



PLANNING COMMISSION STAFF REPORT

Originator Cary Teague Community Development Director	Meeting Date May 22, 2013	Agenda # VI.B.
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INFORMATION/BACKGROUND

Project Description

Lund Food Holdings is requesting final review for a proposal to tear down the existing 59,000 Byerly's grocery store, located at 7171 France Avenue and build the following:

Phase 1

- A new 47,119 square foot Byerly's store.
- A six/seven-story 109-unit apartment building with two levels of underground parking. (Building A.)

Phase 2

- A six/seven-story, 77-unit apartment building with a first floor 10,711 square foot retail area and two levels of underground parking. (Building B.)
 - A six-story, 48-unit apartment building with 11,162 square feet of retail space on the first level and two levels of underground parking. (Building C.)
- (See site location on pages A1–A10)

In building the first phase, the new Byerly's store would be constructed in the parking lot of the existing store at the northwest corner of the site, and Building A would be constructed in the southeast corner of the site. The existing store would remain open. When the new store is finished the existing store would be removed, and then Buildings B & C would be constructed. (See the applicant's narrative on pages A14a–A14k; the site plan and building renderings on pages A15–A28.)

This request has already received the following approvals from the City Council:

- Preliminary Rezoning from PCD-3, Planned Commercial District to PUD, Planned Unit Development;
- Preliminary Development Plan; and
- Preliminary Plat

(See attached Preliminary Development Plans on pages A11–A14.)

There were 18 conditions attached to the preliminary approvals. The applicant has addressed each of those conditions, and how they meet those conditions in an addendum to their narrative on pages A14e–A14k.)

The following is therefore requested for this final review:

- Final Rezoning from PCD-3, Planned Commercial District to PUD, Planned Unit Development; (Including a Zoning Ordinance Amendment to establish the PUD Zoning District.)
- Final Development Plan; and
- Final Plat

SUPPORTING INFORMATION

Surrounding Land Uses

- Northerly: Rue De France retail center; zoned PCD-3, Planned Commercial District and guided Mixed Use.
- Easterly: Vacant restaurant; zoned PCD-3, Planned Commercial District and Mixed Use.
- Southerly: Macy's Home Store; zoned PCD-3, Planned Commercial District and Mixed Use.
- Westerly: Sunrise senior living; zoned PSR-4, Planned Senior Residential and guided Office/Residential.

Existing Site Features

The subject property is 9.67 acres in size, is relatively flat and contains Byerly's Grocery Store with a large parking lot in front. (See pages A3–A4.)

Planning

- Guide Plan designation: MXC – Mixed Use Center.
- Zoning: PCD-3, Planned Commercial District (See page A5.)

Site Circulation

Access to the site would be from France Avenue and Hazelton Road. A new right-in and right-out would be added at the south lot line on France Avenue. (See page A15.) Hennepin County is agreeable to this new access point. (See County approval letter on page A14k.)

There are two existing access points on Hazelton Road. These would be slightly adjusted. A future round-a-bout is planned for the easternmost access point, and has been realigned to match the access to the north. The property owner to the west is now agreeable to this arrangement. (See page A16.) The westernmost access on Hazelton Road would be a right-in and right-out only.

As was approved as part of the Preliminary Development Plan, extensive pedestrian paths are planned for the site. Along France Avenue and Hazelton Road, a boulevard style sidewalk would be constructed with green space separating them from traffic. An interior sidewalk is planned from the France Avenue sidewalk, to the east in front of the Byerly's store, to the south side of Building C, and east to connect to the Promenade. (See page A15.) An additional connection to the Promenade is available from Building A, along the south lot line. Sidewalks are proposed around each of the housing buildings to provide pedestrian connections to the France and Hazelton Road sidewalks and the Promenade. A covered walkway is proposed in the Byerly's parking lot. (See page A15, A16 and A18.)

Traffic & Parking Study

RLK Incorporated is conducting a parking and traffic study. (See the attached study dated January 16, 2013.) The Study concludes that the proposed development could be supported by the existing roads subject to conditions.

The proposed new right-in and right-out on France Avenue would greatly benefit traffic operations around the site. The level of service on adjacent roadways and intersections would remain the same. The Study further concludes that the project would have adequate parking.

Based on the proposal for 68,992 square feet of retail and 234 apartment units, 362 spaces are required for the retail, 19 bike parking stalls, and 234 enclosed units for the apartments. The applicant is proposing 307 spaces for the retail space, 351 enclosed and 23 surface stalls for the apartments. There are 126 enclosed bike racks in the apartment building, and a total of 24 bike racks show at the entrances of each retail building. A shared parking arrangement between all the lots for parking and drive aisle access would be established by a blanket easement over the properties. A parking study was done for the proposed project, which has concluded that the amount of parking provided would adequately serve the proposed development. (See the attached parking study by RLK.)

Bike Stalls

Based on the square footage of the retail space, 19 spaces for bikes is required. The applicant is proposing 24 spaces to be located in three bike

racks with 8 stalls available in each rack. They are conveniently located in front of each of the three retail buildings. (See page A15.)

Landscaping

Based on the perimeter of the site, the applicant is required to have 68 over story trees and a full complement of under story shrubs. The applicant is proposing 98 over story trees. They would include a mixture of Maple, Lindens, Oak, Crabapple, Birch and Spruce. (See pages A39–A40.) A full complement of understory landscaping is proposed around the buildings. The proposal complies with the City’s landscaping regulations. Additionally, per a condition of preliminary approval, the applicant has enhanced the landscaping at the southeast corner of the site by addition Black Hills Spruce and Juniper.

Loading Dock/Trash Enclosures

The proposed plans would locate the loading area on the east side of the building along Hazelton Road. (See proposed location on page A15.) This is similar to the arrangement of the loading dock for the recently construction Whole Foods to the south on France Avenue. The loading area would be screened from Hazelton with a brick wall that would match the building. (See page A15, A18 & A39.) Additional landscaping is also proposed along Hazelton to screen the loading area from the Hazelton and adjacent property. This is an improvement over the current location of the loading dock facing the Promenade, and the plans reviewed during the Sketch Plan, which were also facing the Promenade. (See previous Sketch Plan on page A7 & A8.) Loading for the retail space would take place through the main entrances. Trash areas would be located inside of the retail buildings B and C. The Byerly’s trash would be located within the loading dock area which would be screened as described above.

Grading/Drainage/Utilities

The city engineer has reviewed the proposed plans and found them to be generally acceptable subject to the comments and conditions outlined on the attached page A99-A100. A developer’s agreement would be required for the construction of the proposed sidewalks, public water main, sewer and any other public improvements. Any approvals of this project would be subject to review and approval of the Nine Mile Creek Watershed District, as they are the City’s review authority over the grading of the site. It should be mentioned that the water feature, as recommended in the Promenade Plan, is not specifically designed in the grading plan. (See Promenade Plan on pages A9–A10.) The Promenade Plan ponding area is located on a high point on the site, and therefore, is difficult to get drainage to flow naturally to that spot. The applicant is indicating that roof storm water could be routed to the area, with

the City of Edina constructing the water feature. (See page A14b of the applicant narrative.)

Lighting

The applicant has indicated that all site lighting would conform to City Code requirements. Pole lights, 25 feet tall would be used to light the parking lot areas, and 14 foot tall decorative fixtures would light pedestrian walkways. All exterior lighting and illuminating devices would include lenses, reflectors or shades so as to limit light spill and glare on to adjacent properties. The proposed plans conform to all minimum City Code requirements for lighting.

Building/Building Material

The proposed building plans and materials are consistent with the Preliminary Plans. The Byerly's store would be constructed of brick on all four sides, with a stone base and concrete panels. (See rendering on pages A16–A29.) The apartment buildings would be constructed of brick, stucco, metal-panel, and glass. The applicant believes that the mass of the building walls, in particular the west and north elevations, have been broken up by the variety of brick, concrete, windows and landscaping proposed.

Signage

The underlying zoning of the property would be PCD-3, therefore, would be subject to signage requirements of that zoning district. The applicant has submitted a signage plan to approve for the overall development. The locations and size of pylon signs, wall signs and way finding signs are included in the PUD regulations for this site. This site is unique as a unified development, with four separate lots, four buildings, including three separate retail and three separate residential buildings. Site way finding will be important.

The proposed signs generally meet the City Code requirements in regard to size; however, due to the mixed use of the property, with multiple lots, additional signs are allowed through the PUD to assist with way finding on the site. Those include the "71 France" signs on both France and Hazelton, and the monument sign facing the Promenade. (See proposed Ordinance on pages A56- A58 and the plans on page A15 and A28.) A sign proposed that would be too large under the PCD-3 Zoning District is the freestanding sign on Hazelton Road, near the entrance to the apartments. As proposed that sign is 100 square feet; zoning ordinance maximum is 50 square feet for a secondary free standing sign off a side street. The applicant believes that 100 square foot sign is critical for the retail portion of the site for way finding, given the three separate commercial buildings.

Density

The proposed density would be 24 units per acre would be toward the lower middle end of the density range for the City's high density residential development as indicated in the table on the following page.

Development	Address	Units	Units Per Acre
Yorktown Continental	7151 York	264	45
The Durham	7201 York	264	46
York Plaza Condos	7200-20 York	260	34
York Plaza Apartments	7240-60 York	260	29
Edina Place Apartments	7300-50 York	139	15
Walker Elder Suites	7400 York	72	40
7500 York Cooperative	7500 York	416	36
Edinborough Condos	76xx York	392	36
South Haven	3400 Parklawn	100	42
69 th & York Apartments	3121 69 th Street	114	30

Final Plat

The applicant is also requesting a Plat to create separate lots for each of the proposed buildings. (See the plat on page A33.) The site exists as two lots today; the proposed subdivision would create four lots. The subdivision would meet all minimum lot standards and subdivision requirements. Shared parking and drive-aisle agreements would need to be established across the four lots. Preliminary Plat approval has already been granted

Park Dedication

Per Minnesota State Statute 462.353, Subd. 4(a) and Section 810.13 of the City Code, the applicant is required to dedicate land for public use as parks, playgrounds, recreation facilities, trails, or public open space.

Per Section 810.13. Subd. 5 of the City Code, the fees in lieu of land dedication is \$5,000 per dwelling unit. The development would create 234 new dwelling units; therefore, a \$1,170,000 parking dedication would be required. A portion of the park dedication funds would be used to develop the

water feature, and maintain an easement over it. The value of the easement would be deducted from the park dedication requirement.

The park dedication fees would be paid prior to issuance of a building permit per each unit.

Planned Unit Development (PUD)

Per Section 850.04. Subd. 4 D provides the following regulations for a PUD:

- 1. Purpose and Intent. The purpose of the PUD District is to provide comprehensive procedures and standards intended to allow more creativity and flexibility in site plan design than would be possible under a conventional zoning district. The decision to zone property to PUD is a public policy decision for the City Council to make in its legislative capacity. The purpose and intent of a PUD is to include most or all of the following:**
 - a. provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan;**
 - b. promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City;**
 - c. provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian oriented design, and podium height at a street or transition to residential neighborhoods, parks or other sensitive uses;**
 - d. ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;**
 - e. maintain or improve the efficiency of public streets and utilities;**

- f. preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;***
- g. allow for mixing of land uses within a development;***
- h. encourage a variety of housing types including affordable housing; and***
- i. ensure the establishment of appropriate transitions between differing land uses.***

The proposal would meet the purpose and intent of the PUD, as most of the above criteria would be met. The site is guided in the Comprehensive Plan as "Mixed Use Center – MXC," which encourages a mixing of uses, including retail and multifamily residential.

The proposal would create a more efficient and creative use of the property. The Byerly's store would be pulled up close to the street, with sidewalks in front, and separated from the street by green space to promote a more walkable environment. A patio is proposed along France at the front of Byerly's, and the front walkway in front of the building would be covered with a canopy. (See page A15.) The store configuration is similar to the recently constructed Whole Foods, except that there is an entry into the store closer to France Avenue. (See page A15 & A20.)

Pedestrian connections would be made from France Avenue to the Promenade from the north and south sides of the site, as well as through the middle. (See sidewalk plan on page A15.)

The applicant is also proposing some sustainability principles within their project narrative. (See pages A14c–A14d.) There was concern raised by the Planning Commission in regard to sustainability of the project, and how it would be monitored in the future. The store and housing projects would participate in the Energy Design Assistance program through Xcel Energy, and a wide array of sustainable measures will be integrated into the project. The development team is committed to an outcome for both the store and housing component that would exceed code-driven energy efficiency standards by a minimum of 5%.

The proposed buildings would be a high quality brick, stone, precast concrete, metal and glass building. (See pages A16–A29.)

The site circulation would be improved on the site with a right-in and right-out added along France Avenue.

2. Applicability/Criteria

- a. Uses. All permitted uses, permitted accessory uses, conditional uses, and uses allowed by administrative permit contained in the various zoning districts defined in Section 850 of this Title shall be treated as potentially allowable uses within a PUD district, provided they would be allowable on the site under the Comprehensive Plan. Property currently zoned R-1, R-2 and PRD-1 shall not be eligible for a PUD.**

The proposed uses, retail and multiple-family residential housing are uses allowed in the Mixed Use Center, as described in the Comprehensive Plan, and within the underlying PCD-3 Zoning District.

- b. Eligibility Standards. To be eligible for a PUD district, all development should be in compliance with the following:**
- i. where the site of a proposed PUD is designated for more than one (1) land use in the Comprehensive Plan, the City may require that the PUD include all the land uses so designated or such combination of the designated uses as the City Council shall deem appropriate to achieve the purposes of this ordinance and the Comprehensive Plan;**

The site is guided in the Comprehensive Plan for "Mixed Use Center – MXC," which encourages the mixing of retail and multi-family residential uses. The proposed plans are therefore, consistent with the Comprehensive Plan.

- ii. any PUD which involves a single land use type or housing type may be permitted provided that it is otherwise consistent with the objectives of this ordinance and the Comprehensive Plan;**

Again, the proposal is for a mixture of land uses.

- iii. permitted densities may be specifically stated in the appropriate planned development designation and shall be in general conformance with the Comprehensive Plan; and**

As indicated in table earlier within this report, and the fact that the site is located in a commercial area on France Avenue and the Promenade, the proposed density of 24 units per acre is appropriate for this site.

iv. the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent described in #1 above.

The following page shows a compliance table demonstrating how the proposed new building would comply with the underlying PCD-3 Zoning Ordinance Standards. Should the City decide to rezone this site to PUD, the proposed setbacks, height of the building and number of parking stalls would become the standards for the lots. Please note that a few City Standards are not met under conventional zoning. However, by relaxing these standards, the purpose and intent, as described in #1 above would be met.

The site layout would be improved by bringing the building up to the street; pedestrian connections would be incorporated through and around the site, including green space between the road and the sidewalk; locating the taller buildings farther back from France Avenue, into the site; introducing a mixture of uses on the site.

The design of the building is of a high quality. Byerly's would be brick on all four sides, with a stone base and concrete panels. (See rendering on pages A16–A29.) The apartment buildings would be constructed of brick, stucco, metal panels, and large windows. The development would incorporate improved landscaping and green space. Traffic would be improved in the area with a right-in and right-out on France Avenue.

Compliance Table

	City Standard (PCD-3)	Proposed
<u>Building Setbacks</u>		
<i>Byerly's</i>		
Front – France Avenue	50 feet	51 feet
Side Street – Hazelton Road	50 feet	24 feet*
Side – South	NA	200+ feet
Rear – East (Promenade)	NA	200+ feet
<i>Building A</i>		
Front – France Avenue	81 feet	500+ feet
Side Street – Hazelton Road	81 feet	300+ feet
Side – South	NA	11 & 41 feet
Rear – East (Promenade)	81 feet	11 & 22 feet*
<i>Building B</i>		
Front – France Avenue	85 feet	300+ feet
Side Street – Hazelton Road	85 feet	200+ feet
Side – South	NA	NA
Rear – East (Promenade)	85 feet	200+ feet
<i>Building C</i>		
Front – France Avenue	79 feet	300+ feet
Side Street – Hazelton Road	79 feet	43 feet*
Side – South	NA	NA
Rear – East	79 feet	46 feet*
<u>Parking Lot Setbacks</u>		
Front – France Avenue	20 feet	51 feet
Front – Hazelton Road	20 feet	20 feet
Side – South & East	NA	NA
Building Height		
Byerly's		One story – 21 feet
Building A	Eight stories or 108 feet	Six/seven stories – 81 feet
Building B		Six/seven stories – 85 feet
Building C		Six stories – 79 feet
Maximum Floor Area Ratio (FAR)	50% Lot size = 9.67 acres	80%* 338,164 s.f.
Parking Stalls		
Retail	68,992 s.f. = 357 spaces	307 spaces retail*
Housing	234 units = 234 enclosed units	351 enclosed & 23 surface
Parking Stall Size	8.5' x 18'	8.5 x 18'
Drive Aisle Width	24 feet	24 feet

* **Would require a variance under PCD-3 Zoning**

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issues

- **Is the proposed development reasonable for this site?**

Yes. Staff believes the proposal is reasonable for the following reasons:

1. The proposed uses would fit in to the neighborhood. As mentioned, this site is guided in the Comprehensive Plan for "Mixed Use Center – MXC," which encourages mixing land uses, including retail and multiple family residential, on one site.
2. The proposed six/seven-story buildings are one-two stories lower than the height allowed by City Code, and would fit into this area. They are consistent with some of the high density residential development to the east and south.
3. The existing roadways would support the project. RLK conducted a traffic impact study, and concluded that the proposed development could be supported by the existing roads subject to conditions. The proposed new right-in and right-out on France Avenue would greatly benefit traffic operations around the site. The level of service on adjacent roadways and intersections would remain the same.
4. The proposed project would meet the following goals and policies of the Comprehensive Plan:
 - a. Building Placement and Design. Where appropriate, building facades should form a consistent street wall that helps to define the street and enhance the pedestrian environment. On existing auto-oriented development sites, encourage placement of liner buildings close to the street to encourage pedestrian movement.
 - Locate prominent buildings to visually define corners and screen parking lots.
 - Encourage or require placement of surface parking to the rear or side of buildings, rather than between buildings and the street.
 - b. Movement Patterns.
 - Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
 - A Pedestrian-Friendly Environment. Improving the auto-oriented design pattern discussed above under "Issues" will call for

guidelines that change the relationship between parking, pedestrian movement and building placement.

- c. Appropriate Parking Standards. Mixed use developments often produce an internal capture rate. This refers to residents and workers who obtain goods and services from within the development without making additional vehicle trips. Parking ratios for mixed use development should reflect the internal capture rate and the shared parking opportunities this type of development offers.
- d. Encourage infill/redevelopment opportunities that optimize use of city infrastructure and that complement area, neighborhood, and/or corridor context and character.

- **Is the PUD Zoning District appropriate for the site?**

Yes. Staff believes that the PUD is appropriate for the site. As highlighted above on pages 6-10, the proposal meets the City's criteria for PUD zoning. In summary the PUD zoning would:

1. The site is guided in the Comprehensive Plan as "Mixed Use Center – MXC," which encourages a mixing of uses, including retail and multifamily residential.
2. The proposal would create a more efficient and creative use of the property. The Byerly's store would be pulled up close to the street, with sidewalks in front, and separated from the street by green space to promote a more walkable environment. A patio is proposed along France at the front of Byerly's, and the front walkway in front of the building would be covered with a canopy. (See page A32.) The store configuration is similar to the recently constructed Whole Foods, except that there is an entry into the store closer to France Avenue. (See page A43 & A52.)
3. Pedestrian connections would be made from France Avenue to the Promenade from the north and south sides of the site, as well as through the middle. (See sidewalk plan on pages A43 & A45.)
4. The store and housing projects would participate in the Energy Design Assistance program through Xcel Energy, and a wide array of sustainable measures will be integrated into the project. (See pages A14c–A14d.) The development team is committed to an outcome for both the store and housing component that would exceed code-driven energy efficiency standards by a minimum of 5%.

5. The proposed buildings would be a high quality brick, stone, precast concrete, metal and glass building. (See pages A16–A29.)
 6. The site circulation would be improved with a right-in and right-out added along France Avenue.
 7. Ensure that the building proposed would be the only building built on the site, unless an amendment to the PUD is approved by City Council.
- **Are the Proposed Plans consistent with the approved Preliminary Development Plans?**

Yes. The proposed plans are consistent with the Preliminary Plans. Buildings are all in the same location, pedestrian connections are the same, building materials and landscaping are also the same. Additionally, the applicant has met each of the 18 conditions that were required in the Preliminary Development Plan review. (See pages A14e-A14k.)

Staff Recommendation

Final Rezoning to PUD & Final Development Plan

Recommend that the City Council approve the Final Rezoning from PCD-3, Planned Commercial District to PUD, and Final Development Plan to build a new 47,119 square foot Byerly's store; a 6/7-story 109-unit apartment building with two levels of underground parking; a 6/7-story, 77-unit apartment building with first floor 10,711 square foot retail area and two levels of underground parking; and a six-story, 48-unit apartment building with 11,162 square feet of retail space on the first level and two levels of underground parking.

Approval is subject to the following findings:

1. The proposed land uses are consistent with the Comprehensive Plan.
2. The proposal would meet the purpose and intent of the PUD, as most of the above criteria would be met. The site is guided in the Comprehensive Plan as "Mixed Use Center – MXC," which encourages a mixing of uses, including retail and multifamily residential. The proposed uses are therefore consistent with the Comprehensive Plan.
4. The proposal would create a more efficient and creative use of the property. The Byerly's store would be pulled up close to the street, with sidewalks in front, and separated from the street by green space to promote a more walkable environment.

5. Pedestrian connections would be made from France Avenue to the Promenade from the north and south sides of the site, as well as through the middle.
6. The applicant is also proposing sustainability principles within their project narrative, and pledges an energy efficiency standard that would exceed the code driven energy efficiency standard by a minimum of 5%.
7. The proposed buildings would be a high quality brick, stone, precast concrete, metal and glass building.
8. The site circulation would be improved with a right-in and right-out added along France Avenue.
9. The proposed project would meet the following goals and policies of the Comprehensive Plan:
 - a. Building Placement and Design. Where appropriate, building facades should form a consistent street wall that helps to define the street and enhance the pedestrian environment. On existing auto-oriented development sites, encourage placement of liner buildings close to the street to encourage pedestrian movement.
 - Locate prominent buildings to visually define corners and screen parking lots.
 - Encourage or require placement of surface parking to the rear or side of buildings, rather than between buildings and the street.
 - b. Movement Patterns.
 - Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
 - A Pedestrian-Friendly Environment. Improving the auto-oriented design pattern discussed above under "Issues" will call for guidelines that change the relationship between parking, pedestrian movement and building placement.
 - c. Appropriate Parking Standards. Mixed use developments often produce an internal capture rate. This refers to residents and workers who obtain goods and services from within the development without making additional vehicle trips. Parking ratios for mixed use development should reflect the internal capture rate and the shared parking opportunities this type of development offers.

- d. Encourage infill/redevelopment opportunities that optimize use of city infrastructure and that complement area, neighborhood, and/or corridor context and character.

Approval is subject to the following Conditions:

1. Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, unless modified by the conditions below:
 - Site plan date stamped April 26, 2013.
 - Grading plan date stamped April 26, 2013.
 - Utility plan date stamped April 26, 2013.
 - Landscaping plan date stamped April 26, 2013.
 - Building elevations date stamped April 26, 2013
 - Building materials board as presented at the Planning Commission and City Council meeting.
2. Prior the issuance of a building permit, a final landscape plan must be submitted, subject to staff approval. Additionally, a performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures.
3. The property owner is responsible for replacing any required landscaping that dies.
4. Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
5. The Final Plat must be filed at the County within one-year after City Council approval. If the plat is not filed it shall be deemed null and void.
6. A shared parking and access agreement must be established across the Plat.
7. A park dedication fee of \$5,000 per dwelling unit is required. A portion of the park dedication funds shall be used to develop the water feature, and maintain an easement over it. The value of the easement shall be deducted from the park dedication requirement. The park dedication fees are due prior to issuance of a building permit per each unit.
8. Compliance with the conditions required by the city engineer in his memo dated May 17, 2012.

