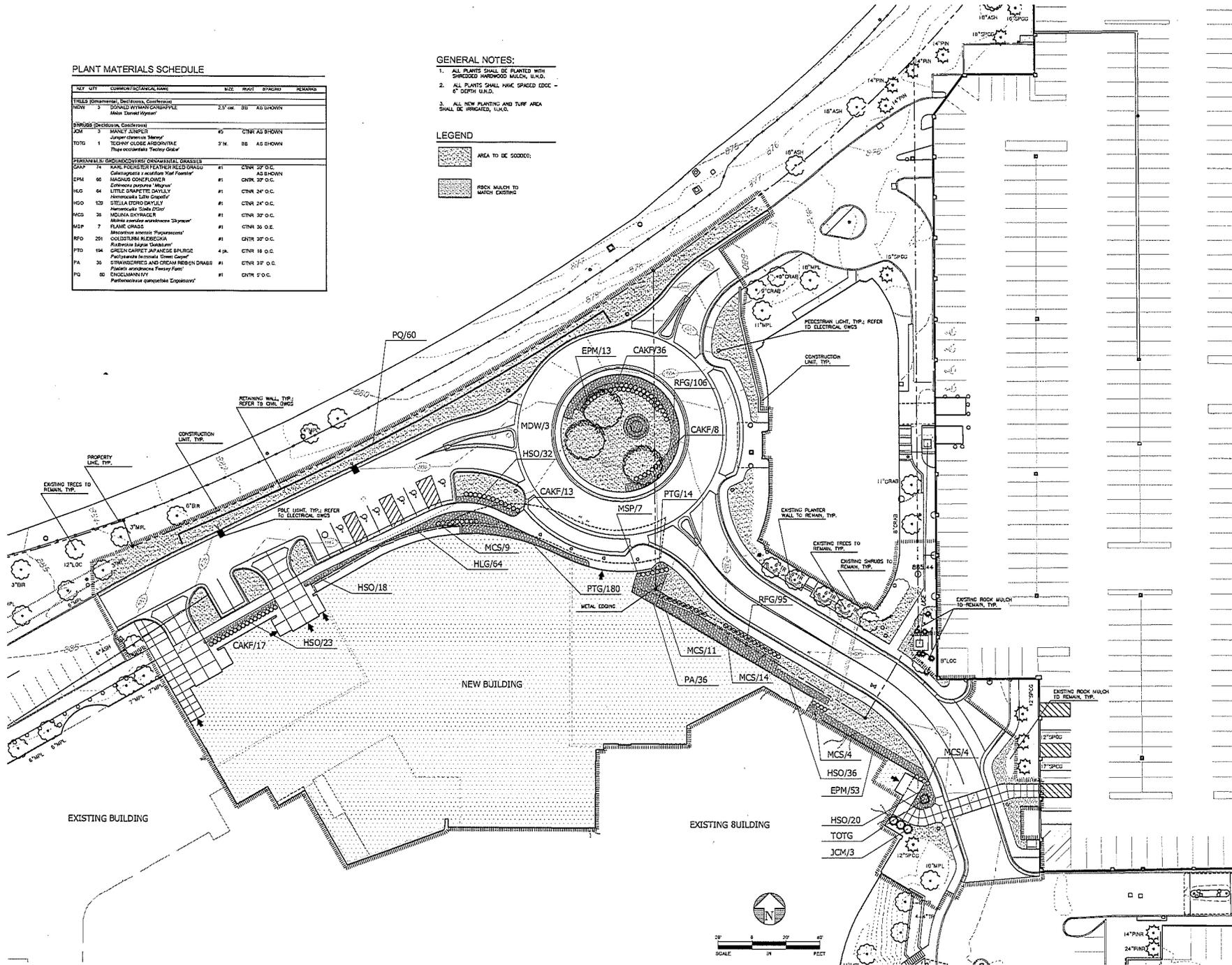
GENERAL NOTES:

1. ALL PLANTS SHALL BE PLANTED WITH SPREADS AND ROOTS MAINTAINED UNLESS OTHERWISE NOTED.
2. ALL PLANTS SHALL HAVE GRADED EDGE - 6" DEPTH UNLESS OTHERWISE NOTED.
3. ALL NEW PLANTING AND TURF AREA SHALL BE IRRIGATED, UNLESS OTHERWISE NOTED.

LEGEND

- AREA TO BE CONCRETE
- ROCK MULCH TO MATCH EXISTING

A32



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FAIRVIEW

Fairview
Southdale
Emergency
Center
Expansion

6401 FRANCE AVENUE
SOUTH EDINA, MINNESOTA

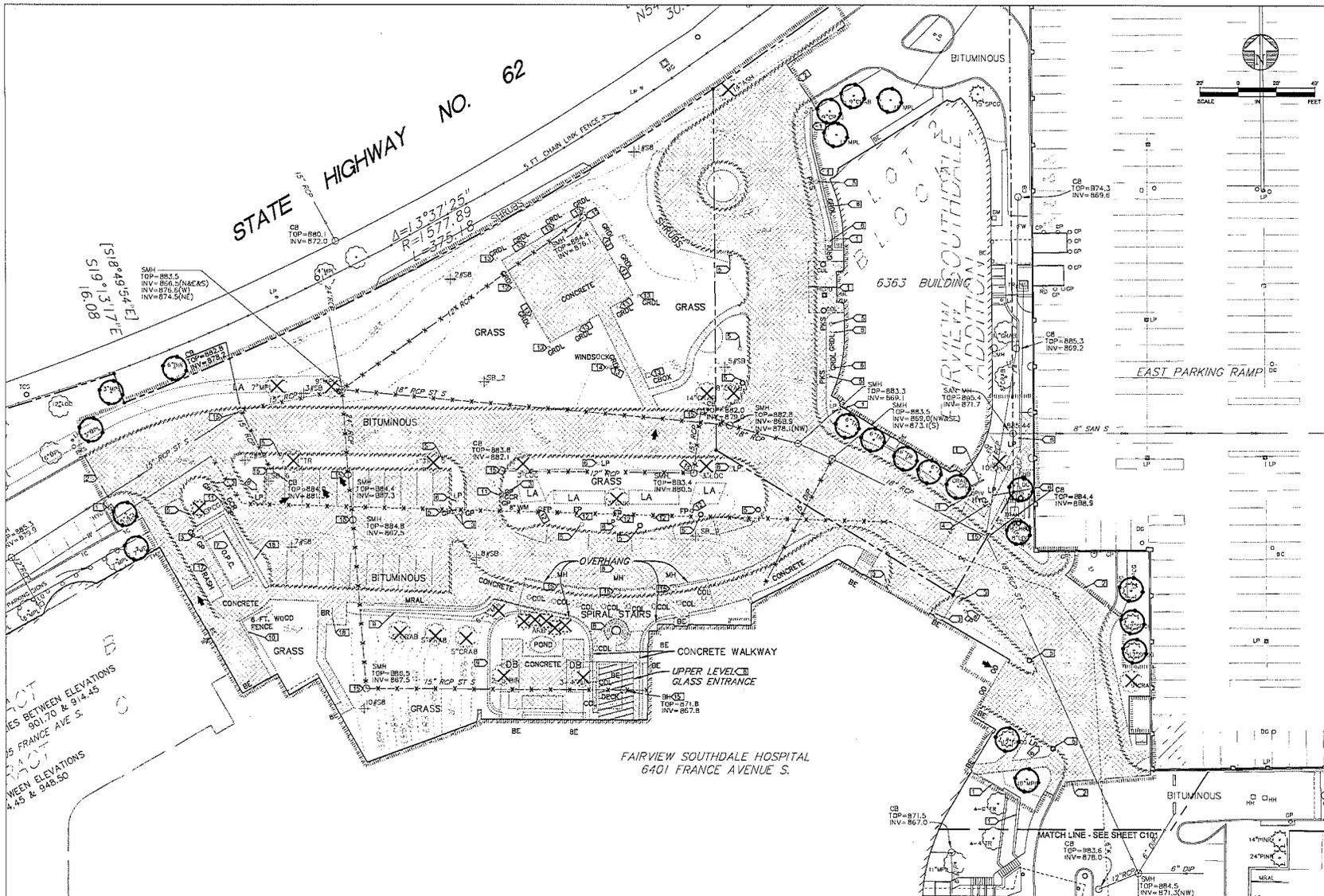
HGA NO. 2500-213-03

ISSUE DATE: APRIL 24, 2013

PLANTING PLAN

SITE PLAN APPLICATION

L101



DUNHAM
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FAIRVIEW

Fairview
 Southdale
 Emergency
 Center
 Expansion

6401 FRANCE AVENUE
 SOUTH DALE, MINNESOTA

A33

NOTES BETWEEN ELEVATIONS
 901.70 & 914.45
 FACT
 NOT
 BETWEEN ELEVATIONS
 945 & 948.50

- DEMOLITION NOTES:**
- UTILIZE THE GORIER STATE ONE CALL SYSTEM @ 1-800-252-4166, 48 HOURS PRIOR TO PERFORMING ANY EXCAVATION.
 - EXISTING UNDERGROUND UTILITIES ARE SHOWN AT APPROXIMATE LOCATIONS. ALL EXISTING UTILITIES MAY NOT BE SHOWN. PRIOR TO CONSTRUCTION FIELD VERIFY LOCATION OF UTILITIES THAT MAY BE IMPACTED BY CONSTRUCTION.
 - PROVIDE TEMPORARY ROCK CONSTRUCTION EXIT AT ALL LOCATIONS WHERE CONSTRUCTION TRAFFIC ENTERS PUBLIC ROADWAY.
 - STORM SEWER INLETS MUST BE PROTECTED WITH SILT FENCE, BAILE CHECKS, OR OTHER EQUIVALENT METHODS UNTIL PERMANENT GROUND COVER IS ESTABLISHED.
 - PROVIDE TRAFFIC CONTROL IN ACCORDANCE WITH THE MINNESOTA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING THE TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS FIELD MANUAL, DATED FEBRUARY 2011, AT ALL TIMES THAT CONSTRUCTION ACTIVITY DISRUPTS TRAFFIC ON THE PROJECT SITE AND PUBLIC ROADWAYS. MAINTAIN TWO 11' WIDE LANES FOR ACCESS TO AMBULANCE GARAGE, THE 6363 BUILDING AND PARKING STRUCTURE AT ALL TIMES.

- COORDINATE DISRUPTION OF PEDESTRIAN AND VEHICULAR TRAFFIC WITH OWNER.
- CARE MUST BE TAKEN DURING CONSTRUCTION TO PROTECT SURVEY MONUMENTS AND PROPERTY IRONS.
- PROTECT ALL TREES NOT SPECIFICALLY DESIGNATED FOR REMOVAL.
- SAWCUT PAVEMENT THAT REMAINS AT REMOVAL LIMITS.
- RESTORE OR REPAIR DAMAGED ROADS, WALKS, CURBS, UTILITIES, OR PLANT MATERIALS TO CONDITION EQUAL TO OR BETTER THAN PRE-CONSTRUCTION CONDITION AND AS REQUIRED BY OWNER.
- COORDINATE DISRUPTION OF UTILITY SERVICES WITH OWNER. LIMIT SERVICE DISRUPTION TO TIMES APPROVED BY OWNER.

- KEY NOTES**
- PROTECT
 - SAWCUT EXISTING PAVEMENT
 - REMOVE GUARD POST
 - RELOCATE HYDRANT
 - REMOVE SIGN
 - REMOVE LIGHT POLE
 - MOVE GENERATOR TO LOCATION IN BUILDING ADDITION BASEMENT
 - SEE ARCHITECTURAL DRAWINGS FOR BUILDING DEMOLITION
 - REMOVE RETAINING WALL
 - REMOVE WOOD FENCE
 - REMOVE GATE CARD READER

- SALVAGE FLAPDOLE AND REINSTALL AT LOCATION APPROVED BY OWNER
- REMOVE HELIPORT AND APPURTENANCES. PROVIDE TEMPORARY HELIPORT AT LOCATION CONTROLLED BY OWNER
- SALVAGE WINDROCK AND RETURN TO OWNER
- REMOVE SEWER STRUCTURE
- REMOVE STONE WALL
- RELOCATE EXISTING TRASH COMPACTOR
- SALVAGE BIKE RACK AND RETURN TO OWNER

- LEGEND**
- REMOVE TREESHURB
 - REMOVE CLUMP
 - REMOVE UNDERGROUND UTILITY
 - REMOVE PAVEMENT
 - REMOVE BUILDING
 - SAW CUT LINE
 - TREE PROTECTION

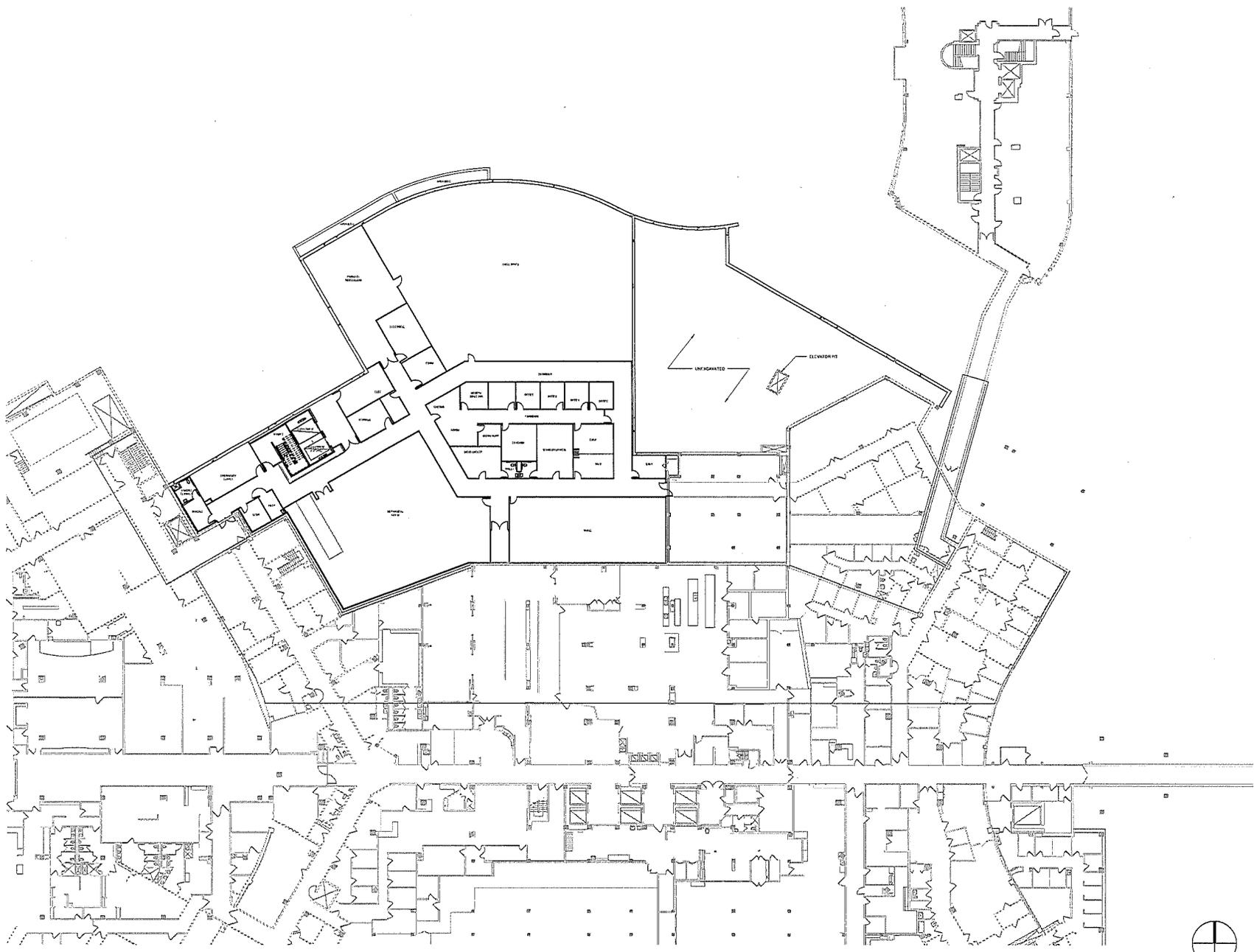
HGA NO: 2500-215-00
 ISSUE DATE: APRIL 24, 2013

SITE DEMOLITION PLAN

SITE PLAN APPLICATION

C100

A34



FAIRVIEW
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 Center
 Expansion
 6401 FRANCE AVENUE SOUTH
 EDINA, MINNESOTA

HGA NO: 2500-213-00
 ISSUE DATE: APRIL 24, 2013

**BASEMENT
 LEVEL OVERALL
 PLAN**



1 OVERALL BASEMENT LEVEL FLOOR PLAN
 100% 4/24

SITE PLAN APPLICATION

A200

A35



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**Fairview
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8401 FRANCE AVENUE SOUTH
 EDINA, MINNESOTA

HGA NO. 2568-213-08

ISSUE DATE: APRIL 24, 2013

**FIRST LEVEL
 OVERALL PLAN**

SITE PLAN APPLICATION



1 OVERALL FIRST LEVEL FLOOR PLAN
 1/16" = 1'-0"

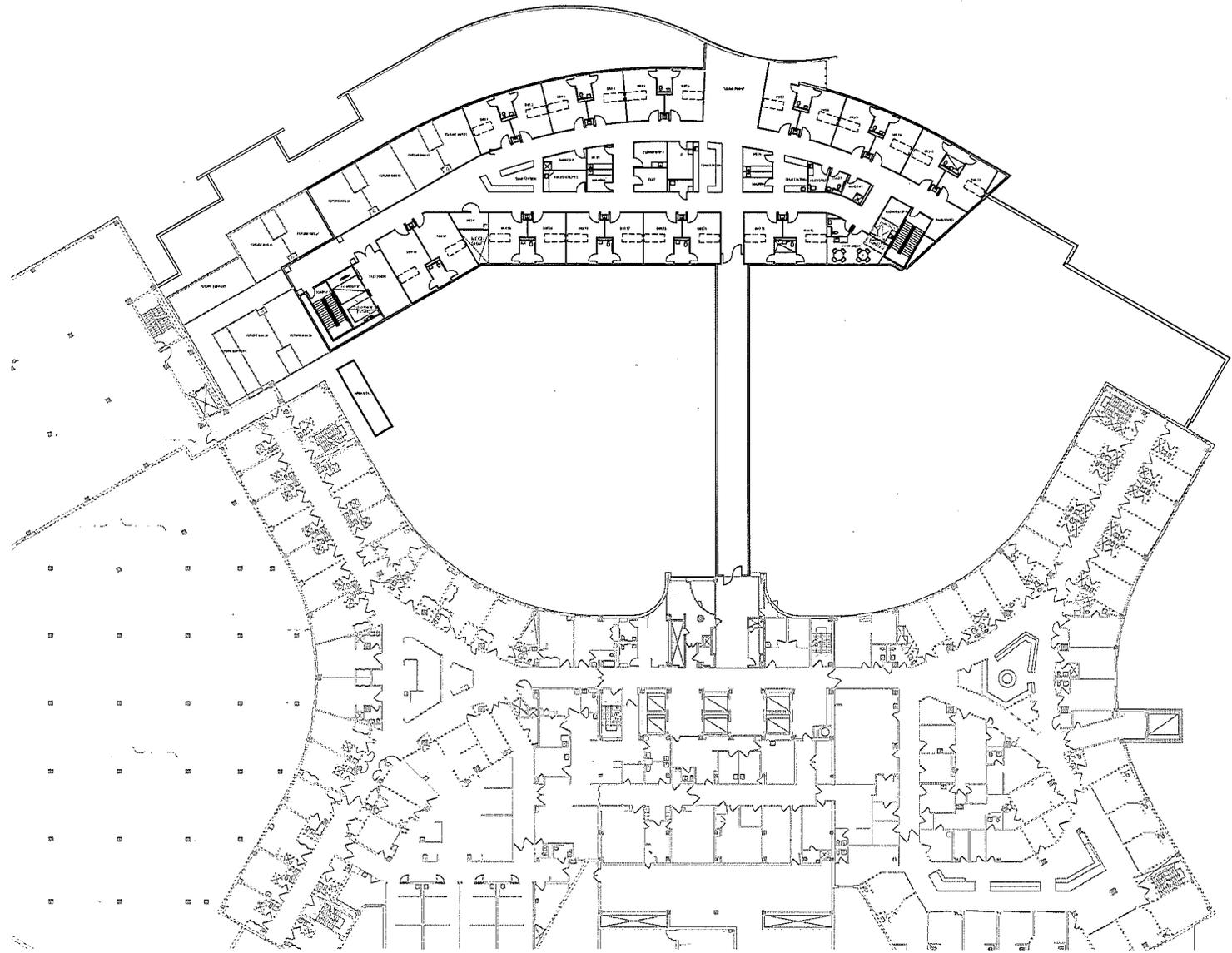
A210

481213131221AN.E:\DWG\A04\101013\101013_01\A04_101013_01_01.dwg



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A36



Fairview
 Southdale
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 Expansion

6401 FRANCE AVENUE SOUTH
 EDINA, MINNESOTA

HGA NO: 2590-213-00
 ISSUE DATE: APRIL 24, 2013

SECOND LEVEL
 OVERALL PLAN

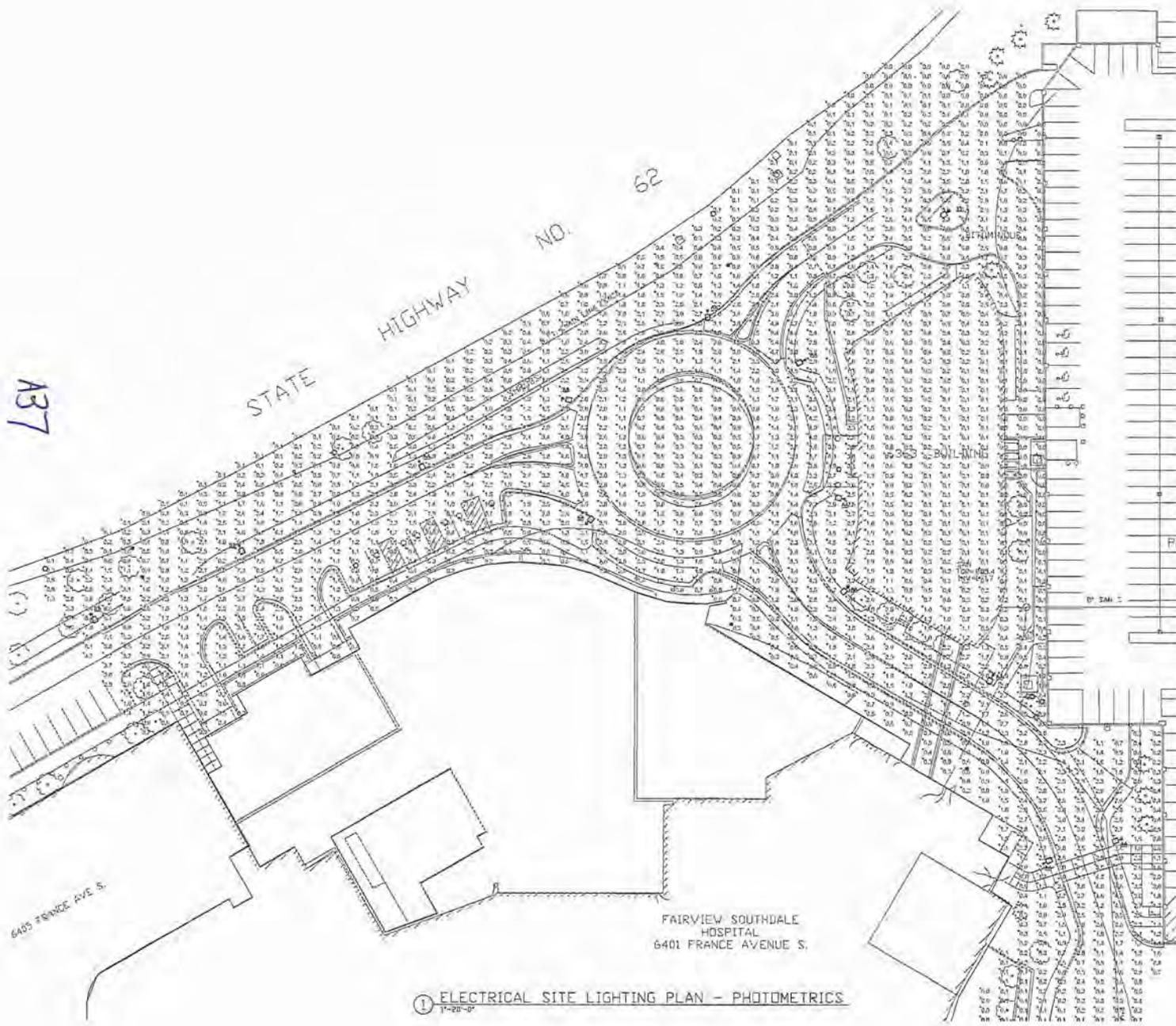


SITE PLAN APPLICATION

A220

1 OVERALL SECOND LEVEL FLOOR PLAN
 1/16" = 1'-0"

A:\2013\131313\131313.dwg User: hga\hga\p001 Date: 4/23/13 10:00:00 AM Plot Date: 4/23/13 10:00:00 AM



GENERAL NOTES:

TYPE #1 IS AN EXISTING FIXTURE TO REMAIN.

TYPE #2 IS A 250W METAL HALIDE FIXTURE MOUNTED AT 20'. THE FIXTURE IS TO BE EQUAL TO THE EXISTING.

TYPE #3 IS TO MATCH THE EXISTING SITE FIXTURES. IT IS A 200W METAL HALIDE MOUNTED AT 20'. THE FIXTURE IS TO BE EQUAL TO EXISTING BRANDED FIXTURES.

HGA

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FAIRVIEW

Fairview
 Southdale
 Emergency
 Center
 Expansion

6401 FRANCE AVENUE
 SOUTH DALE, MINNESOTA

**NOT FOR
 CONSTRUCTION**

1 ELECTRICAL SITE LIGHTING PLAN - PHOTOMETRICS
 1-26-07

HOA NO: 250021340

**ELECTRICAL SITE
 LIGHTING PLAN -
 PHOTOMETRICS**

SITE PLAN APPLICATION

E102



Memorandum

DATE: *May 14, 2013*

TO: *Mr. Cary Teague, Planning Director
Mr. Wayne Houle, Director of Engineering
City of Edina*

FROM: *Charles Rickart, P.E., PTOE*

RE: *Fairview Southdale Hospital Emergency Center Expansion
Traffic and Parking Study
City of Edina, MN
WSB Project No. 1686-38*

Background

The purpose of this study is to determine the potential traffic and parking impacts the proposed expansion of the Fairview Southdale Hospital Emergency Center would have on the existing hospital parking, site circulation and the adjacent roadway system. The site is located in the southeast quadrant of TH 62 and France Avenue. The project location is shown on *Figure 1*.

The proposed plan includes the expansion of the emergency center, urgent care, behavioral health and observation area. The proposed expansion consists of a 77,500 sf (gross area), two-story building located on the north side of the existing hospital building. Existing access to this area of the hospital is from 65th Street at two access locations. No modification to the access planned with the expansions project. However the ring road is planned to be modified to include a new roundabout that will accommodate the new expansion and provide improved circulation and access to the 6363 building and the east parking ramp. The proposed site plan including the ring road improvements is shown in *Figure 2*.

The traffic impacts of the existing and proposed site expansion were evaluated at the following locations.

- France Avenue at 66th Street
- France Avenue at 65th Street
- 65th Street at Hospital Driveway
- 65th Street at Drew Avenue
- 66th Street at Drew Avenue

The following sections of this report document the analysis and anticipated impacts of the proposed redevelopment.

Existing Traffic Characteristics

The existing lane configuration and traffic control include:

France Avenue (CSAH 17) is north/south a 6-lane divided Arterial roadway from south of 66th Street to north of 65th Street. Primary access to York Avenue is by local streets and development driveways. The posted speed limit on France Avenue in the vicinity of the site is 40 mph.

66th Street (CSAH 53) is an east/west 4-lane divided Minor Arterial roadway. Street access and access to adjacent developments including the Southdale Shopping Center is provided from this roadway. The speed limit posted on 66th Street is 35 mph.

65th Street 65th Street is an east/west City street with numerous access driveways. The existing roadway configuration includes a single lane in each direction. All the driveway access points are controlled with stop signs, stopping the exiting movements from the developments. A 30 mph speed limit is posted on this roadway.

Drew Avenue is a north/south City street with numerous access driveways. The existing roadway configuration between 66th Street and 65th Street includes a single lane in each direction with a continuous center left turn lane (three lane section). All the driveway access points are controlled with stop signs, stopping the exiting movements from the developments. A 30 mph speed limit is posted on this roadway. Drew Avenue north of 65th Street is a two-lane street providing access around the hospital. Access to the Fairview Southdale parking ramp and several hospital related uses is provided from this roadway.

The lane configurations at each of the study area intersection are as follows:

France Avenue at 66th Street – Traffic Signal Control

- SB France Ave approaching 66th St – one right/through, two through, one left
- NB France Ave approaching 66th St – one right, three through, one left
- EB 66th St approaching France Ave – one right, two through, one left
- WB 66th St approaching France Ave – one right, two through, two left

France Avenue at 65th Street – Traffic Signal Control

- SB France Ave approaching 65th St – one right/through, two through, one left
- NB France Ave approaching 65th St – one right/through, two through, one left
- EB 65th St approaching France Ave – one right/through, one left
- WB 65th St approaching France Ave – one right, one through/left

65th Street at Hospital Driveway – Side Street Stop

- SB Hospital Driveway approaching 65th St – one right/through/left
- EB 65th St approaching Hospital Driveway – one right/through/left
- WB 65th St approaching Hospital Driveway - one right/through/left

65th Street at Drew Avenue - Side Street Stop

SB Drew Ave approaching 65th St – one right, one through/left
NB Drew Ave approaching 65th St – one right/through, one left
EB 65th St approaching Drew Ave – one right/through/left
WB 65th St approaching Drew Ave – one right/through/left

66th Street at Drew Avenue/Southdale Center Access – Traffic Signal Control

SB Drew Ave approaching 66th St – one right/through, one left
NB Southdale Center Access approaching 66th St – inbound lanes only
EB 66th St approaching Drew Ave – one right, two through, one left
WB 66th St approaching France Ave – one right/through, two through, one left

AM and PM peak hour turning movement counts and daily hourly approach counts were conducted during the week on April 1, 2013. These counts were used as the existing baseline conditions for the area. *Figure 3* shows the existing intersections and driveways along each corridor that were analyzed as part of this traffic study, with the existing 2013 AM and PM peak hour and traffic volumes. The traffic count data is included in the *Appendix*.

Background (Non Development) Traffic Growth

Traffic growth in the vicinity of a proposed site will occur between existing conditions (2012) and any given future year due to other development within the region. This background growth must be accounted for and included in future year traffic forecasts. Reviewing the historical traffic counts in the area, traffic has stayed somewhat constant or dropped in the past few years. However, in order to account for some background growth in traffic the Hennepin County State Aid traffic growth projection factor of 1.1 over a 20 year period was used to project traffic to the 2014 and 2030 analysis years.