9. All public utility, roadway and sidewalk easements shall be dedicated to the City.
10. Attempts must be made meet an energy savings goal of 10%.
11. No delivery truck traffic shall be allowed on 70th Street.
12. Final Rezoning is subject to a Zoning Ordinance Amendment creating the PUD, Planned Unit Development for this site.

Subdivision – Final Plat

Recommend that the City Council approve the Preliminary Plat to create a new four lot subdivision at 7171 France for the proposed project.

Approval is subject to the following findings:

1. The proposed plat meets all Zoning and Subdivision Ordinance requirements.

Approval is subject to the following conditions:

1. Approval of the Final Rezoning of the subject property to Planned Unit Development, PUD.
2. The Final Plat must be considered within one-year after approval of the Preliminary Plat, or the Preliminary Plat shall be deemed null and void.
3. A shared parking and access agreement must be established across the Plat.
4. A park dedication fee of \$5,000 per dwelling unit is required. A portion of the park dedication funds shall be used to develop the water feature, and maintain an easement over it. The value of the easement would be deducted from the park dedication requirement. The park dedication fees are due prior to issuance of a building permit per each unit.

Deadline for a city decision: July 21, 2013

ORDINANCE NO. 2013-__

AN ORDINANCE AMENDING THE ZONING ORDINANCE
TO ESTABLISH A PUD, PLANNED UNIT DEVELOPMENT
DISTRICT AT 7171 FRANCE AVENUE

The City Of Edina Ordains:

Section 1. Subsection 850 is hereby amended to add the following Planned Unit Development (PUD) District:

850.23 Planned Unit Development Districts (PUD)

Subd. 4. Planned Unit Development District-4 (PUD-4) – 71 France

A. Legal Description:

Lots 1-4, Block 1, 71 France Addition, Hennepin County.

B. Approved Plans. Incorporated herein by reference are the re-development plans received by the City on April 26, 2013, except as amended by City Council Resolution No. 2013-____, on file in the Office of the Planning Department under file number 2013-004.13a.

C. Principal Uses:

All principal and conditional uses allowed in the Planned Commercial District-3 (PCD-3)

D. Accessory Uses:

All accessory uses allowed in the Planned Commercial District-3 (PCD-3)

E. Conditional Uses:

None

Existing text – XXXX
Stricken text – ~~XXXX~~
Added text – XXXX

F. Development Standards. Development standards per the PCD-3 Zoning District, except the following:

Building Setbacks

Front – France Avenue	50 feet
Front – Hazelton Road	24 feet
Side – North	22 feet
Side – East (lot line setback)	46 feet
Side – East (Promenade setback)	11 feet (Story 1) 22 feet (Story 2-8)
Rear – South	11 feet (Story 1) 40 feet (Story 2-8)

Building Height	eight stories or 108 feet
Maximum Floor Area Ratio	80%

Parking Stalls (Residential)	350 enclosed & 33 surface
Parking Stalls (Retail)	314 stalls

Signage

<u>Type</u>	<u>Max. Number</u>	<u>Max. Area</u>	<u>Max. Hgt.</u>
Building ID Residential	One per building	12 s.f.	6 ft.
Building/store ID Retail	One per bldg. front	15% of wall area	N/A
Freestanding (Retail)	One per entrance	100 s.f. (France Ave) 50 s.f. (Hazelton Rd.)	20 ft. 20 ft.
Neighborhood ID Sign	One per street front	60 s.f.	2 ft.
	Promenade	25 s.f.	7 ft.
Directional monument		6 s.f.	4 ft.

Section 3. This ordinance is effective immediately upon its passage and publication.

First Reading:

Second Reading:

Published:

Existing text – XXXX
 Stricken text – ~~XXXX~~
 Added text – XXXX

ATTEST:

Debra A. Mangen, City Clerk

James B. Hovland, Mayor

Please publish in the Edina Sun Current on:

Send two affidavits of publication.

Bill to Edina City Clerk

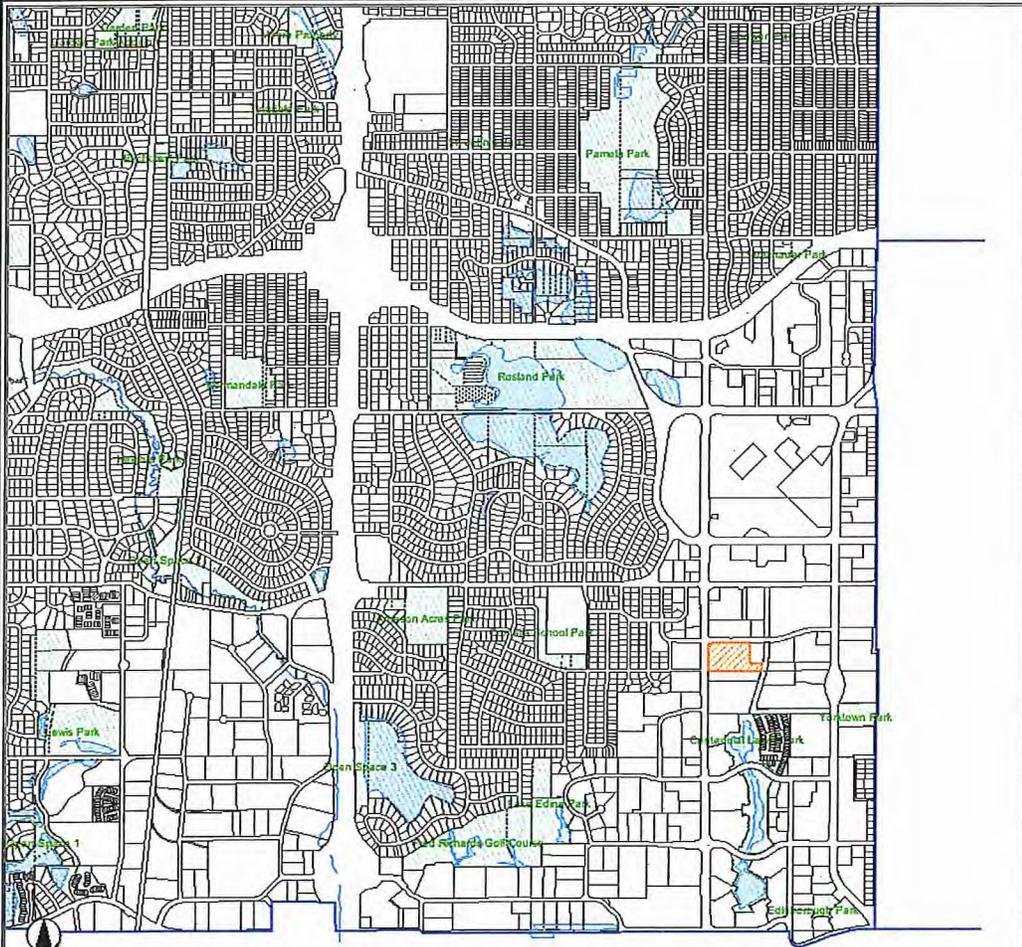
CERTIFICATE OF CITY CLERK

I, the undersigned duly appointed and acting City Clerk for the City of Edina do hereby certify that the attached and foregoing Ordinance was duly adopted by the Edina City Council at its Regular Meeting of _____, 2013, and as recorded in the Minutes of said Regular Meeting.

WITNESS my hand and seal of said City this _____ day of _____, 2013.

City Clerk

City of Edina



- Legend**
- Highlighted Feature
 - City Limits
 - Creeks
 - Lakes
 - Parks
 - Parcels

Map created with ArcGIS. Copyright (C) LOGIS GIS 2015

0 2400ft

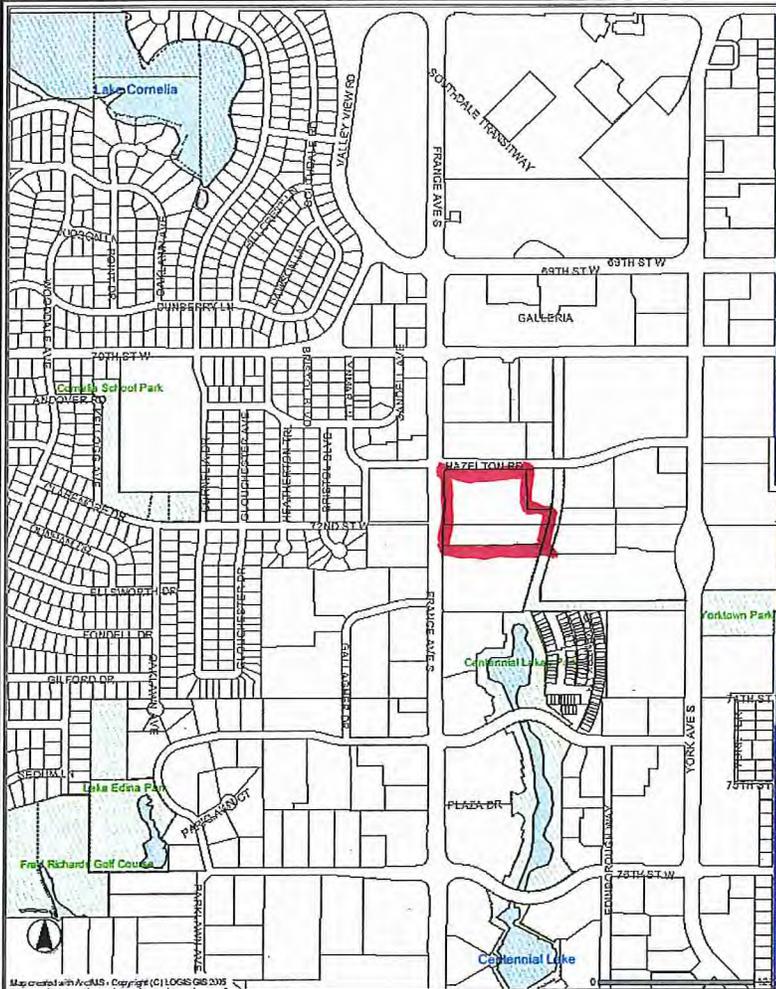


PID: 3202824220009

**7171 France Ave S
Edina, MN 55435**



City of Edina



- Legend**
- Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Parcels

Map created with ArcGIS. Copyright (C) LOGS GIS 2016

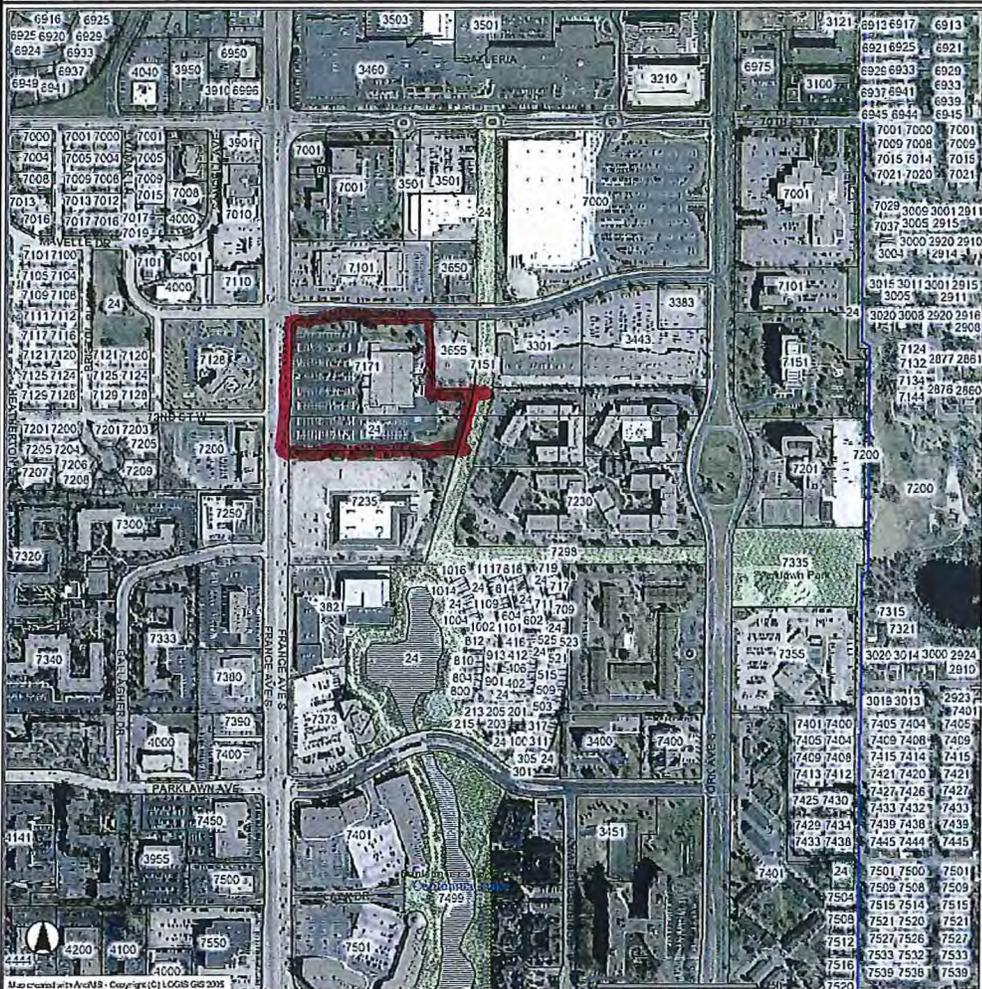


PID: 3202824220009

7171 France Ave S
Edina, MN 55435



City of Edina



- Legend**
- Surrounding House Number Labels
 - House Number Labels
 - Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Parcels
 - 2009 Aerial Photo

Map created with ArcGIS - Copyright (c) LOGGIS GIS 2015



PID: 3202824220009

7171 France Ave S
Edina, MN 55435



City of Edina



- Legend**
- Surrounding House Number Labels
 - House Number Labels
 - Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Parcels
 - 2009 Aerial Photo



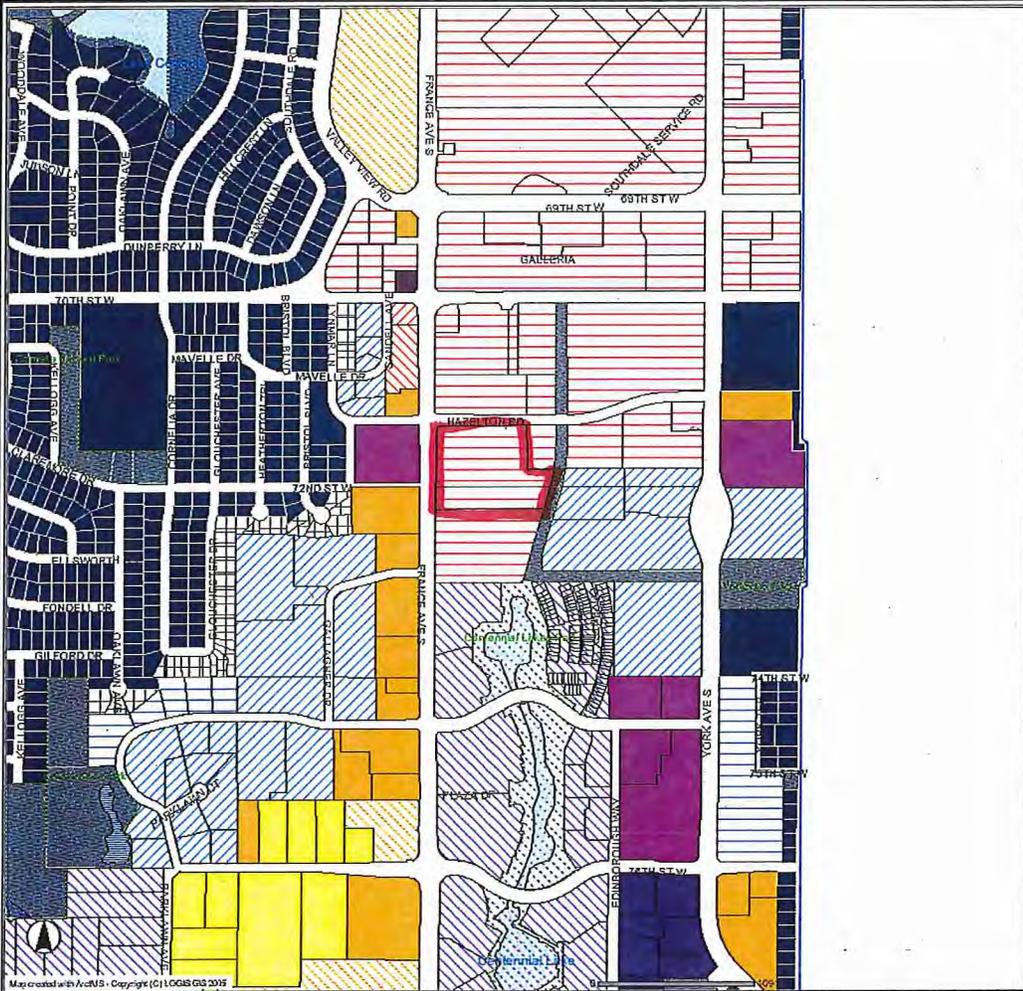
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7171 France Ave S
Edina, MN 55435



AA

City of Edina



- Legend**
- Street Name Labels
 - City Limits
 - Creeks
 - Lake Names
 - Lakes
 - Parks
 - Zoning
 - APD (Automobile Parking District)
 - MDD-4 (Mixed Development District)
 - MDD-5 (Mixed Development District)
 - MDD-6 (Mixed Development District)
 - PCD-1 (Planned Commercial District)
 - PCD-2 (Planned Commercial District)
 - PCD-3 (Planned Commercial District)
 - PCD-4 (Planned Commercial District)
 - PID (Planned Industrial District)
 - PUD (Planned Unit District)
 - POO-1 (Planned Office District)
 - POO-2 (Planned Office District)
 - PRD-1 (Planned Residential District)
 - PRD-2 (Planned Residential District)
 - PRD-3 (Planned Residential District)
 - PRD-4 (Planned Residential District)
 - PRD-5 (Planned Residential District)
 - PGR-4 (Planned Senior District)
 - R-1 (Single Dwelling Unit)
 - R-2 (Double Dwelling Unit)
 - RMD (Regional Medical District)
 - Parcels



PID: 3202824220009
 7171 France Ave S
 Edina, MN 55435



Zoning

A5



A6

A7



View from SE - Macy's Prop line

2012 Sketch Plan

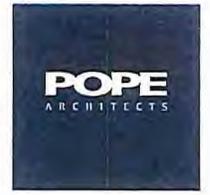


Aerial view from SE

Byerly's Edina Mn

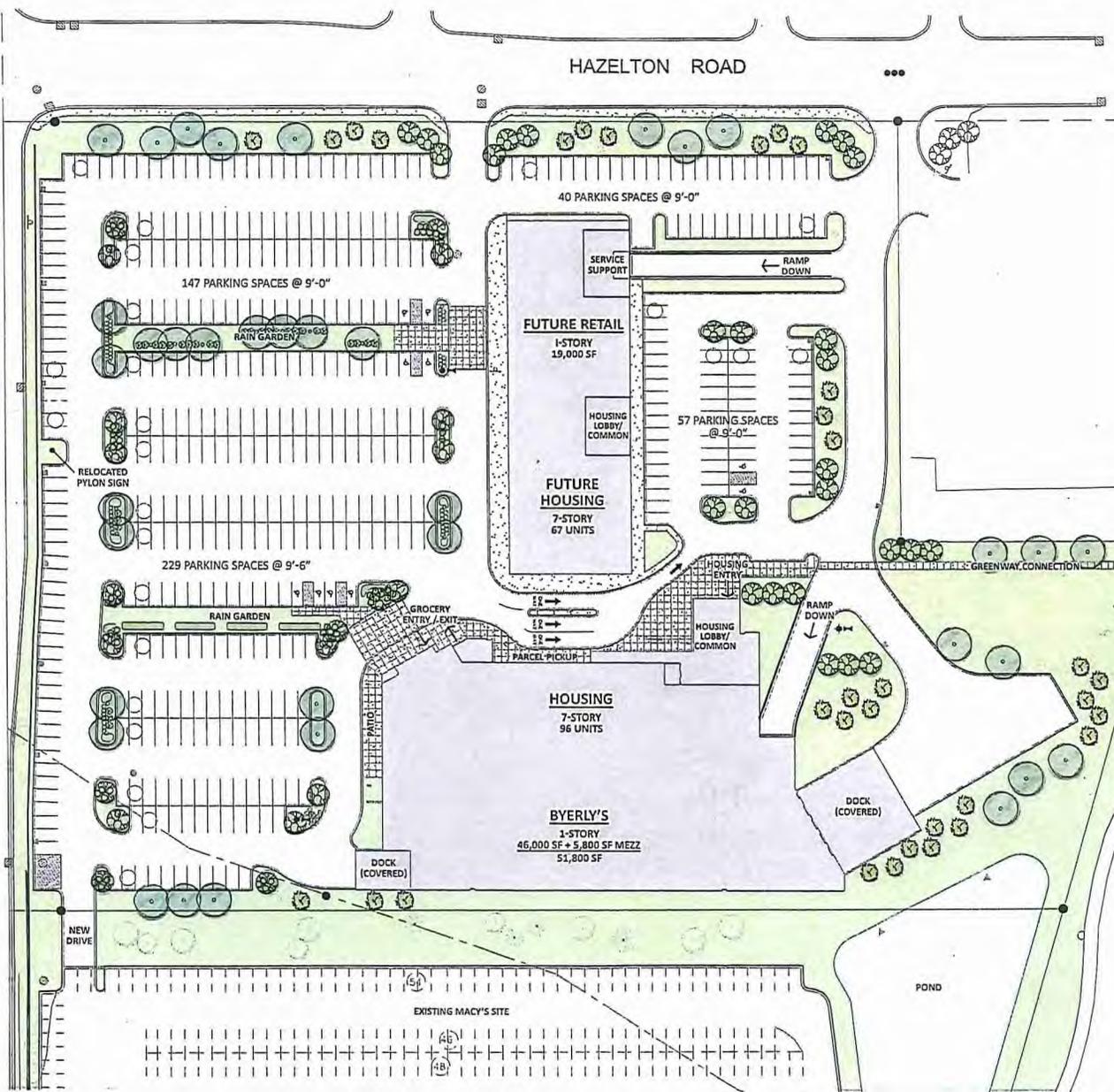
PLANNING DEPARTMENT
FEB 9 & 2012
CITY OF EDINA

02/21/12



A8
2012 Sketch Plan

FRANCE AVENUE

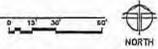


DATA SUMMARY			
Housing A	125,100 sf	96 units	
Housing B (future)	84,800 sf	67 units	
Byerly's	51,800 sf		
Retail (future)	19,000 sf		
PARKING SUMMARY			
		parking req (1.5/unit)	parking provided garage surface
Housing			
Housing A	96 units	144	96
Housing B (future)	67 units	101	50
		245	146
Retail			
Byerly's	51,800 sf	277	
Retail (future)	19,000 sf	95	
		372	354
total required		617	total provided 619

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FEB 23 2012
CITY OF EDINA

Byerly's Edina Mn

1 SITE PLAN
A15 1"=30'-0"



**ANDERSON
KM BUILDERS**
Comprehensive Building & Development Services

**POPE
ARCHITECTS**



02/22/12

PROMENADE PLAN

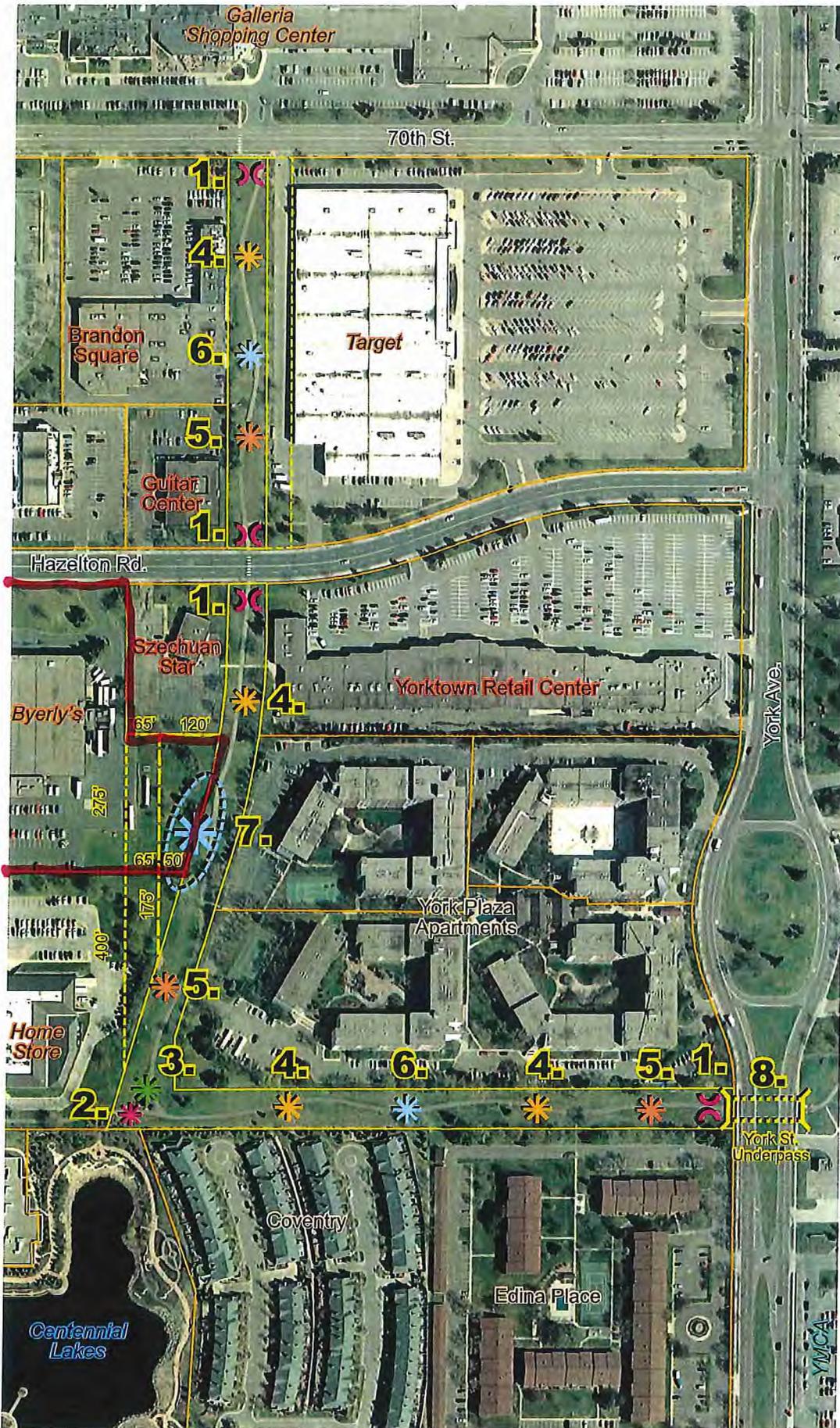


Figure 33

Promenade Component Plan

Promenade Design

Legend

-  **1.** Gateway
-  **2.** Crossroads Feature
-  **3.** Landscaping Feature
-  **4.** Single Sculpture
-  **5.** Sculpture Group
-  **6.** Sculpture Fountain
-  **7.** Pond / Stream and Fountain
-  **8.** York Ave. Underpass



EDINA
PROMENADE

Urban Design Plan

July 2007

URS

AD

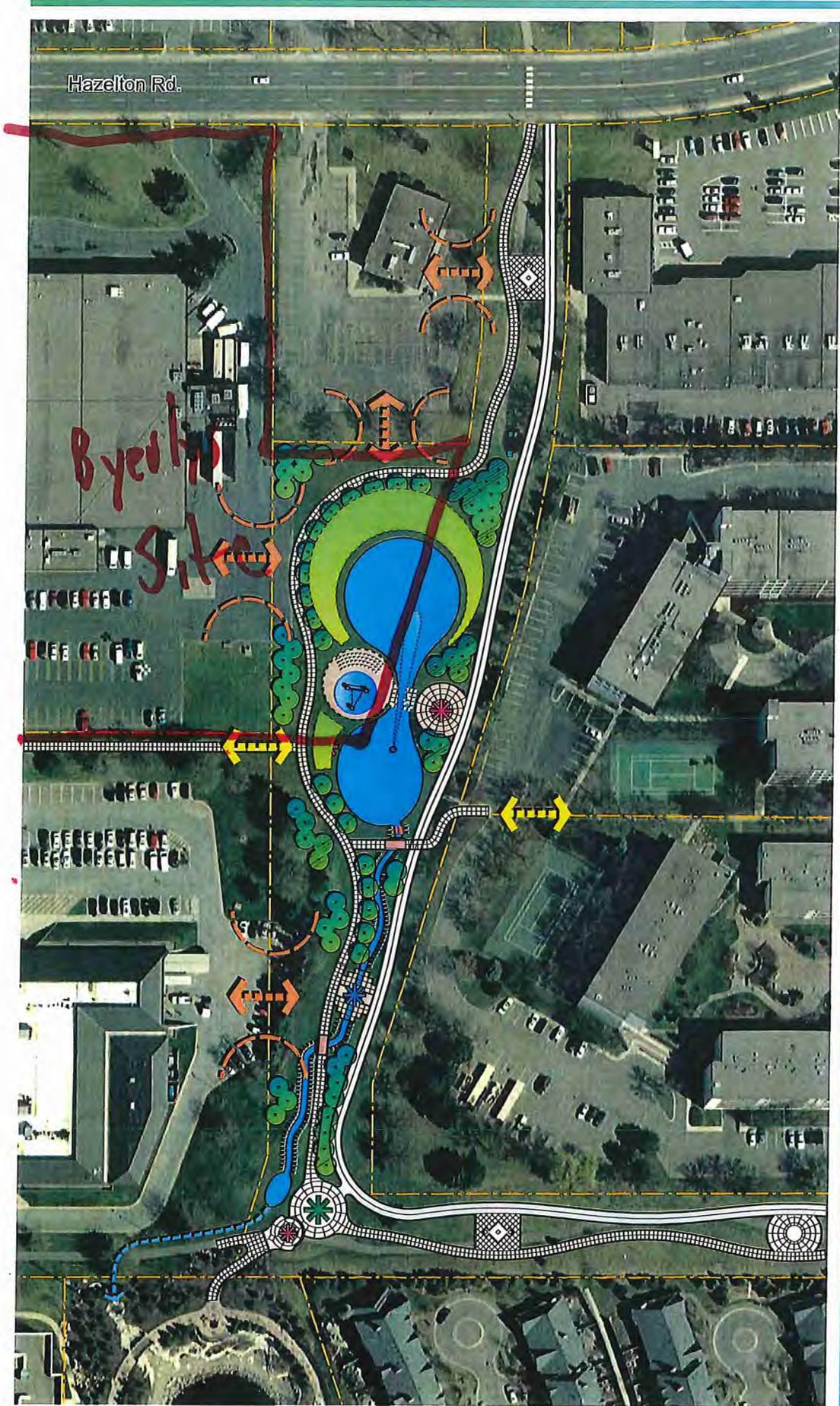
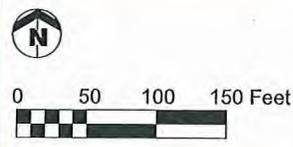


Figure 41
**Stormwater Pond /
 Stream Concept**

Promenade Design

**PROMENADE
 PLAN**



**EDINA
 PROMENADE**

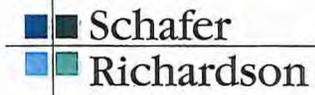
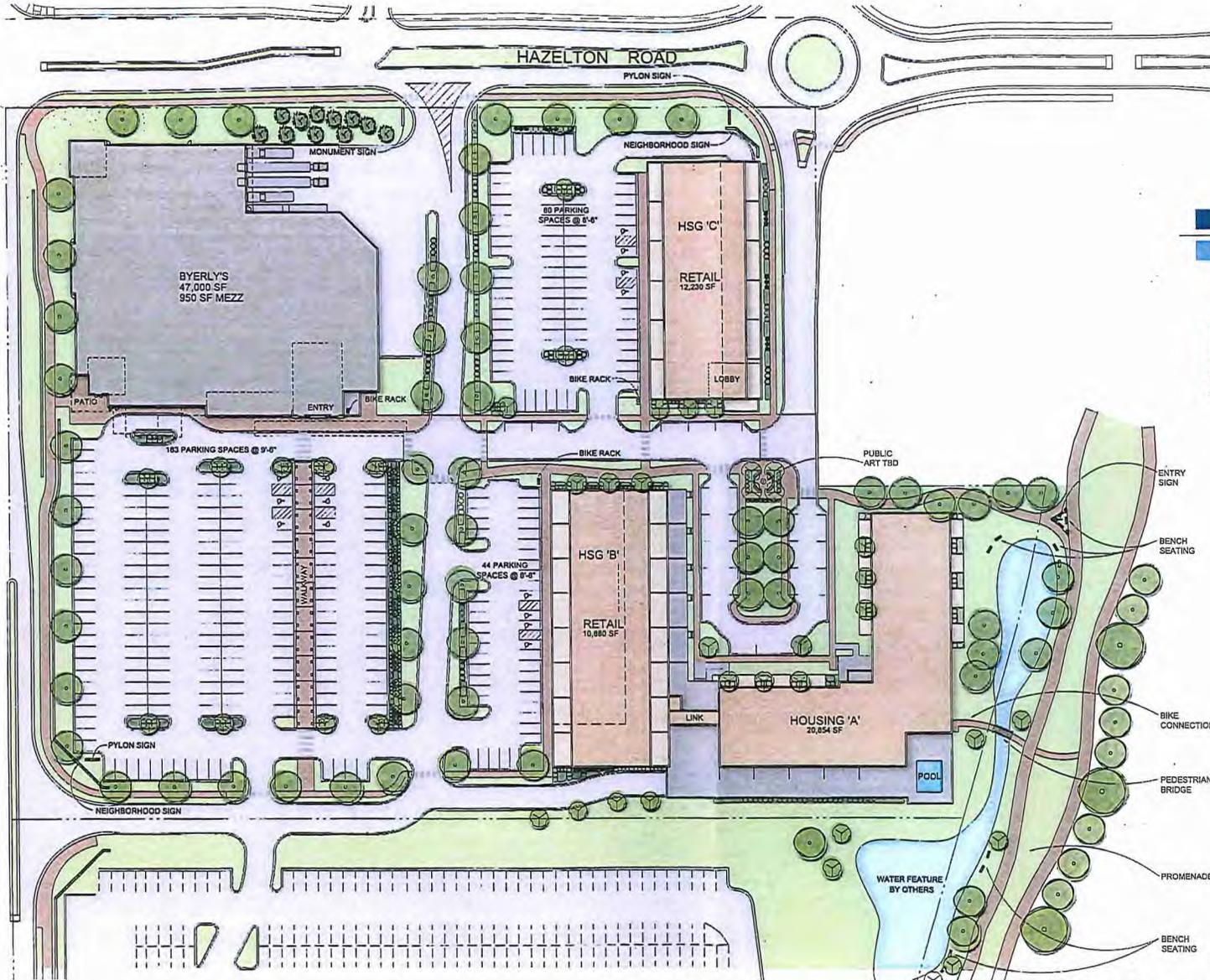
Urban Design Plan

July 2007



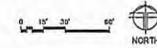
FRANCE AVENUE

HAZELTON ROAD



PLANNING DEPARTMENT
 MAR 08 2013
 CITY OF EDINA

REVISED SITE PLAN



Preliminary Development Plan
 Byerly's France Avenue Re-Development
 EDINA, MN
 3-7-2013 | 15226-11051



PRELIMINARY
 PLAN

CITY



PRELIM. PLAN

PLANNING DEPT
JAN 28 2012
CITY OF EDINA

AERIAL VIEW FROM SOUTHWEST

POPE
Preliminary Development Plan
Byerly's - France Ave. Re-Development
EDINA, MN
01/28/13 15226 - 11051





POPE ARCHITECTS
 1235 BANDANA BLVD N, SUITE 200
 ST. PAUL, MN 55108-8725
 (651) 642-9200 | FAX (651) 642-1101
 www.popearch.com

BYERLY'S
 FRANCE AVENUE
 RE-DEVELOPMENT



Schafer
 Richardson

ANDERSON
 KM BUILDERS
Commercial Building & Development Services



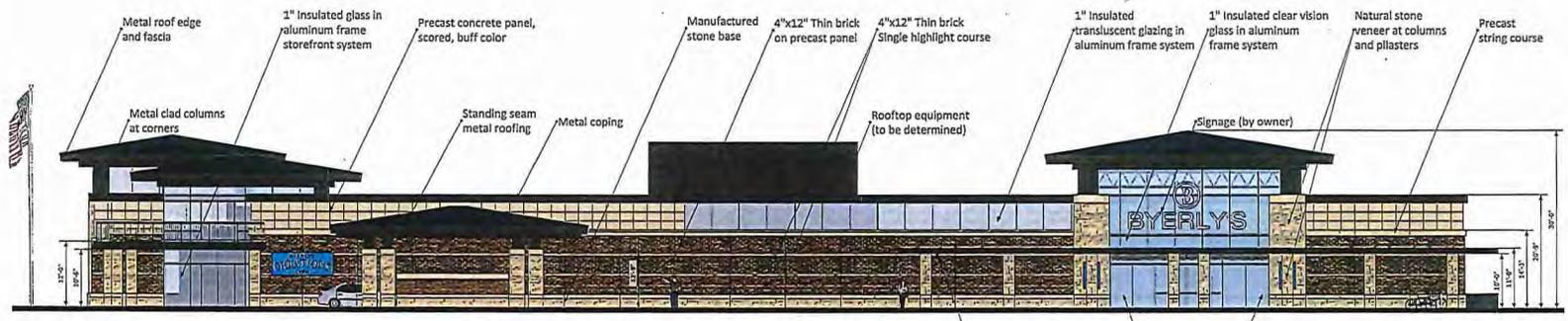
PRELIMINARY
 DEVELOPMENT
 PLAN 1-28-13

Issue #/Date:	01/28/13
Preliminary Development Plan	

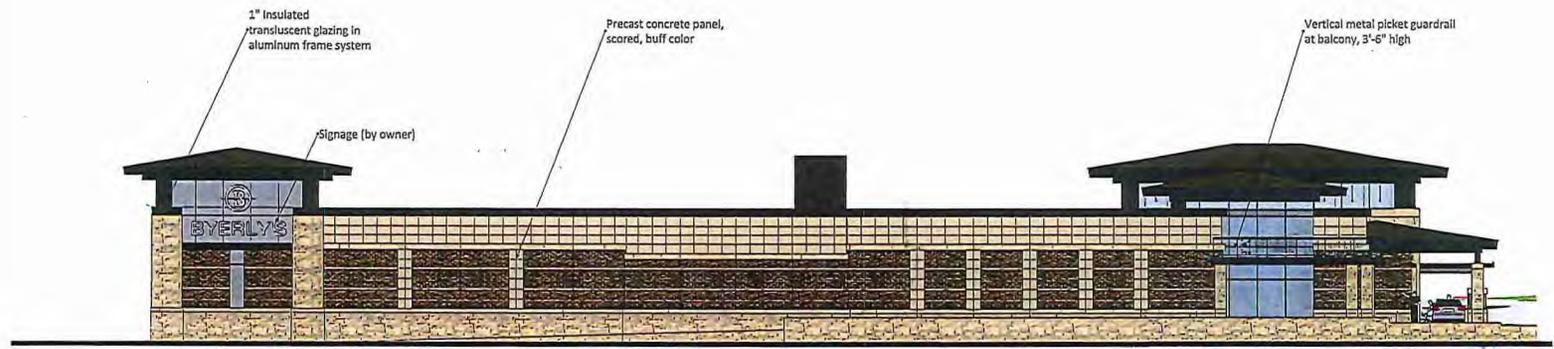
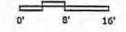
BYERLY'S STORE
 ELEVATIONS

Project No. 15226-11051
 Drawn by MCM
 Date by

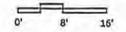
SHEET
A3.1



SOUTH ELEVATION



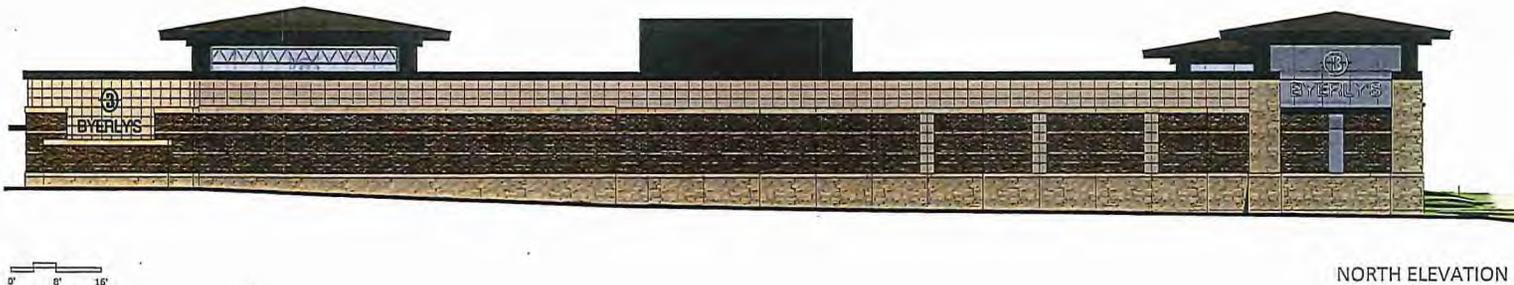
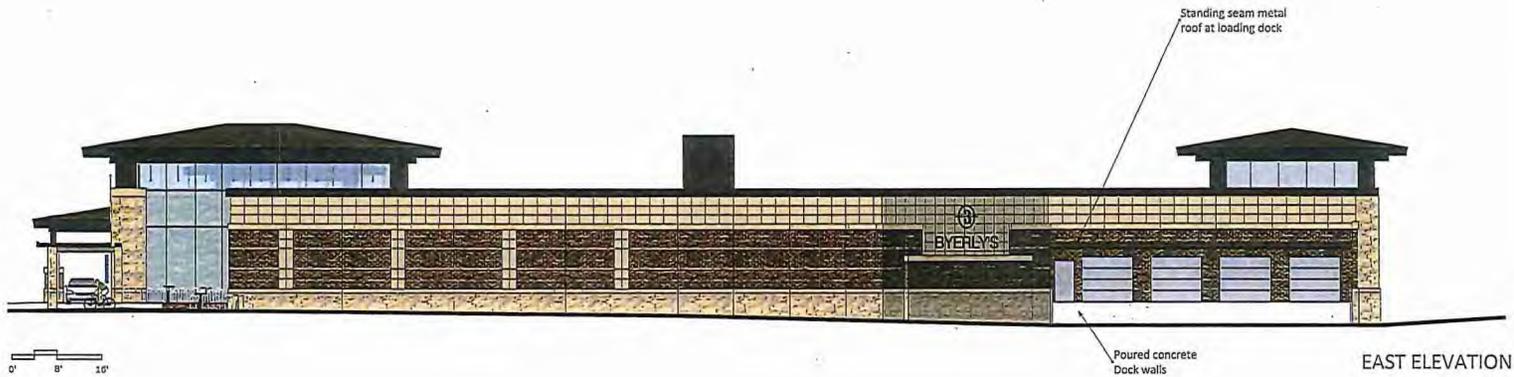
WEST ELEVATION



PRELIM. PLAN

JAN 28 2013

A13



PRELIM. PLAN

A14



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BYERLY'S
FRANCE AVENUE
RE-DEVELOPMENT



Schafer
Richardson

ANDERSON
KM BUILDERS
Construction Building & Development Services



PRELIMINARY
DEVELOPMENT
PLAN 1-28-13

Issues and Revisions
Preliminary 01/28/13
Development Plan

BYERLY'S STORE
ELEVATIONS

Architect: 15226-11051
Drawn by: MCM
Checked by:

SHEET
A3.2

JAN 28 2012

Byerly's France Avenue Redevelopment Final Development Application Narrative

April 24, 2013

Lund Food Holdings, in partnership with Schafer Richardson, seeks to redevelop an existing 9.67 acre parcel at the southeast quadrant of France Avenue and Hazelton Road in Edina. The property is presently occupied by a Byerly's store of just over 59,000 sq. ft. The proposed redevelopment will create an exciting mixed-use community, including a residential village along the Promenade, a new retail destination on France Avenue, and improved connections to and through the site for those driving, as well as for pedestrians, cyclists, and those using mass transit.

Overview:

Current zoning is PCD-3. PUD zoning classification for this project is desired in order to provide flexibility that will enhance redevelopment outcomes and provide benefits as described in the city zoning code 850.04 subpart 4.

The proposed redevelopment plan includes, in Phase 1, a replacement Byerly's store, 47,119 sq. ft. in footprint, and two market-rate rental apartment buildings. Building A is a 6 story, 104-unit Apartment Building, with two levels of underground parking. Building B is a six story, 77-unit Apartment Building including 10,711 sq. ft. of retail space on the west side of the ground level, and two levels of underground parking. A second phase of redevelopment includes an additional apartment building (Building C) containing 48 dwelling units and 11,162 sq. ft. of retail area on the west side of the ground level, and two levels of underground parking. Building A contains 130,181 sq. ft. Building B contains 98,536 sq. ft. and Building C contains 78,029 sq. ft. (Note that underground parking area and enclosed ramps are not included in the totals above.)

On- site parking provided includes 183 spaces serving the Byerly's retail, 44 spaces serving Building B Retail, 80 spaces serving Building C Retail, 278 below-grade spaces serving Building A and B residential, 73 below-grade spaces serving Building C residential, and 23 on-grade residential visitor parking spaces.

The requested building addresses are as follows:

Byerly's Store	7171 France Avenue
Housing Building A	7141 France Avenue
Housing Building B	7131 France Avenue
Housing Building C	7121 France Avenue

PLANNING DEPARTMENT
APR 26 2013
CITY OF EDINA

Byerly's Store Description:

The Byerly's store is a single level, slab-on grade structure, with a mezzanine of 4,500 sq. ft. Building height ranges from 20'6" to 30'. It's exterior architectural expression is rooted in the Prairie Style School and includes the use of natural indigenous stone, brick, architectural precast, glass, stucco and metal panel. The structural system relies on load-bearing, insulated, architectural pre-cast wall panels, finished with a combination of thin-set brick, precast masonry, and exposed, scored, colored-concrete.

The store layout and the need for "back of house" spaces limit the ability of the design team to introduce vision glass in the exterior wall. Vision glass has been added where possible, and further articulation of the exterior has been accomplished through changes in wall plane and materials.

A149

A covered walkway, constructed of materials similar to the store, extends through the primary store parking field on a north/south axis to provide shelter for pedestrians.

Housing Description:

Buildings A, B, and C are all 6 stories in height above grade with an average parapet elevation not in excess of 85'. All three buildings rely on below-grade parking, with building A and B having interior vehicular connection below grade, and an enclosed, heated, pedestrian connection above. The buildings are concrete construction from the parking levels through the first floor, and wood construction (Type 3A) from floors two through six.