In addition to the regional background traffic, other specific none development related traffic was determined and included with the overall background traffic.

Edina Medical Plaza (6500 France Avenue) – The City recently approved the redevelopment of the properties in the southwest quadrant of France Avenue and 65th Street. The proposed site included redevelopment of both the 6500 France Avenue site and the 4005 65th Avenue site with a five story 96,500 sf medical office building with an attached 3 story parking structure. However, recently the City was presented a revised site plan changing the use on the site to a 209 unit senior housing and skilled care facility. With this change, it is assumed that this project will not be open and will not be included with 2014 analysis but, will be fully developed and included in the 2030 background traffic.

Southdale Residential - The City recently approved the addition of 232 apartment units with associated parking in the existing Southdale Shopping Center parking lot. The site is located in the northwest quadrant of 69th Street and York Avenue. It is assumed that this project will not be open and included as part of the 2014 analysis but, will be fully leased and included in the 2030 background traffic.

Additional Southdale Mall Development - Based on the information received from Southdale Center about the current vacancy rates and plans for renovations, it was determined that following the renovations, the mall would have an additional 143,880 sf of leasable space available. This figure includes leasable retail and food court space. The analysis assumes that all leasable space will be occupied by 2014 and included in the background traffic for the 2014 and 2030 analysis.

Future Restaurant Development – A future restaurant is proposed in northeast quadrant of France Avenue and 69th Street in the Southdale Center Parking lot. The restaurant was assumed to be 8,000 sf in size with approximately 300 seats. The analysis assumes the restaurant will not be developed by 2014 but, will be open and included as part of the 2030 background traffic.

The estimated trip generation for the additional background traffic is shown below in *Table 1*. The trip generation rates used to estimate the additional development traffic is based on extensive surveys of the trip-generation rates for other similar land uses as documented in the Institute of Transportation Engineers *Trip Generation Manual*, 9th Edition. The table shows the AM and PM peak hour trip generation for the proposed uses.

Table 1 - Estimated Additional Background Trip Generation

Use	Size	AM Peak Hour			PM Peak Hour		
		Total	In	Out	Total	In	Out
Senior Housing/Skilled	209 units	42	15	27	53	29	24
Apartments	232 units	118	24	94	144	94	50
Shopping Center	143,880 sf	138	86	52	533	256	277
Restaurant	8000 sf	87	48	39	79	47	32
Total New Trips		385	173	212	809	426	383

Source: Institute of Transportation Engineers Trip Generation Manual, 9th Edition

Site Expansion Trip Generation

The trip generation used to estimate the proposed additional site traffic was calculated two ways: (1) Using traffic generation rated from similar land use (i.e. hospital) as documented in the Institute of Transportation Engineers *Trip Generation Manual*, 9th Edition, and; (2) Based on data provided by the hospital on annual and average peak visits to the emergency room and urgent care facilities.

Determining the traffic generation using the existing site conditions was based on data provided by the hospital from November 2011 through August 2012. During this time frame for the emergency room there was an average of 3 visits in the AM peak hour and an average of 7 visits in the PM peak hour. For the urgent care facility, at the peak, there were 7 visits per hour. Currently the urgent care does not operate during the AM peak hour. Each of these visits represents two trips, one in and one out. It is proposed that there will be a 10% increase in services with the proposed expansion and that the urgent care would be operating during the AM peak hour. This will result in an increase of 16 trips in the AM peak hour (assuming a peak condition for the urgent care) and 4 trips in the PM peak hour.

The estimated trip generation from the proposed Fairview Southdale Hospital expansion is shown below in *Table 2*.

Table 2 - Estimated Expansion Trip Generation – ITE Rates

Use	Size	AM Peak Hour			PM Peak Hour		
		Total	In	Out	Total	In	Out
Emergency Room		2	1	1	2	1	1
Urgent Care Facility		14	7	7	2	1	1
Hospital Uses ⁽¹⁾	20, 820 sf	20	13	7	20	8	12
Total Expansion		36	21	15	24	10	14

(1) - Source: Institute of Transportation Engineers Trip Generation Manual, 9th Edition

Traffic Distribution

Background and site-generated trips were distributed to the adjacent roadway system based on several factors including the existing Annual Average Daily Traffic (AADT) and the travel sheds for the major routes that serve the area. In general the Trip Distribution was assumed:

- 30% to the north
- 40% to the south
- 15% to the east
- 15% to the west.

Additional background trips were distributed to 65th Street, 66th Street or France Avenue based on the ratio of existing AADT volumes on each respective roadway.

The generated trips for the proposed Fairview Southdale Hospital expansion were assumed to arrive or exit using driveways on 65th Street and circulate using the existing Drew Avenue ring road. These trips were assigned based on the ratio of existing traffic patterns on each respective roadway.

Future Year Traffic Forecasts

Traffic forecasts were prepared for the year 2014 which is the year after the proposed expansion would be completed and for the 2030 conditions which represents the City's Comprehensive Plan development time frame. Three improvement alternatives were evaluated.

1. Existing – Assuming existing lane configurations and traffic control
2. No Build – Assuming existing lane configurations and traffic control without the hospital expansion
3. Build – Assuming existing lane configuration and traffic control with the hospital expansion.

The traffic forecasts were prepared by adding the projected annual background traffic growth and the projected non-development background traffic growth to the existing 2013 traffic counts to determine the “No-Build” traffic conditions. The anticipated Fairview Southdale Hospital expansion traffic was then added to the no-build to determine the “Build” traffic conditions. *Figures 4 – 7* shows the projected 2014 and 2030 No-Build and Build AM and PM peak hour traffic volumes.

Traffic Operations

Existing and/or forecasted traffic operations were evaluated for the impacted intersections and access driveway adjacent to the hospital. The analysis was conducted for the following scenarios.

1. Existing 2013 Conditions
2. Projected 2014 No Build
3. Projected 2014 Build
4. Projected 2030 No Build
5. Projected 2030 Build

This section describes the methodology used to assess the operations and provides a summary of traffic operations for each scenario.

Analysis Methodology

The traffic operations analysis is derived from established methodologies documented in the *Highway Capacity Manual 2000* (HCM). The HCM provides a series of analysis techniques that are used to evaluate traffic operations.

Intersections are given a Level of Service (LOS) grade from “A” to “F” to describe the average amount of control delay per vehicle as defined in the HCM. The LOS is primarily a function of peak traffic hour turning movement volumes, intersection lane configuration, and the traffic controls at the intersection. LOS A is the best traffic operating condition, and drivers experience minimal delay at an intersection operating at that level. LOS E represents the condition where the intersection is at capacity, and some drivers may have to wait through more than one green phase to make it through an intersection controlled by traffic signals.

LOS F represents a condition where there is more traffic than can be handled by the intersection, and many vehicle operators may have to wait through more than one green phase to make it through the intersection. At a stop sign-controlled intersection, LOS F would be characterized by exceptionally long vehicle queues on each approach at an all-way stop, or long queues and/or great difficulty in finding an acceptable gap for drivers on the minor legs at a through-street intersection.

The LOS ranges for both signalized and un-signalized intersections are shown in *Table 3*. The threshold LOS values for un-signalized intersections are slightly less than for signalized intersections. This variance was instituted because drivers' expectations at intersections differ with the type of traffic control. A given LOS can be altered by increasing (or decreasing) the number of lanes, changing traffic control arrangements, adjusting the timing at signalized intersections, or other lesser geometric improvements. LOS also changes as traffic volumes increase or decrease.

Table 3 - Intersection Level of Service Ranges

	Control Delay (Seconds)	
	Signalized	Un-Signalized
A	≤ 10	≤ 10
B	10 – 20	10 – 15
C	20 – 35	15 – 25
D	35 – 55	25 – 35
E	55 – 80	35 – 50
F	> 80	> 50

Source: HCM

LOS, as described above, can also be determined for the individual legs (sometimes referred to as “approaches”) or lanes (turn lanes in particular) of an intersection. It should be noted that a LOS E or F might be acceptable or justified in those cases where a leg(s) or lane(s) has a very low traffic volume as compared to the volume on the other legs. For example, improving LOS on such low-volume legs by converting a two-way stop condition to an all-way stop, or adjusting timing at a signalized intersection, could result in a significant penalty for the many drivers on the major road while benefiting the few on the minor road. Also, geometric improvements on minor legs, such as additional lanes or longer turn lanes, could have limited positive effects and might be prohibitive in terms of benefit to cost.

Although LOS A represents the best possible level of traffic flow, the cost to construct roadways and intersection to such a high standard often exceeds the benefit to the user. Funding availability might also lead to acceptance of intersection or roadway designs with a lower LOS. LOS D is generally accepted as the lowest acceptable level in urban areas. LOS C is often considered to be the desirable minimum level for rural areas. LOS D or E may be acceptable for limited durations or distances, or for very low-volume legs of some intersections.

The LOS analysis was performed using Synchro/SimTraffic:

- Synchro, a software package that implements Highway Capacity Manual (HCM) methodologies, was used to build each signalized intersection and provide an input database for turning-movement volumes, lane geometrics, and signal design and timing characteristics. In addition, Synchro was used to optimize signal timing parameters for future conditions. Output from Synchro is transferred to SimTraffic, the traffic simulation model.
- SimTraffic is a micro-simulation computer modeling software that simulates each individual vehicle’s characteristics and driver behavior in response to traffic volumes, intersection configuration, and signal operations. The model simulates drivers’ behaviors and responses to surrounding traffic flow as well as different vehicle types and speeds. It outputs estimated vehicle delay and queue lengths at each intersection being analyzed.

Existing Level of Service Summary

Table 4, below, summarizes the existing LOS at the primary intersections in the study area based on the current lane geometry, traffic control and traffic volumes. The table shows that all intersection are operating at an overall LOS D or better during both the AM and PM peak hours with all movements operating at LOS E or better. A table showing the LOS and delays by approach is included in the *Appendix*.

Table 4 - Existing Level of Service

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	Overall Delay (sec/veh)	LOS	Overall Delay (sec/veh)
France Ave at 65 th Street	C (D)	20.1	C (E)	21.5
France Ave at 66 th Street	C (D)	23.0	C (D)	26.1
65 th Street at Hospital Driveway	A (B)	4.5	A (C)	6.2
65 th Street at Drew Avenue	A (B)	6.9	A (B)	5.6
66 th Street at Drew Avenue	C (D)	24.4	C (D)	24.8

C = Overall LOS, (D) = Worst movement LOS Source: WSB & Associates, Inc.

Forecast Traffic Operations

A capacity and LOS analysis was completed for the study area intersections for 2014 which is the year after the proposed Southdale Hospital expansion would be fully developed and for the 2030 conditions which represents the City’s Comprehensive Plan development time frame. The results of the analysis are discussed below and shown in *Tables 5 and 6*. Detailed tables showing the LOS and delays by approach are included in the *Appendix*.

Table 5 – Forecasted No Build, shows that all intersection will continue to operate at overall LOS D or better in 2014 and 2030 during both the AM and PM peak hours. However, with the increase in traffic, some additional movements will be operating at LOS E. Overall delays will also increase slightly from the existing conditions to the 2030 conditions, especially at the major intersections on France Avenue at 65th Street and 66th Street and on 66th Street at Drew Avenue/Southdale entrance.

By 2030 the analysis indicates that at the intersection of 65th Street and France Avenue potential issues on the 65th Street approaches and France Avenue left turns may exist. With minor intersection and signal improvements (additional turn lane length and signal phasing changes), these issues would be minimized, improving the overall intersection LOS back to a C with 20 to 25 sec delays.

Table 5 – Forecasted No Build Level of Service

Intersection	2014				2030			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	LOS	Overall Delay (sec/veh)						
France Ave at 65 th St	C (D)	20.5	C (E)	22.8	C (D)	23.7	D (E)	37.4
France Ave at 66 th St	C (D)	23.1	C (D)	26.3	C (D)	23.5	C (E)	32.8
65 th St at Hospital Driveway	A (B)	4.5	A (B)	7.5	A (C)	5.1	C (D)	14.4
65 th St at Drew Ave	A (B)	7.0	A(B)	5.6	A (B)	7.5	A(B)	6.1
66 th St at Drew Ave	C (D)	24.5	C (D)	24.9	C (D)	26.5	C (D)	27.3

C = Overall LOS, (D) = Worst movement LOS Source: WSB & Associates, Inc.

Table 6 – Forecasted Build with Hospital Expansion, shows that all intersection will continue to operate at overall LOS D or better in 2014 and 2030 during both the AM and PM peak hours with some movements at LOS E. Similar to the Forecasted No-Build conditions the intersection of 65th Street and France Avenue will have potential delay issues. With minor intersection and signal improvements (additional turn lane length and signal phasing changes), these issues would be minimized, improving the overall intersection LOS back to a C with 20 to 25 sec delays. In addition the intersection of 65th Street and the Hospital Entrance will experience increased delays, although they do not indicate a need for potential mitigation.

Table 6 – Forecast Build with Hospital Expansion

Intersection	2014				2030			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	LOS	Overall Delay (sec/veh)						
France Ave at 65 th Street	C (D)	20.5	C (E)	23.3	C (D)	23.7	D (E)	37.4
France Ave at 66 th Street	C (D)	23.1	C (D)	26.6	C (D)	24.0	C (E)	35.0
65 th Street at Hospital Driveway	A (C)	4.7	A (D)	7.9	A (C)	5.1	C (D)	21.8
65 th Street at Drew Ave	A (B)	7.1	A(B)	5.6	A (B)	7.5	A(B)	6.1
66 th Street at Drew Ave	C (D)	24.8	C (D)	24.9	C (D)	26.5	C (D)	27.3

C = Overall LOS, (D) = Worst movement LOS Source: WSB & Associates, Inc.

Vehicle Queuing Analysis

A queuing analysis for the existing and future 2014 and 2030 conditions was prepared evaluating the anticipated vehicle queuing impacts at the intersections in the study area. The analysis was conducted using the SimTraffic simulation software.

The results found that during both the AM and PM peak hours, for the existing, and future no-build and build 2014 and 2030 conditions, the average queues in the corridors do not exceed any of the available turn lane storage. In some cases however, for the 2030 no-build and build conditions, the maximum queues were exceeded, specifically; on 65th Street between France Avenue and the hospital entrance. The maximum queue represents the longest length of queue that was observed during the analysis period. The observations were typically identified one or two times during the peak periods with an extremely short duration of less than 4 to 6 seconds.

The potential future mitigation at the 65th Street and France Avenue intersection discussed above will improve the flow of traffic on 65th Street and also minimize traffic blocking the hospital entrance. Additional signage could also be added indicating “do not block intersection” and/or “no left turns during peak hours” at the hospital entrance should this become an issue.

Tables showing the average and maximum queue lengths by movement and approach are included in the *Appendix*.

Parking Impact Analysis

The parking impacts to the existing Fairview Southdale Hospital and Medical office buildings were analyzed based on the current use of the site and anticipated growth. **Table 7** below shows a summary of the existing parking available on the site. Currently there are two primary parking structures and surface spaces located throughout the site. These locations are shown in **Figure 2**.

Table 7 – Available Parking

Location	Existing Spaces	Proposed Spaces
West Parking Ramp	1018	1018
East Parking Ramp	975	975
Surface Spaces	74	58
Total Parking Spaces	2067	2051

In order to provide a base line of the parking demand for the site, the existing parking was counted. **Table 8** shows the average and peak number of occupied spaces throughout the site.

Table 8 – Parking Use Counted

Location	Average Occupied Spaces	Peak Occupied Spaces
West Parking Ramp	1455	1690
East Parking Ramp		
Surface Spaces	40	58
Total Parking Spaces	1495	1748

Based on the City Code the Fairview Southdale Hospital site would require approximately 2575 parking spaces. If it is assumed that the site will have a 10% increase in staff as a result of the expansion, the site would require 2728. In both case a parking variance is required for site would require 6747total spaces requiring a variance for 524 and 677 respectively. **Table 9** shows the required parking based on the City Code.

Table 9 – Parking Required per City Code

Use	Existing	Parking Required	Proposed	Parking Required
Hospital	326 Beds 1525 Emp	1851	326 Beds 1678 Emp	2004
6405 Medical Office	54,248 sf 49 Doc/Dent	320	54,248 sf 49 Doc/Dent	320
6363 Medical Office	69,442 sf 56 Doc/Dent	404	69,442 sf 56 Doc/Dent	404
Total Parking Spaces Required		2575		2728

The parking demand for the site was also estimated based on the parking demand surveys of the parking generation for other similar land uses as documented in the Institute of Transportation Engineers *Parking Generation Manual*, 4th Edition. **Table 10** below shows a summary of each uses and what the anticipated peak parking demand would be for a typical weekday. This would represent the worst case condition for the parking on the site assuming the existing and proposed uses. Based on this analysis, the existing parking required is very similar to the peak occupied spaces counted.

Table 10 – Parking Demand per ITE

Use	Existing	Parking Required	Proposed	Parking Required
Hospital	1525 Emp	1266	1678 Emp	1393
6405 Medical Office	54,248 sf	192	54,248 sf	192
6363 Medical Office	69,442 sf	246	69,442 sf	246
Total Parking Spaces Required		1704		1831

Based on the above parking summary, there is and would be sufficient parking available on site for the proposed Fairview Southdale expansion.

AA8

Conclusions / Recommendation

Based on the analysis documented in this memorandum, WSB has concluded the following:

- The proposed Fairview Southdale Emergency Center Expansion project includes the expansion of the emergency center, urgent care, behavioral health and observation area. The site is anticipated to generate an additional 36 trips in the AM peak hour and 24 trips in the PM peak hour.
- Additional trips will be generated from other approved or anticipated development in the surrounding area. These uses will generate an additional 385 trips in the AM peak hour and 809 trips in the PM peak hour.
- Existing traffic operations at the intersections and driveways in the study area on 65th Street and 66th Street are all operating at overall LOS D or better for the both the AM and PM peak hours.
- Intersection traffic operations for the No-Build conditions in 2014 and 2030 will continue to operate at an overall LOS D or better for the AM and PM peak hours. However, some movements, specifically at the 65th Street and France Avenue intersection will be operating at LOS E. By 2030 the analysis indicates that at the intersection may have potential issues on the 65th Street approaches and France Avenue left turns. With minor intersection and signal improvements (additional turn lane lengths and signal phasing changes), these issues would be minimized, improving the overall intersection LOS back to a C.
- Intersection traffic operations for the Forecasted Build alternative (with the Hospital Expansion traffic) in 2014 and 2030 will continue to operate at an overall LOS D or better during the AM and PM peak hours. Similar to the Forecasted No-Build conditions the intersection of 65th Street and France Avenue may have potential delay issues. With the minor intersection and signal improvements (additional turn lane length and signal phasing changes), these issues would be minimized, improving the overall intersection LOS back to a C.
- The results of the queuing analysis found that during both the AM and PM peak hours, for the existing, and future no-build and build 2014 and 2030 conditions, the average queues in the corridors do not exceed any of the available turn lane storage. In some cases however, for the 2030 no-build and build conditions, the maximum queues were exceeded, specifically; on 65th Street between France Avenue and the hospital entrance. The potential future mitigation at the 65th Street and France Avenue intersection will improve the flow of traffic on 65th Street and also minimize traffic blocking the hospital entrance. Additional signage could also be added at the hospital entrance should this become an issue.

- The existing or proposed available parking does not or will not meet the City's Code. A With the current and proposed Hospital Expansion, 2728 parking spaces would be required based on City Code. The current plan provides for 2051 parking spaces. This would require a variance for 677 parking spaces.
- Based on the parking count and the ITE parking generation estimates the total parking required for the Fairview Southdale Hospital site would range between 1750 and 1850 spaces. With this estimate, no parking variance would be required.

Based on these conclusions the following is recommended.

1. Provide the proposed roadway internal roadway improvements as shown on the proposed site plan (*Figure 2*).
2. Although no improvements to the France Avenue at 65th Street intersection are specifically required at this time. Should delays and queuing become an issue in the future, minor intersection turn lane and phasing improvements may be necessary. Should these improvements be required in the future Fairview Southdale Hospital will be responsible for their share of those improvements.
3. No additional roadway improvements or additional parking would be required to accommodate the proposed Fairview Southdale Emergency Center Expansion project.

APPENDIX



Traffic and Parking Study

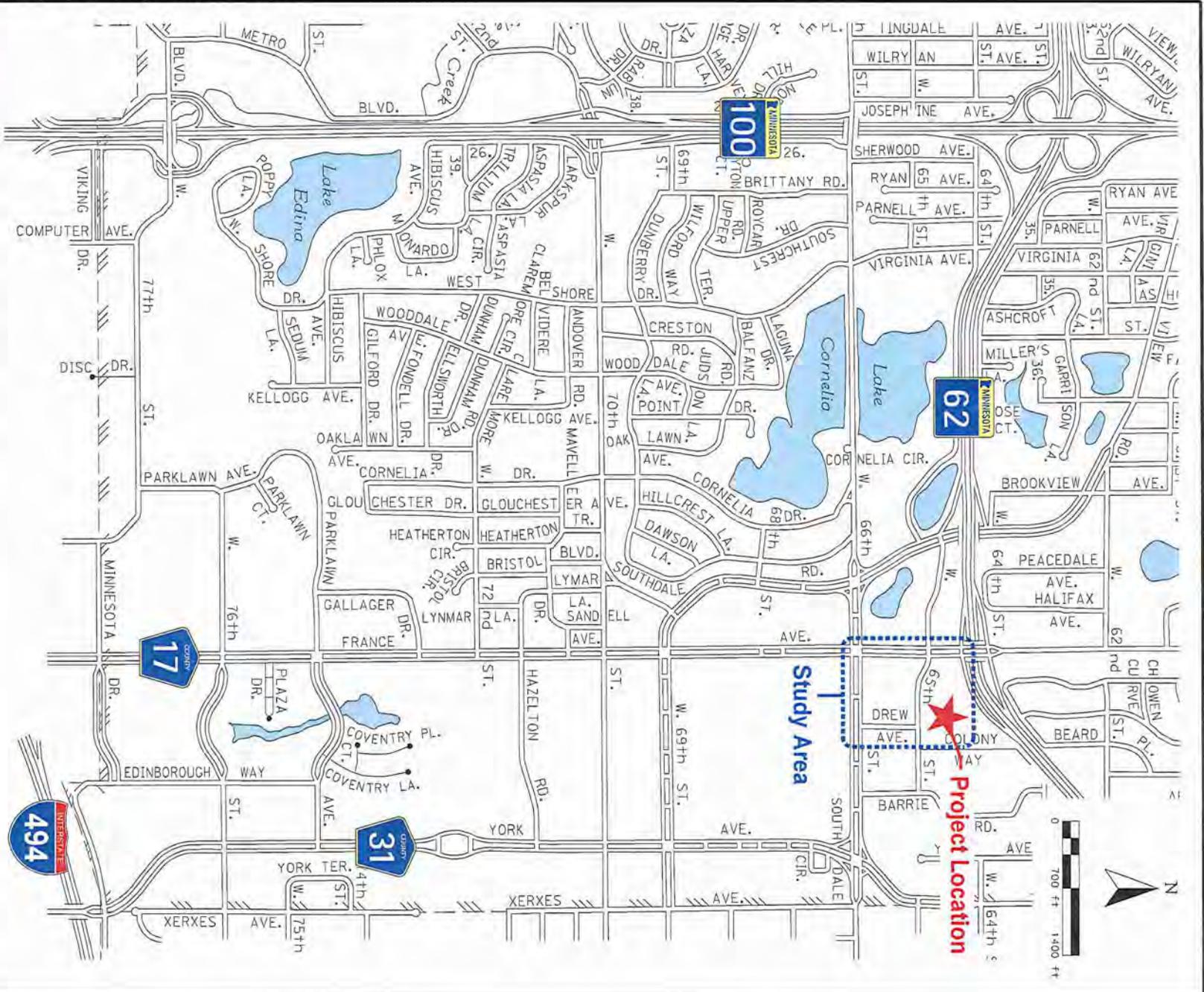
Fairview Southdale Hospital Emergency Center Expansion

City of Edina, Minnesota

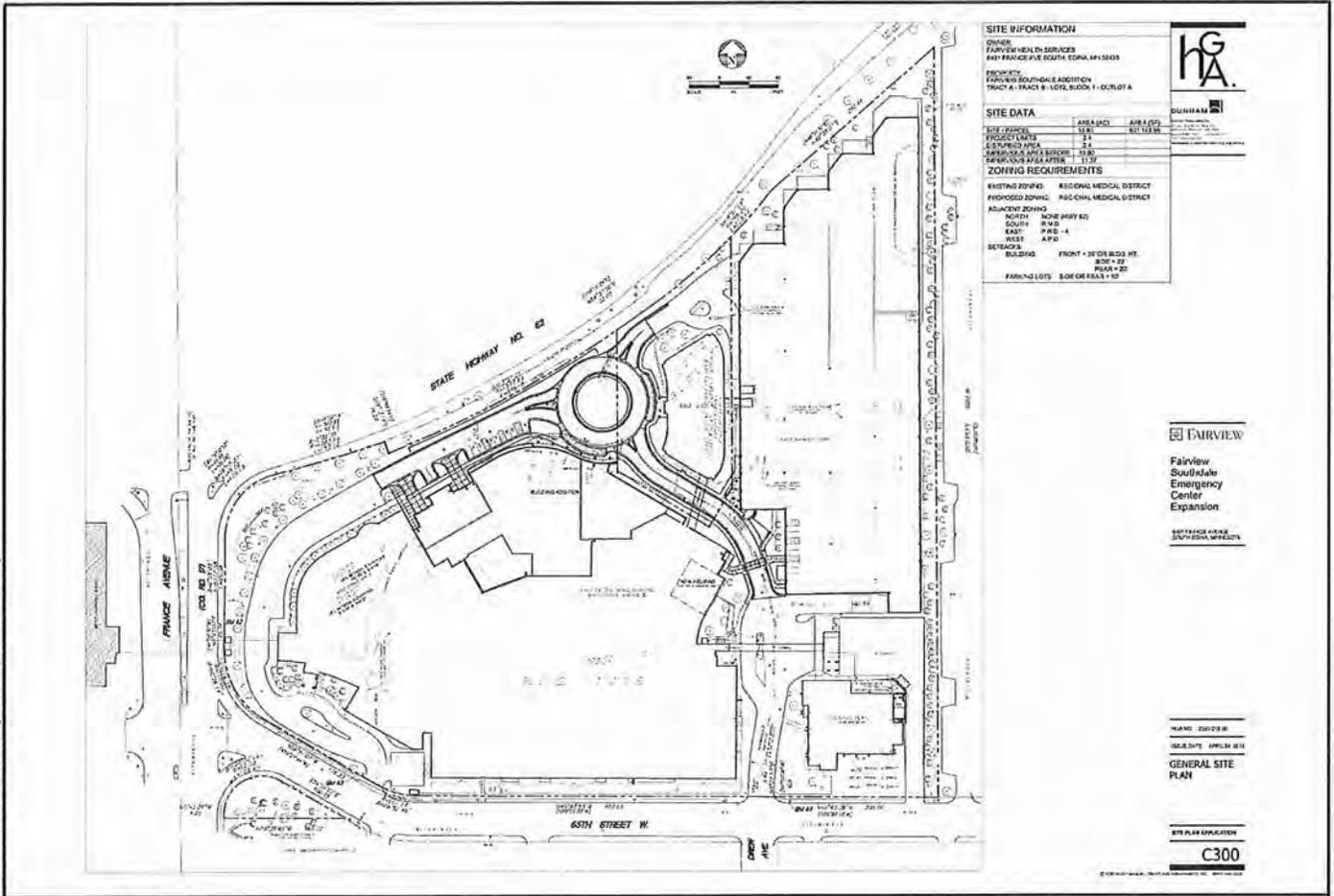
AS2

Project Location Map

Figure 1



Date: 1/14/2014
 Project: 13801 Conf. Exhibit 15.1885-28 fig-02 - Proposed Site Plan.dgn



Traffic and Parking Study
 Fairview Southdale Hospital Emergency Center Expansion
 City of Edina, Minnesota

Figure 2

Proposed Site Plan

AS3

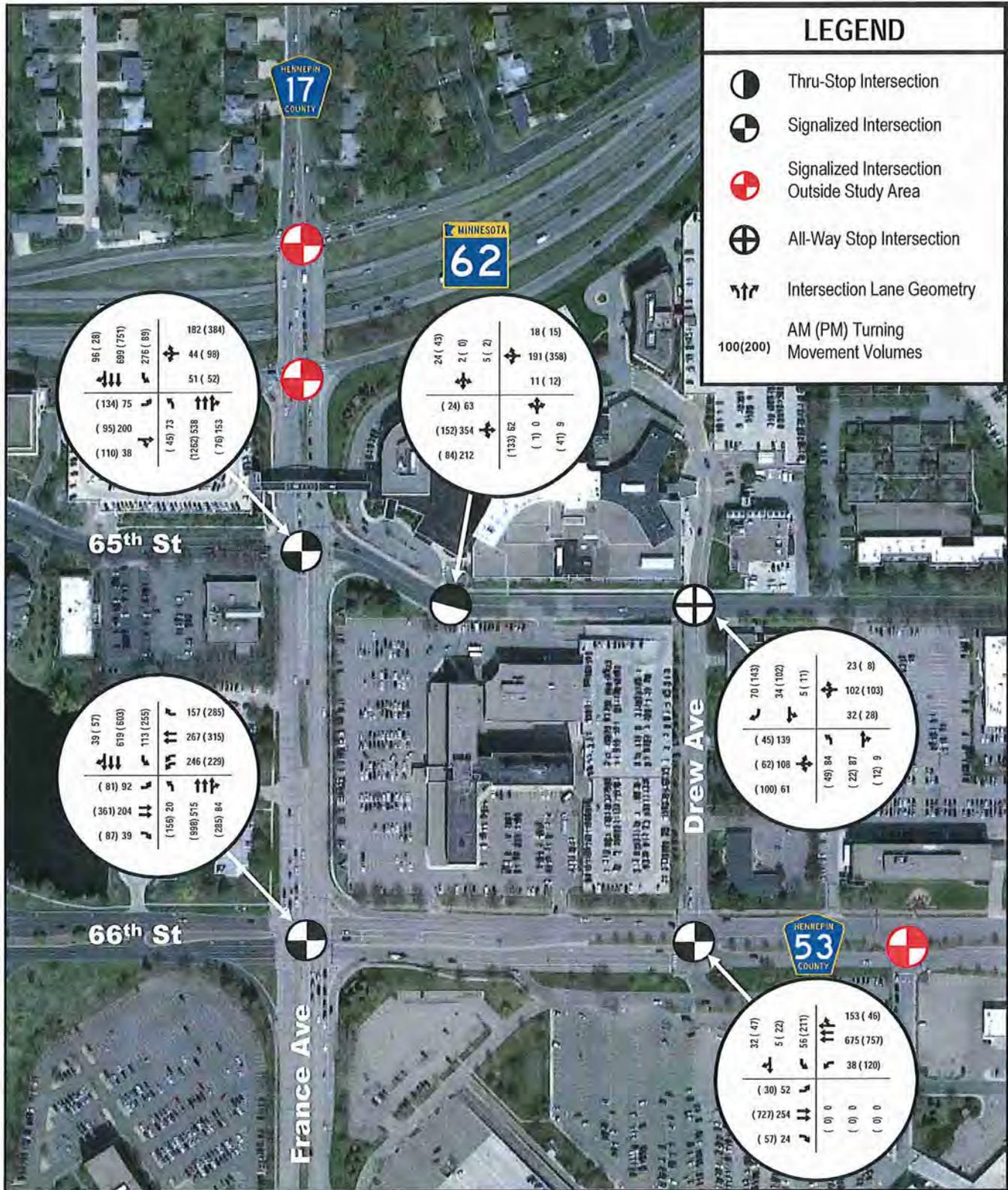


Traffic and Parking Study
 Fairview Southdale Hospital Emergency Center Expansion
 City of Edina, Minnesota

Figure 3

Existing Conditions

AST



Traffic and Parking Study
 Fairview Southdale Hospital Emergency Center Expansion
 City of Edina, Minnesota

Figure 4

2014 No Build Conditions

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Traffic and Parking Study
 Fairview Southdale Hospital Emergency Center Expansion
 City of Edina, Minnesota

Figure 5
 2014 Build Conditions

A56

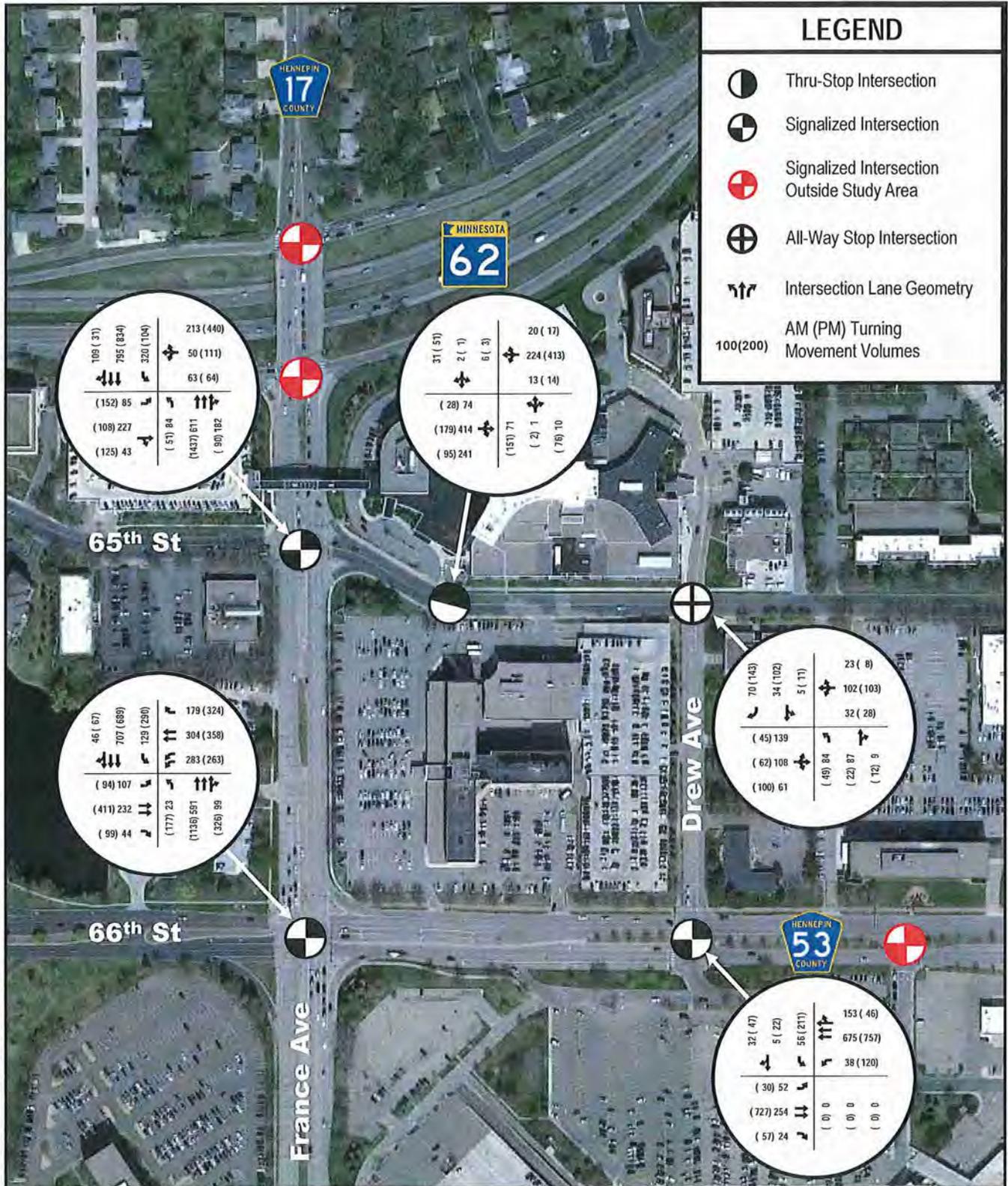


Traffic and Parking Study
 Fairview Southdale Hospital Emergency Center Expansion
 City of Edina, Minnesota

Figure 6

2030 No Build Conditions

A57

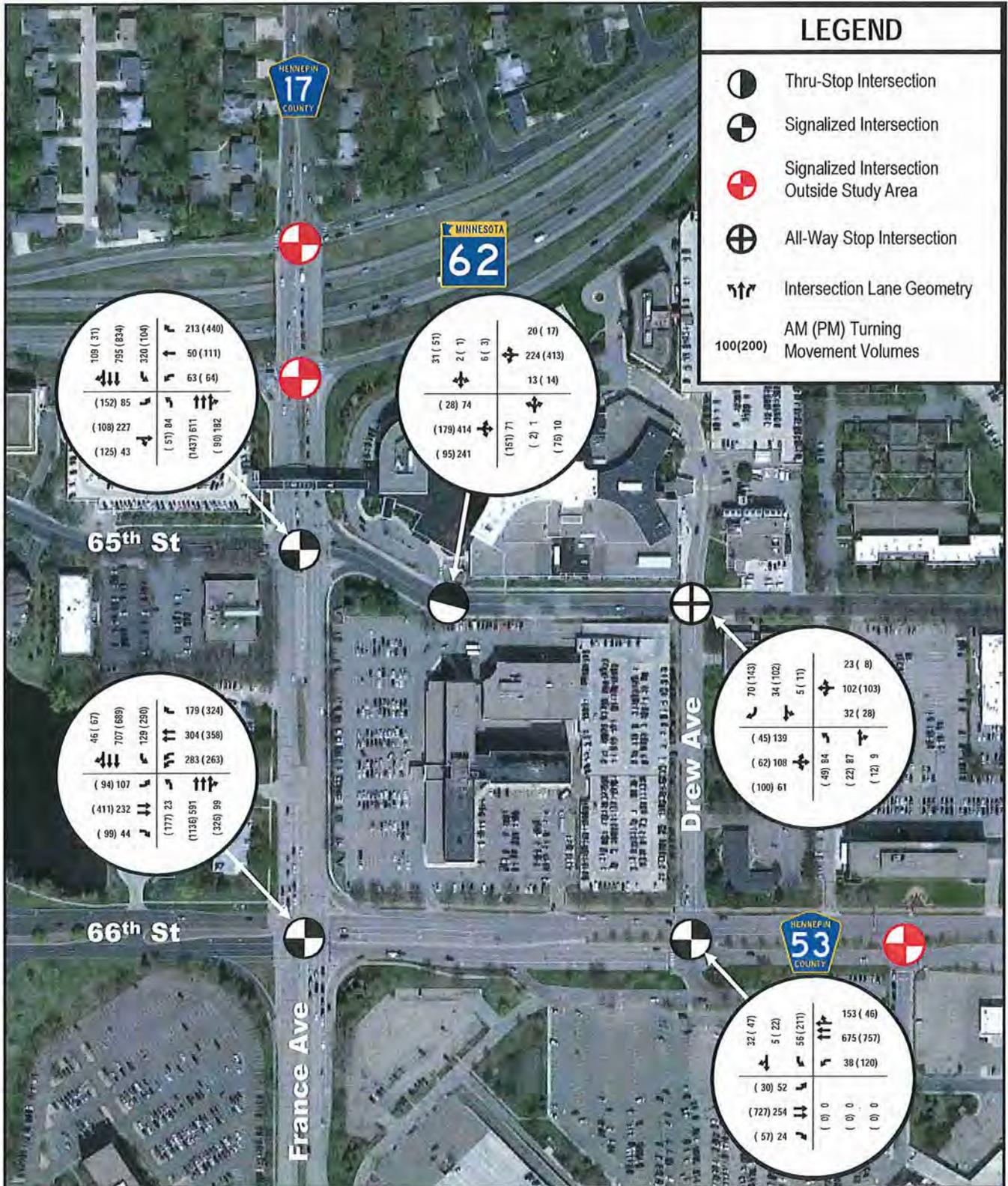


Traffic and Parking Study
 Fairview Southdale Hospital Emergency Center Expansion
 City of Edina, Minnesota

Figure 7

2030 Build Conditions

A58



Traffic and Parking Study
 Fairview Southdale Hospital Emergency Center Expansion
 City of Edina, Minnesota

Figure 8
 2030 Build Conditions
 With Mitigated Lane Geometry

A59

WSB & Associates

701 Xenia Ave S
Minneapolis, MN

France Ave & 65th St
6:30-9:30 am and 3:30-6:30 pm
Edina, MN
30's

File Name : france ave & 65th st
Site Code : 00000001
Start Date : 2/22/2012
Page No : 1

Groups Printed- Unshifted

Start Time	France Ave From North					65th St From East					France Ave From South					65th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
06:30 AM	35	63	61	0	159	10	4	1	0	15	8	41	17	0	66	2	17	4	0	23	263
06:45 AM	40	72	84	0	196	15	4	4	0	23	15	72	19	0	106	2	32	5	0	39	364
Total	75	135	145	0	355	25	8	5	0	38	23	113	36	0	172	4	49	9	0	62	627
07:00 AM	19	93	48	0	160	29	3	1	0	33	12	77	14	0	103	6	35	14	0	55	351
07:15 AM	27	132	68	0	227	33	8	3	0	44	23	76	20	0	119	3	43	20	0	66	456
07:30 AM	18	134	56	0	208	45	12	2	0	59	18	93	34	0	145	9	42	13	0	64	476
07:45 AM	19	172	64	0	255	36	13	5	0	54	29	115	34	0	178	4	54	15	0	73	560
Total	83	531	236	0	850	143	36	11	0	190	82	361	102	0	545	22	174	62	0	258	1843
08:00 AM	19	143	51	0	213	38	15	5	0	58	20	116	25	0	161	9	52	11	0	72	504
08:15 AM	26	173	54	0	253	52	13	15	0	80	26	124	27	1	178	6	50	22	0	78	589
08:30 AM	24	161	58	0	243	54	12	15	0	81	30	123	13	0	166	13	51	21	0	85	575
08:45 AM	21	156	54	0	231	49	9	19	0	77	35	139	16	0	190	9	35	13	1	58	556
Total	90	633	217	0	940	193	49	54	0	296	111	502	81	1	695	37	188	67	1	293	2224
09:00 AM	23	195	55	0	273	61	18	12	0	91	32	141	16	0	189	9	24	17	0	50	603
09:15 AM	26	157	60	0	243	60	10	13	1	84	33	137	14	0	184	8	36	21	0	65	576
Total	49	352	115	0	516	121	28	25	1	175	65	278	30	0	373	17	60	38	0	115	1179
03:30 PM	14	171	44	0	229	98	30	15	0	143	15	241	19	0	275	24	45	39	0	108	755
03:45 PM	11	190	29	0	230	100	29	19	1	149	27	268	15	0	310	20	25	25	0	70	759
Total	25	361	73	0	459	198	59	34	1	292	42	509	34	0	585	44	70	64	0	178	1514
04:00 PM	14	163	34	0	211	100	26	20	0	146	10	258	13	0	281	26	29	44	0	99	737
04:15 PM	6	180	24	0	210	83	17	19	2	121	22	270	8	0	300	20	27	31	0	78	709
04:30 PM	6	178	19	0	203	110	36	14	0	160	14	251	12	0	277	26	26	43	0	95	735
04:45 PM	10	196	27	0	233	92	16	9	0	117	18	309	12	0	339	23	17	26	0	66	755
Total	36	717	104	0	857	385	95	62	2	544	64	1088	45	0	1197	95	99	144	0	338	2936
05:00 PM	6	165	18	0	189	102	25	15	2	144	16	334	10	0	360	38	23	33	0	94	787
05:15 PM	5	197	12	0	214	68	18	12	0	98	19	344	10	0	373	21	18	29	0	68	753
05:30 PM	6	194	12	0	212	56	22	8	0	86	9	274	17	0	300	13	13	19	0	45	643
05:45 PM	5	212	15	0	232	47	3	11	0	61	5	248	11	0	264	8	15	21	0	44	601
Total	22	768	57	0	847	273	68	46	2	389	49	1200	48	0	1297	80	69	102	0	251	2784
06:00 PM	6	168	15	1	190	33	3	1	0	37	6	275	8	0	289	15	12	23	0	50	566
06:15 PM	7	193	17	0	217	51	7	3	0	61	10	289	5	0	304	10	3	15	0	28	610
Grand Total	393	3858	979	1	5231	1422	353	241	6	2022	452	4615	389	1	5457	324	724	524	1	1573	14283
Apprch %	7.5	73.8	18.7	0		70.3	17.5	11.9	0.3		8.3	84.6	7.1	0		20.6	46	33.3	0.1		
Total %	2.8	27	6.9	0	36.6	10	2.5	1.7	0	14.2	3.2	32.3	2.7	0	38.2	2.3	5.1	3.7	0	11	

A60

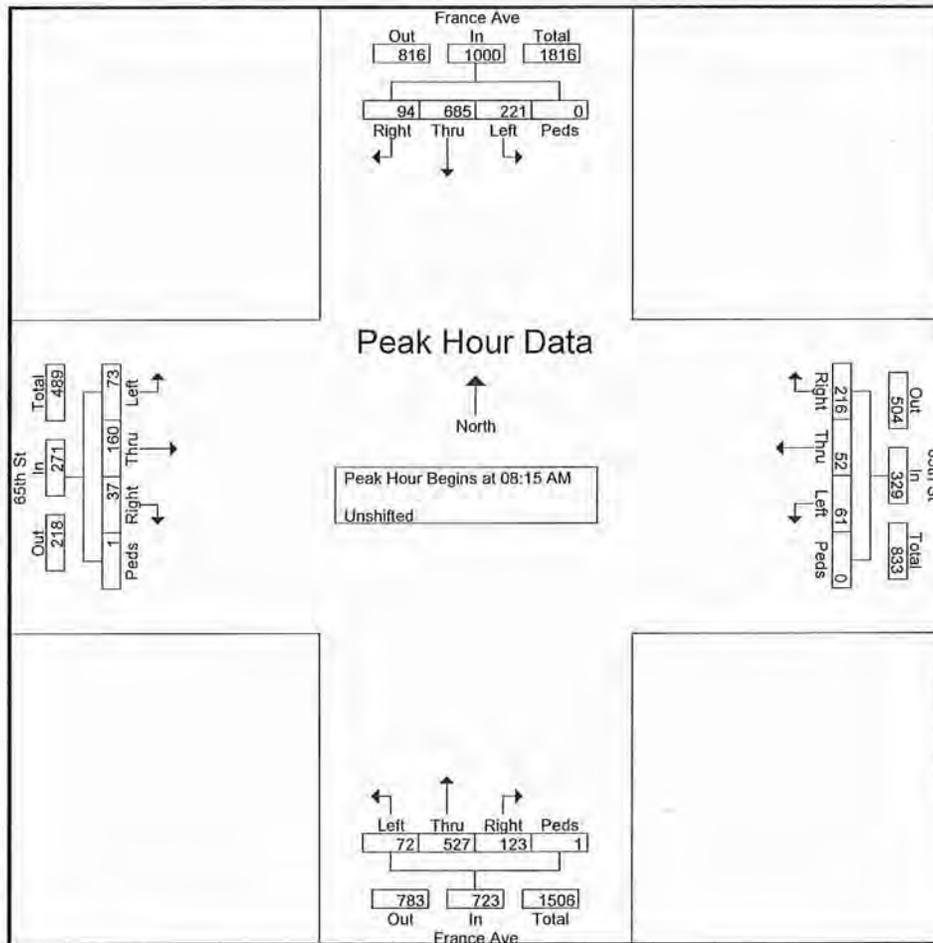
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

France Ave & 65th St
6:30-9:30 am and 3:30-6:30 pm
Edina, MN
30's

File Name : france ave & 65th st
Site Code : 00000001
Start Date : 2/22/2012
Page No : 2

Start Time	France Ave From North					65th St From East					France Ave From South					65th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
Peak Hour Analysis From 06:30 AM to 11:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:15 AM																					
08:15 AM	26	173	54	0	253	52	13	15	0	80	26	124	27	1	178	6	50	22	0	78	589
08:30 AM	24	161	58	0	243	54	12	15	0	81	30	123	13	0	166	13	51	21	0	85	575
08:45 AM	21	156	54	0	231	49	9	19	0	77	35	139	16	0	190	9	35	13	1	58	556
09:00 AM	23	195	55	0	273	61	18	12	0	91	32	141	16	0	189	9	24	17	0	50	603
Total Volume	94	685	221	0	1000	216	52	61	0	329	123	527	72	1	723	37	160	73	1	271	2323
% App. Total	9.4	68.5	22.1	0		65.7	15.8	18.5	0		17	72.9	10	0.1		13.7	59	26.9	0.4		
PHF	.904	.878	.953	.000	.916	.885	.722	.803	.000	.904	.879	.934	.667	.250	.951	.712	.784	.830	.250	.797	.963



AGI

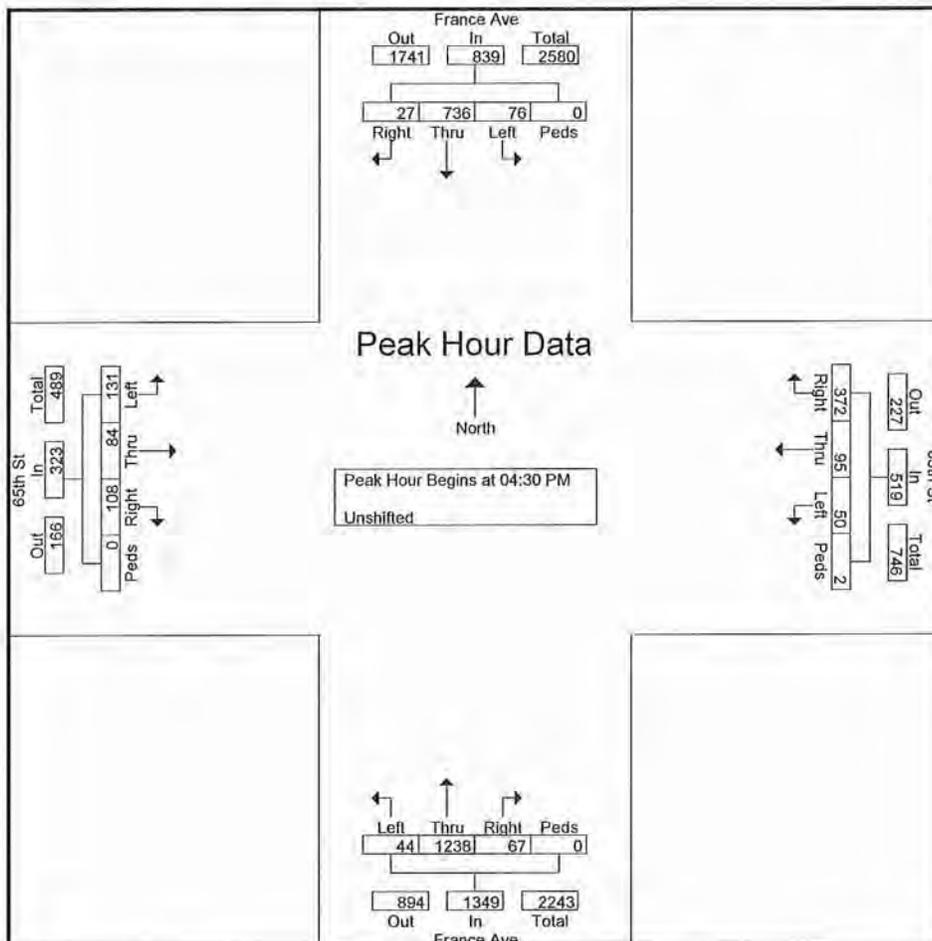
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

France Ave & 65th St
6:30-9:30 am and 3:30-6:30 pm
Edina, MN
30's

File Name : france ave & 65th st
Site Code : 00000001
Start Date : 2/22/2012
Page No : 3

Start Time	France Ave From North					65th St From East					France Ave From South					65th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
Peak Hour Analysis From 12:00 PM to 06:15 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	6	178	19	0	203	110	36	14	0	160	14	251	12	0	277	26	26	43	0	95	735
04:45 PM	10	196	27	0	233	92	16	9	0	117	18	309	12	0	339	23	17	26	0	66	755
05:00 PM	6	165	18	0	189	102	25	15	2	144	16	334	10	0	360	38	23	33	0	94	787
05:15 PM	5	197	12	0	214	68	18	12	0	98	19	344	10	0	373	21	18	29	0	68	753
Total Volume	27	736	76	0	839	372	95	50	2	519	67	1238	44	0	1349	108	84	131	0	323	3030
% App. Total	3.2	87.7	9.1	0		71.7	18.3	9.6	0.4		5	91.8	3.3	0		33.4	26	40.6	0		
PHF	.675	.934	.704	.000	.900	.845	.660	.833	.250	.811	.882	.900	.917	.000	.904	.711	.808	.762	.000	.850	.963



AG2

WSB & Associates

701 Xenia Ave S
Minneapolis, MN

65th St & Southdale Hospital
6-9 am & 3-6pm
Edina, MN
1686-380

File Name : 65th st & parking lot
Site Code : 00000005
Start Date : 4/2/2013
Page No : 1

Groups Printed- Unshifted

Start Time	Southdale Hospital From North					65th St From East					Southdale Hospital From South					65th St From West					Int. Total
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06:00 AM	0	0	0	0	0	0	7	1	0	8	0	0	0	0	0	2	13	7	0	22	30
06:15 AM	4	0	0	0	4	1	6	0	0	7	0	1	0	0	1	6	47	9	0	62	74
06:30 AM	4	0	2	0	6	3	10	0	0	13	0	0	2	0	2	9	60	19	0	88	109
06:45 AM	6	0	1	0	7	1	20	0	0	21	0	1	1	0	2	25	76	24	0	125	155
Total	14	0	3	0	17	5	43	1	0	49	0	2	3	0	5	42	196	59	0	297	368
07:00 AM	3	0	1	0	4	3	18	0	0	21	0	1	6	0	7	18	65	15	0	98	130
07:15 AM	4	0	2	0	6	5	31	1	0	37	1	1	5	0	7	12	76	11	0	99	149
07:30 AM	2	0	1	0	3	1	49	2	0	52	3	0	8	0	11	29	79	10	0	118	184
07:45 AM	5	2	1	0	8	4	36	6	0	46	0	0	11	0	11	43	91	19	0	153	218
Total	14	2	5	0	21	13	134	9	0	156	4	2	30	0	36	102	311	55	0	468	681
08:00 AM	7	0	1	0	8	5	25	1	0	31	2	0	9	0	11	46	70	18	0	134	184
08:15 AM	6	0	2	0	8	2	42	1	0	45	0	0	13	0	13	53	96	15	0	164	230
08:30 AM	6	0	1	0	7	4	49	1	0	54	7	0	28	0	35	66	89	11	0	166	262
08:45 AM	4	1	2	0	7	1	30	4	0	35	2	0	28	0	30	55	64	9	0	128	200
Total	23	1	6	0	30	12	146	7	0	165	11	0	78	0	89	220	319	53	0	592	876
03:00 PM	18	0	1	0	19	3	70	1	0	74	15	1	30	0	46	40	38	9	0	87	226
03:15 PM	14	0	3	0	17	3	54	4	0	61	16	0	45	0	61	37	45	11	0	93	232
03:30 PM	18	0	1	0	19	0	88	2	0	90	13	2	33	0	48	34	48	2	0	84	241
03:45 PM	14	0	2	0	16	1	68	3	0	72	21	1	27	1	50	29	38	15	0	82	220
Total	64	0	7	0	71	7	280	10	0	297	65	4	135	1	205	140	169	37	0	346	919
04:00 PM	15	0	0	0	15	2	84	1	0	87	9	0	36	0	45	20	35	7	1	63	210
04:15 PM	8	0	0	0	8	5	79	4	0	88	5	0	29	0	34	18	37	2	0	57	187
04:30 PM	6	0	0	0	6	4	84	2	0	90	5	0	38	0	43	15	38	0	0	53	192
04:45 PM	8	0	0	0	8	3	76	1	0	80	6	0	24	0	30	17	42	0	0	59	177
Total	37	0	0	0	37	14	323	8	0	345	25	0	127	0	152	70	152	9	1	232	766
05:00 PM	7	0	0	0	7	2	72	2	0	76	7	0	18	0	25	16	31	0	0	47	155
05:15 PM	5	1	0	0	6	2	49	3	0	54	5	0	11	0	16	14	29	0	0	43	119
05:30 PM	5	0	2	0	7	1	47	1	0	49	4	0	9	0	13	9	25	4	0	38	107
05:45 PM	9	1	0	0	10	2	32	1	0	35	3	2	21	0	26	10	24	8	0	42	113
Total	26	2	2	0	30	7	200	7	0	214	19	2	59	0	80	49	109	12	0	170	494
Grand Total	178	5	23	0	206	58	1126	42	0	1226	124	10	432	1	567	623	1256	225	1	2105	4104
Apprch %	86.4	2.4	11.2	0		4.7	91.8	3.4	0		21.9	1.8	76.2	0.2		29.6	59.7	10.7	0		
Total %	4.3	0.1	0.6	0	.5	1.4	27.4	1	0	29.9	3	0.2	10.5	0	13.8	15.2	30.6	5.5	0	51.3	

A63

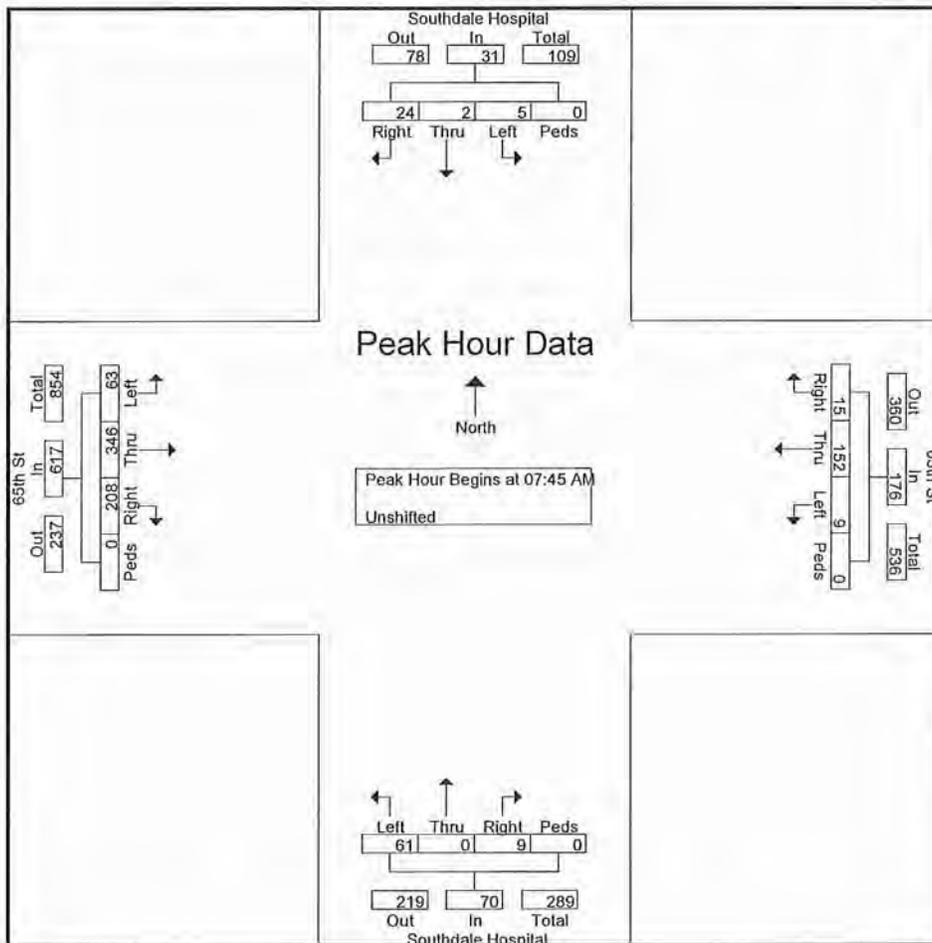
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

65th St & Southdale Hospital
6-9 am & 3-6pm
Edina, MN
1686-380

File Name : 65th st & parking lot
Site Code : 00000005
Start Date : 4/2/2013
Page No : 2

Start Time	Southdale Hospital From North					65th St From East					Southdale Hospital From South					65th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
Peak Hour Analysis From 06:00 AM to 11:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:45 AM																					
07:45 AM	5	2	1	0	8	4	36	6	0	46	0	0	11	0	11	43	91	19	0	153	218
08:00 AM	7	0	1	0	8	5	25	1	0	31	2	0	9	0	11	46	70	18	0	134	184
08:15 AM	6	0	2	0	8	2	42	1	0	45	0	0	13	0	13	53	96	15	0	164	230
08:30 AM	6	0	1	0	7	4	49	1	0	54	7	0	28	0	35	66	89	11	0	166	262
Total Volume	24	2	5	0	31	15	152	9	0	176	9	0	61	0	70	208	346	63	0	617	894
% App. Total	77.4	6.5	16.1	0		8.5	86.4	5.1	0		12.9	0	87.1	0		33.7	56.1	10.2	0		
PHF	.857	.250	.625	.000	.969	.750	.776	.375	.000	.815	.321	.000	.545	.000	.500	.788	.901	.829	.000	.929	.853