The exterior architectural expression of Buildings A, B, and C is clean, sleek and modern. Enclosure materials include brick, stucco, metal-panel, and glass. Continuity between the Byerly's store and the housing buildings is achieved through the use of similar materials and colors. It is the desire of both Lund Food Holdings, and Schafer Richardson that the design of the buildings reflect their differing purposes.

Transit and Site Access:

The redevelopment of the site has been planned with pedestrian, bicycle, and transit use in mind. The layout of the residential buildings takes advantage of their immediate proximity to the Promenade, and pedestrian way-finding through the site is established by the use of a broad, landscaped, walkway, special paving sections at roadway intersections, and other architectural features.

The Byerly's store entry canopy extends east to welcome pedestrians coming from the housing and Promenade, and the design of the store includes outdoor gathering spaces along France Avenue on grade, and on a second level. The planned France Avenue pedestrian and bicycle pathway will be immediately adjacent this outdoor seating space. An existing transit stop on France Avenue will be relocated to the north side of the new Byerly's, on Hazelton Road, for improved access and safety.

Access to the site from north-bound France Avenue is provided via a right-In/right-out connection established between the subject site, and the Macy's Home Store property to the south. An agreement for this shared access is in place between the two property owners. Access to the site from south-bound France Avenue, and from the east, is via two curb cuts on Hazelton Road. The easternmost access is envisioned to be signalized, while the westernmost access will be right-In/right-out only. The easternmost access drive is indicated as spanning the property line between the subject parcel and the neighboring property to the East. An agreement between the two property owners is in place.

Stormwater, Landscaping and Site Lighting:

Storm water management design includes conventional as well as innovative technologies. Rain Gardens, Tree Pits, and a Stockholm soils system will capture surface drainage and promote transpiration and infiltration. The Stockholm soils system utilizes a rock area (tree pit) below grade to promote plant health as well as to provide storm water volume storage. Additional rate control, and below-grade infiltration will be provided through construction of a storm water vault below the Byerly's parking area. Roof water from housing buildings A, B, & C will be routed to a water feature along the Promenade that we understand will be designed and constructed by the City.

The proposed landscape design has a variety of native tree, shrub, and plant types that provide seasonal interest and will exceed the requirements of the City codes. The landscaped areas will have an efficient

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APR 29 2013
CITY OF EDINA

A14b

irrigation system with moisture sensors to reduce the amount of water needed, and to prevent over-watering.

Site Lighting will be provided in accordance with City requirements. Pole lights of 25' in height will be used to light the parking lot areas, and 14' tall decorative fixtures will light pedestrian walkways. All exterior lighting and illuminating devices will include lenses, reflectors or shades so as to limit light spill and glare onto adjacent properties. Illumination measured at the property line abutting residential zoned lots will not be greater than three foot-candles or ten foot-candles measured at property lines abutting streets or property zoned non-residential. A photometric plan is included in the submission. Please note that the design may evolve as final fixture selection occurs, within the confines of the zoning ordinance.

Sustainability:

This mixed use project will incorporate sustainable design principles, materials, system selections, and operating protocols. Redevelopment of an existing site in a densely developed suburban context affords many opportunities.

The Store and Housing projects will participate in the Energy Design Assistance program through Xcel Energy, and a wide array of sustainable measures will be integrated in to the project. The housing design team began the EDA process on April 8, and the Store design team will follow shortly. The entire project team is committed to an outcome for both the store and housing, that exceeds code-driven energy efficiency standards by a minimum of 5%.

Specific sustainable features that will be incorporated into the residential buildings include:

- Energy efficient windows, roof, and wall assemblies
- Occupancy sensor controls in select building locations
- High efficiency heating and cooling
- High efficiency, Energy Star appliances in units
- LED and fluorescent lighting fixtures throughout
- Individually controlled and metered heating and cooling
- Low flow plumbing fixtures and irrigation sensors to reduce water usage
- Use of recycled content materials
- Use of regional materials
- Use of Low-Emitting Materials (VOC) including adhesives and sealants, paintings and coatings, and carpeting
- At least six electric vehicle charging stations in parking areas
- Stormwater system designed to optimize infiltration and support Promenade water feature
- Convenient bicycle storage with a minimum of 150 bicycle parking stalls in dedicated storage areas

Specific sustainable features that will be incorporated into the Byerly's store include:

- An energy-efficient, high-performance, building enclosure system
- Day-lighting
- Occupancy sensors
- High efficiency mechanical and electrical systems with high-performance controls
- The use of locally and regionally sourced materials, components and equipment
- Incorporation of materials high in post-consumer and post industrial recycled content
- The inclusion of 6 car-charging stations in a preferred location in the store parking lot.

PLANNING DEPARTMENT
APR 26 2013
CITY OF EDINA

AIRC

- Night shades will be installed on open refrigerated display cases within the store, resulting in an energy savings of approximately 50%.
- Store lighting, both interior and exterior, will be 20% more efficient than standard systems
- The total refrigerant charge for the grocery store will be 30% less than a standard system
- Specification of Low Emitting materials
- Convenient bicycle parking
- Convenient access to Mass Transit

Signage:

Signage design is included in the submission. The design includes signage applied to retaining walls at the main entry points that identifies the residential neighborhood, coupled with pylon signs in the same locations identifying retail uses. In addition, building-mounted signage will be used to identify other retail partners and tenants. Please note that the signage design is intended to convey the scale and location of various signage components, and that final design (font, colors, etc.) will follow.

Conclusion:

This is an exciting redevelopment opportunity that has been in the planning stages for more than a year. The Sketch Plan Review process completed early in 2012 was of great help to the development team, and this plan changed significantly as a result.

The Preliminary Development Process, likewise, improved the project significantly. The pedestrian connection from the Promenade, in particular, was greatly enhanced during this process.

The outcome is a Final Development Submission that adds 234 housing units along the Promenade, activates France Avenue frontage with a new retail presence, connects the Promenade to the France Avenue Frontage for Pedestrians, and enhances access to and through the site for all who will use it. .

Additional background information regarding Lund Food Holdings and Schafer-Richardson Commercial Real Estate is available on their respective websites; www.lfhi.com and www.sr-re.com .

We appreciate your consideration of the Final Development Plan application, and look forward to meeting with you to discuss the project in greater detail.

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APR 26 2013
CITY OF EDINA

A14d



Cresa Minneapolis
920 Second Avenue South, Suite 900
Minneapolis, MN 55402
612.337.8498 tel
612.337.8459 fax

The Tenant's Advantage
cresa.com

Memorandum

Date May 6, 2013

To Cary Teague, City Planner

Company City of Edina

From Jim Vos

Re 71 France / Byerly's redevelopment
Response to Preliminary Site Plan Approval Conditions

cc

Cary –

Attached is our response to each of the 17 conditions stipulated in your memo to us following the City Council's conditional approval of the preliminary site plan for the redevelopment of the Byerly's property on the corner of France Ave and Hazelton. We've attempted for each item to reference the specific page of the plans where the requested item is shown, or to explain our response to the requirement established. You will see that we believe we have respected each of the 17 conditions (and one which your memo did not include but the council specifically requested.)

As you review the final plans submitted, you will note that the revisions since our previous submission are minor clarifications in most regards. With the exception of the expanded store mezzanine and the addition of the requested architectural detail on the north face, the store remains relatively unchanged. The Housing design is also generally unchanged, though we have upgraded the exterior materials in some areas.

There was some concern previously about our arrangements with the neighboring property owners. At this time, we have presented temporary construction licenses and/or reciprocal access easement agreements to both Macy's and the Wolfson's. We have had cordial discussions with both parties and expect to have these documents finalized by the end of May.

On behalf of the entire development team, I'd be happy to answer any additional questions you may have, or to gather additional information that is necessary for this Final Site Plan Approval and PUD application. Please feel free to contact me at any time.



A14e

71 France – Byerly's France Avenue Redevelopment

1. The Final Development Plans must be generally consistent with the Preliminary Development Plans dated January 28, 2012, the revisions submitted to the Planning Commission March 13, 2013. Final Development plans should include specific locations of trash enclosure areas, number of bike parking spaces provided, and where loading/deliveries are made to the retail space and apartments. *We believe the final Development Plans are generally consistent with the Preliminary Plans. Specific locations of trash enclosure areas and bike parking locations / counts are illustrated on the Site Plan, Sheet A1.1. Loading and deliveries for the retail tenants are intended to be through their primary entrances. Residents will be permitted to move in through the main building entrances – packages and mail deliveries will be located in the first floor mail centers for each building.*

2. The Final Landscape Plan must meet all minimum landscaping requirements per Section 850.04 of the Zoning Ordinance. *The Final Landscape Plan is indicated on sheets C.13 and C.14 of the civil plan set. Section 850.04 of the Zoning Ordinance would require 68 trees on the site, plus additional shrubs or perennial plantings. The submitted plan, consistent with our intent to add green spaces, includes 95 over-story trees and 38 ornamental trees, plus 880 shrubs and over 2700 perennials.*

3. The Final Lighting Plan must meet all minimum landscaping requirements per Section 850.04 of the Zoning Ordinance. *Section 850.04, Subd.3 H.5.f indicates that the planner may require the lighting plan which is included in the plan set as sheet C.12. That section of the zoning ordinance is not specific about how Lighting Plan is to meet landscaping requirements.*

4. Submittal of a complete sign plan for the site as part of the Final Development Plan application. Signage should include monument sign locations and size, way finding signage, and wall signage. *The complete Sign Plan is included in the submitted plan set with the architectural drawings and perspectives.*

5. Architectural features, windows or articulation must be added to the north elevation of the new Byerly's building. *Having studied the interior layout of the store, the determination was made that additional architectural detail would be the most prudent revision to the north elevation of the new Byerly's building. The detail which was originally along the very west edge has now been replicated along the full width of the North wall (shown on Sheet A3.2). Additional pilaster details have also been added to the loading area wall, and additional plantings - both foundation plantings and trees/shrubs – have been added along the north wall (visible in the Landscaping Plan, Sheet C.13)*

6. Compliance with all of the conditions outlined in the city engineer's memo dated February 22, 2013. *The various conditions referenced in the city engineer's Feb. 22 memo have been addressed either on the drawings where requested, or will be addressed in the development agreement.*

71 France – Byerly's France Avenue Redevelopment

7. Final Rezoning is subject to a Zoning Ordinance Amendment creating the PUD, Planned Unit Development for this site. *We understand this requirement and understand the final motion to approve the application would normally include the creation of the requisite PUD district.*
8. The Final Plat must be considered within one-year after approval of the Preliminary Plat, or the Preliminary Plat shall be deemed null and void. *We understand the timing requirements of the final plat process and will comply.*
9. A shared parking and access agreement must be established across the Plat. *All of the access easements between the various parcels, as well as parking easements, and utility easements will be recorded with the final plat.*
10. The Park Dedication fee of \$1,230,00 shall be paid prior to release of the mylars approving the Final Plat. *As a part of the Development Agreement to be executed, Staff and the applicant have negotiated potential offsets to the Park Dedication fee in exchange for necessary easements (other than access points), environmental remediation, construction of the City water feature, and/or other public/private improvements. Staff has also acknowledged that the phasing of the construction will roll out over a period of years, and that any fee due after said offsets would be paid prior the issuance of Occupancy Certificates on a building-by-building basis (based on unit count). The city will receive cash or equivalent value for the Park Dedication Fee due.*
11. Enhanced and taller landscaping in the southeast corner of the site by the pool. *As illustrated on the final Landscape Plan (Sheet C.13) additional planting of various types have been provided on the Southeast corner of the building, including 6 Black Hills Spruce trees and a number of junipers which provide height and texture variations.*
12. All public utility, roadway and sidewalk easements shall be dedicated to the City. *The plat and easement agreements will comply with this requirement.*
13. Detail must be provided for the proposed covered walkways. *The design of the covered walkway in the parking lot is based on a single column design with two flat roof segments as illustrated on the site plan Sheet A1.1 and visible in the Perspective Sheet 3, showing the aerial view from the Southeast. The covered walkway in front of the grocery store is a two-column design with similar materials - also shown on the site plan Sheet A1.1, but also illustrated in Sheet A3.1.*

A14g

71 France – Byerly's France Avenue Redevelopment

14. Attempt an energy saving goal of 10% above Code. *The housing portion of the development has enrolled in the Xcel Energy Design Assistance program and a kick off meeting was held on April 8, 2013. Currently the Weidt group is performing energy modeling and preliminary cost analysis for the development. A strategy results meeting will be held on May 20th and final project selections will be made mid to end of June after the bundle analysis has been completed. Energy saving strategies being considered include: increased percentages a LED lighting in the project, daylighting controls, occupancy sensors, upgraded and alternate unit heating/cooling packages, energy star bath fans, fan system power reductions, and increased water heating efficiency. In addition to our efforts towards a 10% energy savings above code we have also added improved stormwater features to the housing site - a Swedish stormwater system is proposed which would manage virtually all housing-related stormwater while improving tree health.*

The grocery store portion of the Site is now also enrolled in the Xcel Energy EDA program, and we will pursue that to completion over the next 60-90 days. Significant developments in refrigeration efficiency will be evident in the store design, such as individual case controllers and variable capacity compressors which reduce the coolant charge and energy usage, and night shades to contain cooling during non-store hours. The store will also utilize reclaimed heat from the refrigeration system to pre-heat water before it goes into the gas-fired water heaters. The refrigeration systems should be approximately 40% more efficient than traditional methods. We've elsewhere mentioned the LED lighting system that will be used throughout the store, which, in combination with daylighting, variable lighting controls, and motion-controlled lighting where appropriate, should result in energy savings of approx. 20% over traditional systems. While not directly tied to energy consumption that gets measured on site, the new cardboard compactor system allows for three times the capacity of the current system, reducing trips for haulers to the recycling center.

15. Enhanced landscaping within the Byerly's parking field. *We studied the area between islands and the walkway and find it to be quite efficient and relatively small. Target's lot was offered as an example – they have approx. 750 stalls. Our total store parking field is approx. 25% of that, with 1/4 on the east side of the covered walk. That leaves 143 stalls in the open field, including the 4 tree islands – Whole Foods has 136 stalls in their front and no green space or tree islands/screening. Scaled overlay images of how the new Byerly's parking area compares to both Target (landscaped with tree islands) and Whole Foods (no landscaping of any kind) are attached as Exhibit A.*

16. All crosswalks shall be marked with duraprint stamping to clearly identify the pedestrian crossing. *The Duraprint concrete stamping, and all cross walks, are shown where requested on the Site Plan, Sheet A1.1.*

17. No delivery truck traffic shall be allowed on 70th Street. *Byerly's will instruct their corporate drivers not to use 70th street from Hwy 100 to France for any delivery activity. In addition, Byerly's will request their vendor and direct delivery partners to also avoid using 70th. While we support and will cooperate with this request, as we understand Target has also done, we respectfully suggest the city post "No Truck Traffic" signs along the specified route for the benefit of the residents in the area.*

71 France – Byerly's France Avenue Redevelopment

18. County approval of the right-in and right-out on France Avenue. *In the letter from Hennepin County Public Works dated April 9, 2013, Director James N. Grube states, "The county is in support of the proposed right-in/right-out access" (see copy attached.)*

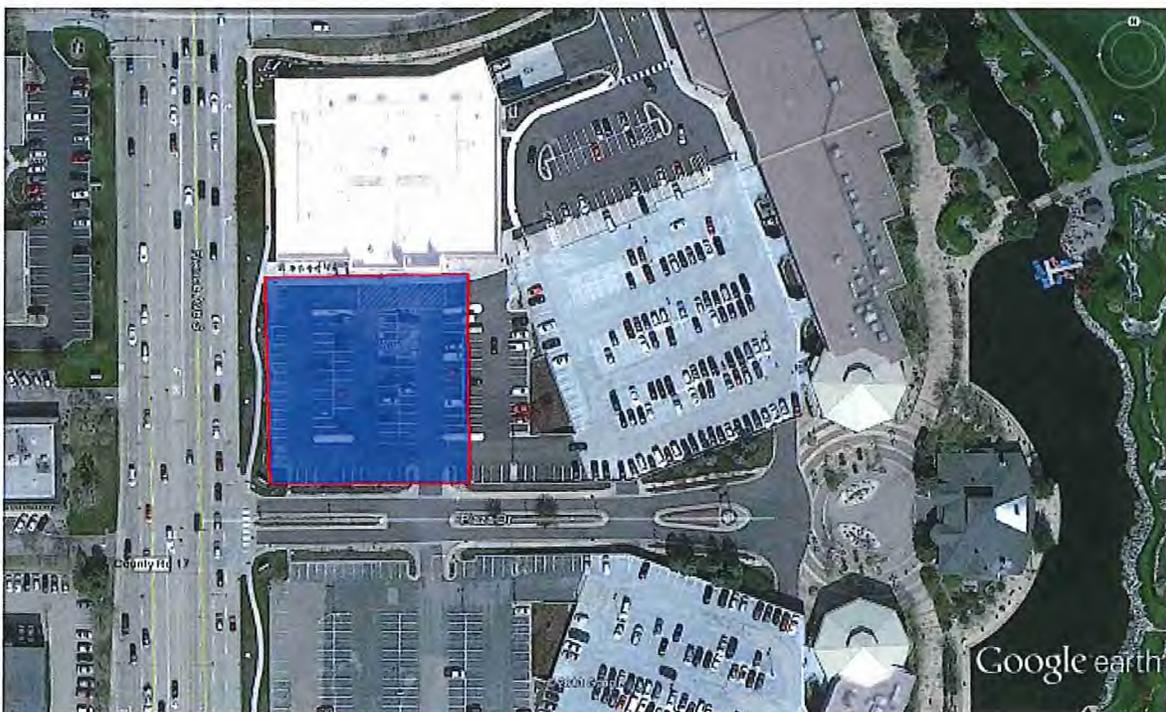
In addition to the conditions listed in the 3/20/13 email from the City Planner , the applicant noted one additional condition articulated by the council during their review/approval of the preliminary site plan. The council requested, specifically, that a sidewalk be added running north from the retail portion of "Building C" to the current Hazelton Avenue sidewalk. This pedestrian link has been added and can be seen on the site plan, Sheet A1.1.

71 France – Byerly's France Avenue Redevelopment

Exhibit A – Parking Comparison Images



Scale comparison of Proposed Byerly's Parking to Target Parking Lot



Scale comparison of Proposed Byerly's Parking to Whole Foods Lot

A14j

71 France – Byerly's France Avenue Redevelopment

County Letter Supporting Right-in/Right-out design
(Emphasis added)



Hennepin County Public Works

Transportation Department
Public Works Facility
1600 Prairie Drive
Medina, MN 55340-5421

Phone: 612-596-0300

Web: www.co.hennepin.mn.us

April 9, 2013

Mr. Cary Teague, Community Development Director
City of Edina
4801 W. 50th Street
Edina, MN 55424

Re: Preliminary Plat Review – Byerly's Right-In/Right-Out Proposed Access
Southeast Quadrant of CSAH 17 (France Avenue) and Hazelton Road
Hennepin County Plat Review No. 3265

Dear Mr. Teague:

Minnesota Statutes 505.02, 505.03, and 462.358, Plats and Surveys, allow up to 30 days for county review of preliminary plats abutting county roads. Over the last few months we've discussed various site plans for the proposed redevelopment of Byerly's with city staff and the developer. A key element of our discussion has been the possible addition of a right-in/right-out access onto France Avenue.

The most recent site plans were sent by the city on March 23, 2013. The city also provided updated concepts for improvements to the France Avenue medians on March 29th. A traffic impact study was completed for this proposed redevelopment by RLK Inc., dated February 22, 2013. Staff from the City of Edina, along with the developer and their consultants, presented information to the county Plat Review Committee on March 5th. The county is in support of the proposed right-in/right-out access with the inclusion of the following comments.

Access Design – From the initial discussions, we have supported a site plan and traffic circulation system that incorporated the adjacent Macy's Home Store. We feel that patrons to the various commercial establishments will need alternative routes to access France Avenue, thus avoiding a concentrated burden on any one access point. The current site plan shows this internal connection.

Based on the current site plan / access design, channelization modifications are being proposed for the France Avenue/72nd Street intersection to accommodate the right-in/right-out for Byerly's and increase the weaving distance on France Avenue. In addition, we have discussed the maintenance and snow plowing for the proposed right-in/right-out island. It is our understanding that the county's maintenance staff will not be responsible for any portion of the maintenance of this proposed entrance/exit.

Right-of-Way – Based on our discussions with the City, the design of the retaining walls will not encroach within the county right-of-way along France Avenue. No additional right-of-way is being requested by the county as part of this proposed redevelopment.

Permits – Please inform the developer that the construction of the right-in/right-out access will require an approved Hennepin County permit prior to beginning construction. Any additional proposed construction within county right-of-way will also require a permit, including, but not limited to, drainage and utility construction, trail development, and landscaping. Permit questions can be directed to Steve Groen at (612) 596-0337 or steve.groen@co.hennepin.mn.us.

Please contact Bob Byers (612) 596-0354 or robert.byers@co.hennepin.mn.us for further discussion of these items.