AG9

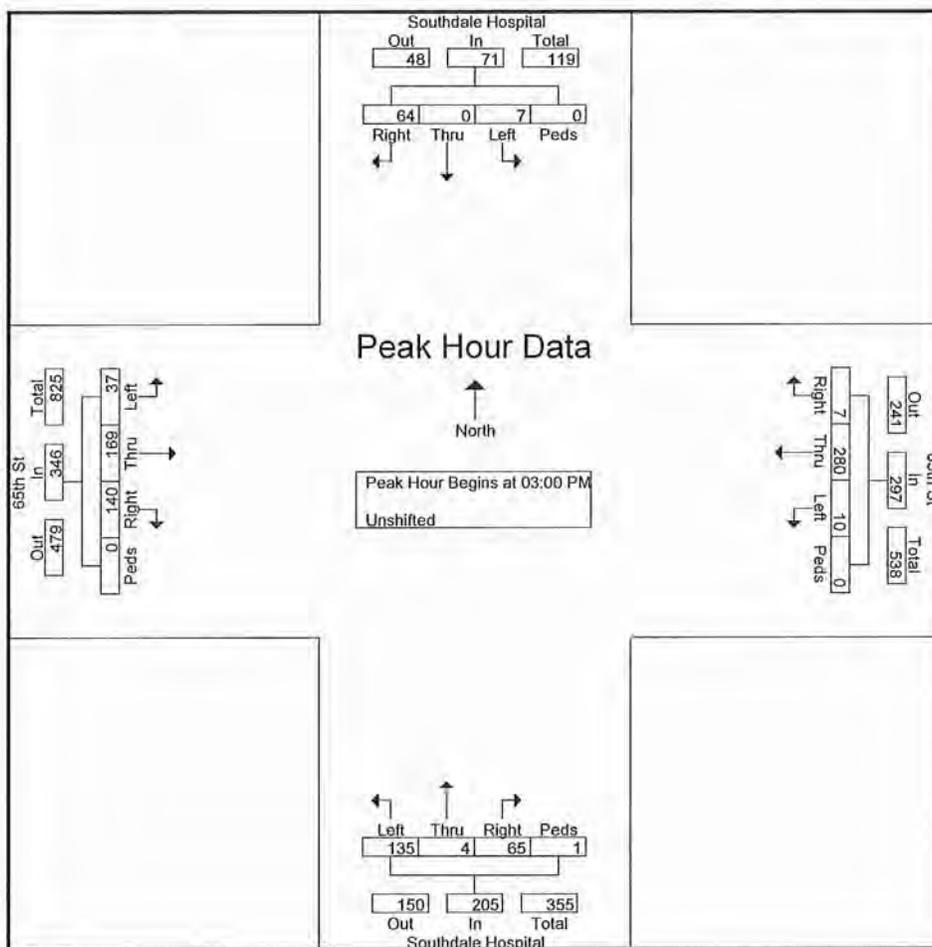
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

65th St & Southdale Hospital
6-9 am & 3-6pm
Edina, MN
1686-380

File Name : 65th st & parking lot
Site Code : 00000005
Start Date : 4/2/2013
Page No : 3

Start Time	Southdale Hospital From North					65th St From East					Southdale Hospital From South					65th St From West					Int. Total
	Rig ht	Thr u	Left	Ped s	App Total	Rig ht	Thr u	Left	Ped s	App Total	Right	Thr u	Left	Peds	App Total	Right	Thr u	Left	Peds	App Total	
Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	18	0	1	0	19	3	70	1	0	74	15	1	30	0	46	40	38	9	0	87	226
03:15 PM	14	0	3	0	17	3	54	4	0	61	16	0	45	0	61	37	45	11	0	93	232
03:30 PM	18	0	1	0	19	0	88	2	0	90	13	2	33	0	48	34	48	2	0	84	241
03:45 PM	14	0	2	0	16	1	68	3	0	72	21	1	27	1	50	29	38	15	0	82	220
Total Volume	64	0	7	0	71	7	280	10	0	297	65	4	135	1	205	140	169	37	0	346	919
% App. Total	90.1	0	9.9	0		2.4	94.3	3.4	0		31.7	2	65.9	0.5		40.5	48.8	10.7	0		
PHF	.889	.000	.583	.000	.934	.583	.795	.625	.000	.825	.774	.500	.750	.250	.840	.875	.880	.617	.000	.930	.953



AGS

WSB & Associates

701 Xenia Ave S
Minneapolis, MN

65th St & Drew Ave
6-9 am & 3-6 pm
Edina, MN
1686-38

File Name : 65th st & drew ave
Site Code : 00000006
Start Date : 4/3/2013
Page No : 1

Groups Printed- Unshifted

Start Time	Drew Ave From North					65th St From East					Drew Ave From South					65th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
06:00 AM	1	1	0	0	2	2	4	1	0	7	0	5	1	0	6	4	5	11	0	20	35
06:15 AM	1	3	0	0	4	4	5	2	0	11	0	11	11	0	22	3	6	20	0	29	66
06:30 AM	5	2	1	0	8	3	9	4	0	16	0	24	9	0	33	6	3	44	0	53	110
06:45 AM	9	4	0	0	13	13	12	3	0	28	0	22	8	0	30	5	16	44	0	65	136
Total	16	10	1	0	27	22	30	10	0	62	0	62	29	0	91	18	30	119	0	167	347
07:00 AM	9	6	0	0	15	4	9	4	0	17	1	13	16	0	30	5	10	21	0	36	98
07:15 AM	9	8	4	0	21	4	17	6	0	27	1	14	20	0	35	8	21	28	0	57	140
07:30 AM	18	16	4	1	39	4	26	6	0	36	4	18	20	0	42	3	20	36	0	59	176
07:45 AM	13	7	1	0	21	8	29	6	0	43	1	29	29	0	59	12	22	50	0	84	207
Total	49	37	9	1	96	20	81	22	0	123	7	74	85	0	166	28	73	135	0	236	621
08:00 AM	21	11	0	0	32	3	25	13	0	41	4	24	17	0	45	14	25	21	0	60	178
08:15 AM	16	8	2	0	26	7	25	3	0	35	1	18	24	0	43	20	31	31	1	83	187
08:30 AM	20	8	2	0	30	5	23	10	0	38	3	16	14	0	33	15	30	37	0	82	183
08:45 AM	10	6	1	0	17	4	21	1	0	26	3	6	10	0	19	9	18	32	0	59	121
Total	67	33	5	0	105	19	94	27	0	140	11	64	65	0	140	58	104	121	1	284	669
03:00 PM	34	22	4	0	60	5	24	7	0	36	3	7	12	0	22	24	11	13	0	48	166
03:15 PM	34	14	2	1	51	0	26	6	0	32	4	11	12	0	27	25	23	17	0	65	175
03:30 PM	45	44	3	0	92	2	17	8	0	27	4	10	16	0	30	25	21	22	0	68	217
03:45 PM	38	27	1	0	66	3	21	3	0	27	0	7	12	0	19	28	16	15	0	59	171
Total	151	107	10	1	269	10	88	24	0	122	11	35	52	0	98	102	71	67	0	240	729
04:00 PM	30	24	4	0	58	1	25	8	0	34	3	4	10	0	17	28	14	14	0	56	165
04:15 PM	26	29	1	0	56	0	23	9	0	32	4	6	15	0	25	16	16	8	0	40	153
04:30 PM	49	22	5	0	76	4	34	8	0	46	5	5	12	0	22	28	16	8	0	52	196
04:45 PM	18	19	2	0	39	3	22	6	0	31	4	11	7	0	22	30	19	11	0	60	152
Total	123	94	12	0	229	8	104	31	0	143	16	26	44	0	86	102	65	41	0	208	666
05:00 PM	27	25	2	0	54	1	30	2	0	33	1	6	10	0	17	36	15	8	0	59	163
05:15 PM	23	10	1	0	34	0	20	10	0	30	1	6	11	0	18	18	24	15	0	57	139
05:30 PM	19	15	0	0	34	1	16	13	0	30	1	7	6	0	14	26	23	13	1	63	141
05:45 PM	19	11	2	0	32	0	20	1	0	21	0	5	4	0	9	7	10	12	0	29	91
Total	88	61	5	0	154	2	86	26	0	114	3	24	31	0	58	87	72	48	1	208	534
Grand Total	494	342	42	2	880	81	483	140	0	704	48	285	306	0	639	395	415	531	2	1343	3566
Apprch %	56.1	38.9	4.8	0.2		11.5	68.6	19.9	0		7.5	44.6	47.9	0		29.4	30.9	39.5	0.1		
Total %	13.9	9.6	1.2	0.1	24.7	2.3	13.5	3.9	0	19.7	1.3	8	8.6	0	17.9	11.1	11.6	14.9	0.1	37.7	

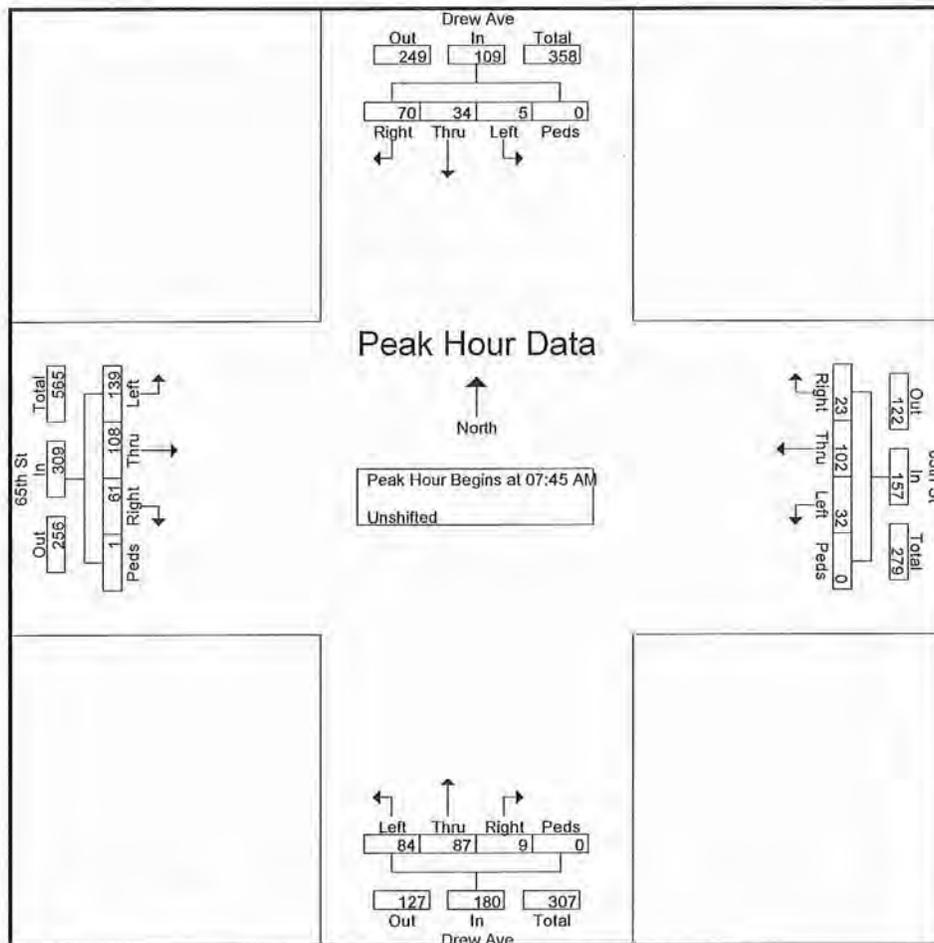
X66

WSB & Associates
701 Xenia Ave S
Minneapolis, MN

65th St & Drew Ave
6-9 am & 3-6 pm
Edina, MN
1686-38

File Name : 65th st & drew ave
Site Code : 00000006
Start Date : 4/3/2013
Page No : 2

Start Time	Drew Ave From North					65th St From East					Drew Ave From South					65th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
Peak Hour Analysis From 06:00 AM to 11:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:45 AM																					
07:45 AM	13	7	1	0	21	8	29	6	0	43	1	29	29	0	59	12	22	50	0	84	207
08:00 AM	21	11	0	0	32	3	25	13	0	41	4	24	17	0	45	14	25	21	0	60	178
08:15 AM	16	8	2	0	26	7	25	3	0	35	1	18	24	0	43	20	31	31	1	83	187
08:30 AM	20	8	2	0	30	5	23	10	0	38	3	16	14	0	33	15	30	37	0	82	183
Total Volume	70	34	5	0	109	23	102	32	0	157	9	87	84	0	180	61	108	139	1	309	755
% App. Total	64.2	31.2	4.6	0		14.6	65	20.4	0		5	48.3	46.7	0		19.7	35	45	0.3		
PHF	.833	.773	.625	.000	.852	.719	.879	.615	.000	.913	.563	.750	.724	.000	.763	.763	.871	.695	.250	.920	.912



AG7

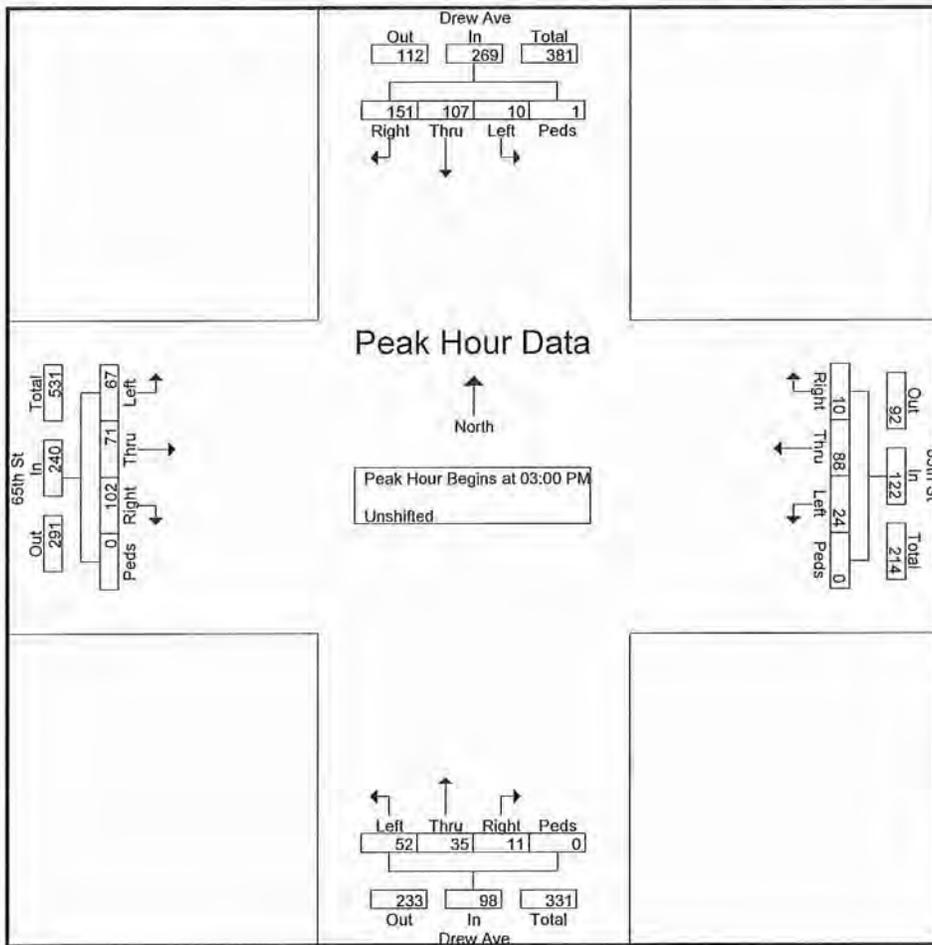
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

65th St & Drew Ave
6-9 am & 3-6 pm
Edina, MN
1686-38

File Name : 65th st & drew ave
Site Code : 00000006
Start Date : 4/3/2013
Page No : 3

Start Time	Drew Ave From North					65th St From East					Drew Ave From South					65th St From West					Int. Total
	Rig ht	Thr u	Left	Ped s	App Total	Rig ht	Thr u	Left	Ped s	App Total	Right	Thr u	Left	Peds	App. Total	Right	Thr u	Left	Peds	App. Total	
Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	34	22	4	0	60	5	24	7	0	36	3	7	12	0	22	24	11	13	0	48	166
03:15 PM	34	14	2	1	51	0	26	6	0	32	4	11	12	0	27	25	23	17	0	65	175
03:30 PM	45	44	3	0	92	2	17	8	0	27	4	10	16	0	30	25	21	22	0	68	217
03:45 PM	38	27	1	0	66	3	21	3	0	27	0	7	12	0	19	28	16	15	0	59	171
Total Volume	151	107	10	1	269	10	88	24	0	122	11	35	52	0	98	102	71	67	0	240	729
% App. Total	56.1	39.8	3.7	0.4		8.2	72.1	19.7	0		11.2	35.7	53.1	0		42.5	29.6	27.9	0		
PHF	.839	.608	.625	.250	.731	.500	.846	.750	.000	.847	.688	.795	.813	.000	.817	.911	.772	.761	.000	.882	.840



AG8

WSB & Associates

701 Xenia Ave S
Minneapolis, MN

66th St & Drew Ave
6-9 am & 3-6 pm
Edina, MN
1686-38

File Name : 66th st & drew
Site Code : 00000007
Start Date : 4/4/2013
Page No : 1

Groups Printed- Unshifted

Start Time	Drew Ave From North					66th St From East					Drew Ave From South					66th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
06:00 AM	1	1	5	0	7	7	27	2	0	36	0	0	0	0	0	3	14	1	0	18	61
06:15 AM	2	0	5	0	7	18	36	1	0	55	0	0	0	0	0	5	16	3	0	24	86
06:30 AM	0	2	5	0	7	25	47	4	0	76	0	0	0	0	0	3	26	5	0	34	117
06:45 AM	2	1	6	0	9	29	46	3	0	78	0	0	0	0	0	3	44	11	0	58	145
Total	5	4	21	0	30	79	156	10	0	245	0	0	0	0	0	14	100	20	0	134	409
07:00 AM	3	1	7	0	11	26	78	5	0	109	0	0	0	0	0	6	29	5	0	40	160
07:15 AM	7	1	6	0	14	35	117	4	0	156	0	0	0	0	0	2	53	5	0	60	230
07:30 AM	3	1	23	0	27	50	189	6	0	245	0	0	0	0	0	6	56	9	0	71	343
07:45 AM	9	2	12	0	23	50	225	12	0	287	0	0	0	0	0	8	61	13	0	82	392
Total	22	5	48	0	75	161	609	27	0	797	0	0	0	0	0	22	199	32	0	253	1125
08:00 AM	2	2	15	0	19	27	171	11	0	209	0	0	0	0	0	6	73	15	0	94	322
08:15 AM	7	0	13	0	20	57	160	7	2	226	0	0	0	0	0	8	56	9	0	73	319
08:30 AM	14	1	16	0	31	19	119	8	0	146	0	0	0	0	0	2	64	15	0	81	258
08:45 AM	6	4	20	0	30	30	99	18	0	147	0	0	0	0	0	4	72	22	0	98	275
Total	29	7	64	0	100	133	549	44	2	728	0	0	0	0	0	20	265	61	0	346	1174
03:00 PM	12	5	35	0	52	21	132	26	1	180	0	0	0	0	0	11	107	8	0	126	358
03:15 PM	19	6	40	0	65	23	162	22	0	207	0	0	0	0	0	10	84	11	0	105	377
03:30 PM	23	5	54	0	82	21	148	41	0	210	0	0	0	0	0	11	105	7	0	123	415
03:45 PM	12	7	49	0	68	11	180	21	0	212	0	0	0	0	0	12	104	10	0	126	406
Total	66	23	178	0	267	76	622	110	1	809	0	0	0	0	0	44	400	36	0	480	1556
04:00 PM	10	4	62	0	76	14	182	36	0	232	0	0	0	0	0	4	138	14	0	156	464
04:15 PM	17	2	51	0	70	8	184	24	1	217	0	0	0	0	0	6	146	9	0	161	448
04:30 PM	13	8	73	0	94	12	202	23	0	237	0	0	0	0	0	3	174	13	0	190	521
04:45 PM	9	4	47	0	60	18	200	28	0	246	2	0	0	0	2	5	173	7	0	185	493
Total	49	18	233	0	300	52	768	111	1	932	2	0	0	0	2	18	631	43	0	692	1926
05:00 PM	15	3	51	0	69	9	176	34	0	219	0	0	0	0	0	12	204	6	0	222	510
05:15 PM	10	7	40	0	57	7	179	35	0	221	0	0	0	0	0	37	176	4	3	220	498
05:30 PM	7	4	27	0	38	6	174	27	0	207	0	0	0	0	0	11	185	3	0	199	444
05:45 PM	8	5	31	0	44	5	165	24	0	194	0	0	5	0	5	12	155	5	0	172	415
Total	40	19	149	0	208	27	694	120	0	841	0	0	5	0	5	72	720	18	3	813	1867
Grand Total	211	76	693	0	980	528	3398	422	4	4352	2	0	5	0	7	190	2315	210	3	2718	8057
Apprch %	21.5	7.8	70.7	0		12.1	78.1	9.7	0.1		28.6	0	71.4	0		7	85.2	7.7	0.1		
Total %	2.6	0.9	8.6	0	12.2	6.6	42.2	5.2	0	54	0	0	0.1	0	0.1	2.4	28.7	2.6	0	33.7	

AG9

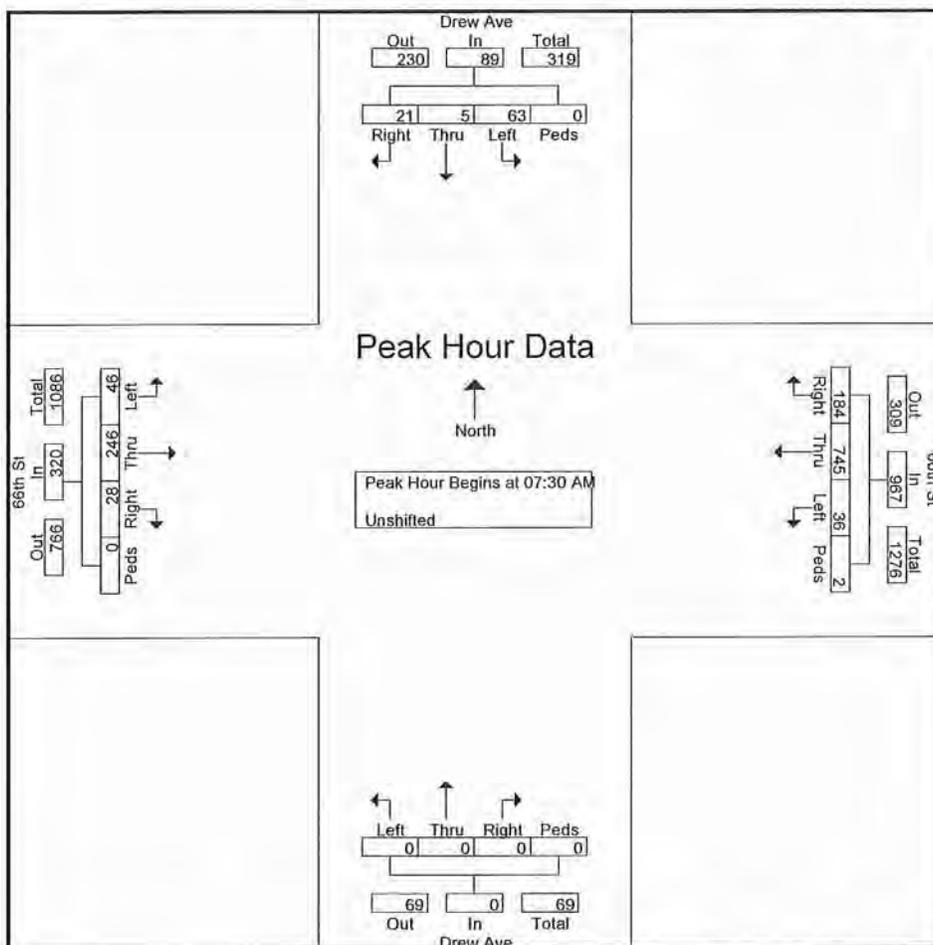
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

66th St & Drew Ave
6-9 am & 3-6 pm
Edina, MN
1686-38

File Name : 66th st & drew
Site Code : 00000007
Start Date : 4/4/2013
Page No : 2

Start Time	Drew Ave From North					66th St From East					Drew Ave From South					66th St From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
Peak Hour Analysis From 06:00 AM to 11:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:30 AM																					
07:30 AM	3	1	23	0	27	50	189	6	0	245	0	0	0	0	0	6	56	9	0	71	343
07:45 AM	9	2	12	0	23	50	225	12	0	287	0	0	0	0	0	8	61	13	0	82	392
08:00 AM	2	2	15	0	19	27	171	11	0	209	0	0	0	0	0	6	73	15	0	94	322
08:15 AM	7	0	13	0	20	57	160	7	2	226	0	0	0	0	0	8	56	9	0	73	319
Total Volume	21	5	63	0	89	184	745	36	2	967	0	0	0	0	0	28	246	46	0	320	1376
% App. Total	23.6	5.6	70.8	0		19	77	3.7	0.2		0	0	0	0		8.8	76.9	14.4	0		
PHF	.583	.625	.685	.000	.824	.807	.828	.750	.250	.842	.000	.000	.000	.000	.000	.875	.842	.767	.000	.851	.878



A70

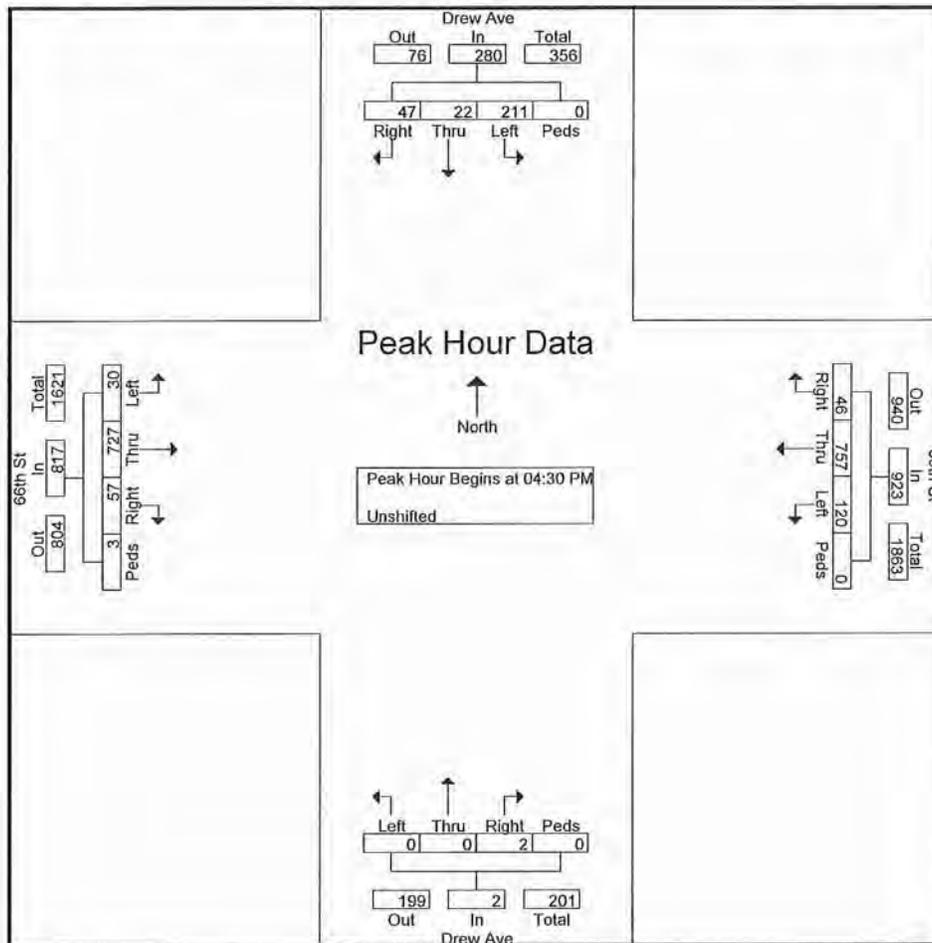
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

66th St & Drew Ave
6-9 am & 3-6 pm
Edina, MN
1686-38

File Name : 66th st & drew
Site Code : 00000007
Start Date : 4/4/2013
Page No : 3

Start Time	Drew Ave From North					66th St From East					Drew Ave From South					66th St From West					Inl. Total
	Rig ht	Thr u	Left	Ped s	App Total	Rig ht	Thr u	Left	Ped s	App Total	Right	Thr u	Left	Peds	App. Total	Right	Thr u	Left	Peds	App. Total	
Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	13	8	73	0	94	12	202	23	0	237	0	0	0	0	0	3	174	13	0	190	521
04:45 PM	9	4	47	0	60	18	200	28	0	246	2	0	0	0	2	5	173	7	0	185	493
05:00 PM	15	3	51	0	69	9	176	34	0	219	0	0	0	0	0	12	204	6	0	222	510
05:15 PM	10	7	40	0	57	7	179	35	0	221	0	0	0	0	0	37	176	4	3	220	498
Total Volume	47	22	211	0	280	46	757	120	0	923	2	0	0	0	2	57	727	30	3	817	2022
% App. Total	16.8	7.9	75.4	0		5	82	13	0		100	0	0	0		7	89	3.7	0.4		
PHF	.783	.688	.723	.000	.745	.639	.937	.857	.000	.938	.250	.000	.000	.000	.250	.385	.891	.577	.250	.920	.970



A-71

WSB & Associates

701 Xenia Ave S
Minneapolis, MN

France Ave & 66th St
6-9, 11-1, 3-6
Edina, MN

File Name : france & 66th
Site Code : 00000104
Start Date : 1/10/2007
Page No : 1

Groups Printed- Unshifted

Start Time	France Ave From North					66th St W From East					France Ave From South					66th St W From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
06:00 AM	0	35	8	0	43	9	3	6	0	18	3	21	2	0	26	3	12	4	0	19	106
06:15 AM	3	35	7	0	45	11	10	6	0	27	11	34	1	0	46	0	10	11	0	21	139
06:30 AM	6	58	12	0	76	20	23	14	0	57	12	59	2	0	73	5	22	8	0	35	241
06:45 AM	2	60	13	0	75	21	30	13	0	64	22	47	1	0	70	7	34	10	0	51	260
Total	11	188	40	0	239	61	66	39	0	166	48	161	6	0	215	15	78	33	0	126	746
07:00 AM	7	80	20	0	107	22	36	24	0	82	12	62	2	0	76	4	26	7	0	37	302
07:15 AM	12	105	17	0	134	26	51	28	0	105	13	66	2	0	81	3	44	16	0	63	383
07:30 AM	8	142	21	0	171	47	73	52	0	172	16	93	3	0	112	8	48	20	0	76	531
07:45 AM	8	161	19	0	188	35	81	62	0	178	17	66	4	0	87	10	50	10	0	70	523
Total	35	488	77	0	600	130	241	166	0	537	58	287	11	0	356	25	168	53	0	246	1739
08:00 AM	13	161	32	0	206	38	69	60	0	167	19	76	4	0	99	14	42	11	0	67	539
08:15 AM	9	130	25	0	164	30	47	46	0	123	20	99	8	0	127	7	57	21	0	85	499
08:30 AM	8	155	35	0	198	35	38	48	0	121	26	102	4	0	132	7	51	19	0	77	528
*** BREAK ***																					
Total	30	446	92	0	568	103	154	154	0	411	65	277	16	0	358	28	150	51	0	229	1566
*** BREAK ***																					
11:00 AM	10	192	45	0	247	54	43	70	0	167	40	138	24	0	202	26	41	12	0	79	695
11:15 AM	15	182	43	0	240	58	30	55	0	143	38	157	21	0	216	33	55	20	0	108	707
11:30 AM	12	165	46	0	223	64	52	71	0	187	62	150	30	0	242	29	60	8	0	97	749
11:45 AM	7	175	49	0	231	44	59	66	0	169	59	171	27	0	257	33	67	11	0	111	768
Total	44	714	183	0	941	220	184	262	0	666	199	616	102	0	917	121	223	51	0	395	2919
12:00 PM	10	177	65	0	252	60	55	76	0	191	53	158	43	0	254	31	68	14	0	113	810
12:15 PM	15	176	54	0	245	64	65	82	0	211	62	218	28	0	308	30	60	14	0	104	868
12:30 PM	12	159	51	0	222	60	59	73	0	192	47	183	33	0	263	21	67	18	0	106	783
12:45 PM	8	208	45	0	261	68	52	80	0	200	47	191	33	0	271	33	65	11	0	109	841
Total	45	720	215	0	980	252	231	311	0	794	209	750	137	0	1096	115	260	57	0	432	3302
*** BREAK ***																					
03:00 PM	9	134	62	0	205	44	68	19	0	131	59	186	72	0	317	6	59	18	0	83	736
03:15 PM	13	161	78	0	252	43	64	29	0	136	60	208	58	0	326	12	46	17	0	75	789
03:30 PM	10	141	64	0	215	52	82	56	0	190	55	187	69	0	311	23	67	7	0	97	813
03:45 PM	21	153	43	0	217	70	58	70	0	198	50	189	38	0	277	21	78	13	0	112	804
Total	53	589	247	0	889	209	272	174	0	655	224	770	237	0	1231	62	250	55	0	367	3142
04:00 PM	18	156	72	0	246	31	88	66	0	185	49	163	24	0	236	25	69	19	0	113	780
04:15 PM	8	192	63	0	263	61	60	46	0	167	61	189	32	0	282	14	66	9	0	89	801
04:30 PM	10	149	61	0	220	51	75	59	0	185	76	185	34	0	295	24	77	14	0	115	815
04:45 PM	11	147	46	0	204	64	77	64	0	205	64	163	28	0	255	20	80	15	0	115	779
Total	47	644	242	0	933	207	300	235	0	742	250	700	118	0	1068	83	292	57	0	432	3175
05:00 PM	18	152	69	0	239	68	88	63	0	219	84	228	46	0	358	17	104	23	0	144	960
05:15 PM	17	143	74	0	234	63	69	38	0	170	55	230	45	0	330	24	93	13	0	130	864
05:30 PM	10	167	67	0	244	52	76	53	0	181	51	180	23	0	254	30	87	6	0	123	802
05:45 PM	19	143	82	0	244	58	51	35	0	144	47	190	38	0	275	21	95	7	0	123	786
Total	64	605	292	0	961	241	284	189	0	714	237	828	152	0	1217	92	379	49	0	520	3412
Grand Total	329	4394	1388	0	6111	1423	1732	1530	0	4685	1290	4389	779	0	6458	541	1800	406	0	2747	20001
Apprch %	5.4	71.9	22.7	0		30.4	37	32.7	0		20	68	12.1	0		19.7	65.5	14.8	0		
Total %	1.6	22	6.9	0	30.6	7.1	8.7	7.6	0	23.4	6.4	21.9	3.9	0	32.3	2.7	9	2	0	13.7	

A72

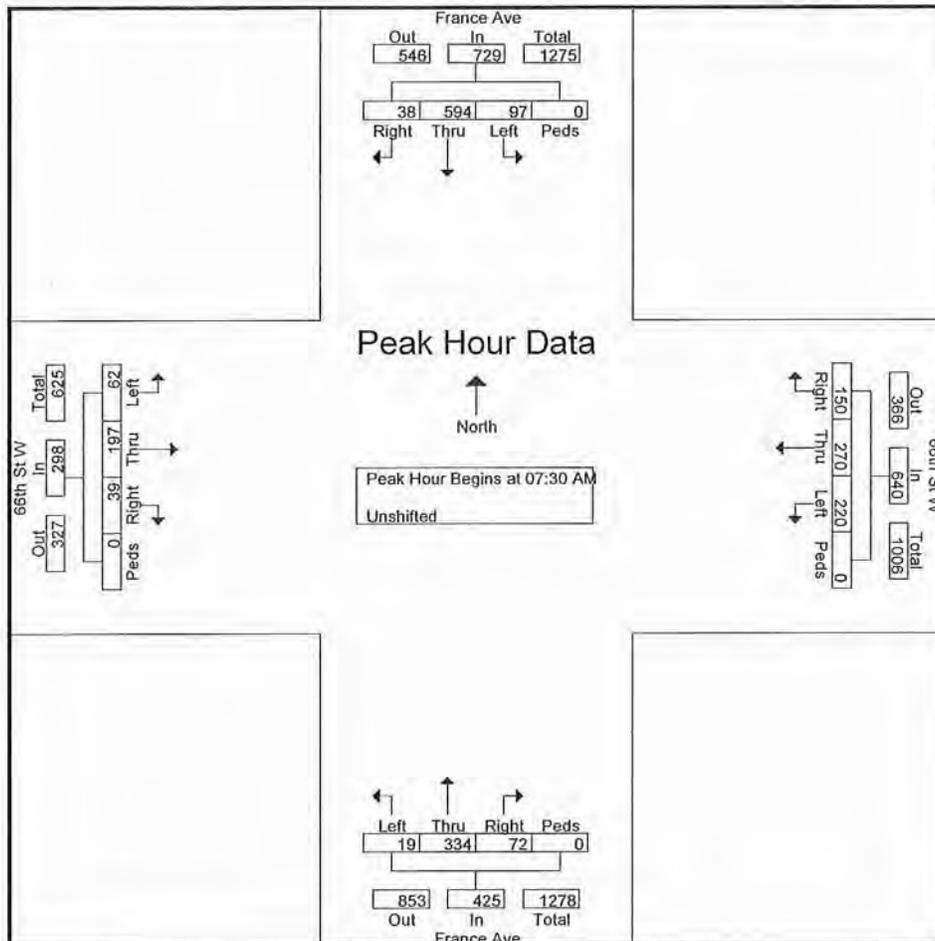
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

France Ave & 66th St
6-9, 11-1, 3-6
Edina, MN

File Name : france & 66th
Site Code : 00000104
Start Date : 1/10/2007
Page No : 2

Start Time	France Ave From North					66th St W From East					France Ave From South					66th St W From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
Peak Hour Analysis From 06:00 AM to 09:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:30 AM																					
07:30 AM	8	142	21	0	171	47	73	52	0	172	16	93	3	0	112	8	48	20	0	76	531
07:45 AM	8	161	19	0	188	35	81	62	0	178	17	66	4	0	87	10	50	10	0	70	523
08:00 AM	13	161	32	0	206	38	69	60	0	167	19	76	4	0	99	14	42	11	0	67	539
08:15 AM	9	130	25	0	164	30	47	46	0	123	20	99	8	0	127	7	57	21	0	85	499
Total Volume	38	594	97	0	729	150	270	220	0	640	72	334	19	0	425	39	197	62	0	298	2092
% App. Total	5.2	81.5	13.3	0		23.4	42.2	34.4	0		16.9	78.6	4.5	0		13.1	66.1	20.8	0		
PHF	.731	.922	.758	.000	.885	.798	.833	.887	.000	.899	.900	.843	.594	.000	.837	.696	.864	.738	.000	.876	.970



A73

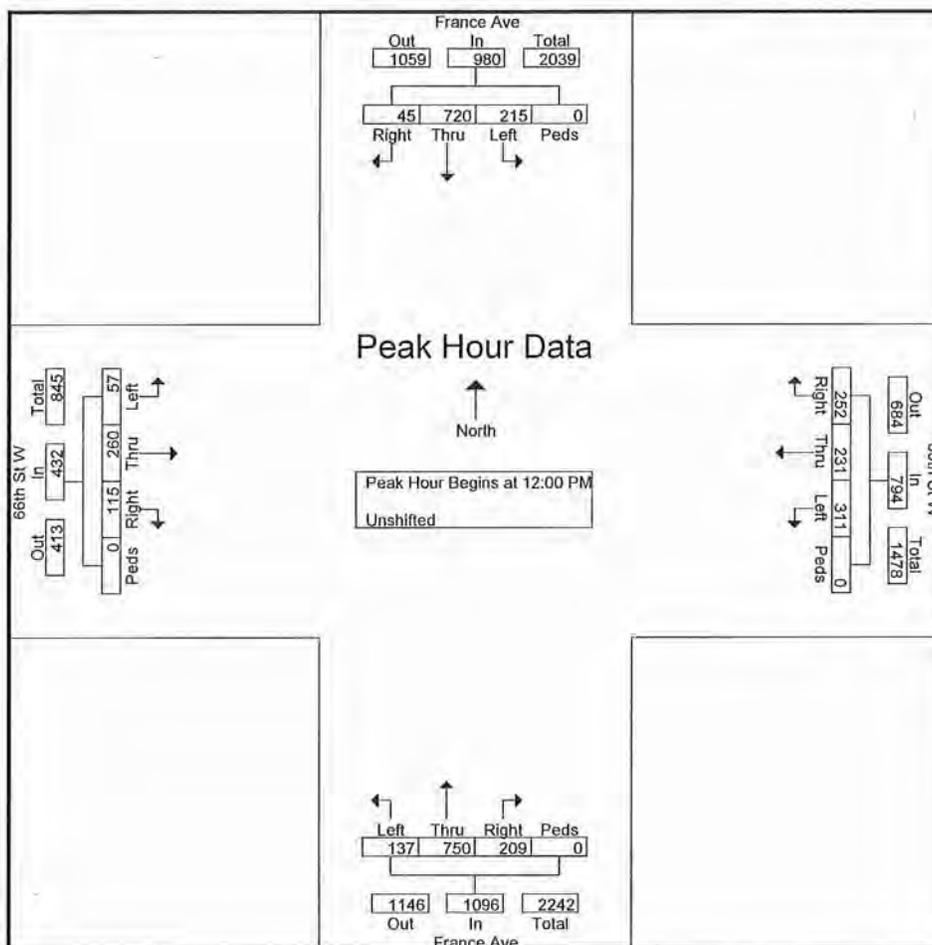
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

France Ave & 66th St
6-9, 11-1, 3-6
Edina, MN

File Name : france & 66th
Site Code : 00000104
Start Date : 1/10/2007
Page No : 3

Start Time	France Ave From North					66th St W From East					France Ave From South					66th St W From West					Int. Total
	Rig ht	Thr u	Left	Ped s	App Total	Rig ht	Thr u	Left	Ped s	App Total	Right	Thr u	Left	Peds	App Total	Right	Thr u	Left	Peds	App Total	
Peak Hour Analysis From 10:00 AM to 01:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 12:00 PM																					
12:00 PM	10	177	65	0	252	60	55	76	0	191	53	158	43	0	254	31	68	14	0	113	810
12:15 PM	15	176	54	0	245	64	65	82	0	211	62	218	28	0	308	30	60	14	0	104	868
12:30 PM	12	159	51	0	222	60	59	73	0	192	47	183	33	0	263	21	67	18	0	106	783
12:45 PM	8	208	45	0	261	68	52	80	0	200	47	191	33	0	271	33	65	11	0	109	841
Total Volume	45	720	215	0	980	252	231	311	0	794	209	750	137	0	1096	115	260	57	0	432	3302
% App. Total	4.6	73.5	21.9	0		31.7	29.1	39.2	0		19.1	68.4	12.5	0		26.6	60.2	13.2	0		
PHF	.750	.865	.827	.000	.939	.926	.888	.948	.000	.941	.843	.860	.797	.000	.890	.871	.956	.792	.000	.956	.951



A74

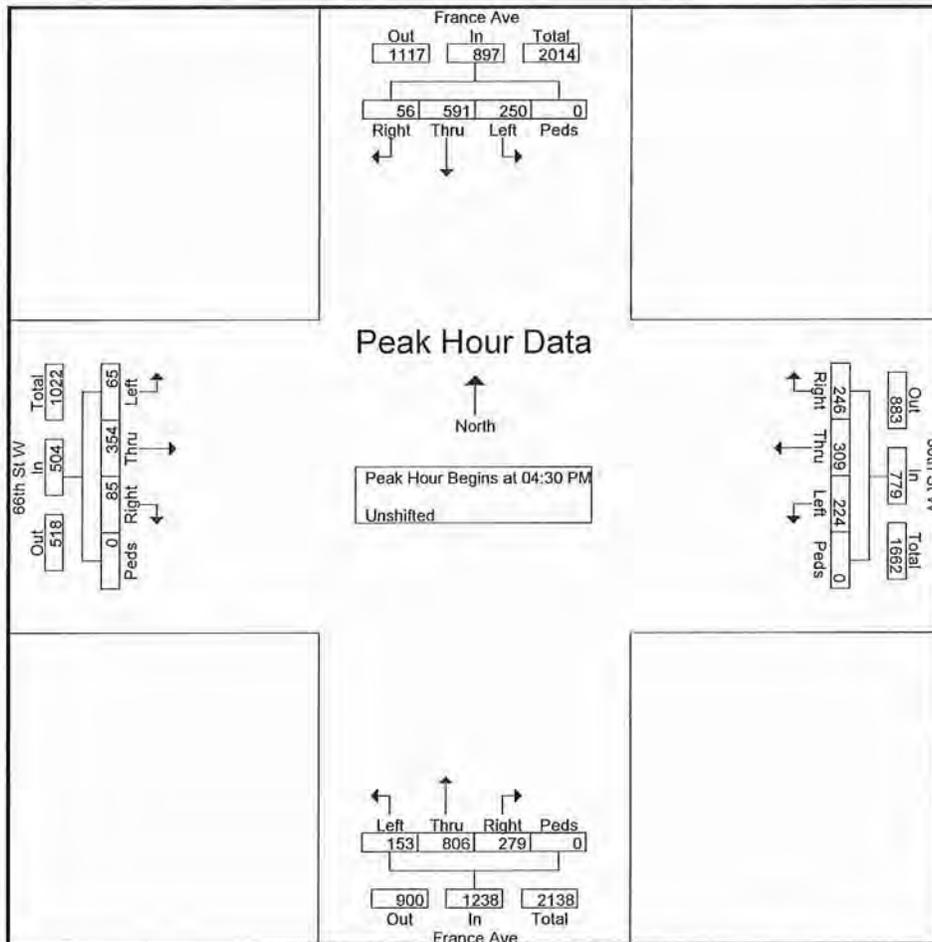
WSB & Associates

701 Xenia Ave S
Minneapolis, MN

France Ave & 66th St
6-9, 11-1, 3-6
Edina, MN

File Name : france & 66th
Site Code : 00000104
Start Date : 1/10/2007
Page No : 4

Start Time	France Ave From North					66th St W From East					France Ave From South					66th St W From West					Int. Total
	Rig ht	Thr u	Left	Ped s	App Total	Rig ht	Thr u	Left	Ped s	App Total	Right	Thr u	Left	Peds	App Total	Right	Thr u	Left	Peds	App Total	
Peak Hour Analysis From 02:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	10	149	61	0	220	51	75	59	0	185	76	185	34	0	295	24	77	14	0	115	815
04:45 PM	11	147	46	0	204	64	77	64	0	205	64	163	28	0	255	20	80	15	0	115	779
05:00 PM	18	152	69	0	239	68	88	63	0	219	84	228	46	0	358	17	104	23	0	144	960
05:15 PM	17	143	74	0	234	63	69	38	0	170	55	230	45	0	330	24	93	13	0	130	864
Total Volume	56	591	250	0	897	246	309	224	0	779	279	806	153	0	1238	85	354	65	0	504	3418
% App. Total	6.2	65.9	27.9	0		31.6	39.7	28.8	0		22.5	65.1	12.4	0		16.9	70.2	12.9	0		
PHF	.778	.972	.845	.000	.938	.904	.878	.875	.000	.889	.830	.876	.832	.000	.865	.885	.851	.707	.000	.875	.890



A75

SanTraffic Simulation Summary
 4/18/2013
 Existing PM Peak Hour Conditions
 Measures of Effectiveness

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Control	Intersection		Appr	Forecast Volumes				Modeled Volumes				Error Volumes				Volume Error by Approach		Appr	Total Delay by Movement (Sec/Veh)			Level of Service by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queuing (feet)									
	Location	L		T	R	Total	L	T	R	Total	L	T	R	Total	%	L	T		R	L	T	R	Delay	LOS	Delay	LOS	Left-Turn		Through		Right-Turn							
																											Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Ave Queue	Max Queue	Storage				
Signalized	1: 65th Street & France Ave	NB	44	1238	74	1356	40	1237	78	1355	-4	-1	4	-1	0%	NB	63.1	21.8	20.4	E	C	C	22.9	C	21.5	C	NB	35	170	200	164	366						
		WB	51	96	376	523	47	94	370	511	-4	-2	-6	-12	-2%	WB	42.9	43	8.1	D	D	A	17.7	B	21.5	C	WB				127	210						
		SB	87	736	27	850	81	712	28	821	-6	-24	1	-29	-3%	SB	55.2	132	9.8	E	B	A	17.2	B	21.5	C	SB	59	162	165	83	193						
		EB	131	93	108	332	129	92	107	328	-2	-1	-1	-4	-1%	EB	41.5	34.8	18.2	D	C	B	32.0	C	21.5	C	EB	89	154	138	111	297						
Thru-Stop	2: 65th Street & Southdale Hospital Entrance	NB	130	1	40	171	119	2	45	169	-11	1	5	-5	-3%	NB	19	5.4	10.7	C	A	B	18.6	C	8.2	A	NB					65	195					
		WB	12	390	15	377	10	349	12	371	-2	-1	-3	-6	-2%	WB	4.1	2.9	0.7	A	A	A	2.9	A	8.2	A	WB				26	153						
		SB	2	0	43	45	1	0	45	46	-1	0	2	1	2%	SB	10.1	0	11.1	B	A	B	11.1	B	8.2	A	SB	26	78									
		EB	24	148	82	254	23	146	83	252	-1	-2	1	-2	-1%	EB	5.7	3.4	2.3	A	A	A	3.2	A	8.2	A	EB				11	67						
All-Way Stop	3: 65th Street & Drew Ave	NB	49	22	12	83	43	21	11	75	-6	-1	-1	-8	-10%	NB	5.7	6	3.2	A	A	A	5.4	A	5.6	A	NB	26	51	78	21	42						
		WB	28	103	8	139	30	105	8	144	2	3	0	5	4%	WB	6	7.3	3.9	A	A	A	6.8	A	5.6	A	WB				42	76						
		SB	11	102	143	256	11	103	137	251	0	1	-6	-5	-2%	SB	5	6.7	4	A	A	A	5.2	A	5.6	A	SB				36	77	40	72	75			
		EB	45	62	100	207	45	66	103	214	0	4	3	7	3%	EB	5.7	7.2	3.7	A	A	A	5.2	A	5.6	A	EB				40	90						
Signalized	4: 68th Street & France Ave	NB	153	978	279	1410	156	974	271	1401	3	-4	-8	-9	-1%	NB	43.8	25.9	4.3	D	C	A	23.7	C	26.1	C	NB	107	238	250	144	267	25	199	250			
		WB	224	399	299	832	223	315	301	840	-1	7	2	8	1%	WB	54.7	31.9	4	D	C	A	28.0	C	26.1	C	WB	65	163	315	78	179	19	132				
		SB	250	591	56	697	245	567	57	669	-5	-24	1	-28	-3%	SB	42.4	19.9	17	D	B	B	26.1	C	26.1	C	SB	152	244	250	84	264						
		EB	79	354	85	518	74	358	83	515	-5	4	-2	-3	-1%	EB	40	33.3	3.8	D	C	A	29.5	C	26.1	C	EB	57	149	150	102	175	12	79	135			
Signalized	5: 68th Street & Drew Ave	NB	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	24.8	C	NB												
		WB	120	757	46	923	114	754	43	911	-6	-3	-3	-12	-1%	WB	39.4	19.4	14.5	D	B	B	21.7	C	24.8	C	WB	68	181	200	112	202						
		SB	211	22	47	280	214	22	51	287	3	0	4	7	3%	SB	18.6	15	5.2	B	B	A	15.9	B	24.8	C	SB	63	125	150	43	216						
		EB	30	727	57	814	26	719	59	804	-4	-8	2	-10	-1%	EB	38.8	33.4	3.8	D	C	A	31.4	C	24.8	C	EB	26	151	220	189	267	24	53				
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB											
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A	0.0	A	WB											
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A	0.0	A	SB											
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A	0.0	A	EB											
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB											
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A	0.0	A	WB											
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A	0.0	A	SB											
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A	0.0	A	EB											
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB											
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A	0.0	A	WB											
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A	0.0	A	SB											
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A	0.0	A	EB											

A77

SimTraffic Simulation Summary
 5/14/2013
 2014 No Build PM Peak Hour
 Measures of Effectiveness

K:\01686-3801Traffic\Synchro\2014 NB MOEs

Control	Intersection		Forecast Volumes												Modeled Volumes				Error Volumes				Volume Error by Approach		Total Delay by Movement (Sec/Veh)			Level of Service by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queuing (feet)															
	Location	Appr	L				T				R				Total				Total				%				L			T			R			Delay		LOS		Delay	LOS	Left-Turn			Through			Right-Turn		
			L	T	R	Total	L	T	R	Total	L	T	R	Total	L	T	R	Total	L	T	R	Total	L	T	R	L	T	R	L	T	R	L	T	R	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue			Storage	Ave Queue	Max Queue	Storage					
Signalized	1: 65th Street & France Ave	NB	45	1262	76	1383	43	1254	77	1374	-2	-8	1	-9	-1%	NB	68	239	238	E	C	C	25.3	C	22.8	C	NB	37	131	200	190	412	7.5	A	NB				134	222	49	67	35							
		WB	52	98	384	534	50	96	393	539	-2	-2	9	5	1%	WB	44.8	40.6	6.6	D	D	A	17.7	B			WB				165	93			150															
		SB	89	751	28	868	94	757	28	879	5	6	0	11	1%	SB	57.7	14.4	11.1	E	B	B	18.9	B			SB	70	164	165	117	304																		
		EB	134	95	119	338	124	97	112	333	-10	2	2	-8	-2%	EB	39.3	34.2	20.7	D	C	C	31.6	C			EB	89	154	126	75	171																		
Thru-Stop	2: 65th Street & Southdale Hospital Entrance	NB	133	1	41	175	137	2	40	179	4	1	-1	4	2%	NB	22.9	24	16.9	C	C	C	21.6	C	7.5	A	NB				25	149																		
		WB	12	358	15	385	9	350	15	384	-3	2	0	-1	0%	WB	5.8	2.9	3	A	A	A	3.0	A			WB				7	67																		
		SB	2	0	43	45	2	0	44	46	0	0	1	1	2%	SB	11.4	0	15.8	B	A	C	15.6	C			SB	29	78																					
		EB	24	152	84	260	22	155	82	259	-2	3	8	9	3%	EB	5.1	3.3	2.2	A	A	A	3.1	A			EB				21	46																		
All-Way Stop	3: 66th Street & Drew Ave	NB	50	22	12	84	53	20	11	84	3	-2	-1	0	0%	NB	5.6	6.1	2.9	A	A	A	5.4	A	5.6	A	NB	28	60	75	21	46																		
		WB	28	105	8	141	26	100	10	136	-2	-5	2	-5	-4%	WB	5.7	7.3	3.2	A	A	A	6.7	A			WB				42	93																		
		SB	11	102	143	256	10	99	138	246	-1	-4	-5	-10	-4%	SB	5.7	6.7	4	A	A	A	5.1	A			SB				36	71	41	72	75															
		EB	45	63	102	210	44	55	110	209	-1	-6	6	-1	0%	EB	5.7	7.1	3.6	A	A	A	5.0	A			EB				39	71																		
Signalized	4: 66th Street & France Ave	NB	150	699	285	1439	162	694	285	1431	8	-14	0	-8	-1%	NB	41.6	26.7	4.6	D	C	A	23.3	C	26.3	C	NB	110	225	250	143	227	22	197	250															
		WB	229	315	285	829	234	318	292	844	5	3	7	15	2%	WB	54.8	38.9	4.3	D	D	A	30.6	C			WB	92	174	315	88	158	24	149																
		SB	255	603	87	945	252	607	61	920	-3	4	4	5	1%	SB	41.8	19.6	17.6	D	B	B	25.5	C			SB	156	244	250	85	245																		
		EB	81	361	87	529	77	341	88	506	-4	-20	1	-23	-4%	EB	39.9	33.5	3.4	D	C	A	29.2	C			EB	58	136	150	87	175	9	133	135															
Signalized	5: 66th Street & Drew Ave	NB	0	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	24.8	C	NB																							
		WB	122	772	47	941	120	765	48	953	-2	13	1	12	1%	WB	40.6	18.6	12.6	D	B	B	21.1	C			WB	78	192	200	111	244																		
		SB	225	72	49	296	222	24	50	296	-3	2	1	0	0%	SB	20.7	16.7	5.8	C	B	A	17.9	B			SB	88	125	150	55	230																		
		EB	31	742	58	831	28	723	58	807	-3	-19	-2	-24	-3%	EB	39.2	32.3	3.5	D	C	A	39.5	C			EB	25	94	220	185	280	25	77																
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																							
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB																							
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB																							
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB																							
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																							
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB																							
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB																							
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB																							
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																							
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB																							
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB																							
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB																							

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SynTraffic Simulation Summary
 5/14/2013
 2014 Build AM Peak Hour
 Measures of Effectiveness
 C:\GIS\2014TrafficSynchro\2014 Build\MOEs\MOE1

Control	Location	Appr	Forecast Volumes				Modeled Volumes				Error Volumes				Volume Error by Approach		Appr	Total Delay by Movement (Sec/Veh)			Level of Service by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queuing (feet)								
			L	T	R	Total	L	T	R	Total	L	T	R	Total	%	L		T	R	L	T	R	Delay	LOS	Delay	LOS	Left-Turn			Through		Right-Turn				
																											Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Ave Queue	Max Queue	Storage		
Signalized	1: 65th Street & France Ave	NB	73	538	161	772	69	548	158	775	-4	10	-3	3	0%	NB	39.7	23.7	11.3	D	C	B	22.6	C	20.5	C	NB	45	100	200	62	142				
		WB	56	44	187	287	60	43	185	288	4	-1	-2	1	0%	WB	40.9	33.6	4.1	D	C	A	16.2	B			WB				70	171	46	58	23	
		SB	284	699	95	1078	285	697	95	1078	2	-2	-1	-1	0%	SB	37.1	11.7	9.8	D	B	A	18.3	B			SB	145	189	148	94	353				
		EB	75	200	38	313	69	194	41	307	-8	-8	6	-6	-2%	EB	28.5	28.5	19.3	C	C	B	27.2	C			EB	52	154	148	125	271				
Thru-Stop	2: 65th Street & Southdale Hospital Entrance	NB	62	0	9	71	60	0	8	68	-2	0	-1	-3	-4%	NB	14.3	0	6.7	B	A	A	13.4	B	4.7	A	NB	35	66							
		WB	11	106	18	225	11	204	19	234	0	8	1	9	4%	WB	6.3	0.9	0.6	A	A	A	1.1	A			WB				8	79				
		SB	6	2	29	36	4	1	26	31	-1	-1	-3	-5	-14%	SB	14.6	25	4.4	B	C	A	6.4	A			SB				22	54				
		EB	68	363	212	643	73	391	202	636	5	-2	-10	-7	-1%	EB	5.2	5.2	3.3	A	A	A	4.6	A			EB				23	107				
All-Way Stop	3: 65th Street & Drew Ave	NB	85	95	9	190	88	89	8	185	2	-6	-1	-5	-3%	NB	6.7	7.7	4.5	A	A	A	7.1	A	7.1	A	NB	37	71	75						
		WB	33	104	23	160	33	113	26	172	0	9	3	12	8%	WB	6	7.9	4.8	A	A	A	7.1	A			WB				46	87				
		SB	5	39	75	119	5	37	77	119	0	-2	2	0	0%	SB	5.6	6.7	3.8	A	A	A	4.8	A			SB				23	49	32	62	75	
		EB	148	112	62	322	143	111	61	315	-5	-1	-1	-7	-2%	EB	7.7	9.2	5.9	A	A	A	7.9	A			EB				63	128				
Signalized	4: 66th Street & France Ave	NB	20	520	88	628	17	518	82	617	-3	-2	-6	-11	-2%	NB	45.9	15.5	2.2	D	B	A	14.6	B	23.1	C	NB	18	59	250	60	122	1	27	250	
		WB	249	267	157	673	249	265	157	671	0	-2	0	-2	0%	WB	55	41	2.8	D	D	A	37.3	D			WB	95	165	315	80	164	9	50		
		SB	113	622	41	776	108	652	41	801	-5	30	0	25	3%	SB	49.3	7.3	6.5	D	A	A	12.9	B			SB	82	166	250	29	101				
		EB	95	204	39	338	99	201	43	343	-4	-3	4	5	1%	EB	38.0	32.4	2.6	D	C	A	30.5	C			EB	69	147	150	91	139	2	53	135	
Signalized	5: 66th Street & Drew Ave	NB	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	24.8	C	NB										
		WB	30	589	159	897	36	683	160	879	-3	-6	1	-6	-1%	WB	44.3	23.9	14.5	D	C	B	23.0			C	WB	27	124	209	122	241				
		SB	59	5	36	100	54	6	37	97	-5	1	1	-3	-3%	SB	15.8	7.7	3.2	B	A	A	10.5			B	SB	26	75	150	16	54				
		EB	57	259	25	341	50	253	28	328	-7	-6	1	-12	-4%	EB	39.1	29.7	2.9	D	C	A	29.0			C	EB	42	107	220	65	149	16	46		
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB									
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB									
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB									
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB									
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB									
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB									
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB									
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB									
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB									
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB									
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB									
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB									

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SINTRAFFIC Simulation Summary
 5/15/2013
 3:00 PM - 3:15 PM Peak Hour
 Measures of Effectiveness