Sincerely,

James N. Grube, P.E.
Director of Transportation and County Engineer

JNG/ejs

cc: Wayne Houle, PE, Director of Engineering
Plat Review Committee
Mark Larson, Hennepin County Survey Office

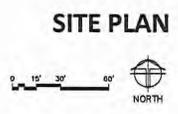
An Equal Opportunity Employer

AHK



FRANCE AVENUE

ATS



Final Development Plan
Byerly's France Avenue Re-Development
 EDINA, MN
 4-25-2013 | 15226-11051



A16



AERIAL VIEW FROM SOUTHWEST



VIEW FROM PARKING, LOOKING NORTHEAST

A17



STREET LEVEL VIEW FROM FRANCE AVE

A18



AERIAL VIEW FROM NORTHEAST



AERIAL VIEW FROM SOUTHEAST



VIEW FROM PROMENADE, LOOKING NORTHWEST



VIEW FROM PARKING, LOOKING EAST

AVA



STREET LEVEL VIEW FROM HAZELTON ROAD



Final Development Plan
Byerly's - France Ave. Re-Development
 EDINA, MN
 04/25/13 15226 - 11051



A3.1

SHEET

15226-11051
MCM
Date: 04/25/13

ELEVATIONS
BYERLY'S STORE

Final
Date: 04/25/13
Development Plan

FINAL
DEVELOPMENT
PLAN 4-25-13



Anderson Companies
Commercial Building & Development Services

Scharter
Richardson

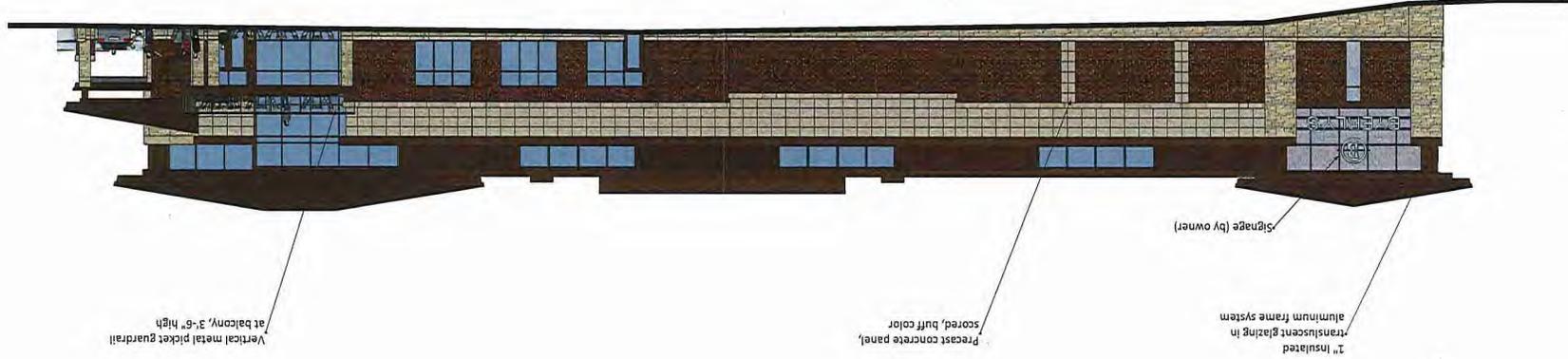
BYERLY'S

BYERLY'S
FRANCE AVENUE
REDEVELOPMENT

POPE ARCHITECTS
1225 SANDY HOLLOW BLVD. SUITE 200
ST. PAUL, MN 55108-2732
www.popearch.com
(651) 642-9200 | Fax (651) 642-1101

POPE
ARCHITECTS

WEST ELEVATION

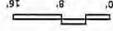


Vertical metal picket guardrail
at balcony, 3'-6" high

Precast concrete panel,
scored, buff color

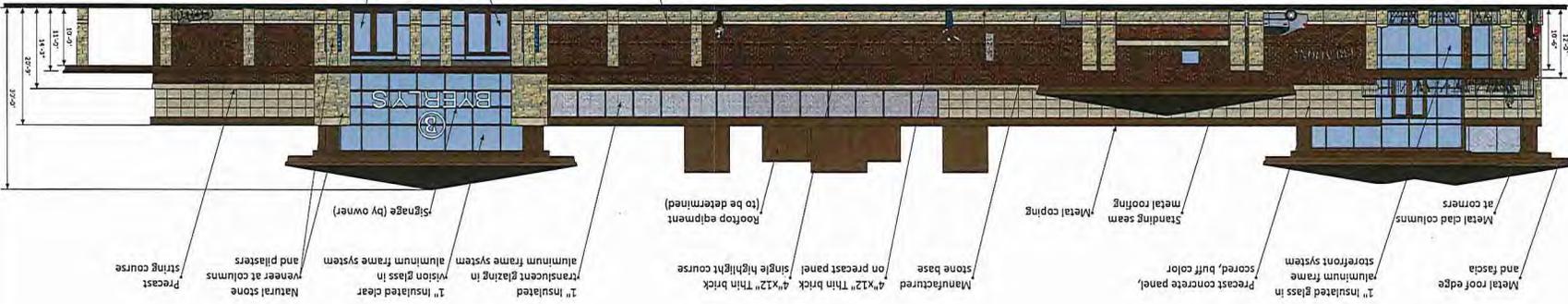
1" Insulated
translucent glazing in
aluminum frame system

Signage (by owner)



A20

SOUTH ELEVATION



Precast
string course
Natural stone
veneer at columns
and pilasters

1" Insulated clear
aluminum frame system
aluminum frame system

1" Insulated
translucent glazing in
aluminum frame system

4"x12" Thin brick
single highlight course
(to be determined)

Manufactured
stone base

Standing seam
metal roofing

Scored, buff color
Precast concrete panel,

1" Insulated glass in
aluminum frame
storefront system

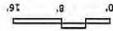
and fascia
Metal roof edge
Metal clad columns
at corners

Power operated
sliding entry doors

Pedestrian canopy with
4 ply built-up roof and
metal roof edge

Rooftop equipment
(to be determined)

12'-0"



A3.2

SHEET

15226-11051
MCM
Date: 04/25/13

BYERLY'S STORE
ELEVATIONS

Date of Revision: 04/25/13
Development Plan

FINAL
DEVELOPMENT
PLAN 4-25-13



ANDERSON
COMPANIES

Scharer
Richardson

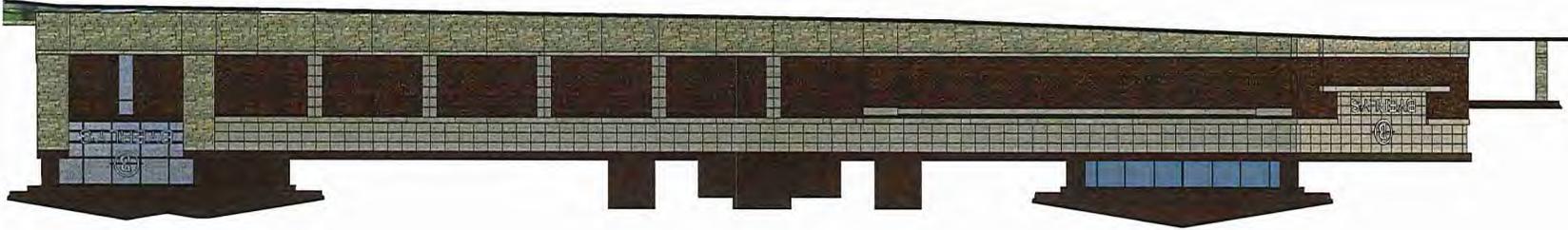
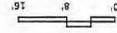


BYERLY'S
FRANCE AVENUE
RE-DEVELOPMENT

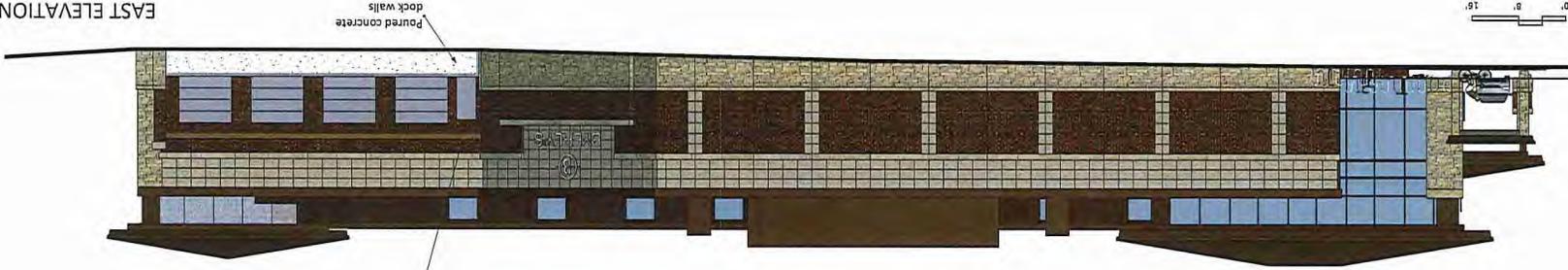
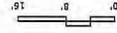
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WWW.POPEARCHT.COM

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NORTH ELEVATION



EAST ELEVATION



A21



Scale: 1" = 8' - 0"

SOUTH ELEVATION



Scale: 1" = 8' - 0"

WEST ELEVATION

A22

- HVAC grill (paint to match adjacent material)
- Brick B
- Brick A

Scale: 1" = 8' - 0"



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BYERLY'S
FRANCE AVENUE
RE-DEVELOPMENT



Schafer
Richardson

ANDERSON
COMPANIES
Commercial Building & Investment Services



FINAL
DEVELOPMENT
PLAN 4-25-13

Issue and Revision:	
Final	04/25/13
Development Plan	
Supplemental Information	05/07/13

BUILDING A
ELEVATIONS

Drawn by: 15226-11051
RM
Checked by:

SHEET
A3.3



EAST ELEVATION



NORTH ELEVATION



A23

A3.5

SHEET

15226-11051
Date: 05/07/13
Rev: 01

BUILDING B
ELEVATIONS

Date of Release: 04/25/13
Final Development Plan 05/07/13
Information

FINAL DEVELOPMENT
PLAN 4-25-13



ANDERSON COMPANIES
Richardson
Schafar



BEVERLY'S
FRANCE AVENUE
RE-DEVELOPMENT

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1255 BANANA BLVD N, SUITE 200
ST. PAUL, MN 55108-2325
TEL: (651) 461-2200 FAX: (651) 461-1800
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POPE ARCHITECTS

WEST ELEVATION



SOUTH ELEVATION



A24



0' 8' 16' Scale: 1" = 8' - 0"

NORTH ELEVATION



0' 8' 16' Scale: 1" = 8' - 0"

EAST ELEVATION

POPE
ARCHITECTS

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BYERLYS
FRANCE AVENUE
RE-DEVELOPMENT

BYERLYS

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ANDERSON
COMPANIES
Commercial Real Estate & Development Services

cresa

FINAL
DEVELOPMENT
PLAN 4-25-13

Issues and Revisions:
Final 04/25/13
Development Plan
Supplemental Information 05/07/13

BUILDING B
ELEVATIONS

Revision No. 15226-11051
Drawn by RH
Checked by

SHEET

A3.6

A35



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BYERLYS
FRANCE AVENUE
RE-DEVELOPMENT



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Richardson

ANDERSON
COMPANIES



FINAL
DEVELOPMENT
PLAN 4-25-13

Issue and Revision:	
Final	04/25/13
Development Plan	
Supplemental Information	05/07/13

BUILDING C
ELEVATIONS

Contract No. 15226-11051
Drawn by RH
Checked by
SHEET

A3.7



SOUTH ELEVATION



WEST ELEVATION

A3.6

Issue and Revision:	
Final	04/25/13
Development Plan	
Supplemental Information	05/07/13

BUILDING C
ELEVATIONS

Contract No.	15226-11051
Drawn by	RH
Checked by	
SHEET	



NORTH ELEVATION

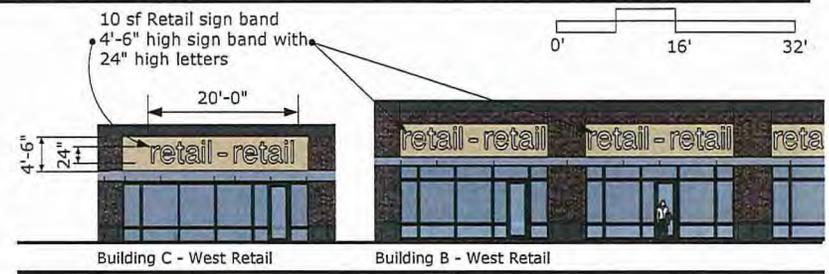
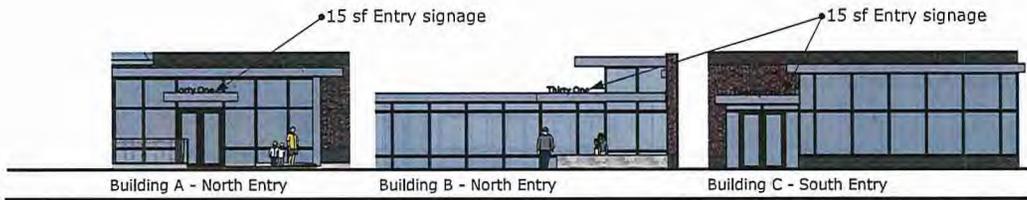


EAST ELEVATION

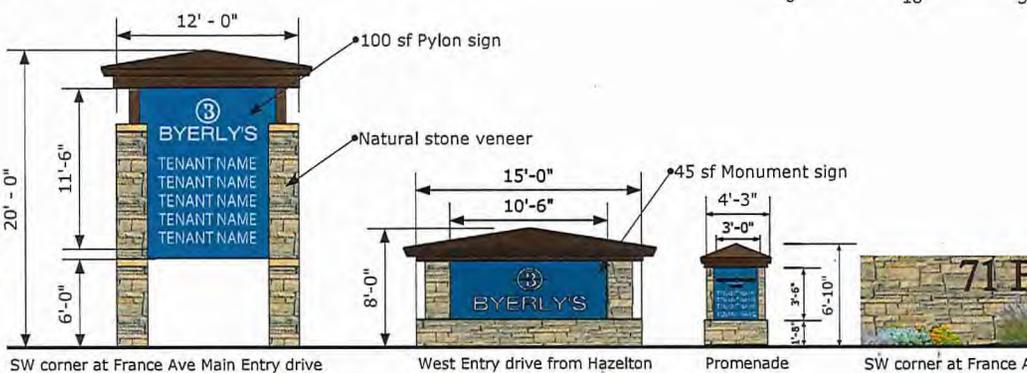
- *Metal panel, silver metallic
- *Stucco, color C
- *HVAC grill, (paint to match adjacent color)
- *Brick A
- *Brick B



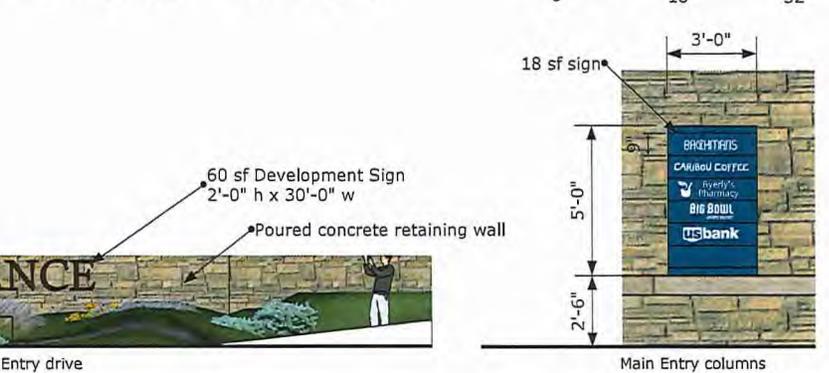
Byerly's Store Signage



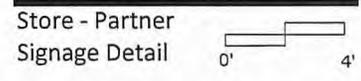
Housing - Entry Signage



Housing - Typical Retail Signage



Development Signage





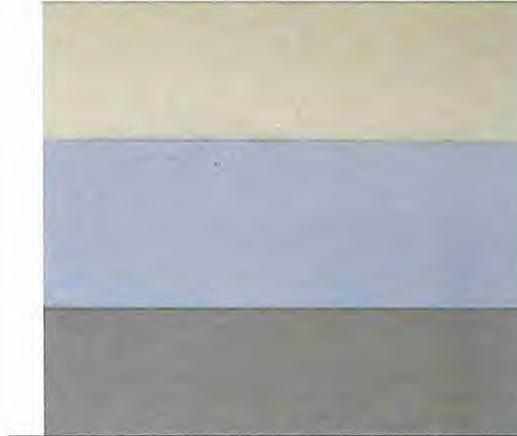
Metal Coping and standing seam metal roofing



Precast Concrete Panels, scored, Buff color



4" x 12" Thin Brick - single highlight course



Stucco, colors B (top), A, and C (bottom)



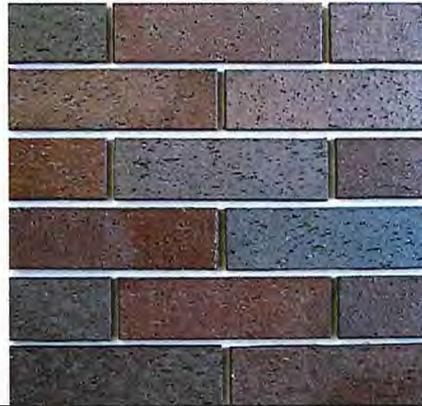
Metal Panel and coping, gunmetal gray



Metal Panel, silver metallic



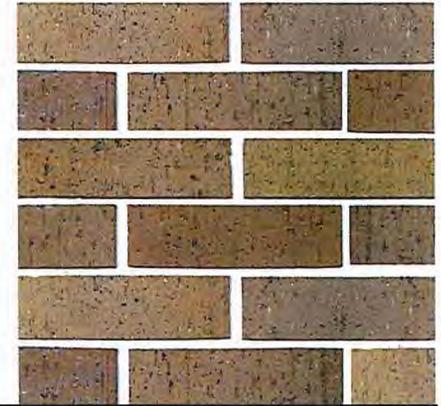
Kasota Stone -
Natural and manufactured



4" x 12" Brick Blend



4" x 12" Brick Blend A



4" x 12" Brick Blend B

Byerly's Store Materials

Housing Materials



Final Development Plan
Byerly's - France Ave. Re-Development
EDINA, MN
04/24/13 15226 - 11051

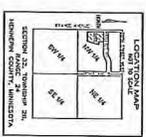
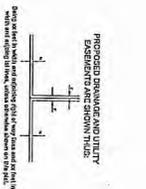
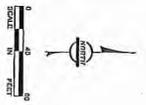
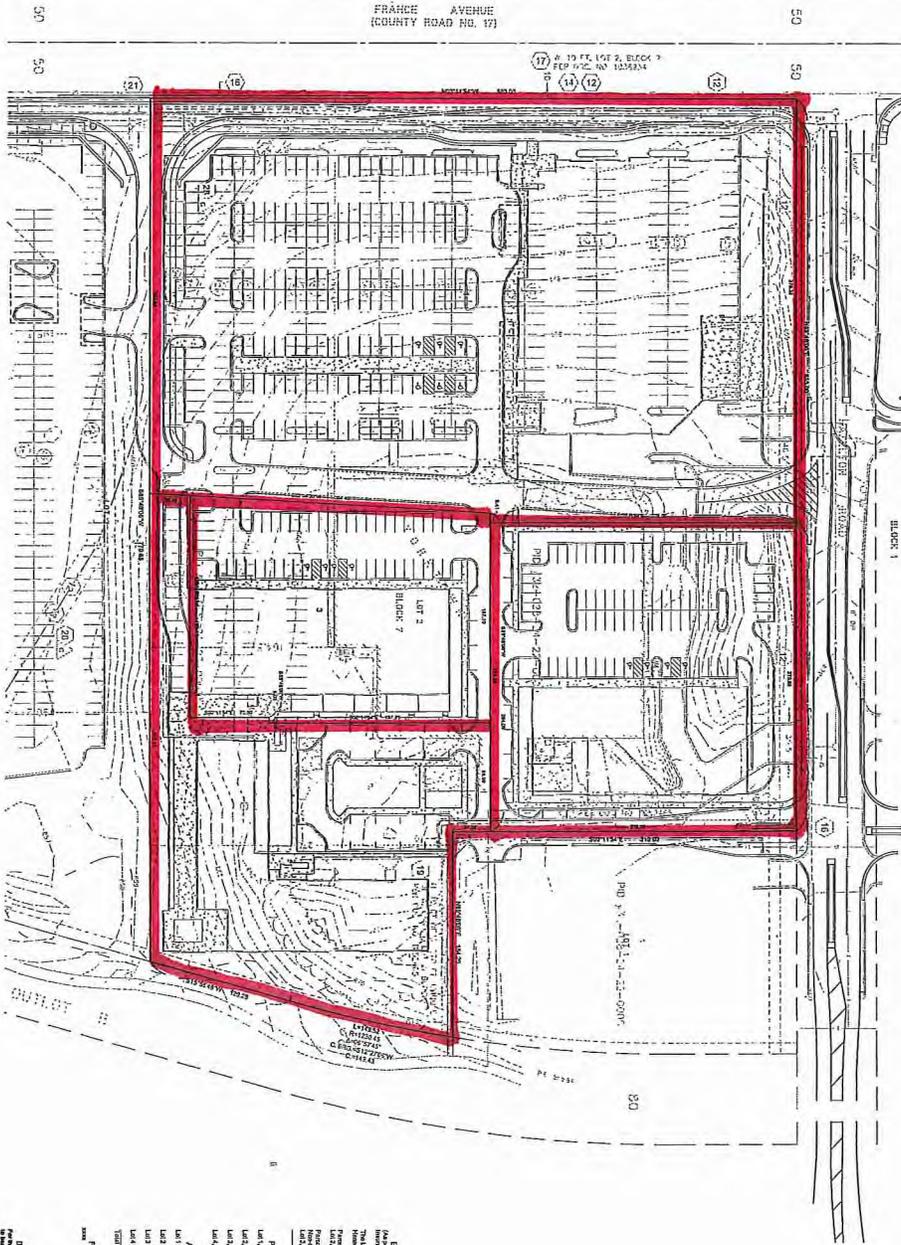


A32

PRELIM. PLAT

BYERLY'S ADDITION

SHEET 1



EXISTING LEGAL DESCRIPTION
 The subject property is located in the City of Eden, Minnesota, within the boundaries of the Eden Township, Hennepin County, Minnesota. The subject property is bounded by the City of Eden, County of Hennepin, State of Minnesota, and the Eden Township, Hennepin County, Minnesota.

PROPOSED LEGAL DESCRIPTION
 The subject property is located in the City of Eden, Minnesota, within the boundaries of the Eden Township, Hennepin County, Minnesota. The subject property is bounded by the City of Eden, County of Hennepin, State of Minnesota, and the Eden Township, Hennepin County, Minnesota.

APPROXIMATE AREA
 The subject property is approximately 100,000 square feet in area.

PROPOSED ZONING
 The subject property is zoned as a residential zone.

REVISIONS

NO. 1	DATE	DESCRIPTION
1	1/2/17	PRELIMINARY PLAT

BYERLY'S
 7171 FRANCE AVENUE S.
 EDINA, MINNESOTA
 Preliminary Plat

ANDERSON KM BUILDERS
 4220 PARK GLEN ROAD
 ST. LOUIS PARK, MN 55416

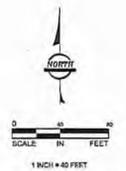
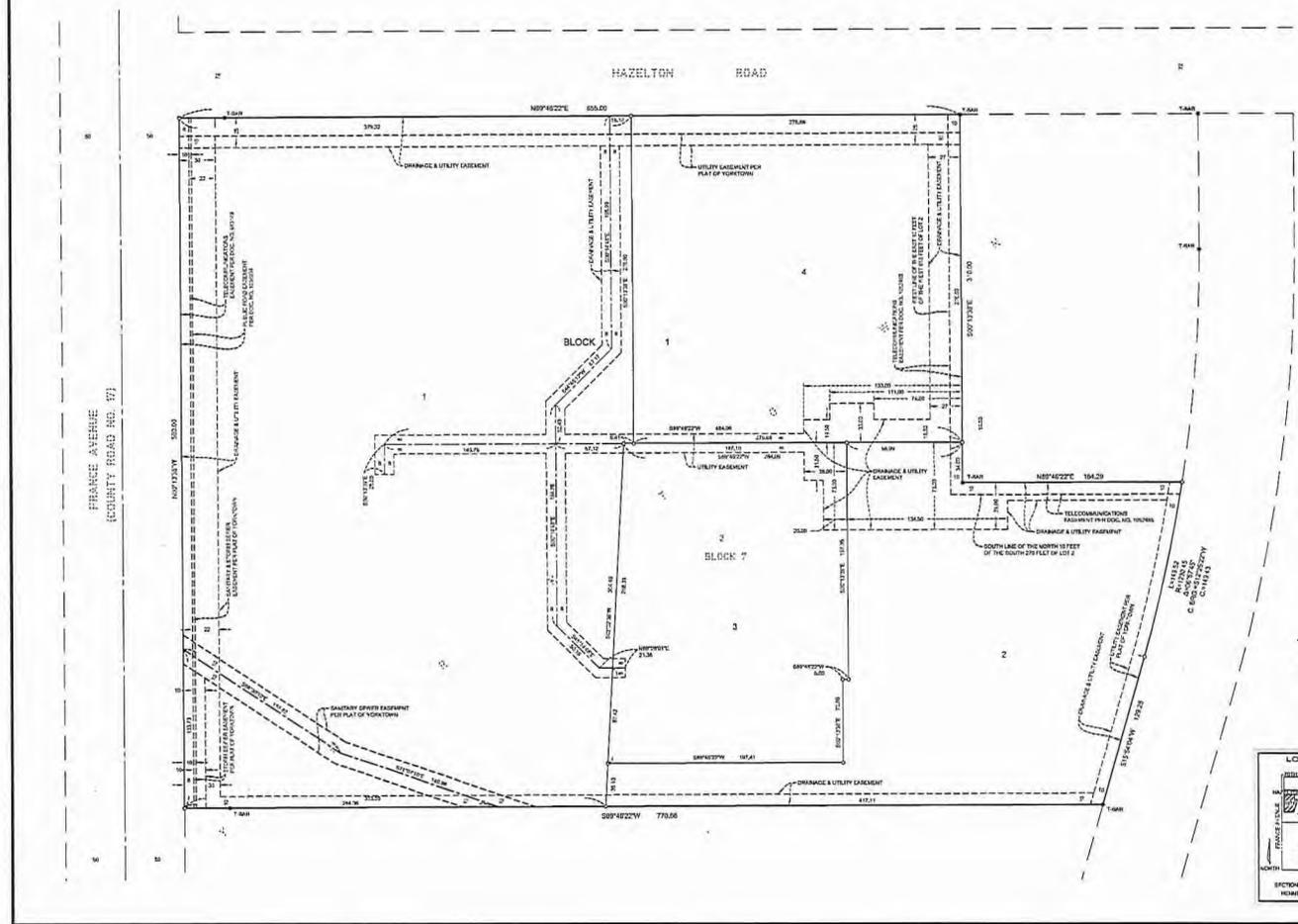


SHEET # 2
 1/2/17
 2017/1/17

A33

71 FRANCE

PLAT FILE NO.
R.T. FILE NO.