Key: Yellow=Level of Control is Adequate
 Green=Level of Control is Excellent

Intersection	Location	Forecast Volumes			Modelled Volumes			Error Volumes			Volume Error by Approach		Total Delay by Movement (sec/Veh)			Level of Service by Movement			LOS by Approach (sec/Veh)		LOS by Intersection (sec/Veh)		Average & Maximum Traffic Queuing (feet)															
		L	T	R	Total	L	T	R	Total	L	T	R	Total	%	L	T	R	L	T	R	Delay	LOS	Delay	LOS	Ave	Max	Ave	Max	Ave	Max	Ave	Max						
Control	L 65th Street & France Ave	NB	45	1302	84	1391	45	1292	83	1390	0	-10	-1	-11	-1%	NB	45.4	42.2	8.2	D	C	C	25.6	C	NB	39	172	200	138	195	428	49	65	83				
		WB	67	98	388	543	64	90	382	529	-3	-5	-6	-14	-4%	WB	48.4	42.1	8.2	D	B	B	18.7	B	WB	65	173	118	90	182	206	49	65	83				
		SB	92	751	78	871	89	743	31	853	-3	-8	3	-8	-1%	SB	57.5	14.6	1.2	E	B	B	18.8	B	SB	60	134	138	121	308	167	28	197					
		EB	131	95	110	339	134	89	113	348	0	4	3	7	2%	EB	41.7	33.4	16.4	D	C	B	31.5	C	EB	30	77											
Thru-Stop	7- 65th Street & Southside Hospital Entrance	NB	13	1	41	175	131	1	42	174	-2	0	1	-1	-1%	NB	29.1	29.7	2.7	F	A	A	3.7	A	NB	30	77											
		WB	12	303	15	330	0	353	18	378	-3	-10	-1	-12	-3%	WB	5.3	3.7	0	D	A	A	1.9	B	WB													
		SB	2	0	47	48	2	0	44	46	0	0	-3	-3	-5%	SB	10.8	0	13	B	A	B	12.9	B	SB													
		EB	26	197	84	287	26	197	87	289	-1	0	3	2	1%	EB	5.5	3.2	2.5	A	A	A	3.2	A	EB													
All-Way Stop	3- 65th Street & Drew Ave	NB	50	28	12	88	49	28	14	89	-1	0	2	1	1%	NB	5.7	6.5	3	A	A	A	5.5	A	NB	27	60	78	24	62	41	76						
		WB	28	105	8	141	28	105	9	143	0	1	1	2	1%	WB	5.8	7.1	3.8	A	A	A	6.0	A	WB													
		SB	11	107	148	268	8	109	139	256	-3	2	-9	-10	-4%	SB	5.5	6.7	4.1	A	A	A	5.3	A	SB													
		EB	50	83	102	235	48	58	107	213	-2	-5	5	-2	-1%	EB	7.2	3.8	3	A	A	A	5.2	A	EB													
Signalized	4- 65th Street & France Ave	NB	156	1000	289	1445	133	998	226	1427	-3	-12	-3	-18	-1%	NB	41.3	28.4	4.6	D	C	A	23.6	C	NB	104	222	250	148	247	29	215	220					
		WB	232	315	269	827	216	315	292	823	-16	0	7	-9	-1%	WB	53.6	38.6	4.3	D	D	A	29.6	C	WB	84	148	318	85	183	20	140						
		SB	255	695	59	920	208	598	62	874	11	-20	3	-6	-1%	SB	42.7	20.9	18.5	D	C	B	26.9	C	SB	163	244	280	92	280								
		EB	83	351	87	521	60	352	89	528	-3	1	-1	-3	-1%	EB	39.4	33.7	3.4	D	C	A	29.6	C	EB	62	138	150	106	206	6	108	135					
Signalized	6- 65th Street & Drew Ave	NB	0	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	NB	77	174	200	105	202								
		WB	122	772	40	933	122	783	50	935	0	-9	1	-8	-1%	WB	39.5	18.4	12.2	D	B	B	20.8	C	WB	95	125	150	86	236								
		SB	227	22	52	301	230	23	57	310	3	1	5	0	3%	SB	21.6	16.5	5.5	C	B	A	18.3	B	SB	33	101	220	160	287	26	122						
		EB	33	742	58	833	34	749	66	843	1	7	2	10	1%	EB	38.5	31.6	3.3	D	C	A	29.9	C	EB													
Thru-Stop	RtVA	NB	0	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	NB													
		WB	0	0	0	0	0	0	0	0	0	0	0	0	0%	WB	0	0	0	A	A	A	0.0	A	WB													
		SB	0	0	0	0	0	0	0	0	0	0	0	0	0%	SB	0	0	0	A	A	A	0.0	A	SB													
		EB	0	0	0	0	0	0	0	0	0	0	0	0	0%	EB	0	0	0	A	A	A	0.0	A	EB													
Thru-Stop	RtVA	NB	0	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	NB													
		WB	0	0	0	0	0	0	0	0	0	0	0	0	0%	WB	0	0	0	A	A	A	0.0	A	WB													
		SB	0	0	0	0	0	0	0	0	0	0	0	0	0%	SB	0	0	0	A	A	A	0.0	A	SB													
		EB	0	0	0	0	0	0	0	0	0	0	0	0	0%	EB	0	0	0	A	A	A	0.0	A	EB													
Thru-Stop	RtVA	NB	0	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	NB													
		WB	0	0	0	0	0	0	0	0	0	0	0	0	0%	WB	0	0	0	A	A	A	0.0	A	WB													
		SB	0	0	0	0	0	0	0	0	0	0	0	0	0%	SB	0	0	0	A	A	A	0.0	A	SB													
		EB	0	0	0	0	0	0	0	0	0	0	0	0	0%	EB	0	0	0	A	A	A	0.0	A	EB													

AS1

SimTraffic Simulation Summary
 5/14/2013
 2010 No Build AM Peak Hour
 Measures of Effectiveness

K:\01686-3801Traffic\Synchro\2010 NB MOEs - TAB: [AM MOEs]

Intersection		Control	Location	Appr	Forecast Volumes				Modeled Volumes				Error Volumes				Volume Error by Approach		Total Delay by Movement (Sec/Veh)			Level of Service by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queuing (feet)																			
					L	T	R	Total	L	T	R	Total	L	T	R	Total	%	L	T	R	L	T	R	Delay	LOS	Delay	LOS	Left-Turn			Through		Right-Turn															
																												Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Ave Queue	Max Queue	Storage													
Signalized	1: 65th Street & France Ave	NB	84	611	174	869	85	605	191	881	1	-6	17	12	1%	NB	40.8	30	17.6	D	C	B	28.4	C	23.7	C	NB	55	133	200	85	275																
		WB	59	50	208	316	61	52	206	319	3	2	-2	3	1%	WB	41.9	33.1	4.5	D	C	A	16.3	B	5.1	A	WB				77	188	46	63	23													
		SB	314	795	109	1218	308	602	102	1212	-6	7	-7	-6	0%	SB	38	15.9	13.9	D	B	B	21.3	C	23.7	C	SB	156	169	165	128	348																
		EB	85	227	43	355	82	222	46	350	-3	-6	3	-5	-1%	EB	27.4	28.2	18.7	C	C	B	26.8	C	23.7	C	EB	58	154	118	143	295																
Thru-Stop	2: 65th Street & Southdale Hospital Entrance	NB	71	1	10	82	70	0	12	82	-1	-1	2	0	0%	NB	18.9	0	7.8	C	A	A	17.3	C	5.1	A	NB				42	100																
		WB	13	218	20	252	9	218	20	247	-4	-1	0	-5	-2%	WB	5.8	0.8	0.3	A	A	A	0.9	A	5.1	A	WB				7	73																
		SB	6	2	26	34	7	4	32	43	1	2	6	9	25%	SB	14	14.9	5.1	B	B	A	7.5	A	5.1	A	SB				26	67																
		EB	69	405	241	715	71	410	240	721	2	5	-1	6	1%	EB	6	5.6	3.8	A	A	A	5.0	A	5.1	A	EB				24	143																
All-Way Stop	3: 65th Street & Drew Ave	NB	97	91	10	198	58	102	11	209	-1	11	1	11	6%	NB	7.1	8.3	4.9	A	A	A	7.6	A	7.5	A	NB				38	74	75	41	93													
		WB	37	118	25	180	39	110	26	175	2	-8	1	-5	-3%	WB	6.9	8.2	5.1	A	A	A	7.4	A	7.5	A	WB				48	89																
		SB	6	36	75	117	5	40	78	123	-1	4	3	6	5%	SB	4.8	6.8	4.1	A	A	A	5.0	A	7.5	A	SB				25	40	33	63	75													
		EB	150	125	71	346	148	129	72	349	-2	3	1	3	1%	EB	8.2	9.4	6.3	A	A	A	8.3	A	7.5	A	EB				66	135																
Signalized	4: 66th Street & France Ave	NB	23	556	95	704	23	591	108	722	0	5	13	18	3%	NB	42.4	18.8	2.5	D	B	A	17.1	B	23.5	C	NB	23	66	250	75	141	1	18	250													
		WB	230	304	179	763	278	295	178	751	-2	-9	-1	-12	-2%	WB	52.2	41.8	3.4	D	D	A	38.5	D	23.5	C	WB	103	171	315	68	172	18	123														
		SB	129	704	44	877	131	730	47	908	2	26	3	31	4%	SB	50.7	6.1	7.6	D	A	A	14.2	B	23.5	C	SB	91	171	250	32	118																
		EB	104	232	44	380	111	231	48	390	7	-1	4	10	3%	EB	39.1	34.1	2.8	D	C	A	31.7	C	23.5	C	EB	75	159	175	72	153	1	26	135													
Signalized	5: 66th Street & Drew Ave	NB	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	26.5	C	NB																						
		WB	44	783	177	1004	44	762	185	991	0	-21	8	-13	-1%	WB	42.7	23.7	15.6	D	C	B	23.0	C	26.5	C	WB	27	84	200	132	261																
		SB	65	6	37	108	64	6	40	110	-1	0	3	2	2%	SB	18.3	10.3	3.1	B	B	A	12.3	B	26.5	C	SB	33	92	150	16	44																
		EB	60	295	28	383	67	301	26	394	7	6	-2	11	3%	EB	39.8	27.2	2.9	D	C	A	27.7	C	26.5	C	EB	51	118	220	79	171	14	45														
Thru-Stop	N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																					
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A	0.0	A	WB																					
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A	0.0	A	SB																					
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A	0.0	A	EB																					
Thru-Stop	N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																					
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A	0.0	A	WB																					
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A	0.0	A	SB																					
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A	0.0	A	EB																					
Thru-Stop	N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																					
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A	0.0	A	WB																					
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A	0.0	A	SB																					
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A	0.0	A	EB																					

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Control	Intersection	Location	Appr	Forecast Volumes				Modeled Volumes				Error Volumes				Volume Error by Approach		Appr	Total Delay by Movement (Sec/Veh)			Level of Service by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queuing (feet)															
				L	T	R	Total	L	T	R	Total	L	T	R	Total	%	L		T	R	L	T	R	Delay	LOS	Delay	LOS	Left-Turn		Through		Right-Turn												
				Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Ave Queue	Max Queue	Storage																																	
Signalized	1: 65th Street & France Ave	NB	51	1437	86	1574	48	1429	81	1561	-3	-8	-2	-13	-1%	NB	109	407	403	E	D	D	42.7	D	37.4	D	NB	57	185	200	304	626												
		WB	59	111	436	606	60	104	422	596	1	-7	-14	-20	-3%	WB	56.3	55.6	16	E	E	B	27.2	C			WB				187	212	49	73	28									
		SB	101	824	31	966	105	625	31	961	4	9	0	-5	-1%	SB	95.2	26.2	24.8	E	C	C	33.7	C			SB	111	159	158	145	313												
		EB	152	108	125	385	149	105	129	384	-3	-2	4	-1	0%	EB	48	44.1	28.4	D	D	C	40.3	D			EB	101	154	128	190	411												
Thru-Stop	2: 65th Street & Southdale Hospital Entrance	NB	151	2	76	229	140	1	78	217	-11	-1	0	-12	-5%	NB	81.5	64.3	63.7	D	D	D	45.7	D	14.4	C	NB				144	192												
		WB	14	408	17	439	13	403	16	432	-1	-5	-1	-7	-2%	WB	8.9	12	5.4	A	B	A	11.7	B			WB				102	174												
		SB	3	1	47	51	2	1	47	50	-1	0	0	-1	-2%	SB	26.6	46.9	39	D	D	D	38.7	D			SB				36	88												
		EB	26	174	95	295	30	178	89	297	4	4	-6	2	1%	EB	5.4	3.7	2.3	A	A	A	3.5	A			EB				12	107												
All-Way Stop	3: 65th Street & Drew Ave	NB	57	24	14	95	50	27	14	91	-7	3	0	-4	-4%	NB	5.6	6.6	3.1	A	A	A	5.6	A	6.1	A	NB	27	61	75	34	53												
		WB	32	119	10	161	32	121	11	164	0	2	1	3	2%	WB	6.3	7.7	3.4	A	A	A	7.1	A			WB				43	89												
		SB	13	110	148	271	12	114	145	272	-1	4	-2	1	0%	SB	5.2	7.3	4.7	A	A	A	5.8	A			SB				39	65	44	75	75									
		EB	49	72	116	236	50	72	114	236	2	0	-2	0	0%	EB	6.4	7.6	4.4	A	A	A	5.8	A			EB				45	105												
Signalized	4: 66th Street & France Ave	NB	177	1134	324	1835	169	1128	324	1621	-8	-6	0	-14	-1%	NB	52.9	33.7	6.2	D	C	A	30.2	C	32.8	C	NB	129	260	275	211	344	94	275	275									
		WB	260	358	324	942	236	367	314	919	-22	9	-10	-23	-2%	WB	49.4	39.6	6.3	D	D	A	30.8	C			WB	93	182	315	121	243	68	205										
		SB	290	686	65	1041	290	666	60	1038	0	2	-5	-3	0%	SB	55.8	24.9	24.2	E	C	C	33.6	C			SB	184	244	250	151	375												
		EB	92	411	99	602	91	404	63	596	2	-7	-1	-6	-1%	EB	58.2	47.1	6.3	E	D	A	42.1	D			EB	91	159	175	147	258	39	160	175									
Signalized	5: 66th Street & Drew Ave	NB	0	0	0	0	0	0	0	0	0	0	0	0	0%	NB	0	0	0	A	A	A	0.0	A	27.3	C	NB																	
		WB	139	878	53	1070	142	850	56	1048	3	-28	3	-22	-2%	WB	39.8	15.7	9.8	D	B	A	19.5	B			WB	88	203	229	108	233												
		SB	256	26	55	337	257	28	56	341	1	2	1	4	1%	SB	28	21.5	8.6	C	C	A	22.5	C			SB	102	125	150	86	270												
		EB	35	843	66	944	28	837	68	933	-7	-6	2	-11	-1%	EB	48.2	23.4	3.4	D	C	A	32.7	C			EB	34	219	220	194	301	25	130										
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																	
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB																	
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB																	
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB																	
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																	
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB																	
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB																	
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB																	
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB																	
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB																	
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB																	
		EB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	EB	0	0	0	A	A	A	0.0	A			EB																	

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SanTraffic Simulation Summary
 5/14/2013
 2030 Build PM Peak Hour
 Measures of Effectiveness

K:\101686-3801\Traffic\Synchro\2030 Build\MOEs

Control	Intersection		Forecast Volumes												Modeled Volumes				Error Volumes				Volume Error by Approach		Total Delay by Movement (Sec/Veh)				Level of Service by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queuing (feet)									
	Location	Appr	Forecast Volumes				Modeled Volumes				Error Volumes				Volume Error by Approach		Total Delay by Movement (Sec/Veh)				Level of Service by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Left-Turn			Through			Right-Turn											
			L	T	R	Total	L	T	R	Total	L	T	R	Total	%	L	T	R	L	T	R	Delay	LOS	Delay	LOS	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage											
Signalized	1: 65th Street & France Ave	NB	51	1437	80	1578	46	1437	87	1570	-5	0	-3	-8	-1%	NB	104	384	395	E	D	D	40.4	D	37.4	D	NB	52	182	200	301	641	21.8	C	NB	111	178	6.1	A	NB	27	57	75	26	56
		WB	64	111	440	615	59	104	418	581	-5	-7	-22	-34	-6%	WB	59.7	57.4	17	E	E	B	28.6	C			WB	115	169	144	144	351			WB	111	178								
		SB	104	834	31	969	108	829	32	869	4	-5	1	0	0%	SB	99.4	252	25.5	E	C	C	33.5	C			SB	101	154	158	201	407			SB	14	97								
Thru-Stop	2: 65th Street & Southdale Hospital Entrance	NB	151	2	76	229	128	1	66	195	-23	-1	-10	-34	-15%	NB	107	61.4	93.5	D	D	D	32.3	D	21.8	C	NB	151	2	76	155	193	6.1	A	NB	27	57	75	26	56					
		WB	14	413	17	444	14	409	16	439	0	-4	-1	-5	-1%	WB	14.8	14	6.6	B	B	A	13.8	B			WB	111	178																
		SB	3	1	51	55	2	1	47	50	-1	0	-4	-5	-9%	SB	42.5	76.7	77.4	D	D	D	43.0	D			SB	50	137																
Signalized	3: 65th Street & Drew Ave	NB	177	1136	326	1639	172	1128	329	1629	-5	-8	-3	-10	-1%	NB	52.1	35.2	13.2	D	D	B	32.5	C	35.0	C	NB	127	250	275	213	339	27.3	C	NB	29	112	220	190	307					
		WB	283	358	324	945	242	368	312	922	-21	10	-12	-23	-2%	WB	51.3	39.2	18.1	D	D	B	35.2	D			WB	64	187	315	112	240													
		SB	290	689	67	1046	296	687	60	1043	6	-2	-7	-3	0%	SB	55.8	26.2	25.4	E	C	C	34.6	C			SB	168	245	250	150	376													
Signalized	4: 66th Street & France Ave	NB	94	411	89	604	94	405	99	599	0	-5	0	-5	-1%	NB	58.7	45.5	11	E	D	R	41.8	D	35.0	C	NB	92	159	175	147	274	27.3	C	NB	29	112	220	190	307					
		WB	0	0	0	0	0	0	0	0	0	0	0	0	0%	WB	0	0	0	A	A	A	0.0	A			WB	90	190	200	108	220													
		SB	258	26	58	342	251	28	57	336	-7	2	-1	-6	-2%	SB	26.3	21	8.7	C	C	A	22.9	C			SB	99	125	150	84	279													
Thru-Stop	5: 66th Street & Drew Ave	NB	37	843	66	946	31	844	69	944	-6	1	3	-2	0%	NB	46.9	23.6	3.6	D	C	A	22.9	C	0.0	A	NB	37	843	66	31	844	0.0	A	NB	37	843	66	31	844					
		WB	0	0	0	0	0	0	0	0	0	0	0	0	0%	WB	0	0	0	A	A	A	0.0	A			WB	0	0	0	0	0													
		SB	0	0	0	0	0	0	0	0	0	0	0	0	0%	SB	0	0	0	A	A	A	0.0	A			SB	0	0	0	0	0													
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB	0	0	0	0	0	0.0	A	NB	0	0	0	0	0					
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB	0	0	0	0	0													
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB	0	0	0	0	0													
Thru-Stop	#N/A	NB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	NB	0	0	0	A	A	A	0.0	A	0.0	A	NB	0	0	0	0	0	0.0	A	NB	0	0	0	0	0					
		WB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	WB	0	0	0	A	A	A	0.0	A			WB	0	0	0	0	0													
		SB	#N/A	#N/A	#N/A	#N/A	0	0	0	0	#N/A	#N/A	#N/A	#N/A	0%	SB	0	0	0	A	A	A	0.0	A			SB	0	0	0	0	0													

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**DRAFT MINUTES OF THE PLANNING COMMISSION MEETING JANUARY 26,
2000**

**P-00-1 Fairview Southdale Hospital
6401 France Avenue South, Edina, MN**

**Request: Final Development Plan for hospital expansion and
Conditional Use Permit for construction of a parking
ramp at 6400 France Avenue South**

Mr. Larsen informed the Commission Fairview Southdale hospital is seeking approval for an expansion that includes remodeling existing space, expanding outpatient services, additional medical office space, and construction of structured parking on the existing surface parking lot west of France Avenue. The expansion would occur in three phases. Phase I would be a single story with lower level addition to the west side of the building. Phase II would further expand the first floor, would add three levels of medical office space (MOB) above the addition, would construct a 984 space parking structure west of France Avenue which would be connected to the hospital by a skyway over France. Phase III would reorient the hospital's main entrance from the north to the west side and complete the interior remodeling.

The proposed 4 level, 984 space parking structure requires 2 setback variances. The first is along the north property line which is the right of way for the Crosstown highway. The ramp has been pushed as far north as possible to maximize the setback from 65th Street. As a result a 32 foot setback variance is requested which is equal to the height of the ramp. The second variance occurs at the southwesterly corner of the ramp where a 4 foot setback variance is requested. The ramp provides a 31 foot setback where 35 feet is required.

Mr. Larsen explained when all 3 phases of the hospital expansion are complete the Floor Area Ratio for the site will be 1.06 which exceeds the maximum FAR of 1.0 for the Regional Medical District. This ultimate FAR, however, does not take into account the property west of France owned by the hospital (the parking ramp property). If given credit for this property the FAR would drop to 0.88 and would not require a variance. The relocated ring road along the west and north sides of the building comes within 3 feet of the property line on the north side and within 5 feet along the west side of the building. The required setback is 20 feet. Thus, a 17 foot parking setback variance is required. The ring road is necessary because the hospital intends to limit access to the parking areas to the west and north of the building.

ASL

Mr. Larsen pointed out the Zoning Ordinance bases parking requirements for hospitals on the number of beds and employees. We have found this standard does not work to evaluate parking demand for a modern hospital campus. Rather we have relied on parking counts and the experience at other suburban hospitals to determine how much parking is needed. This method has worked well. The hospital's parking consultant estimates a overall demand of 5 spaces per 1,000 square feet for the completed project. The proposed ramp would provide parking adequate to meet expected demand. According to the analysis existing parking is adequate to meet Phase I demand.

Mr. Larsen told the Commission the traffic impact of the proposed development is addressed in the attached Traffic Impact Study prepared by BRW, Inc. The study is attached for Commission review. The study has been reviewed by the City Engineer. Recommendations for area improvements are detailed on page 30 of the report. Improvements to Drew Avenue should occur coincident with Phase I, while improvements west of France should occur when the ramp is built in Phase II.

Mr. Larsen stated Fairview Southdale is a community asset that provides vital services to the community and region. Staff believes it is essential that the hospital have the flexibility to react and change as the delivery of health care evolves. Acquiring the site west of France Avenue allows the hospital this flexibility. Construction of the skyway will allow the two sites to function as one. Traffic is, and will continue to be an issue in this area. Proposed improvements in the Traffic Study are consistent with the city's master planning for the area. Staff recommends approval of the 3 phase plan for the expansion and remodeling, and also recommends approval of the Conditional Use Permit for the 4 level ramp conditioned on:

1. Permits from 9 Mile Creek Watershed District
2. Hospital participation in street and intersection improvements as set out in the Traffic Impact Study
3. Refer ring road design to staff to examine possible modification which could occur with Phase II of the project

Mr. Mark Enger, Mr. Daniel Weber were present representing Fairview Southdale Hospital. Mr. Richard Dahl was present representing BWBR Architects, and Mr. Howard Preston of BRW was present.

Commissioner McClelland asked Mr. Larsen why 6400 France Avenue (site of proposed ramp) requires a Conditional Use Permit. Mr. Larsen explained the site is zoned APD, Automobile Parking District, and constructing a ramp in an APD zone requires a Conditional Use Permit.

Commissioner McClelland commented that she has some concern that in the future the ramp could be sold to a separate party. Mr. Larsen responded once the ramp is physically connected to the hospital via the skyway that concern would not be an issue.

Commissioner Swenson noted Phase I is aggressive and eliminates parking during the construction phase. She asked if staff believes parking demands will be met during the construction process. Mr. Larsen agreed that providing adequate parking will be a balancing act, adding he believes during the construction period parking will be adequate. Commissioner Swenson suggested that the parking ramp structure be constructed first, thereby providing parking during construction phases.

Mr. Mark Enger addressed the Commission and informed them Fairview Southdale Hospital (FSH) is 35 years old, has experienced many changes, and is committed to the City of Edina and the community. Mr. Enger said the main site is 15 acres, and was donated to FSH by the Dayton family for health care to the community. Mr. Enger told the Commission for the past three years FSH has looked at numerous expansion scenarios, and believes the plan before the Commission this evening fulfills the hospitals and communities needs. Mr. Enger said FSH is now operating at its maximum capacity due to the growth of the southwest suburbs, and FSH needs to meet the demands of the community.

Mr. Enger explained the proposed campus expansion will be accomplished in three phases, and it is the goal of the hospital to accomplish the three phases with as little disruption as possible. Mr. Enger introduced to the Commission, Mr. Daniel Weber also of FSH.

Mr. Weber informed the Commission the goal of the hospital is to orient the hospital toward the west, France Avenue. He said the front entrance for the hospital will be moved from the north to the west taking full advantage of France Avenue. Mr. Weber added new landscaping will be incorporated on site and a "green area" will be developed on the first floor allowing visitors and patients to enjoy the outdoors. Mr. Weber also noted the hospital will be creating quasi-public spaces that the hospital intends to offer the community for use. Mr. Weber said the hospital wants to develop a strong purpose of campus. Mr. Weber introduced Mr. Rick Dahl, architect for the project to further explain the proposal.

Mr. Dahl told the Commission the goal of the project is to create a recognized campus that is approachable and flexible. Mr. Dahl added the project is also cost effective, and re-enforces the sense of campus. Continuing, Mr. Dahl said the master plan will include; clinical components and support spaces, educational facilities, quasi-public spaces, medical office building,

parking ramp and a skyway spanning France Avenue. Mr. Dahl said the master plan is proposed to be implemented over the course of three phases.

Mr. Dahl explained Phase I is proposed to begin in the spring of 2000 and completed by the winter of 2002. Phase I will include 74,000 square feet of expansion which will occur on the first and lower levels. Phase I also includes 52,000 square feet of remodeling on the first floor. Phase I will include the heart center, including a cardiology suite of seven procedure rooms. Surgery facilities are also expanded by adding eight ORs and MRI is being relocated adjacent to the Emergency Department. Phase II adds the medical mall, parking ramp and skyway. This phase is targeted to begin in the spring of 2001 and end in the winter of 2002. Mr. Dahl said Phase III includes the completion of the medical mall, expansion of the post-operative recover area, a 20 foot wide lower level concourse will be created to route public traffic from the new outpatient entrance. Construction of Phase III is anticipated to begin in the spring of 2002 and completed in the fall of 2003. Concluding Mr. Dahl explained the building design will be respectful of the existing hospital, and the proposed materials for expansion include brick to match the existing building, precast concrete, and green marble. Similar materials will be used on the proposed skyway and parking ramp.

Mr. Enger told the Commission traffic is a concern in the area, and the hospital is working with City staff on signage to aid in the better flow of traffic, and to divert some traffic to Valley View Road. Mr. Enger said there will be some temporary parking inconveniences that the hospital hopes to mitigate by providing shuttle services for staff and temporary parking solutions.

Chairman Johnson questioned if the bulk of patient and visitor parking will be located in the proposed new ramp west of France Avenue and will the existing ramp be used by staff. Mr. Enger responded the existing ramp will be used by tenants and patients of the existing medical office building. Mr. Enger said he believes the vast majority of staff will park in the new parking structure.

Chairman Johnson said the last time the hospital came before the Commission there was some talk about West 65th Street and Drew Avenue. Chairman Johnson questioned what happened to that plan. Mr. Enger said a previous option of the hospital was an expansion that included the closure and re-routing West 65th Street. Mr. Enger said that scenario has died. Mr. Enger introduced Mr. Howard Preston of BRW.

Mr. Preston said as part of the traffic study BRW conducted a technical analysis of what is occurring now, and what will occur in the future. Mr. Preston said he has worked closely with Mr. Hoffman from the City's Engineering Department, and has suggested that Drew Avenue be widened from 66th Street to 65th Street to accommodate traffic.

Chairman Johnson asked if there has been any discussion between the hospital and the neighborhood. Mr. Preston said prior to the change in scenarios BRW conducted a vehicle trace survey in the area. This survey indicated that much of the traffic on West 65th Street appeared to be destined for the hospital. Mr. Preston said it has been their goal to focus traffic coming to and going from the hospital to West 66th Street and Drew Avenue.

Mr. Enger, interjected and told the Commission FSH has held two community forums inviting the neighbors giving them an opportunity to discuss the proposed expansion, and in addition the hospital has kept in contact with Pacey Irk, Chair of the Colony Association. Mr. Enger said at this time there appears to be no issues.

Chairman Johnson stated traffic on Valley View Road coming west from the proposed ramp concerns him. He pointed out Valley View Road is 300 to 400 feet from the ramp, and if a vehicle turns right onto Valley View Road, and turns left onto Crosstown a stacking problem will occur. Chairman Johnson noted it is already difficult to get onto Crosstown. Mr. Preston, said FSH is looking into the possibility of creating signalization at the south and north ramp terminals. Mr. Preston said it is hoped this will assist vehicles in getting from 65th onto Valley View Road going north and turning west onto the Crosstown. Mr. Preston added congestion on the Crosstown highway is beyond the scope of the hospital. Chairman Johnson stated during certain hours one can't get on the Crosstown ramp. Continuing, Chairman Johnson said he has a concern especially during rush hour that travelers will figure out a way to wind through the neighborhood and pick up north bound 100 at Benton Avenue. Mr. Preston agreed that is a possibility. Mr. Enger interjected it is difficult to define when rush hours starts and stops, adding hospital shift changes are during different hours, and it is our hope that the staggered time frame distributes traffic. Mr. Enger said the hospital shares the same concerns regarding traffic. He stated it is important to the hospital that patients and visitors gain easy access to the campus. Mr. Enger concluded hospital staff is very interested in working with the City, County and State, to resolve traffic issues, reiterating finding solutions is very important to FSH and the community.

Commissioner Swenson questioned Mr. Hoffman if it is possible to put a traffic light at the bottom of the Valley View Road exit from Crosstown Highway. Mr. Hoffman said that can be accomplished, adding there is a plan to construct three lanes at this point because the exit lane is so short merging into flowing traffic with so many turns required to gain access to France Avenue. Commissioner Swenson said she also has a concern with the ramp entrance from Valley View Road traveling westbound on Crosstown Highway. She said it is difficult to accelerate fast enough because the merge lane is so short. Mr.

Hoffman said he agrees with that observation, and explained there is an idea using additional land to making the merging easier.

Commissioner McClelland said she noticed the hospital has stressed it is a non for profit organization, and pointed out when the hospital requested the office building and ramp in the NE corner of the campus it was felt at that time that that would be the last expansion to the campus. Mr. Enger explained BTO, owner of the medical office building is not a non profit organization. He said they are for profit and pay property taxes, and will continue to pay taxes on the existing building, and the proposed new office suites. Continuing, Mr. Enger pointed out the demand for medical facilities has increased during the last decade which can be noted in the occupancy rates of medical facilities along France Avenue. Mr. Enger said he cannot address what occurred before he came to FSH, and how the hospital presented itself at that time, but believes this expansion is needed and is an asset to the community.

Commissioner McClelland questioned Mr. Enger on the temporary structure variance the hospital requested in December. Mr. Enger said at this time FSH is considering other options. It was found that the cost of the temporary structure exceeds the budget of the hospital.