For the purposes of this survey the north line of Lot 2, Block 7, Townlot, is assumed to bear N89°42'22\"/>

LEGEND

- Denotes monument found (as noted)
- Denotes 3/4 inch x 1/4 inch iron pipe set and marked with Minnesota License No. 17255. (monuments to be set within one year after recording of this plat)



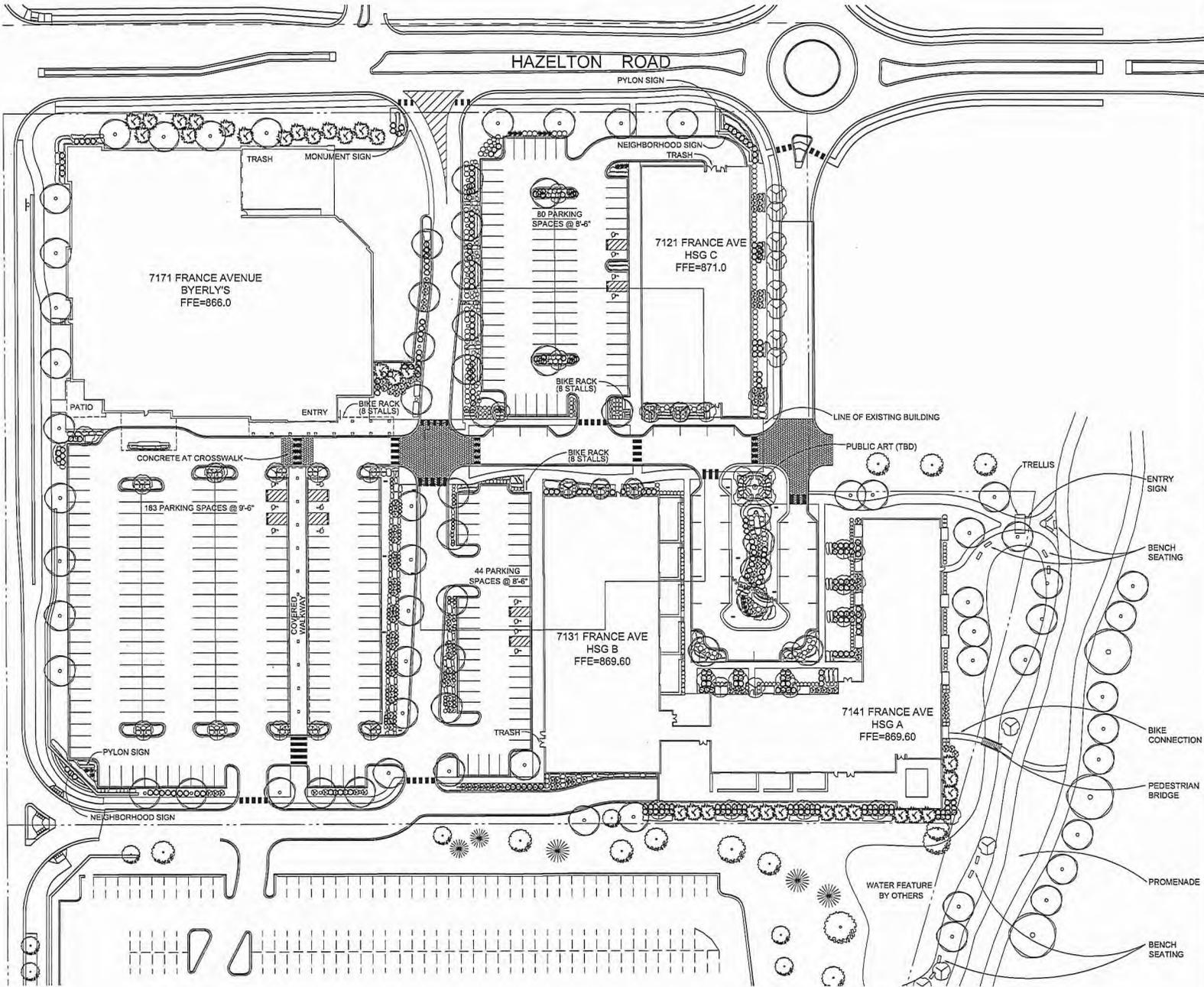
SECTION 23, TOWNSHIP 24, RANGE 24
HENNEPIN COUNTY, MINNESOTA



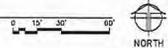
A34

FRANCE AVENUE

HAZELTON ROAD



1 SITE PLAN
A1.1 1"=30'-0"



POPE
ARCHITECTS

POPE ARCHITECTS
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ST. PAUL, MN 55108-2720
(651) 642-9205 / FAX (651) 642-1351
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BYERLY'S
FRANCE AVENUE
RE-DEVELOPMENT

BYERLY'S

Schafer
Richardson

ANDERSON
COMPANIES
Commercial Building & Development Services

cresa

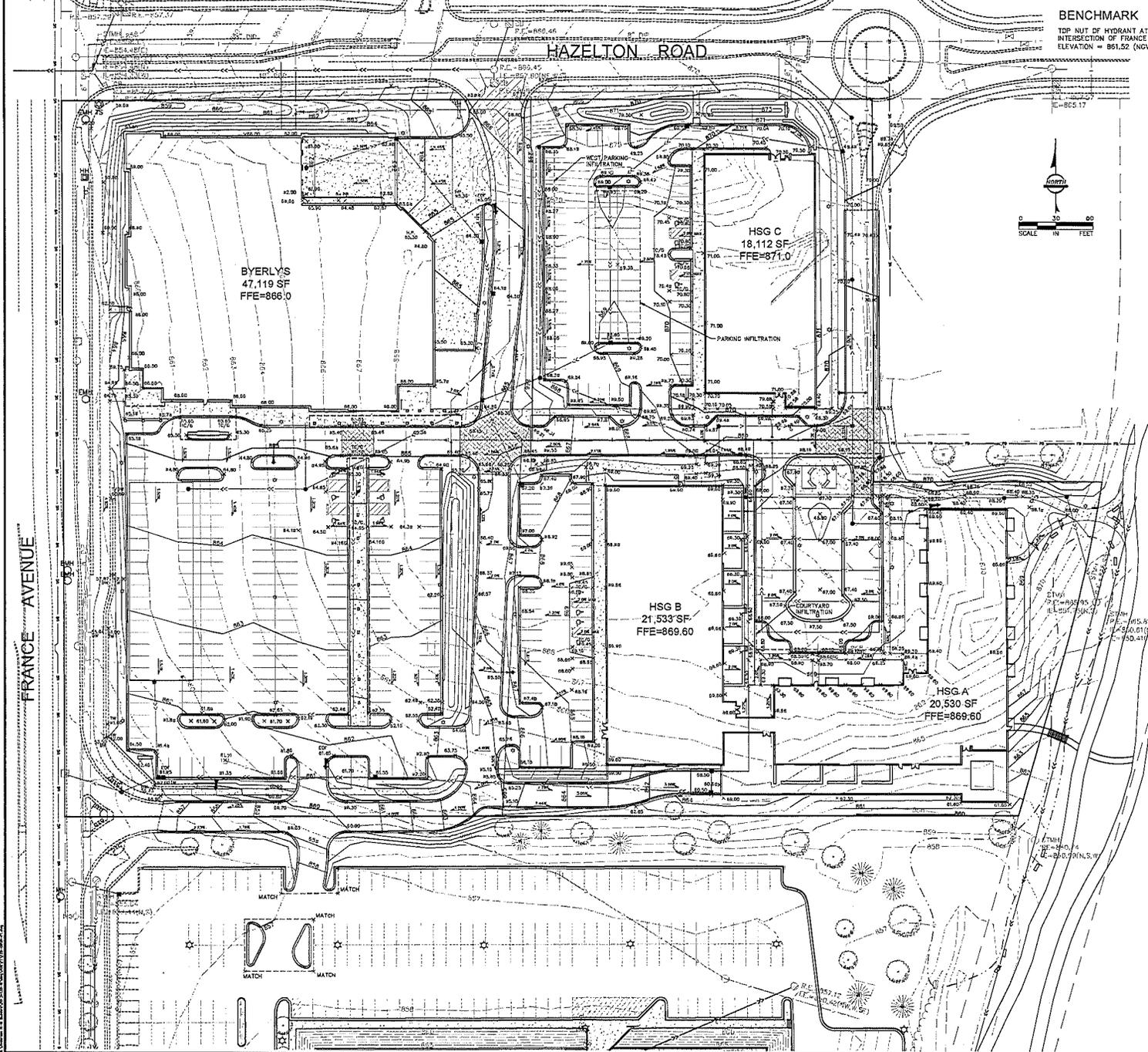
FINAL
DEVELOPMENT
PLAN 4-24-13

SITE PLAN

Item or Feature	Date
Final	4-24-13
Development Plan	
SUPPLEMENTAL PLAN	3-15-13

Companion to	15226-11051
Drawn by	
Checked by	MJM

SHEET
A1.1



BENCHMARK

TOP MUT OF HYDRANT AT NE CORNER OF THE INTERSECTION OF FRANCE AVENUE & HAZELTON ROAD. ELEVATION = 861.52 (NGVD28)

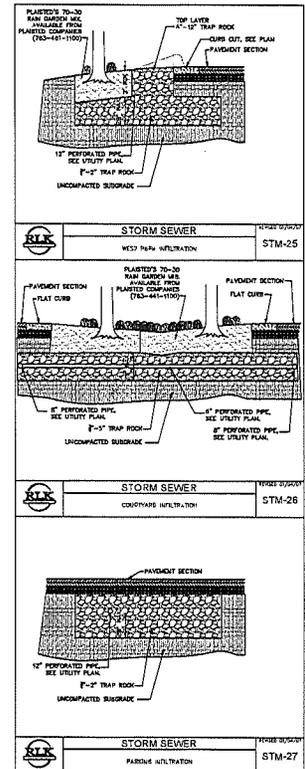
Call 48 Hours before digging
GOPHER STATE ONE CALL
 Twin Cities Area 651-454-0002
 MN Toll Free 1-800-252-1166

LEGEND

	PROPOSED	EXISTING
PROPERTY LINE	---	---
EXISTING LINE	---	---
CLIP LINE	---	---
TOPOGRAPHIC INDEX CONTOUR	---	---
TOPOGRAPHIC CENTERLINE	---	---
SPOT ELEVATION	X 150.00	X 000.00
BRASSIUM SLOPE	---	---
STORM SEWER	---	---
CATCH BASIN	○	○
CATCH BASIN MANHOLE	●	●
STORM MANHOLE	●	●

GRADING NOTES

1. CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO THE COMMENCEMENT OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
2. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "TOWNSHIP STATE ONE CALL" (651-454-0002) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO EXCAVATION.
3. CONTRACTOR SHALL STRIP, STOCKPILE AND RE-SREAD SUFFICIENT TOPSOIL TO PROVIDE A MINIMUM 8 INCH DEPTH (COMPACTED) TO ALL DISTURBED AREAS TO BE SOCCER OR SEEDED.
4. THE SITE GRADING OPERATIONS, WHEN COMPLETED, SHALL RESULT IN ALL AREAS BEING GRADED TO "PLAN SUBGRADE ELEVATION". THIS "PLAN SUBGRADE ELEVATION" SHALL BE CONSISTENT WITH THE "GEOTECHNICAL REPORT FOR BUILDING ELEVATION" IN THE PARKING LOT AND DRIVEWAY AREAS SHALL BE DETERMINED BY CHECKING THE PAVEMENT SECTION DETAILS AND REFERRING TO PAVING PLAN FOR LOCATIONS AND LIMITS OF VARIOUS PAVEMENT SECTIONS.
5. ALL SPOT ELEVATIONS REPRESENT FINISHED SURFACE OR GUTTER LINE GRADE UNLESS OTHERWISE NOTED.
6. EXISTING TOPOGRAPHY PROVIDED BY RLK INC.



REVISIONS

NO.	DATE	DESCRIPTION
1	08/20/12	ISSUED FOR PERMITS
2	09/10/12	REVISED PER COMMENTS
3	09/10/12	REVISED PER COMMENTS
4	09/10/12	REVISED PER COMMENTS
5	09/10/12	REVISED PER COMMENTS
6	09/10/12	REVISED PER COMMENTS
7	09/10/12	REVISED PER COMMENTS
8	09/10/12	REVISED PER COMMENTS
9	09/10/12	REVISED PER COMMENTS
10	09/10/12	REVISED PER COMMENTS
11	09/10/12	REVISED PER COMMENTS
12	09/10/12	REVISED PER COMMENTS
13	09/10/12	REVISED PER COMMENTS
14	09/10/12	REVISED PER COMMENTS
15	09/10/12	REVISED PER COMMENTS
16	09/10/12	REVISED PER COMMENTS
17	09/10/12	REVISED PER COMMENTS
18	09/10/12	REVISED PER COMMENTS
19	09/10/12	REVISED PER COMMENTS
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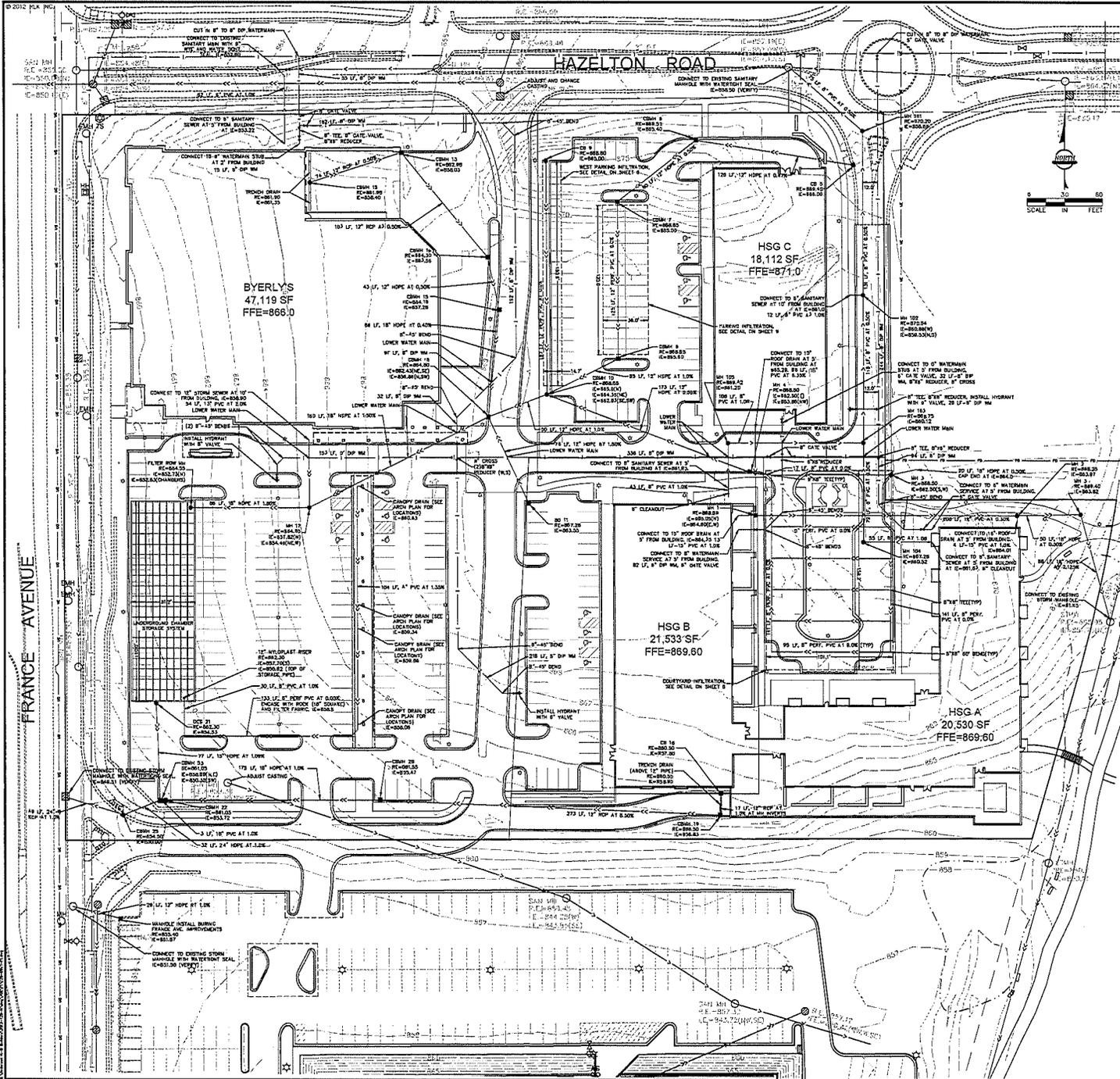
ANDERSON KM BUILDERS
 4220 PARK GLEN ROAD
 ST. LOUIS PARK, MN 55416

ANDERSON COMPANIES
 600 W. WASHINGTON ST. SUITE 200
 ST. LOUIS, MO 63101

7171 FRANCE AVENUE S.
 BURNSVILLE, MN 55337
GRADING PLAN

SHEET 6
 2012.03.04
 4/24/13

A31



LEGEND

	PROPOSED	EXISTING
PROPERTY LINE	---	---
EASEMENT LINE	---	---
SANITARY SEWER	---	---
SANITARY MANHOLE	○	○
SANITARY CLEANOUT	○	○
STORM SEWER	---	---
CATCH BASIN	■	■
CATCH BASIN MANHOLE	■	■
STORM MANHOLE	■	■
WATERMAIN	---	---
FIRE HYDRANT w/ VALVE	■	■
EXIST. VALVE	○	○
REDUCER	○	○
GATE VALVE	○	○
UNDERGROUND ELECTRIC	---	---
UNDERGROUND TELEPHONE	---	---

BENCHMARK

Top Nut of Hydrant
 Location: NE corner of the Intersection of France Avenue and
 Hazelton Road.
 Elevation = 881.52 (NGVD29)

UTILITY NOTES

- CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO THE COMMENCEMENT OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
- ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO EXCAVATION.
- WATER MAIN, SANITARY SEWER & STORM SEWER CONSTRUCTION SHALL CONFORM TO THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM).
- CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.19 FEET PER DETAIL. RM ELEVATIONS SHOWN ON THIS PLAN DO NOT REFLECT SUMPED ELEVATIONS.
- ALL MATERIALS SHALL BE SPECIFIED IN CEAM SPECIFICATIONS EXCEPT AS MODIFIED HEREIN.
 - ALL MATERIALS SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY.
 - ALL SANITARY SEWER TO BE PVC SDR-35, UNLESS NOTED OTHERWISE.
 - ALL WATERMAIN TO BE DUCTILE IRON - CLASS 52.
 - ALL STORM SEWER TO BE REINFORCED CONCRETE PIPE WITH R-4 JOINTS, AND RUBBER GASKETS, UNLESS OTHERWISE NOTED.
- ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB UNLESS OTHERWISE NOTED.
- TYPICAL HYDRANT CONSISTS OF: HYDRANT, GATE VALVE, 2.5" ØP ØIP WATERMAIN AND ANY HYDRANT EXTENSION. HYDRANT EXTENSIONS SHALL BE INCIDENTAL.
- A MINIMUM OF 7.5 FEET OF COVER IS REQUIRED OVER ALL WATERMAIN, UNLESS OTHERWISE NOTED. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM OF 10" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES.
- A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES, UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL MAINTAIN AS-BUILT PLANS THROUGHOUT CONSTRUCTION AND SUBMIT THESE PLANS TO ENGINEER UPON COMPLETION OF WORK.
- CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, EXITS, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- ALL AREAS OUTSIDE THE PROPERTY BOUNDARIES THAT ARE DISTURBED BY UTILITY CONSTRUCTION SHALL BE RESTORED IN KIND. SOD AREAS SHALL BE RESTORED WITH 8 INCHES OF TOPSOIL PLACED BENEATH THE SOD.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL. DEVICES TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES MUST CONFORM TO MN DEPARTMENT OF TRANSPORTATION STANDARDS.
- PRIOR TO PLACEMENT OF AGGREGATE BASE, A TEST HOLE WILL BE REQUIRED ON THE STREET AND PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 20 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
- THE CONTRACTOR SHALL SUBMIT TWO COPIES OF SHOP DRAWINGS FOR MAINHOLE AND CATCH BASIN STRUCTURES TO THE ENGINEER. CONTRACTOR SHALL ALLOW FIVE WORKING DAYS FOR SHOP DRAWING REVIEW.
- THE CONTRACTOR AND THEIR SUPPLIER SHALL DETERMINE THE MINIMUM DIAMETER REQUIRED FOR EACH STORM SEWER STRUCTURE.
- ALL UTILITIES ARE 10' WITHIN 10 FEET OF BUILDING. COORDINATE FINAL BUILDING CONNECTIONS WITH MECHANICAL CONTRACTOR.
- PIPE LENGTHS ARE ESTIMATED FOR DESIGN PURPOSES. DEFLECTING WATER MAIN JOINTS TO A MAXIMUM OF 2" TO ACHIEVE ALIGNMENT IS ALLOWED.
- ALL MANHOLE AND CATCH BASIN CASTINGS SHALL BE NEMA# DR APPROVED EQUAL.
- CONTRACTOR SHALL PROTECT ALL UTILITIES NOT REMOVED BY DEMOLITION OPERATIONS. TREE PROTECTION SHALL BE MAINTAINED UNTIL ALL CONSTRUCTION IS COMPLETE.

Call 48 Hours before digging
GOPHER STATE ONE CALL
 Twin Cities Area 651-454-0002
 MN. Toll Free 1-800-252-1166

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 5641 FM
 P.O. BOX 500
 WYOMING, MN 55092
 TEL: 507-431-1100
 FAX: 507-431-1101

RLK
 CONSULTING ENGINEERS

PROJECT NO. 12-001
 SHEET NO. 12-001-01

DATE: 12/15/11

REVISIONS:

NO.	DATE	DESCRIPTION
1	12/15/11	ISSUED FOR PERMIT
2	12/15/11	ISSUED FOR CONSTRUCTION
3	12/15/11	ISSUED FOR CONSTRUCTION
4	12/15/11	ISSUED FOR CONSTRUCTION

ANDERSON KM BUILDERS
 4220 PARK GLEN ROAD
 ST. LOUIS PARK, MN 55416

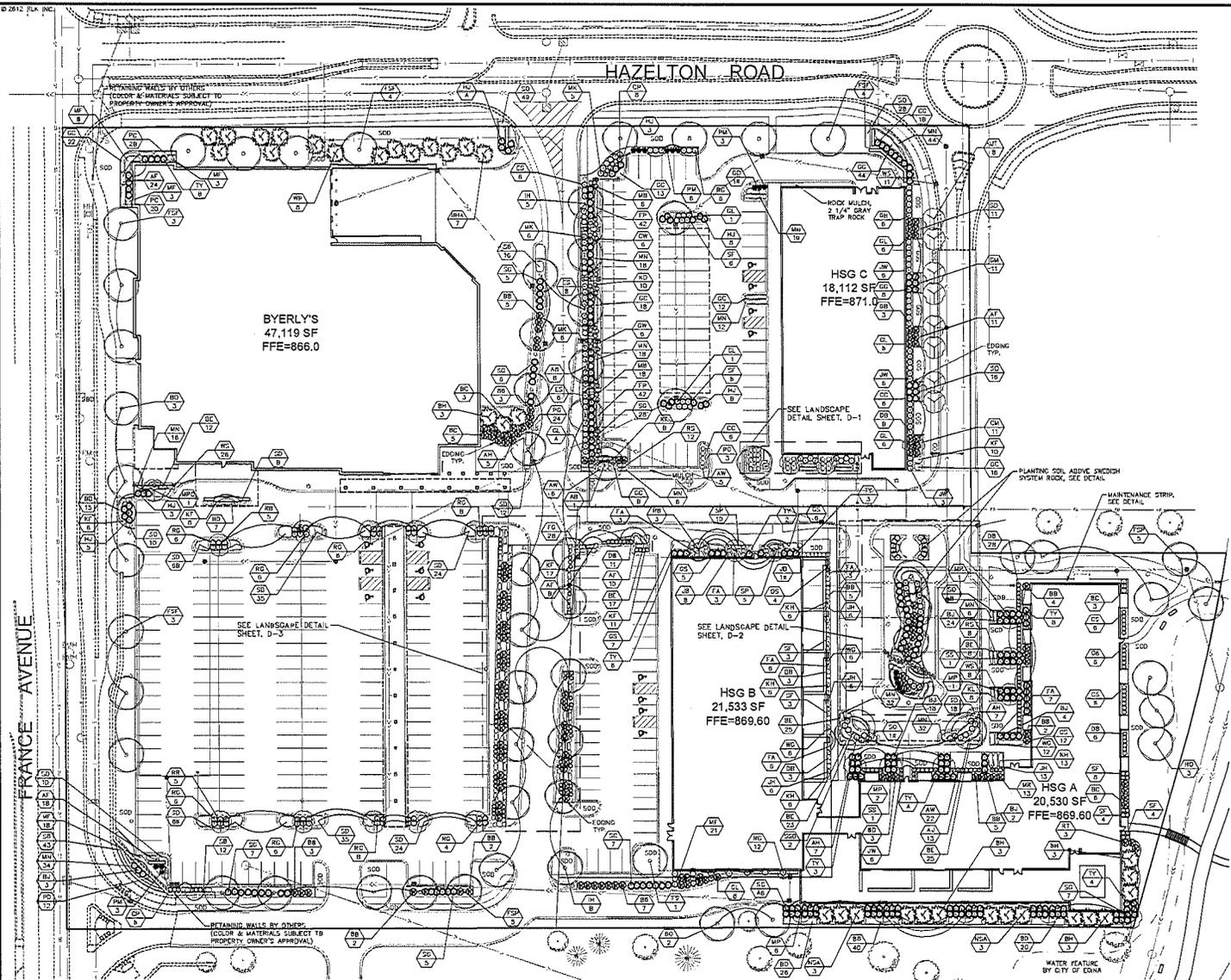
ANDERSON COMPANIES
 Construction Planning & Development Services

7171 FRANCE AVENUE S.
 EDINA, MINNESOTA
UTILITY PLAN

Call 48 Hours before digging
GOPHER STATE ONE CALL
 Twin Cities Area 651-454-0002
 MN Toll Free 1-800-252-1186

PLANT SCHEDULE

CODE	QTY	COMMON NAME/LATIN NAME	SIZE	ROOT	REMARKS
BR 10	1	BRUNELLIA SPICATA	10	BR	PLANT 10' DIA. 10' HT.
BR 11	1	BRUNELLIA SPICATA	11	BR	PLANT 11' DIA. 11' HT.
BR 12	1	BRUNELLIA SPICATA	12	BR	PLANT 12' DIA. 12' HT.
BR 13	1	BRUNELLIA SPICATA	13	BR	PLANT 13' DIA. 13' HT.
BR 14	1	BRUNELLIA SPICATA	14	BR	PLANT 14' DIA. 14' HT.
BR 15	1	BRUNELLIA SPICATA	15	BR	PLANT 15' DIA. 15' HT.
BR 16	1	BRUNELLIA SPICATA	16	BR	PLANT 16' DIA. 16' HT.
BR 17	1	BRUNELLIA SPICATA	17	BR	PLANT 17' DIA. 17' HT.
BR 18	1	BRUNELLIA SPICATA	18	BR	PLANT 18' DIA. 18' HT.
BR 19	1	BRUNELLIA SPICATA	19	BR	PLANT 19' DIA. 19' HT.
BR 20	1	BRUNELLIA SPICATA	20	BR	PLANT 20' DIA. 20' HT.
BR 21	1	BRUNELLIA SPICATA	21	BR	PLANT 21' DIA. 21' HT.
BR 22	1	BRUNELLIA SPICATA	22	BR	PLANT 22' DIA. 22' HT.
BR 23	1	BRUNELLIA SPICATA	23	BR	PLANT 23' DIA. 23' HT.
BR 24	1	BRUNELLIA SPICATA	24	BR	PLANT 24' DIA. 24' HT.
BR 25	1	BRUNELLIA SPICATA	25	BR	PLANT 25' DIA. 25' HT.
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BR 27	1	BRUNELLIA SPICATA	27	BR	PLANT 27' DIA. 27' HT.
BR 28	1	BRUNELLIA SPICATA	28	BR	PLANT 28' DIA. 28' HT.
BR 29	1	BRUNELLIA SPICATA	29	BR	PLANT 29' DIA. 29' HT.
BR 30	1	BRUNELLIA SPICATA	30	BR	PLANT 30' DIA. 30' HT.
BR 31	1	BRUNELLIA SPICATA	31	BR	PLANT 31' DIA. 31' HT.
BR 32	1	BRUNELLIA SPICATA	32	BR	PLANT 32' DIA. 32' HT.
BR 33	1	BRUNELLIA SPICATA	33	BR	PLANT 33' DIA. 33' HT.
BR 34	1	BRUNELLIA SPICATA	34	BR	PLANT 34' DIA. 34' HT.
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BR 98	1	BRUNELLIA SPICATA	98	BR	PLANT 98' DIA. 98' HT.
BR 99	1	BRUNELLIA SPICATA	99	BR	PLANT 99' DIA. 99' HT.
BR 100	1	BRUNELLIA SPICATA	100	BR	PLANT 100' DIA. 100' HT.



LANDSCAPE NOTES

ALL TREES NOT IN PLANTING BEDS TO RECEIVE SHREDDED HARDWOOD MULCH RINGS WITH A MIN. OF 3" IN DIA.
 EDGER SHALL SEPARATE ALL SOG AND MULCH AREAS WHERE SHRUBS AND MULCH MEET SOG.
 REFER TO LANDSCAPE DETAIL SHEET FOR GENERAL PLANTING NOTES, IRRIGATION NOTES AND PLANTING DETAILS.
 ALL OPEN SPACE NOT IN PARKING LOT ISLANDS OR PLANTERS TO BE SOGGED UNLESS OTHERWISE NOTED.
 ALL PARKING LOT ISLANDS AND PLANTING BEDS SHALL RECEIVE 4" OF SHREDDED HARDWOOD MULCH OVER LANDSCAPE FABRIC UNLESS OTHERWISE NOTED.
 ALL PLANTING AREAS TO BE IRRIGATED.
 FIRE HYDRANTS SHALL HAVE MIN. 3' CLEARANCE OF ANY LANDSCAPING TO ENSURE FIRE DEPARTMENT ACCESS.

IRRIGATION NOTES

LANDSCAPE CONTRACTOR SHALL SUBMIT A SHOP DRAWING OF THE IRRIGATION PLAN TO THE LANDSCAPE ARCHITECT FOR REVIEW.
 CONTRACTOR SHALL PROVIDE PVC SLEEVES UNDER PAVEMENT AND DRIVE AISLE FOR IRRIGATION AND ELECTRIC POWER.
 LIMIT OF IRRIGATION IS AREA OF IMPROVEMENT FOR THE NEW SITE DEVELOPMENT AS SHOWN ON PLAN. VERIFY LIMITS WITH LANDSCAPE ARCHITECT.
 IRRIGATION DESIGN AND CONSTRUCTION IS TO BE PROVIDED BY CONTRACTOR FOR TURF AND PLANTING AREAS.
 IRRIGATION SYSTEM IS TO BE DESIGNED AND CONSTRUCTED PER REGIONAL CONDITIONS AND LOCAL REGULATIONS.

LANDSCAPE REQUIREMENTS

TREES REQUIRED ON SITE IS DETERMINED BY THE SITE PERIMETER OF THE LOT DIVIDED BY 40.
 LOT PERIMETER = 2,749 LF / 40 = 68.73
 IN ADDITION TO THE REQUIRED NUMBER OF OVERSTORY TREES, A FULL COMPLEMENT OF UNDERSTORY TREES AND SHRUBS SHALL BE PROVIDED TO COMPLETE A QUALITY LANDSCAPE TREATMENT.
 MINIMUM SIZE OF REQUIRED OVERSTORY TREES FOR BUILDING HEIGHT GREATER THAN 24' AS FOLLOWS:

ORNAMENTAL	COMPLEMENTARY	ACCENT	PRIMARY	FULL	DIECODIOUS	CONFEROUS	% OF TOTAL	MIN. REQ.
2" CAL.	2.5" CAL.	3.5" CAL.	4.5" CAL.	5.5" CAL.	2" CAL.	5" HT. OR LESS	5%	5
							25%	18
							20%	14
							20%	14

PROPOSED LANDSCAPE PLANTINGS

OVERSTORY TREES PROPOSED ON SITE = 95
 ORNAMENTAL TREES PROPOSED ON SITE = 38
 SHRUBS PROPOSED ON SITE = 880
 PERENNIALS PROPOSED ON SITE = 2,792

A34

7171 FRANCE AVENUE S.
 EDINA, MINNESOTA
 LANDSCAPE PLAN

ANDERSON KM BUILDERS
 4220 PARK GLEN ROAD
 ST. LOUIS PARK, MN 55418

ANDERSON COMPANIES
 Generalist in Landscape Management Services

REVISIONS

DATE: 4/24/13

7171 FRANCE AVENUE S.
 EDINA, MINNESOTA
 LANDSCAPE PLAN

DATE: 4/24/13

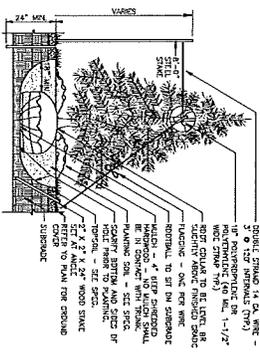
GENERAL NOTES

1. LANDSCAPE CONTRACTOR TO VERIFY ALL UTIL LOCATIONS OR PROPERTY WITH THE GENERAL CONTRACTOR AND BY OBTAINING COPIES OF THE CITY AND COUNTY RECORDS FOR THE WORKING AREA. VERIFY ALL UTIL LOCATIONS AND DEPT. RECORDS FOR THE WORKING AREA.
2. VERIFY ALL UTIL LOCATIONS AND DEPT. RECORDS FOR THE WORKING AREA.
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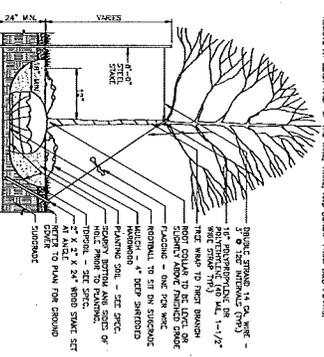
IRRIGATION NOTES

1. LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A SLOPE INDICATING THE DIRECTION OF DRAINAGE. THE SLOPE SHALL BE INDICATED BY THE LANDSCAPE CONTRACTOR TO THE GENERAL CONTRACTOR.
2. VERIFY DRAINAGE/IRRIGATION SYSTEMS LOCATION.
3. VERIFY DRAINAGE/IRRIGATION SYSTEMS LOCATION.
4. VERIFY DRAINAGE/IRRIGATION SYSTEMS LOCATION.
5. VERIFY DRAINAGE/IRRIGATION SYSTEMS LOCATION.

CONIFEROUS TREE



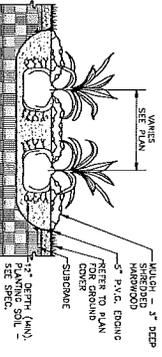
DECIDUOUS TREE



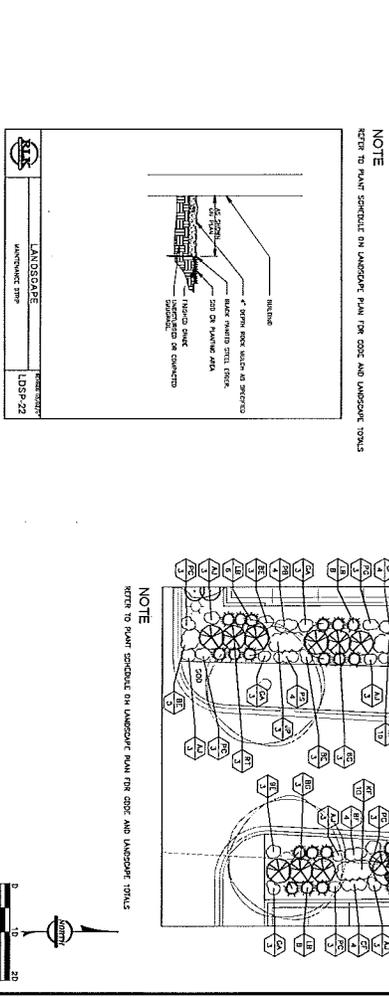
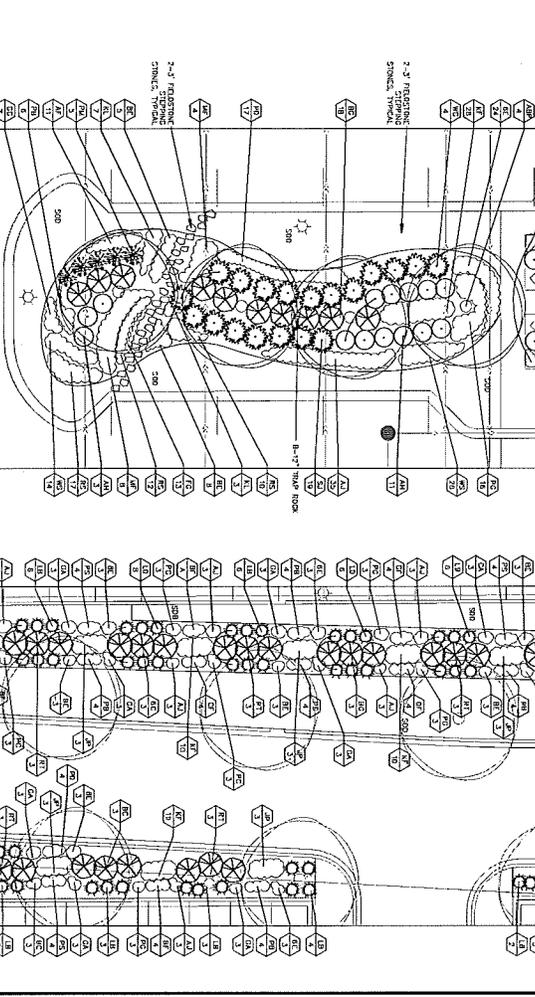
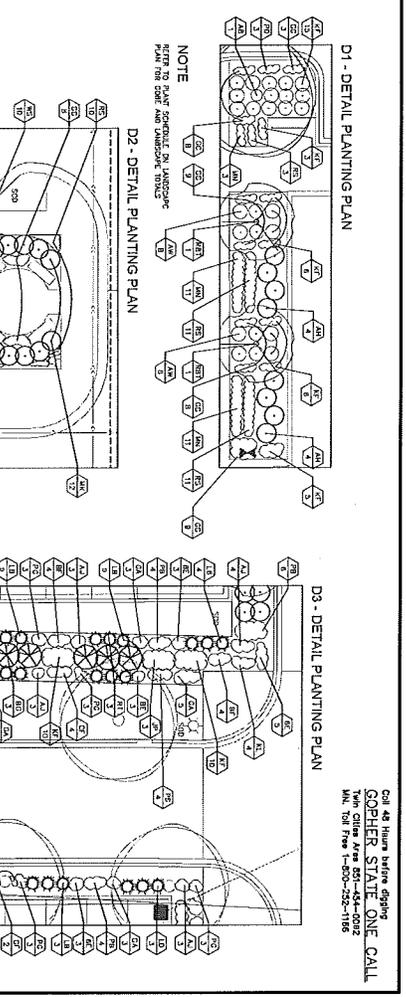
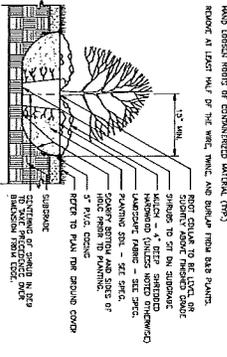
PLANTING SPECIFICATIONS

1. PLANTING AND MAINTENANCE SHALL BE PERFORMED BY A LICENSED LANDSCAPE CONTRACTOR.
2. PLANTING AND MAINTENANCE SHALL BE PERFORMED BY A LICENSED LANDSCAPE CONTRACTOR.
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PERENNIAL



SHRUB



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GOPHER STATE ONE CALL
 10th Chiles Ave. Ste. 400-4002
 St. Louis, MO 63116
 Toll Free 1-800-252-7166

PREVIOUS	DATE	BY
DESIGNED	10/29/22	ANDREW R. WESTERLUND
DRAWN		
CHECKED		

PROJECT NO. 44018

ANDERSON KM BUILDERS
 4220 PARK GLEN ROAD
 ST. LOUIS PARK, MN 55416

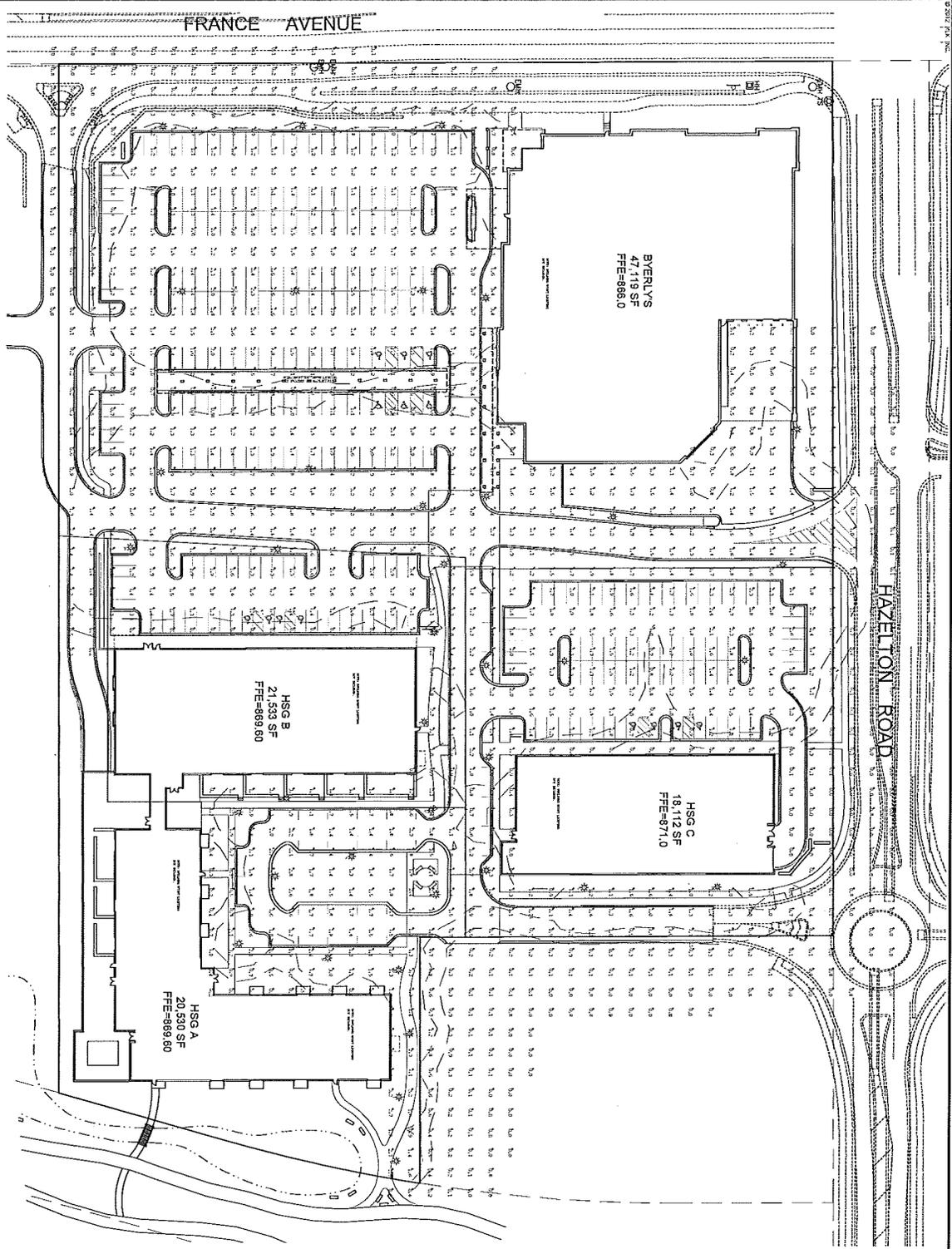
ANDERSON COMPANIES
 2025 W. Bank of America Development Dr. St. Louis, MO 63103

71 FRANCE
 7171 FRANCE AVENUE S.
 EDINA, MINNESOTA

LANDSCAPE DETAILS

DATE: 4/24/13

144



BVERLY'S
47,119 SF
FFE=886.0

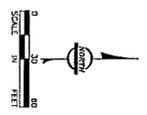
HSG B
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FFE=889.80

HSG C
18,112 SF
FFE=871.0

HSG A
20,530 SF
FFE=889.80

FINISHES LISTED ARE THE FOLLOWING:

NO.	FINISH	DESCRIPTION	UNIT	AMOUNT	FINISH	UNIT	AMOUNT
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2	AC	ACRYLIC	SQ. YD.	1,200	2	AC	ACRYLIC
3	AC	ACRYLIC	SQ. YD.	1,200	3	AC	ACRYLIC
4	AC	ACRYLIC	SQ. YD.	1,200	4	AC	ACRYLIC
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50	AC	ACRYLIC	SQ. YD.	1,200	50	AC	ACRYLIC



This lighting plan specifies luminaires shown on fixture schedule. Any deviation from this plan or type of luminaire shall be the responsibility of the contractor, and shall be noted on the fixture schedule for prior approval.

These calculations are not valid for cases or other situations not shown on this plan.

Calculations have been performed with our best interpretation of the information provided. The information is not intended to be used for legal purposes. The information is provided for informational purposes only. The information is not intended to be used for legal purposes. The information is provided for informational purposes only.



THIS PLAN IS A COPY OF THE ORIGINAL PLAN. ANY CHANGES TO THIS PLAN SHALL BE MADE BY THE ORIGINAL DESIGNER. THIS PLAN IS NOT TO BE USED FOR CONSTRUCTION WITHOUT THE WRITTEN APPROVAL OF THE ORIGINAL DESIGNER.