Mr. Dahl interjected he was present at the December Zoning Board hearing and explained as designed the temporary structure would be required to conform to building codes and cost became a factor. Concluding, Mr. Dahl said they are in the process of discussing other temporary options.

Commissioner Lonsbury asked Mr. Enger is he knows the percentage breakdown of residents that use the hospital and It's campus facilities. Mr. Enger explained FSH encompasses the SW metro area, which includes Edina, parts of southwest Minneapolis, Bloomington, and Richfield. Mr. Enger said that percentage is roughly 85%. 15% comes from other regions.

Commissioner Swenson asked Mr. Larsen the difference between Floor Area Ratio (FAR) and lot coverage. Mr. Larsen explained lot coverage (building footprint coverage) calculations are used in the residential districts. FAR is used in commercial and other zones and measures the gross floor area divided by the lot area. Commissioner Swenson asked Mr. Larsen if he knows of any areas in the City where the FAR is as intense. Mr. Larsen said many of our commercial zones are at the maximum FAR. Mr. Larsen explained what is different in this situation is that France Avenue divides the campus creating a situation where the property west of France Avenue cannot be used in FAR calculations, even if it is owned by the hospital and considered part of the campus.

Commissioner Swenson asked Mr. Larsen if he has a concern about setting a FAR precedent. Mr. Larsen said by law variances are not precedent setting, they are always unique to each site.

Commissioner Bergman asked Mr. Larsen if he is aware of future development plans on the Southdale Medical Building site. Mr. Larsen said that is a real possibility. He added the traffic analysis was required to assume an addition will occur on that site.

Commissioner Lonsbury asked Mr. Larsen if City Code allows skyways. Mr. Larsen said skyways are not addressed in the code. Continuing, Mr. Larsen said France Avenue is a County road and the County has indicated they have no problem with the proposed skyway as long as it meets height requirements. Commissioner Lonsbury questioned if any code modifications should occur regarding skyways. Mr. Larsen stated that is worth thinking about, adding at the present time he does not believe there is another skyway imminent.

Commissioner Byron moved to recommend Final Development Plan and Conditional Use approval, subject to: permits from the Nine Mile Creek Watershed District; hospital participation in street and intersection improvements as set out in the Traffic Impact Study; refer ring road design to staff to examine possible modification which could occur with Phase II of the project, and subject to variance approval as they relate to the parking ramp and ring road. Commissioner Bergman seconded the motion.

Chairman Johnson asked Commissioner's Byron and Bergman if they would accept an amendment to the motion that would include the further study and discussion of Drew Avenue and its intersections. Commissioner's Byron and Bergman accepted the amendment. All voted aye; motion carried.

Passed and adopted this 15th day of February, 2000.

Member Johnson seconded the motion.

Rollcall:

Ayes: Faust, Hovland, Johnson, Kelly, Maetzold

Ordinance No. 850-A-16 adopted.

PUBLIC HEARING CONTINUED TO APRIL 4, 2000, FINAL DEVELOPMENT PLAN, FAIRVIEW SOUTHDAL HOSPITAL Affidavits of Notice were presented, approved and ordered placed on file.

Presentation by Planner

Mr. Larsen reported that Fairview Southdale Hospital has requested approval for an expansion that would include remodeling existing space, expanding outpatient services, additional medical office space and construction of structured parking on the existing surface parking lot west of France Avenue. The proposal requires the Council approve a Final Development Plan and a Conditional Use Permit. The expansion would take place in three phases. Phase 1 would be a two-story addition (one-story with basement) to the west side of the existing building. Phase 2 would further expand the first floor, add three levels of medical office space (65,700 square feet) above this expansion and would construct a 984 space parking structure west of France Avenue connected to the hospital by a skyway over France. Phase 3 would reorient the hospital's main entrance from the north to the west side of the building and complete the proposed interior remodeling. Mr. Larsen added that City staff requested the hospital complete an environmental assessment worksheet which the Council has received. He said that Statute requires no action for 30 days after publication of the worksheet which was published February 7, 2000. Therefore, Mr. Larsen recommended that action would need to be continued until at the earliest March 21, 2000. The proposal requires two variances. He explained that the ramp has been pushed as far north as possible to maximize the setback from 65th Street. As a result, a 32-foot setback variance is requested is equal to the height of the ramp. The second variance is at the southwesterly corner of the ramp where the ramp provides 31 feet of setback and 35 feet are required resulting in a variance of 4 feet.

Mr. Larsen said that when all three phases of the expansion are complete, the Floor Area Ratio (FAR) for the site will be 1.06 which exceeds the maximum FAR of 1.0 for the Regional Medical District. He noted that this FAR does not take into account the property west of France owned by the hospital (the parking structure). When given credit for the property west of France the FAR drops to 0.88 and would comply with Edina Code.

Parking regulations base requirements upon the number of beds and employees. Staff has found this standard does not work to evaluate parking for a modern hospital campus. The parking consultant has estimated an overall demand of 5 spaces for 1,000 square feet of completed project. The proposed ramp would provide 1,137 (originally 984) parking spaces and based upon parking counts and experience at other suburban hospitals it is believed the proposal will adequately meet the demand.

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Mr. Larsen reported that the Planning Commission recommended approval of the Final Development Plan and Conditional Use Permit for the Fairview Southdale Hospital expansion subject to the following conditions:

1. Watershed District permits;
2. Hospital participation in street and intersection improvements set out in the traffic impact analysis section of the EAW.; and
3. Redesign of ring route and parking area west of the addition to be reviewed by staff.

Following the Planning Commission meeting, Planning and Engineering staff met with hospital representatives to consider site plan changes which would reduce hard surface and add landscaped area along the France Avenue side of the hospital. As a result of those discussions, a revised site plan and landscaping plan have been submitted. The revised plan eliminates the separate parking and ring route areas west of the building, reduces parking and hard surface area, while providing additional landscaped areas. The parking area west of the building will be used primarily for handicapped parking. The parking north of the building will continue to be short term physician parking. Mr. Larsen said the changes appear to represent a significant improvement and staff recommends approval of the change.

He added that partially in consideration for the site plan changes; the hospital is requesting consideration of a slightly larger parking ramp. The expanded ramp would increase capacity from 984 stalls originally proposed to 1,137 stalls. The expansion would occur on the west side of the structure. This expansion would meet setbacks and not require a variance. The additional spaces would replace those lost through the redesign of the site plan, and would provide a cushion to handle occasional peak demand times for the hospital and clinic. The expanded ramp would remain at four levels. He concluded that staff recommended approval of the revised ramp plan.

Member Faust asked if the parking west of France would be free or fee parking; if the Phase 2 parking could be required before the Phase 1; whether the expansion to the ramp requires a variance; and how high would the skyway be above France Avenue? Mr. Larsen replied he assumed it would be fee parking, that the parking would be completed as soon as possible, however, according to the traffic study of the EAW the surface lot should be able to handle Phase 1 parking needs. He added that the expansion to the parking ramp does not require a variance, and that the skyway was planned to be 18 feet above France Avenue. Member Faust commented that she wants to see the parking constructed during the first phase.

Proponent Presentation

Mark Enger, Administrator, Fairview Southdale Hospital, thanked the Council for the opportunity to present the hospital's proposed expansion plans. He noted that Fairview will celebrate its 35th year of service during 2000. Mr. Enger said the proposed expansion was in response to additional demands for service. As the suburban population has increased, demographics changed and technologies have advanced. Hospitals see large increases in calls for services in many areas.

Mr. Enger said the team has been working for almost two years to develop the proposed expansion plans. He showed the Council and audience the architectural renderings of the proposed expansion. Mr. Enger reported the Board has approved \$35,000,000 of improvement in the Phase 1 & 2 expansions. The hospital intends to expand its surgical capacity and its cardiovascular services; to build structured parking on the surface lot, west of France Avenue to be attached to the hospital via a skyway; and add medical office space.

Mr. Enger stated that the hospital has attempted to be diligent in working with neighbors. They have hosted two community forums. Mr. Enger reiterated that the hospital is very committed to the community. Their intent is to create space available to the community when not used by the hospital. They are also very concerned about traffic. For this reason the ramp has been designed to disperse traffic.

Mr. Enger asked the Council for their support of the project. He introduced Mr. Richard Dahl, BWBR Architects, and Mr. Howard Preston of BRW Traffic Consultants.

Richard Dahl, BWBR Architects, explained he had been working with Fairview Southdale Hospital for two years. Mr. Dahl said four principles guided the proposal: proactive wellness; accessible facilities; flexibility of use; and cost effectiveness. Mr. Dahl reported the building design responded to space allocation, would be respectful of the existing hospital, and re-enforces the sense of campus. The master plan will include: clinical components and support spaces, education facilities, quasi-public spaces, medical office building, parking ramp and a skyway spanning France Avenue.

Mr. Dahl explained Phase 1 is proposed to begin in Spring 2000 and be completed by Winter 2002. It will include 74,000 square feet of expansion occurring on the first and lower levels. Phase 1 also includes 52,000 square feet of remodeling on the first floor of the hospital. It will include the heart center (a cardiology suite of seven procedure rooms), surgery facilities will also be expanded by adding eight OR's and MRI will be relocated adjacent to the Emergency Department. Phase 2 will add the medical mall, parking ramp and skyway. Mr. Dahl said this phase would be targeted to begin construction during Spring 2001 and be completed Winter 2002. Phase 3 would include the completion of the medical mall, expansion of the post-operative recovery area, a 20-foot wide lower-level concourse to route public traffic from the new outpatient entrance. Mr. Dahl stated that Phase 3 would begin Spring 2002 and be completed Fall 2003. He added that during the third phase the main entrance would be reoriented to France Avenue and the interior space will be changed, thus necessitating the greatest demand for additional parking.

Member Johnson asked the proponents what the need was for additional medical offices? Mr. Enger replied there was an increased demand for office space, and that the plan would create office space around certain clinical specialties. Mayor Maetzold commented that Southdale was the only five star heart hospital in the upper midwest.

Member Faust expressed concern with construction parking. Where are all the construction workers parking? This could create a very difficult situation in an already strained area. She

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asked that by the time the hospital came back for its final approval in April; the construction parking should be worked out and reported to the Council.

Mr. Enger acknowledged the concern for construction parking. He said off-site parking would most likely be needed. Southdale Shopping Center may be a possible location.

Member Hovland said that Fairview Southdale has been a good corporate citizen. He applauded their desire to expand, improve accessibility and approachability. Member Hovland acknowledges the ramp west of France Avenue promotes these issues. He asked how many patients are expected daily? Mr. Enger replied estimates are 240 beds per day plus 120 emergency room visits. In addition, there will be increased outpatient use. It is anticipated that there will be up to 19,000 car visits per day. Member Hovland asked if the proposal looked to 2005 and beyond and about the demographics of the potential patient base. Mr. Enger said yes, it had and assumed a 3% annual growth factor. He added that 70% of the patients come from Edina with the remaining 30% from the Minnesota Valley area.

Member Hovland expressed concern with growth and increased traffic demands. He asked if perhaps MnDOT should be asked to create direct access to Valley View and to the ramp. Member Hovland also asked if the increased number of employees had been factored into the traffic calculations.

Howard Preston of BRW explained that as part of the traffic study, BRW conducted a technical analysis of what the current traffic is and what the future will bring. He said he worked with the City Engineering Department and has suggested that Drew Avenue be widened from 66th Street to 65th Street to accommodate traffic. Mr. Hoffman added there is a plan to construct three lanes at the bottom of Valley View Road's exit from Crosstown Highway and to bring the traffic directly into the ramp without cars ever needing to go as far east as France.

Public Comment

Pacy Erck , 6315 Colony Way. Ms. Erck, President Colony Homeowners Association, stated she represented 500 residents from 308 units. They are concerned about traffic. Ms. Erck reported that parking issues already exist. Colony resident are very concerned with the congestion on Drew Avenue and about pedestrian traffic coming from the parking ramp. With the redesign of 66th Street now only right turns are allowed onto Barrie Road. Ms. Erck urged the Council to require the parking structure be in place before any other expansion is built. Construction of this magnitude will take two to three years and keep 500 people frustrated and in jeopardy.

Linda Schmidt, 6483 Barrie Road, complained that the morning traffic on Barrie Road speeds now. She urged that traffic be directed to avoid Barrie Road.

Doug Murray, 6566 France Avenue, stated he is the Legal Chair for the Point of France Board of Directors. Mr. Murray echoed the traffic concerns of the previous speakers. He stated he had a number of questions about the parking structure and stated it will decrease the value of

a number of Point of France homes. Mr. Murray urged that the structure be extensively screened and that any trees planted be mature trees. Point of France has concern with the ramp's height and its rectangular shape. Mr. Murray asked what about future expansion of the ramp. Mayor Maetzold noted that the Council may only deal with the current proposal. Mr. Murray continued questioning whether the proposal was the best use of the space. He pointed out that many accidents occur at 65th Street and France Avenue and urged the Council to proceed with caution.

Doug Erickson, 5020 Richmond Drive, stated he believed that the proposed expansion was a nightmare waiting to happen. Mr. Erickson urged caution in any approval because of the impact on France Avenue traffic.

Katherine Abbott, 6566 France Avenue South, stated she lives overlooking a traffic jam. Ms. Abbott said she had no notice of any proposed expansion until last week. She asked the City to get control over the height of the ramp and to ensure there is adequate screening. Ms. Abbott asked if there would be another opportunity to review the proposal.

Unidentified Women, 6315 Colony Way, asked if there were any guarantee the parking would be built. She questioned whether people will be able to cross France Avenue after the construction.

Council Discussion/ Action

Mayor Maetzold asked if the Council wished to close the public hearing.

Member Johnson made a motion to close the public hearing. No second. Member Johnson withdrew his motion for lack of second.

Member Kelly stated he did not support the hospital expansion as proposed.

Member Faust asked if there was a height restriction on the ramp and if the hospital increased the traffic on Drew Avenue could they be required to install sidewalk on Drew? Mr. Larsen answered the parking structure could have three additional levels added in the future He added it could be recommended that the hospital participate in the cost of installing sidewalk. Manager Hughes said that sidewalk could be looked at now and added as a condition of approval. Member Faust stated that she believed if the hospital is creating a problem, they should be required to solve it.

Member Hovland commented the expansion is extremely complex. He indicated he had concerns with traffic related issues. Mr. Hoffman explained that it has been intended to bring traffic from Valley View to the proposed parking structure. This would attempt to keep the parking traffic off France Avenue. Guidance signs would be installed on the Crosstown Highway.

Mr. Preston of BRW added the traffic has been analyzed and it is the hospitals desire to divert as much traffic from France Avenue as possible. He noted that perhaps 30-35%

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hospital traffic comes from the west and will be directed to Valley View Road. He added that about 36% of the traffic comes from the east and it was hoped to direct them onto 66th and Drew using lights and intersection improvements to move traffic off France.

Member Hovland stated he continued to be troubled by the statistics shown in the traffic analysis.

Mayor Maetzold said he could not be at the March 21, 2000, Council meeting and since it seemed more discussion of this issue was needed; he suggested the item be carried over until April 4, 2000. Consensus of the Council was to carry the item over to April 4, 2000.

Linda Schmidt, 6483 Barrie Road, said she frequently travels on Valley View and she is concerned that increased traffic will cause traffic back-up onto the freeway.

Member Johnson moved to continue the public hearing until April 4, 2000, and to grant the City a sixty day extension from action until, May 11, 2000.

Member Kelly stated he was opposed to granting a 60-day extension from action. He continued adding he could support a hospital expansion, however, in his estimation, the proposal makes a bad situation worse. He suspects the site will be overbuilt. Member Kelly said he found nothing aesthetically appealing for the Highway 62/France major city intersection. The landscaping proposed was poor and the set of variances requested very ambitious. He challenged the architects to redo their design and come up with answers to the concerns expressed by Council and citizens and urged them to not only address the hospital's desires, but also the communities'. Member Hovland seconded the motion.

Ayes: Faust, Hovland, Johnson, Maetzold

Nay: Kelly

Motion carried.

PRELIMINARY REZONING AND PRELIMINARY PLAT APPROVED, FRAUENSHUH COMPANIES, NORTHEASTERN QUADRANT OF HIGHWAY 169 & I-494 Affidavits of Notice were presented, approved and ordered placed on file.

Presentation by Planner

Mr. Larsen explained the subject property is approximately 13 acres and zoned Planned Office District, POD-1. The property is vacant. Its northerly 75 feet are subject to an NSP easement for an overhead transmission line. The central portion of the site contains a pond covered by an easement in favor of the state highway department and a large portion of the site is impacted by wetlands and flood plain.

Mr. Larsen noted the City of Edina owns the property having acquired it from the FDIC in 1991. He said the City has entered into a purchase agreement with the Frauenshuh Company to acquire and develop the property. The site became part of the City of Edina as a result of a land exchange with the City of Bloomington that included portions of the Raddison South

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WHEREAS, the Council considered the extent the vacation affects existing easements within the area of the vacation and the extent to which the vacation affects the authority of any person, corporation, or municipality owning or controlling electric, telephone or cable television poles and lines, gas and sewer lines, or water pipes, mains, and hydrants on or under the area of the proposed vacation to continue maintaining the same, or to enter upon such easement area or portion thereof vacated to maintain, repair, replace, remove or otherwise attend thereto;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Edina, Hennepin County, Minnesota, that the following public street right-of-way, and utility and drainage easements are hereby vacated effective as of April 4, 2000:

All drainage and utility easements, created by the plat of Braewood Park, which are contained within the following: Lot 1, except the Easterly 175 feet thereof, Block 1, Braewood Park;

Part of that certain drainage and utility easement, created by Document No. 1456628 of record in Hennepin County, Minnesota, being the West 10 feet of the East 185 feet of Lot 1, Block 1, Braewood Park;

All of 78th Street, as dedicated in Braewood Park, according to the plat thereof of record in Hennepin County, Minnesota, which lies northerly of the following described line:

Commencing at the southeast corner of Lot 1, Block 1, Braewood Park; thence westerly along a southerly line of said Lot, and along a curve concave northerly and having a radius of 7489.44 feet, a central angle of 1 degree 19 minutes 29 seconds and a chord of 173.16 feet bearing South 87 degrees 30 minutes 47 seconds West, an arc distance of 173.16 feet to the intersection of said southerly line with the easterly line of said 78th Street, being the point of beginning of the line to be described; then South 88 degrees 10 minutes 30 seconds West 150.00 feet to the intersection of the westerly line of said 78th Street with the southerly line of said Lot, and there terminating.

BE IT FURTHER RESOLVED that said vacation does not affect, and there are continued, reserved, and retained, by the said resolution ordering the vacation, the following existing easements and authority in, on and under the above vacated area:

The authority of Northern States Power Company, US West Communications, Paragon Cable Minnesota, or Minnegasco to enter upon the above vacated area for the maintenance, replacement, repair and removal of and for otherwise attending to, underground conduit, manholes, cables, wires and poles required for utility service now in, on or under the above vacated area.

BE IT FURTHER RESOLVED that the City Clerk is authorized and directed to cause a notice of completion of proceedings to be prepared, entered in the transfer record of the County Auditor, and filed with the County Recorder, in accordance with Minnesota Statutes, Section 412.851. Member Johnson seconded the motion.

Rollcall:

Ayes: Faust, Hovland, Johnson, Kelly, Maetzold

Motion carried.

RESOLUTION 2000-38 ADOPTED APPROVING NEGATIVE DECLARATION FOR NEED OF ENVIRONMENTAL IMPACT STATEMENT; FINAL DEVELOPMENT PLAN & CONDITIONAL USE PERMIT CONTINUED UNTIL APRIL 18, 2000 - FAIRVIEW SOUTHDAL E HOSPITAL BUILDING EXPANSION AND PARKING RAMP CONSTRUCTION (6401 FRANCE AVENUE SOUTH) Affidavits of Notice were presented, approved and ordered placed on file.

Presentation by Planner

Planner Larsen stated there were three items requiring action all related to the proposed expansion of the hospital, the added medical office space, and the parking ramp proposed west of France Avenue. At the first hearing for the hospital expansion, the comment period for the Environmental Assessment Worksheet (EAW) was still open so the hearing was continued.

Mr. Larsen explained the EAW, was a study of the impact of the development, to ascertain whether a full Environmental Impact Statement (EIS) would be needed. The hospital voluntarily completed the EAW, prepared by BRW, at the request of City staff. The EAW's 30 day comment period for the draft document ended on March 8, 2000. All comments received on the EAW were sent to the Council. The Council was also sent a response to the comments, that lists some findings of fact, supporting a negative declaration, meaning that no EIS would be necessary. Mr. Larsen stated that if the Council decided to support the negative declaration, they would be able to act on the local zoning requests consisting of a Final Development Plan and Conditional Use Permit. The Final Development Plan would allow the expansion of the hospital on the east side of France Avenue. The Conditional Use Permit would allow the hospital to construct a four-level parking structure on the west side of France Avenue connected by a skyway to the hospital.

Mr. Larsen noted that a major part of the EAW was a traffic study. He introduced Rick Nau of BRW who would review the traffic study, its findings, and BRW's recommendations.

Proponent Comment

Richard Nau, BRW, Traffic Consultant for Fairview Southdale reviewed the Fairview Southdale and Southdale Medical Office Building (MOB) Traffic Study. Mr. Nau said the traffic study included the following elements:

- Counts of 1999 existing traffic volumes
- Estimate background traffic growth between 1999 and 2005
- Estimate traffic generated by hospital and Southdale MOB expansions
- Analyze traffic operations with and without expansion traffic
- Determine needed roadway improvements

Member Faust asked which medical office building Mr. Nau was referring to in his report; the proposed Fairview expansion or the office building owned by Northco. Mr. Nau said he was referring to the office building owned by Northco. The report included both the expansion of Fairview Hospital and the Southdale Medical Office Building (MOB) owned by Northco.

Mr. Nau stated it was determined that the hospital expansion would generate an additional 330 trips per day or a 55% increase and the MOB expansion would generate 264 trips per day or a 45% increase in area traffic. Mr. Nau displayed a graphic illustration of the estimated traffic impact on area intersections after the expansions, showing increases ranging from 2% to 12% by the year 2005, with the intersection at West 65th Street and France Avenue receiving the largest increase at 12%. He explained that increases over 5% were significant. Mr. Nau noted that existing traffic operating deficiencies included close intersection spacing resulting in lack of stacking distance at ramp meters; and a lack of signalization that results in cross street traffic delays. These deficiencies will remain after the expansion and the additional traffic generated will warrant more signalization of the area. Proposed hospital related mitigation measures would include:

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- New guide signs on TH 62 directing westbound hospital traffic to exit at Xerxes Avenue
- New guide signs on TH 62 directing eastbound hospital traffic to exit at Valley View Road
- Installation of a signal at the intersection of the Valley View ramp to TH 62
- Installation of a signal and reconfiguration of intersection to allow through and left turn movement on all four approaches at Valley View and West 65th Street
- Widen Drew Avenue to 40 feet to accommodate three lanes.

Mr. Nau added the intent of the proposed mitigation measures would be to bring traffic into the hospital and MOB through Xerxes Avenue and Valley View Road attempting to keep it off France Avenue.

Member Hovland asked about MnDOT's comment over lack of a.m. traffic data in the study. Mr. Nau said this was discussed with MnDOT. He said the hospital is generating more traffic during the p.m. peak hour than during the a.m. peak hour. The traffic associated with the hospital does not actually coincide with the peak hours; that is, work shifts of 7-3. For this reason, a.m. traffic data was not collected. Mr. Nau reported that after this review MnDOT was satisfied with the data provided.

Member Hovland asked if there was any expectation that MnDOT would be resistant to the proposed new signage, or the curb cut and signalization at Valley View for the eastbound traffic. Mr. Nau replied that no significant resistance from MnDOT was anticipated relative to the proposed signage. He said that concern was expressed about the curb cut and will reserve judgement until a plan is submitted for their approval, however, in Mr. Nau's opinion this is still a viable solution.

Member Hovland asked Mr. Nau if he had any traffic projections for volumes when the recommended mitigation measures were put into place. Mr. Nau replied that the volumes would change to some extent, but not a great deal because most of the percentage increases are relatively low. Member Hovland asked if there were other long range traffic measures that should be investigated in case of additional medical facility expansion in that quadrant. Mr. Nau said he could not comment unless he had a proposal to analyze.

Member Faust expressed concern that the traffic study did not include peak a.m. traffic counts. She also asked who would pay for the recommended traffic mitigation measures such as the signals, and if sidewalks were still proposed for installation on Drew Avenue. Member Faust asked if there would still be room to install sidewalks if Drew Avenue were widened to three lanes. Member Kelly asked if Mr. Nau could comment on proposed funding of all the recommended traffic mitigation improvements. Mr. Nau replied that he did believe that there would be room for sidewalks on Drew Avenue and he stated that his traffic study did not address the funding of any of the proposed mitigation measures. He stated his assumption that because the streets involved are state aid streets there should be some funding available, but that a portion of the costs would need to be paid for locally, and that determination would be the Council's.

Member Kelly added that he shared the concerns expressed earlier about the lack of peak a.m. traffic data, especially since there are indications that additional development may occur in the area. Secondly, funding of the needed improvements must be discussed. Member Kelly also

noted his concern about storm water run off. He added the Council has dealt with storm water throughout his tenure. MnDOT said they were very concerned about run off, and he noted this was not mentioned in the response to comments, however, it was stated that the net impervious area would decrease. Would this decrease include the parking structure. Mr. Hoffman explained that the run off from the west side of France would be directed to a new pipe directly down to the pond and would not go into any MnDOT pipes. In his experience in the France Avenue interchange, if a 12-inch rainfall occurs some flooding will happen, but otherwise it should not be a problem.

Member Johnson questioned how BRW could propose to install a signal at the eastbound ramp of TH 62 and West 65th Street in light of the March 8, 2000, MnDOT letter opposing the proposed eastbound signal at West 65th Street. Mr. Nau said that the plan has been reviewed by MnDOT and will require their approval, which BRW believes will be granted. Mr. Nau said that providing a signal and cued detection device to prevent traffic backing up on TH 62 would allay the MnDOT concern. Member Johnson asked if any consideration had been giving to providing direct access from TH 62 to the parking facility similar to downtown Minneapolis's Target Center exits. Mr. Nau responded that this had not been considered. He added that the downtown facilities are public not private such as the proposed hospital ramp. Mr. Nau said that he believed a direct access would not be consistent with MnDOT's policy relative to access to trunk highways.

Mayor Maetzold asked if the proposed widening of the exit ramp from TH 62 eastbound that link with Valley View Road would necessitate filling the holding pond on Valley View. Mr. Nau replied that at this time there has not been detailed engineering, but a cursory look at the air photos indicated that it could fit within the existing right-of-way. Mayor Maetzold continued asking if encroachment into the pond became necessary would that be considered a significant environmental event. Mr. Nau hesitated to give a definitive answer, but suggested that it would most likely be a minor incursion into a storm water holding pond.

Member Faust asked if an additional lane off the eastbound TH 62 had been considered. Mr. Hoffman explained that this additional lane was already planned by the City's consultant SRF to widen the ramp. He added this improvement would be done in conjunction with the improvement of Valley View Road by the pond. He added that the pond should not be impacted by any proposed improvements to the Valley View interchange.

Mark Enger, Fairview Hospital Administrator, 6401 France Avenue South, said that as a result of discussions with City staff, the City Council, citizen input, and Fairview's development staff, the hospital listened to and attempted to respond to most of the questions. Mr. Enger said that changes have occurred in how hospitals deliver service to patients. Reimbursements for charges have changed. Technological advances have greatly changed patient services. The focus has shifted from in-patient care to more ambulatory based services. He added that there are procedures being performed at Fairview Southdale that two years ago were not dreamed of. Patients are being discharged on the day of service that five years ago would have spent seven days in the hospital after the procedure. One in three households in Edina has an elderly member. There has been a great increase in demand for cardiovascular care. Three years ago Fairview began planning in response to the health care needs. Mr. Enger stated that in 1998, expansion plans were begun looking at every vertical and horizontal possibility to fit expansion on a very tight hospital campus. Mr. Enger noted that the first option was to expand southerly, but the hospital could not come to terms with the other principal so they had to look at other options. He noted there are many medical office buildings on France Avenue and suggested that

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the proposed medical office building expansion be viewed not as a MOB, but as a heart/vascular care center. This center will allow Edina to offer state-of-the-art care to its residents.

Mr. Enger said the hospital is prepared to install sidewalks at Drew Avenue and West 65th Street. They have decided to build the parking ramp at the same time as the other Phase I expansion. Their contractor, Knutson, has arranged for workers' parking off-site and not on city streets. Fairview has contracted 430 parking spots at Southdale by Key Cadillac for ten months and intends to shuttle hospital employees during construction. He concluded urging the Council to move forward.

Richard Dahl, BWBR Architects, Fairview Southdale's architects took over the presentation. Mr. Dahl used graphics to demonstrate the design changes made in response to comments received earlier. The redesigned project has redistributed the green space on the east side of France, bringing it out as a well-landscaped edge to the campus. On the west side of France Avenue the ramp has been pushed north increasing the green space which will be bermed and landscaped to 40 feet. The variance on West 65th Street was eliminated, however, there is a 0 lot line variance necessary on the north side bordering the Crosstown Highway. Mr. Dahl showed a sample of the textured concrete that will be the exterior material of the ramp. The ramp will also use glass and aluminum wall for a portion of its exterior. He presented the proposed design of the skyway over France Avenue.

Member Kelly asked if any signage was proposed for the skyway. Mr. Enger replied that the Skyway would not have any signage.

Dr. Karen Porth, 6363 Fairview Physicians Office Building, Fairview Hospital Chief of Staff, urged the Council to approve the expansion plans. She reported an increased need for medical staff and technology to improve patient care. Dr. Porth said there has been a large increase in heart and vascular illness as our population ages. Technological advances also have increased the amount of procedures available to care for patients. Physicians and patients want this care available to them in an integrated facility and Fairview must expand to meet this need. Fairview has a wonderful group of physicians committed to serving patient needs, but to stay competitive and retain these physicians they must expand.

Mr. Enger concluded thanking the Council for the time and urged Council move the expansion plans forward as soon as possible.

Public Comment

Bruce Carlson, owner of Southdale Medical Building, stated he wanted to present their future plans. Currently Mr. Carlson said his building is at fifty percent capacity for the allowed floor area ratio (FAR) under Edina's zoning code. Mr. Carlson indicated that he intends to expand up to sixty-nine percent of the allowed FAR. He said he supported the hospital's request for expansion. Mr. Carlson showed graphic boards depicting a conceptual potential expansion of his medical office building. It would include a 69,000 square foot office expansion with 75 underground parking spaces and a ½ half level ramp addition. The proposed three-story building would permit the Minnesota Heart Group to occupy the entire first floor. Mr. Carlson indicated that his building is presently 95% occupied and he is not currently before the Council with an expansion request. He added he supported the hospital's expansion, but wished that Fairview Southdale would allow the Southdale Medical Office Building to expand.

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Norbert Weninger, 6566 France Avenue South, Board Chair, Point of France, reviewed the points made in his letter to the Council. Mr. Weninger pointed out the ramp and expansion will increase traffic that is already a nightmare. He added the height of the ramp is of great concern to the fifty residents who have homes facing the ramp. Point of France residents' other concern include the exterior surface of the parking ramp, surface water runoff, and the precedent set by allowing a skyway.

Elmer Johnson, 6566 France Avenue, Point of France resident, for twenty years asked if the hospital's ramp could be depressed into the ground.

Bob Clinton, 6712 Rosemary Lane, asked if the proposed ramp will be free or a pay to park ramp.

Mike Callas, 4930 West 77th Street, Edina Chamber of Commerce, stated he was representing Pat Tucker. Mr. Callas explained the Chamber understood the issues and concerns, however they still support the proposed expansion. He read the following Chamber resolution into the record:

"EDINA CHAMBER OF COMMERCE

Fairview Southdale 2000-2002 Expansion Project

WHEREAS, Fairview Southdale Hospital is the Twin Cities' eighth largest hospital and has a reputation for outstanding patient care; and

WHEREAS, Fairview Southdale Hospital, a non-profit organization, is Edina's largest year-round employer at more than 2000 employees, a good steward of the Edina Community and committed to meeting the needs of its immediate service area; and

WHEREAS, the demographics of the Edina area point to increased demands for cardiovascular services.

NOW, THEREFORE, BE IT RESOLVED, that the Board of Directors of the Edina Chamber of Commerce does hereby support the Fairview Southdale 2000-2002 Expansion Project to increase its capacity to serve patients, families and the community.

Adopted by the Edina Chamber of Commerce the 15th day of March, 2000."

Hugh Thompson, 6301 Colony Way, stated he had two concerns. The first is traffic which is horrendous and secondly, what is the purpose of the surveying on Drew and 65th Street.

Bill Bellows, 6566 France Avenue, asked about what type of greenery was planned for the parking structure's roof. Mr. Bellows also expressed concern over reflection from the aluminum used in the skyway and glass wall design of the elevator and atrium.

Laurel Thompson, 6301 Colony Way, added she was concerned about all the costs incurred by the proposed expansion. Ms. Thompson asked who would pay for the building. Mayor Maetzold explained the hospital was responsible for any building expansion. The only costs to be decided would be any needed public improvements, such as streets, sidewalks, etc.

Iver Ross, 7504 Colony Way, said that he has an office in the 4010 Building and many times now it takes 15 minutes to get home. Mr. Ross said the Colony already has problems with outside cars on their private street.

Council Discussion/Action

Member Hovland made a motion seconded by Member Johnson to close the public hearing.

Ayes: Faust, Hovland, Johnson, Kelly, Maetzold

Motion carried.

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Mayor Maetzold asked if the hospital had considered building the parking structure underground and why if there are no plans to go higher is the ramp being oversized in the plans. Mr. Enger replied that it would have added over \$1,000,000 to the cost of the ramp to build one level of the ramp underground. He added that while at this time the proposed expansion would serve the hospital for many years, they want to maintain the flexibility of further expansion if it becomes necessary in the future.

Member Faust asked if a variance would be required to add to the ramp in the future. Mr. Larsen said that any future expansion to the ramp would require conditional use permit approval in the future, even without any need for a variance.

Member Hovland asked about the lighting proposed for the ramp. Mr. Dahl said that the lighting would be downcast and the fixtures would be cut off fixtures to keep the light confined within the ramp to avoid any glare at Point of France. He added the lights could be directed northward also.

Member Faust asked if all levels of the ramp would have handicapped access, whether security issues have been looked at with such a parking ramp and if employees will be charged to park in the ramp. Mr. Dahl said that there will be handicapped parking stalls clustered on each level of the ramp near the elevator. He added that his firm has designed a number of parking ramps and a number of security measures are rolled into each design. He said that his firm follows guidance during the design stage from a Minneapolis ordinance developed to deal specifically with such security issues. Mr. Enger added that the hospital currently has 24-hour security and may need to add more resources when this expansion occurs. He informed the Council that hospital employees will be charged at a modest rate to park in the ramp.

Mayor Maetzold asked how to ensure the Drew Avenue sidewalks are made a part of the project. Mr. Hughes answered that the sidewalk installation could be made a condition of the approval or Council could direct City staff to come back and hold a project hearing for the purpose of installing the improvement and assessing the cost to benefited property owners.

Mayor Maetzold asked about the larger issue of requiring the hospital to pay a portion of the traffic improvements identified for the area. Mr. Hoffman explained that only portions of the improvements would be state aid eligible. He said that West 65th Street from Valley View is not a state aid street, so no gas tax funds could be used for that leg of the intersection improvements. He proposed that possible sources of funds would come from MnDOT for ramp improvements, gas tax on state aid street, special assessments to larger area, and a specified amount to be assigned to the ramp because of its location and impact on the surrounding area. Mr. Hughes explained that three legs of the intersection would be eligible for state aid funding and one would not. So one-fourth of the total cost must be funded locally as a special assessment to an area determined to be benefited from the improvement. Member Faust asked if the one-fourth share for the installation of the two stop lights would be around \$100,000. Mr. Hoffman said that no feasibility has been done, but that could be a rough estimate.

Member Hovland asked about potential costs of signaling Xerxes Avenue and 64th Street. Mr. Hoffman said that again a portion could be funded through state aid and the rest would be local funding. He added this signal also involves Richfield. Member Hovland continued asking about the France Avenue bridge deck over the Crosstown Highway. Mr. Hoffman said this project currently is entirely funded by MnDOT, Hennepin County and the City.

Member Faust asked how an assessment might be calculated for widening of Drew Avenue. Mr. Hoffman said that would be determined by the Council. An assessment could be either a front foot or an area assessment.

Mayor Maetzold asked if the Council should become involved in the exterior finish of the parking ramp and landscaping in light of the Point of France residents' concerns. Mr. Hughes answered that Council could as part of the final development plan approval, specify building materials and to the extent that it pleases the Council to have said materials presented. He added that the City has typically not become involved in the colors, but has requested certain types of materials be used for exterior finishes. Mr. Hughes said that landscaping is also a part of final development plan approval. The Council may specify the type and or size of plantings they would like to see. He reminded the Council that developers must post landscaping bonds to ensure compliance with their approved plans.

Member Hovland asked about Mr. Weninger's concern about storm water from the ramp flowing into Point of France's pond. Mr. Hoffman explained that would not happen, that the storm water would flow into a pipe directed further west along Valley View, leaving less water actually getting to the Point of France pond.

Member Johnson asked if procedurally the Council should become involved in the details as raised this evening when either approving or denying such a request. He added that he still has concern about traffic issues and who is planning to pay for the improvements discussed this evening. Member Johnson indicated that he supported the hospital's expansion project, but wants assurance that the details will be taken care when approval is given.

Mayor Maetzold noted these are very legitimate concerns. He said that perhaps the expansion of the hospital will act as a catalyst starting some necessary improvements in the area. Mayor Maetzold added that he drives France Avenue daily and in his experience at 7:00 a.m., there is not a problem with traffic. However, from 2:30 p.m. and later, gridlock is a problem in the area.

Member Kelly stated he has a number of comments. He thanked the hospital for responding to concerns raised earlier. Elimination of the 65th Street variance and the addition of green space, changing the time of the ramp construction and providing additional ramp design were laudable. He stated he appreciated all the changes that indicate the hospital development team has listened and responded. Member Kelly also stated that he thinks everyone agrees that the hospital is a very important and valuable asset to the community; and expansion should go forward on this site. The question is what expansion should be approved and how should it look. Member Kelly voiced concern over the collateral impact of approving the expansion. He expressed grave concern over how all the pieces fit together and their impact on the entire area of the City. This has been Member Kelly's concern all along. Are the traffic solutions proposed today, solutions that will address continued growth in the particular area and the development that is contemplated from the Crosstown to the Galleria. Member Kelly stated that the Council must be responsible and take a global look at development in that particular area because it has already seen one additional plan come forward. He added that Southdale has been kicking around various plans and he would be reluctant to pursue development in a piecemeal fashion, not considering the collateral effect of such development. Member Kelly said he was deeply concerned that the collateral effect of the development has not been studied. In particular, he said that no a.m. traffic counts had been done. He said that addressing developer costs in the assessment period would be akin to closing the barn door after the horse is gone. In order to adequately address development, you must address how the costs are going to be covered in the

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non state aid areas. Everyone has differing views of what the hospital's fair share of the improvement would be and waiting until the assessment leaves the City open to legal challenges if the hospital determines they are being unduly burdened. However, if an agreement is reached while working out the conditional use permit, the City avoids any further potential disagreement down the line. Member Kelly said that an agreement is needed prior to granting approval, and more detail is also needed on the traffic counts prior to approval. Member Kelly said that he did not want his comments interpreted as meaning that he thought the proposed expansion was a poorly designed landscape plan, or that he does not favor expansion in the area. However, he stated that as discussed earlier; the Council should pause and study the area from the Galleria to the Crosstown, knowing that there will be expansion of medical and retail uses in the area. It should be found out how the pieces best fit together comprehensively. For this reason he would not support approval at this time, what he favors instead is taking some time to really study the area comprehensively. If, however, the Council decides to move forward he urged that they consider in the Conditional Use Permit the material and landscaping approval discussed, and make sure no signage be allowed on the skyway.

Member Faust stated her main concern is the parking ramp and the Point of France residents. She said she believed the plans, as presented, for the parking ramp are very preliminary. The Council has not seen a definitive ramp plan and that should be presented before any approval is granted. Member Faust said that looking at the overall development in the area should have been done twenty years ago. She agreed it will be hard to develop further in this area because of all the traffic issues, but changing development standards now seems counter-productive.

Member Hovland said he believes the core problem is traffic just like in many other parts of the City. However, the hospital is not the generator of most of the traffic. Hindsight says that better planning twenty years ago would leave us with more options. However, the City must do the best it can with what is available in a compressed area. Personally, Member Hovland thinks the hospital has come up with good ideas in a tight area. He likes the parking ramp on the west side of France Avenue with the skyway across to the hospital. He agrees with no signage being allowed on the skyway. Member Hovland stated that conditions should be inserted into the conditional use permit or developer's agreement covering: landscape plan being satisfactory to staff, making sure that lighting is appropriately downcast to protect neighbors, and that the exterior building materials and skyway design be incorporated into the an agreement with the hospital. He said that issues involving state aid, gas tax, or special assessments, could be dealt with subsequent in time to approving the project. Member Hovland asked staff if the entire area of the project could be defined and then the allocation of cost dealt with later, so as not to make it a condition of approving the final development plan or conditional use permit. Mr. Hughes said he thought that separating the improvements made sense because they are all off-site of the hospital campus. They are part of intersections the City controls and the issue later becomes allocation of the cost of the improvements. Mayor Maetzold asked if the cost allocation could be done based upon the number of employees. Mr. Hughes replied he had never heard of that formula, but a close corollary would be assessments levied based upon floor area instead of lot area.

Member Hovland acknowledged the concerns of Point of France and said they could be addressed. However, he questioned whether the City could regulate the structure of the parking ramp foundation. Member Hovland said he believed the Comprehensive Plan has already been put into place with one area of concern, Southdale. He said the quadrant where all the medical buildings are located may in the future become an even more expanded medical facility and maybe that entire expansion would be great news for the entire community. Member Hovland

added that he thought the expansion of the hospital was fantastic, that perhaps Fairview would become a regional medical center. Member Hovland acknowledged traffic is a problem to be resolved. As future developments come forward the Council's job will be to review each project. He expressed concern over the Southdale shopping area. However, if the medical quadrant continues to grow, perhaps an extended stay hotel will develop or some additional shopping capabilities for Southdale. Perhaps the medical quadrant could even become a mini "Mayo". Member Hovland believes that conditions could be placed upon approvals protecting Point of France, which the developer has indicated being amenable to, so he favored moving forward.

Mayor Maetzold agreed with Member Hovland. He also agreed with Member Kelly's concern for France Avenue. Mayor Maetzold supported moving ahead with the hospital expansion at this time. He suggested that Member Kelly's suggestion of more comprehensive plan of the area, however, be pursued with the City's Vision 20/20 planning process.

Member Johnson asked a procedural question about the action required. He wanted to know if a list of conditions could be attached to the approvals. Mayor Maetzold asked staff if the conditions could be contained under the final development plan and conditional use permit. Mr. Hughes responded that the Council should either approve or deny the resolution on the EAW, then conditions with respect to cost of surrounding street system would be best addressed with the final development plan, and any conditions dealing with the ramp should be part of the conditional use permit. Member Kelly asked for clarification from the City Attorney on the best method of handling the cost issues associated with the development. Attorney Boelter said that perhaps traffic issues should be attached to both the final development plan and the conditional use permit.

Member Faust interjected that she wanted to see a parking ramp design, not just the potential materials.

Motion by Member Hovland, seconded by Member Johnson introducing the following resolution and moving its approval:

RESOLUTION NO. 2000-38

CITY OF EDINA

A RESOLUTION APPROVING NEGATIVE DECLARATION

WHEREAS, Fairview Southdale Hospital is the proposer of the project consisting of a 174,000 square foot expansion to the west and north of the existing facility, a four-level, 970-vehicle parking ramp west of France Avenue, and an east/west connection via a skyway, located in the city of Edina (the "City"); and

WHEREAS, the city of Edina is the Responsible Governmental Unit pursuant to Minnesota Rules Part 4410.0500; and

WHEREAS, the EAW was prepared using the form approved by the Minnesota Environmental Quality Board for EAW's in accordance with Minnesota Rules 4410.1300; and

WHEREAS, in accordance with Minnesota Rules 4410.1500, the EAW was completed and distributed on January 31, 2000 to all persons and agencies on the official Environmental Quality Board distribution list; and

WHEREAS, the notification of the EAW was published in the Minnesota EQB Monitor on February 7, 2000; and

WHEREAS, the public review and comment period remained open until March 8, 2000; and

WHEREAS, the comments on the EAW were received and responded to; and

WHEREAS, this decision is made in compliance with the time schedule provided in Minnesota Rules 4410.1700; and

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WHEREAS, the record considered by the City Council for purposes of its decision herein consists of the EAW, related reports and analysis; the comments received thereon, and the responses to such comments, all of which are incorporated herein and made a part of this decision;

NOW, THEREFORE BE IT RESOLVED by the City Council of the City of Edina, acting with respect to the Environmental Assessment Worksheet for the Fairview Southdale Medical Campus Expansion, that it finds and concludes the following:

1. The EAW was prepared in compliance with the procedures of the Minnesota Policy Act and Minnesota Rules, Parts 4410.1000 to 4410.1700; and
2. The EAW satisfactorily addressed all of the issues for which existing information could have been reasonably obtained; and
3. The findings of fact contained in Exhibit A (attached) are made; and
4. Based on criteria established in Minnesota Rules 4410.1700, and the findings of fact, the Project does not have a potential for significant environmental effects; and
5. An Environmental Impact Statement is not required, and the City of Edina therefore makes a "Negative Declaration".

Adopted by the City Council of the City of Edina, Minnesota this 4th day of April, 2000.

Roll call:

Ayes: Faust, Hovland, Johnson, Maetzold

Nay: Kelly

Resolution adopted on a 4-1 vote.

Member Hovland made a motion to approve the final development plan and conditional use permit. Mayor Maetzold seconded the motion.

Motion died.

Member Johnson suggested that conditions be attached to the approval, by either amendment or delay. He stated he wants to see the conditions and wants them well thought out. Member Hovland said that in the past the Council has crafted the conditions at the final hearing. Member Kelly stated he did not see how the cost issues can be addressed at this time and that he had previously pointed out the flaws of moving to a special assessment improvement. Member Hovland asked Members Kelly and Johnson how they would suggest an agreement be reached with the hospital on the cost allocation. Member Kelly said that after the costs are known; a reasonable apportionment be mutually agreed upon with all concerned parties. If this is not done, during the assessment process the City is open to legal challenges. Member Hovland asked why not use the special assessment process to ascertain the allocation. He said he felt the approval should be moved forward and let the special assessment process take care of the allocation of the improvement costs. Otherwise, in his opinion there would be an unfair delay to the hospital. He stated that he felt it would be more fair to use the due process of a special assessment project. Member Kelly disagreed, stating that taxpayers could be subjected to additional expenses of legal defense of an assessment. Mayor Maetzold stated he agreed with Member Hovland that the special assessment process should be allowed to take care of the cost allocation. If approval is delayed two weeks there would still be others who should have a share of the cost that would not be able to be brought into the mix. In Mayor Maetzold's opinion following a special assessment process would be the most equitable way to proceed. Member Kelly stated he was not advocating the hospital pay more than a fair share, just that the share be decided before any approval. Member Johnson noted that Mr. Enger had stated earlier he would be willing to work with the City in paying a fair portion of the needed improvements. He suggested that City staff work with the hospital to achieve an agreement for cost apportionment. Member Johnson said that he would be prepared to vote on the expansion at the next Council

meeting if something can be agreed to between the City and the hospital. Member Faust asked that the Drew Avenue sidewalk be included in the discussion.

Member Kelly made a motion continuing action until April 18, 2000, on the final development plan and conditional use permit request of Fairview Southdale Hospital and directing staff: to negotiate a cost sharing agreement with respect to the improvements at Valley View Road and West 65th Street, and Drew Avenue; to provide additional details as to the ramp design; and to develop the conditions necessary for said approvals. Member Faust seconded the motion.

Member Johnson stated he would support the motion, however, he did not want to dictate materials, etc. to the hospital. Member Hovland noted that a month ago when he saw this plan he was very concerned about costs. His first impression was that the hospital should pay all the costs. However, upon further reflection, he decided using the special assessment method would be better.

Mr. Enger asked that Southdale Medical Center be made a part of any cost sharing negotiation involving the proposed improvements. Mayor Maetzold told Mr. Enger that this would be decided upon by City staff.

Ayes: Faust, Johnson, Kelly

Nay: Hovland, Maetzold

Motion carried on a 3-2 vote.

*RESOLUTION NO. 2000-39 LOT DIVISION APPROVED FOR 5000 AND 5004 KELSEY TERRACE (CARL HANSEN) Motion made by Member Johnson and seconded by Member Hovland approving the following resolution:

RESOLUTION 2000-39

WHEREAS, the following described properties are at present single tracts of land:

Lots 18 and 19, Block 2, PARKWOOD KNOLLS 24TH ADDITION, according to the recorded plat thereof, situated in Hennepin County, Minnesota.

WHEREAS, the owner has requested the subdivision as follows:

PARCEL A:

That part of Lot 18, Block 2, PARKWOOD KNOLLS 24TH ADDITION, according to the recorded plat thereof and situated in Hennepin County, Minnesota, lying north of the south 10.59 feet thereof.

PARCEL B:

Lot 19, and the south 10.59 feet of Lot 18, Block 2, PARKWOOD KNOLLS 24TH ADDITION, according to the recorded plat thereof and situated in Hennepin County, Minnesota.

WHEREAS, the requested subdivision is authorized under Code Section 810 and it has been determined that compliance with the Subdivision and Zoning Regulations of the City of Edina will create an unnecessary hardship and said newly created Parcels as separate tracts of land do not interfere with the purpose of the Subdivision and Zoning Regulations as contained in the City of Edina Code Sections 810 and 850;

NOW THEREFORE, it is hereby resolved by the City Council of the City of Edina that the conveyance and ownership of the above described tracts of land (PARCEL A and PARCEL B) as separate tracts of land are hereby approved and the requirements and provisions of Code Sections 850 and 810 are hereby waived to allow said division and conveyance thereof as separate tracts of land but only to the extent permitted under Code Sections 810 and 850

WHEREAS, the Edina City Council on the 4th of April, 2000, fixed a date for a Council hearing on the proposed street reconstruction, Improvement No. A-189, and

WHEREAS, ten days' mailed notice and two weeks' published notice of the hearing was given, and the hearing was held thereon on the 2nd day of May, 2000, for Improvement No. A-189 resurfacing School Road from West 60th Street to Concord Avenue, Ruth Drive, and West 59th Street, at which time all persons desiring to be heard were given an opportunity to be heard thereon.

NOW, THEREFORE, BE IT RESOLVED that the Council has duly considered the views of all persons interested, and being fully advised of the pertinent facts, does hereby determine to proceed with the construction of said improvement, including all proceedings which may be necessary in eminent domain for the acquisition of necessary easements and rights hereby designated and shall be referred to in all subsequent proceedings as Improvement Project No. A-189 resurfacing School Road from West 60th Street to Concord Avenue, Ruth Drive, and West 59th Street.

BE IT FURTHER RESOLVED Improvement Project No. A-189 resurfacing School Road from West 60th Street to Concord Avenue, Ruth Drive, and West 59th Street is hereby ordered as proposed, pending further review of the Ruth Drive and West 59th Street portion of the improvement, after bids are received.

BE IT FURTHER RESOLVED that the City Engineer is hereby designated as the engineer for this improvement. The engineer shall cause plans and specifications to be prepared for the making of such improvement.

BE IT FURTHER RESOLVED, that the City Engineer is hereby authorized to advertise for bids and receive same for the aforementioned improvements, noting that alternate bids are to be received showing West 59th Street and Ruth Drive separately from School Road.

Passed and adopted this 2nd day of May, 2000.

Member Hovland seconded the motion.

Rollcall:

Ayes: Faust, Hovland, Johnson, Kelly, Maetzold

Motion carried.

RESOLUTION 2000-53 ADOPTED APPROVING FINAL DEVELOPMENT PLAN FOR FAIRVIEW SOUTHDAL E HOSPITAL (6401 FRANCE AVENUE SOUTH) BUILDING EXPANSION AND PARKING RAMP AND RESOLUTION 2000-54 ADOPTED APPROVING A CONDITIONAL USE PERMIT FOR PARKING STRUCTURE Affidavits of Notice were presented, approved and ordered placed on file.

Planner Larsen said that after the April 4, 2000, public hearing Fairview Hospital responded to the concerns raised by the Council by:

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- BRW, Inc. conducted a.m. traffic counts in order to compare them to the p.m. counts used for their original study. The counts demonstrated that p.m. counts were higher than a.m. counts and were appropriate to use for the study.
- Submitted a revised landscaping plan illustrating additional and larger stock than what had been offered in writing at the April 4, 2000, meeting.
- Project architects have done additional work on design and materials for the proposed ramp.
- Two phases for road improvements.
 - ◆ Phase 1 would be a widening of Drew Avenue to accommodate three traffic lanes and a six-foot sidewalk on the west side of the street between 65th and 66th Street. The sidewalk would be pavers to tie into the 66th Street sidewalk. Southdale Medical's owners have agreed to grant the necessary sidewalk easement. Funding for Phase 1 would be 100% through special assessment to the hospital, Southdale Medical, and the medical office building at 6363 France Avenue. The assessment would be allocated on a floor area basis. The estimated project cost is \$155,000.00
 - ◆ Phase 2 would be the reconstruction of West 65th Street west of France Avenue and the reconstruction of the intersection of Valley View Road and West 65th Street. The project would include an expansion of the ramp, addition of signals, reconstruction of the intersection and rehabilitation of the pavement. The West 65th portion of the improvement is not a municipal state aid street so it would be funded by a 100% assessment. This portion of the project is estimated at \$194,000. The remainder of proposed improvements are estimated at \$950,000.00 with approximately \$712,500.00 eligible for state aid funding leaving an additional \$237,500.00 to be assessed. The hospital, Southdale Medical, the medical office building at 6363 France Avenue, and all properties abutting West 65th Street between France Avenue and Valley View Road would be included in the assessment area for this improvement.
- Fairview Southdale Hospital and the owner of the medical office building at 7373 France Avenue have agreed to waive their right of appeal for any special assessment that does not exceed the Engineer's estimate, retaining the right to appeal any portion of an assessment exceeding the same.

Member Johnson asked why only the hospital was used in calculating the floor area, not the parking structure. Mr. Hughes explained that the City views the parking structure as an accessory building to the hospital. For this reason the parking structure was not used in calculating the special assessments the hospital would be responsible for.

Mr. Larsen reviewed the suggested conditions for approval of both the Final Development Plan and the Conditional Use Permit as follows:

Final Development Plan Conditions:

1. Watershed District Permits.
2. Landscaping Plan dated, 4/20/00 and bond.
3. Exterior materials shall be brick, marble and glass matching materials on hospital.

4. Agreement by hospital and the owners of 6363 France Avenue to waive their right of appeal of special assessments for off-site improvements subject to term of the agreement signed by all parties and on file at City Hall.

Conditional Use Permit Conditions:

1. Design and materials detailed on ramp plan as presented May 2, 2000 to City Council.
2. Landscaping Plan dated, 4/20/00 and bond.
3. All lighting in parking structure and on the site containing the structure shall be shaded so as to concentrate illumination on-site and within the structure.
4. No signage on the skyway.

Proponent Presentation

Richard Dahl, BWBR Architects, reviewed the revised plans, using graphics and actual samples of the materials intended for use in the ramp. Mr. Dahl explained the stair tower curtain wall will be clear anodized aluminum and green tinted glazing. The elevator tower will be brick, matching the hospital. This will be covered by a clear anodized aluminum fascia and soffit. The ramp will consist of poured in place concrete walls with sand textured acrylic coating. The wall adjacent to the brick elevator tower and curtain wall will be light taupe with the span of the building a combination of light and dark taupe. The columns will be cast in place concrete painted a dark earth tone. Mr. Dahl also reviewed the proposed landscaping and berm.

Member Faust expressed her disappointment with the design as proposed. She stated she had specifically asked that the parking structure design complement the existing and proposed hospital. Member Faust stated that in her opinion the designers had not done as requested.

Member Hovland and Mayor Maetzold disagreed with Member Faust. They stated that in their opinions and experience the proposed design was a good attempt at blending the new hospital addition and ramp with existing buildings and designs.

Member Kelly stated in his opinion the hospital proposal is an over building of the site and he still would have liked to seen a more comprehensive study of the area before proceeding. However, he believes that the hospital is a good citizen and good for the community.

Member Hovland introduced the following resolution and moved its adoption:

RESOLUTION 2000-53

APPROVING FINAL DEVELOPMENT PLAN

BE IT RESOLVED by the Edina City Council that the Final Development Plan revision dated May 2, 2000, for Fairview Southdale Medical Center, Fairview Health Services, a Minnesota nonprofit corporation, located at 6401 France Avenue presented at the regular meeting of the City Council on May 2, 2000, be and is hereby approved conditioned upon:

1. Watershed District Permits.
2. Landscaping Plan dated, 4/20/00 and bond.
3. Exterior materials shall be brick, marble and glass matching materials on hospital.
4. Agreement by hospital and the owners of 6363 France Avenue to waive their right of appeal of special assessments for off-site improvements subject to terms of the agreement signed by all parties and on file at City Hall.

Member Johnson seconded the motion.

Minutes/Edina City Council/May 2, 2000

Rollcall:

Ayes: Hovland, Johnson, Kelly, Maetzold

Nay: Faust

Motion carried.

Member Johnson made a motion introducing the following resolution and moving its adoption:

**RESOLUTION NO. 2000-54
GRANTING CONDITIONAL USE PERMIT
TO FAIRVIEW HEALTH SERVICES
PARKING STRUCTURE**

WHEREAS, the procedural requirements of Code Section 850 (the Zoning Ordinance) have been met; and

WHEREAS, it has been determined that the Findings as required by Code Section No. 850.04 Subd. 4 have been satisfied:

NOW, THEREFORE, BE IT RESOLVED that the Edina City Council hereby grants a Conditional Use Permit to Fairview Health Services conditioned upon the following:

1. Design and materials detailed on ramp plan as presented May 2, 2000 to City Council.
2. Landscaping Plan dated, 4/20/00 and bond.
3. All lighting in parking structure and on the site containing the structure shall be shaded so as to concentrate illumination on-site and within the structure.
4. No signage on the skyway.

Passed and adopted this 2nd day of May, 2000.

Member Hovland seconded the motion.

Rollcall:

Ayes: Hovland, Johnson, Kelly, Maetzold

Nay: Faust

Motion carried.

~~*PRELIMINARY REZONING AND OVERALL DEVELOPMENT PLAN - OPUS/CLARK, GRANDVIEW SQUARE CONTINUED TO MAY 16, 2000 Motion made by Member Johnson and seconded by Member Hovland continuing Preliminary Rezoning and Overall Development Plan for Grandview Square, Ron Clark/Opus Development until May 16, 2000.~~

~~Motion carried on rollcall vote - five ayes.~~

Mayor Maetzold excused himself from the remainder of the meeting at 8:05 P.M.

~~**ACTION TABLED UNTIL MAY 16, 2000, ON ORDINANCE NO 2000-05 - AMENDING SECTION 450 - SWIMMING POOLS** Sanitarian Velde explained the reason for the proposed ordinance amendment was to bring Edina's code into compliance with state statute. The amendment would address three items in the Swimming Pool Code: spa pool deck elevations, access barriers, and chlorine concentration.~~

~~Mr. Velde stated the first section would allow the required deck to be lower than the top rim of a spa pool. Spa pools are typical 30 to 34 inches in height and no larger than 100 square feet in area. Mr. Velde said staff believed this relation would not interfere with live saving efforts. The~~

Engineering Department • Phone 952-826-0371
Fax 952-826-0392 • www.CityofEdina.com



Date: May 17, 2013
To: Cary Teague – Community Development Director
From: Wayne Houle – Director of Engineering
Re: Fairview Southdale Emergency Center Expansion
Dated April 24, 2013

Engineering has reviewed the above stated proposed plan and offer the following comments:

- ⊙ A Minnehaha Creek Watershed permit will be required, along with other agency permits such as, MNDH, MPCA, MCES.
- ⊙ An agreement should be drafted and executed regarding maintenance of the water system including fire hydrants.
- ⊙ Replace all hydrants on property to new standard.

Boundary Survey:

- The boundary survey shows many easements located within existing building footprint. The easements should be vacated and or modified.
- Show watermain along east side of hospital.

Civil Sheet C303:

- Provide fire-truck turning radius for roundabout.

Civil Sheet C500:

- Inspect sanitary sewer line below parking ramp.
- Provide inflow and infiltration study of parking ramp.

Staff will require a more detail review of the Civil Plans if this project is approved by the City Council. Please contact me if you have any questions regarding this review.

A115

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Jackie Hoogenakker

From: Brandi Westbrook <brandi.granlund@yahoo.com>
Sent: Tuesday, May 14, 2013 6:56 PM
To: Jackie Hoogenakker
Cc: brandi.granlund@yahoo.com
Subject: Case File 2013.011 Site Plan Review

RE: Case File 2013.011
Logal Description: RLS No. 1716

Site Plan Review - Fairview Southdale Hospital

I received a letter recently with a description stating possible site plan development for properties owners within 1000 feet. The expansion includes an 89,000 square foot emergency expansion to the north side of the existing hospital. I personally own property at 6312 Barrie Road. I am NOT opposed to the expansion of the hospital if I am paid fairly for my property. If this is the case, I would be in approval for this Fairview Southdale Hospital expansion.

Thank you,
Brandi Granlund - Westbrook