NO.	FINISH	DESCRIPTION	UNIT	AMOUNT
1	AC	ACRYLIC	SQ. YD.	1,200
2	AC	ACRYLIC	SQ. YD.	1,200
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48	AC	ACRYLIC	SQ. YD.	1,200
49	AC	ACRYLIC	SQ. YD.	1,200
50	AC	ACRYLIC	SQ. YD.	1,200



SOUTH ELEVATION

81'-2" average

Average FFE above grade:
4' - 4"

0' 8' 16' Scale: 1" = 8' - 0"



WEST ELEVATION

81'-2" average

Average FFE above grade:
4' - 4"

0' 8' 16' Scale: 1" = 8' - 0"

A43



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FRANCE AVENUE
RE-DEVELOPMENT



Schafer
Richardson

ANDERSON
COMPANIES



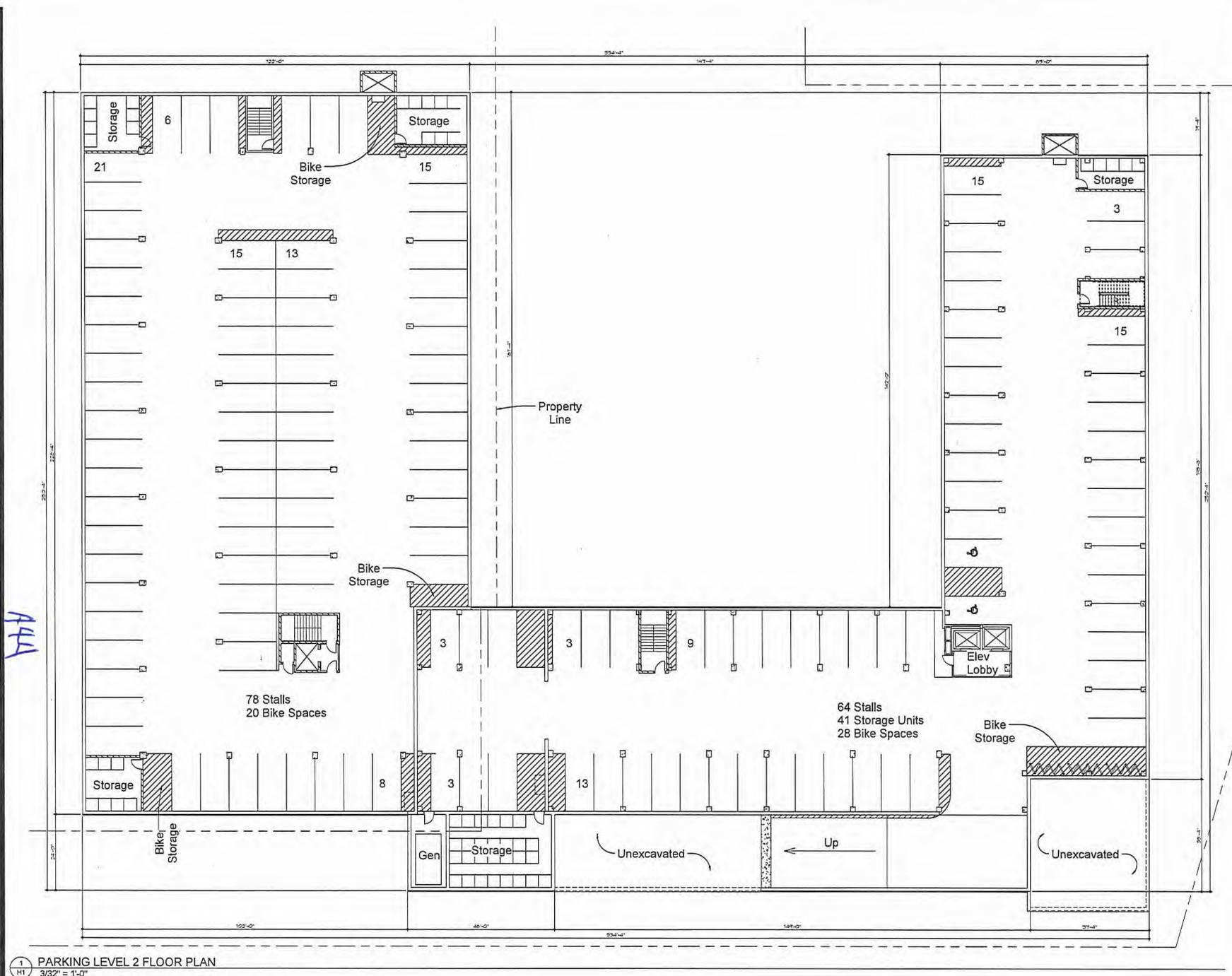
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Issue and Revision:	
Final	04/25/13
Development Plan	

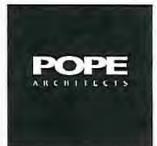
BUILDING A
ELEVATIONS

Revision No. 15226-11051
Drawn by RH
Checked by

A3.3



1
H1 PARKING LEVEL 2 FLOOR PLAN
3/32" = 1'-0"



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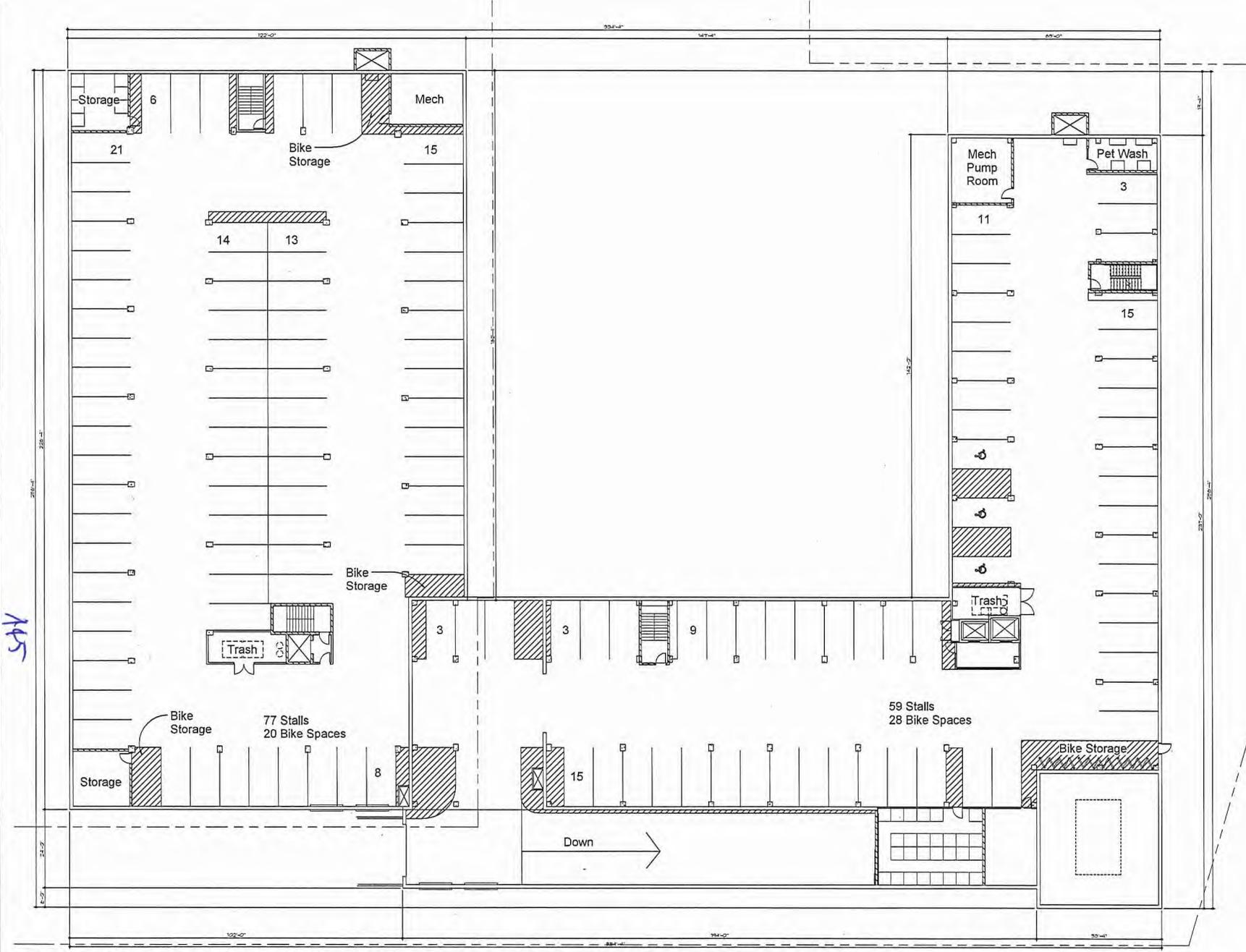
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PLAN 4-24-13

BUILDINGS A & B
PARKING LEVEL 2
FLOOR PLAN

Final Development
4/24/13
Plan

15226-13017
MB
MM

SHEET
H1



1 PARKING LEVEL 1 FLOOR PLAN
 3/32" = 1'-0"



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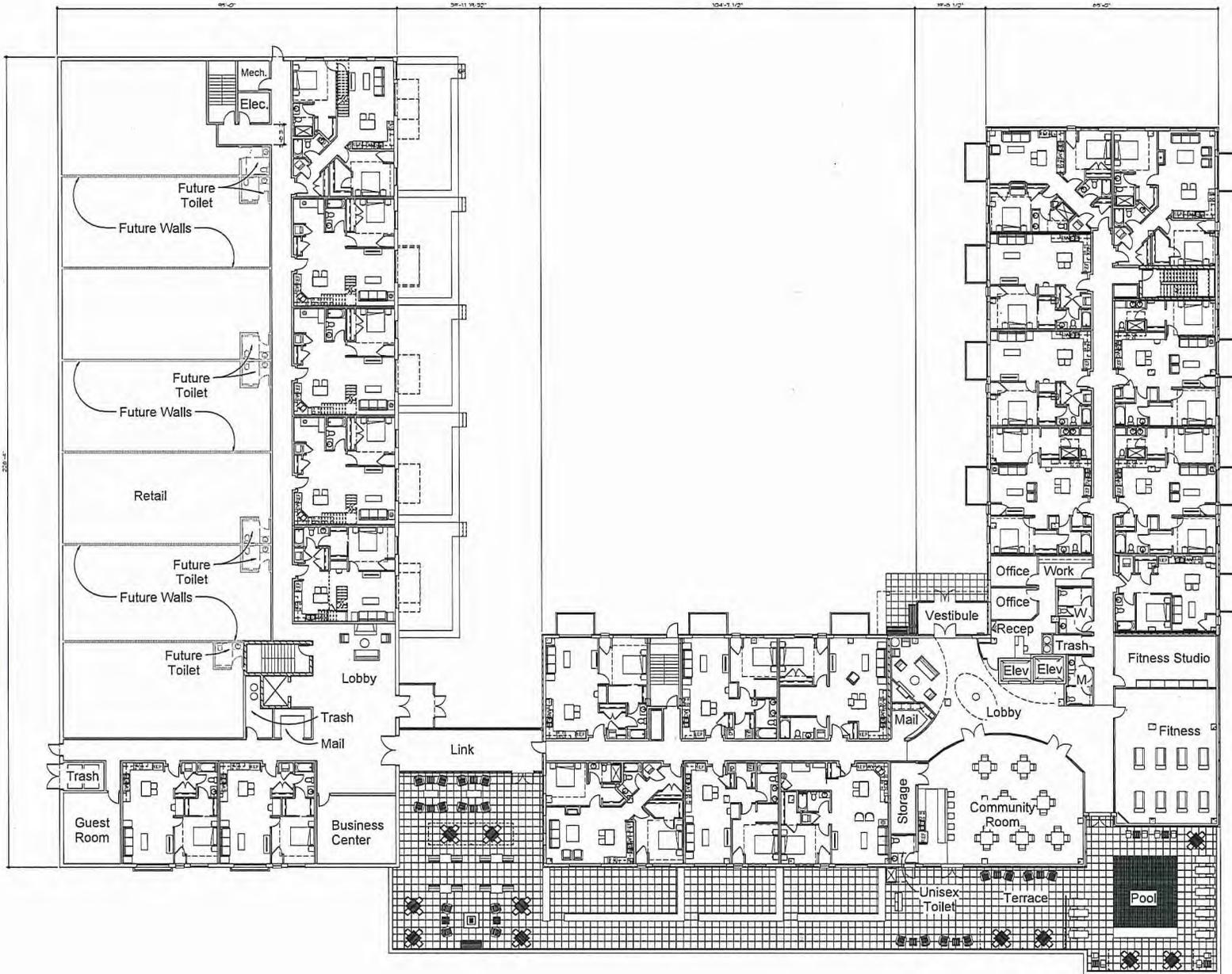
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 PLAN 4-24-13

BUILDINGS A & B
 PARKING LEVEL 1
 FLOOR PLAN

Final Development Plan 4/24/13

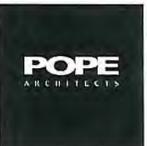
15226-13017
 Title: MB
 Date: MM

H2



A46

1 MAIN LEVEL FLOOR PLAN
3/32" = 1'-0"



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Construction Safety & Equipment Division



FINAL
DEVELOPMENT
PLAN 4-24-13

BUILDINGS A & B
MAIN LEVEL
FLOOR PLAN

15-14
Final Development 4/24/13
Plan

15226-12017
Date MB
Scale MM

010107
H3



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 ST. PAUL, MN 55108-2735
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Commercial Building & Construction Services



FINAL
 DEVELOPMENT
 PLAN 4-25-13

Issue and Revision:	
Final	04/25/13
Development Plan	

BUILDING A
 ELEVATIONS

Contract No.	15226-11051
Drawn by	RH
Checked by	

SHEET
A3.4



NORTH ELEVATION

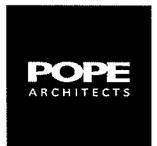
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 Scale: 1" = 8' - 0"

A5.2



EAST ELEVATION

Average FFE above grade: 4' - 4"
 Scale: 1" = 8' - 0"



POPE ARCHITECTS
 2295 BANDANA BLVD N, SUITE 200
 ST. PAUL, MN 55208-2735
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Schafer
 Richardson

ANDERSON
 COMPANIES



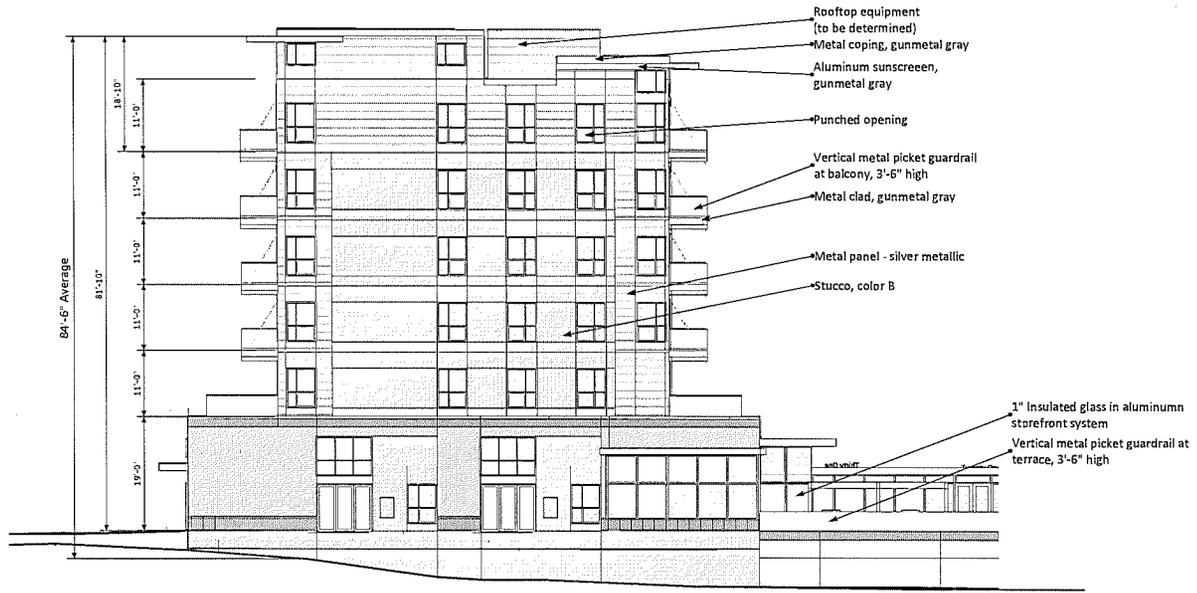
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 PLAN 4-25-13

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Final	04/25/13
Development Plan	

BUILDING B
 ELEVATIONS

Sheet no. 15226-11051
 Date by RH
 Drawn by
 Checked by
 SHEET

A3.5



SOUTH ELEVATION

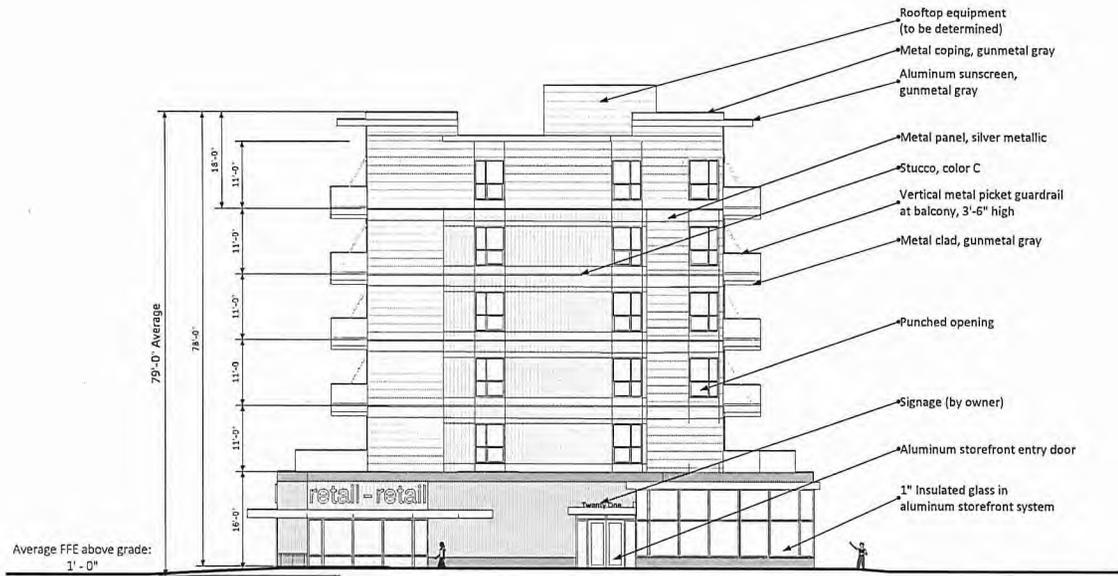
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A3.3



WEST ELEVATION

Scale: 1" = 8' - 0"



SOUTH ELEVATION

Scale: 1" = 8' - 0"



WEST ELEVATION

Scale: 1" = 8' - 0"



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BUILDING C
ELEVATIONS

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SHEET
A3.7



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DEVELOPMENT
PLAN 4-25-13

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Development Plan	

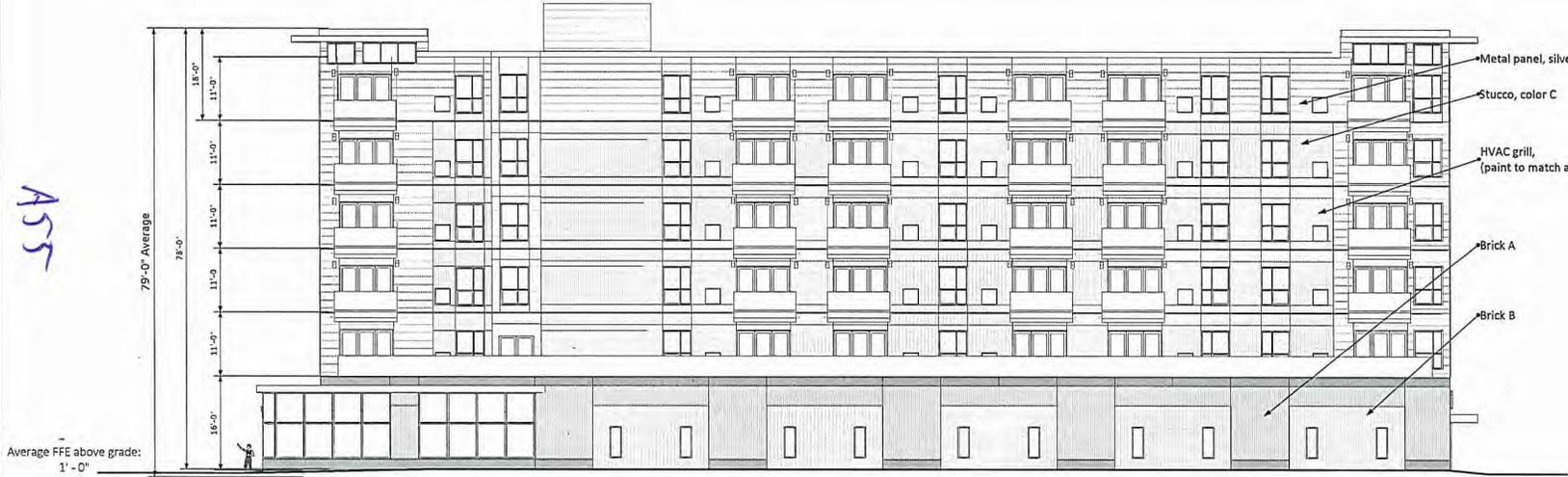
BUILDING C
ELEVATIONS

Revision No.	15226-11051
Drawn By	RH
Checked By	

Sheet
A3.8



NORTH ELEVATION



EAST ELEVATION

TRAFFIC IMPACT ANALYSIS

for

Byerly's France Avenue Redevelopment

in

Edina, MN

January 16, 2013



RLK Project No. 2012-135-M

Creating extraordinary
Communities

A54

TRAFFIC IMPACT ANALYSIS

Byerly's France Avenue Redevelopment

Edina, MN

January 16, 2013

Prepared For:



The CITY of
EDINA

...For living, learning, raising families & doing business

CITY OF EDINA
4801 W. 50th Street
Edina, MN 55424

Prepared By:



RLK INCORPORATED
Minnetonka Office
6110 Blue Circle Drive, Suite 100
Minnetonka, MN 55343
952-933-0972

RLK Project No. 2012-135-M

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A59

A. REPORT LETTER

January 16, 2013

Mr. Wayne Houle
Public Works Director
City of Edina
4801 w. 50th Street
Edina, MN 55424

RE: Traffic Impact Analysis – Byerly’s France Avenue Redevelopment

Dear Mr. Houle:

The attached report details the traffic impact analysis of the Byerly’s France Avenue Redevelopment at southeast corner of France Avenue (County Road 17) and Hazelton Road in Edina. The developer, Anderson-KM Builders, is proposing to redevelop the existing grocery store site into a mixed use development. The proposed redevelopment will encompass a new 52,500 square foot grocery store, an additional 20,950 square feet of retail and 234 units of multi-family housing.

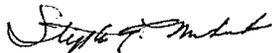
The redevelopment utilizes two full access intersections onto Hazelton Road, as well as a new right-in/right-out intersection on France Avenue. In addition, there is cross-access to be allowed between this site and the Macy’s Furniture Store site to the south.

RLK analyzed two peak times in its analysis – Weekday PM Peak Hour and Weekend Midday Peak. These two peak periods reflected the critical time periods among the three main land uses on the site.

The report found that the Byerly’s France Avenue Redevelopment will result in acceptable levels of service for traffic using these intersections during the tested peak hours. The two access points along Hazelton will adequately serve the trips into and out of the site – especially when the easterly intersection is signalized. The right-in/right-out intersection on France will not adversely affect the northbound operation of France Avenue.

Therefore, it is recommended that the City of Edina accept these findings in their consideration of the Byerly’s France Avenue Redevelopment.

Sincerely,



Stephen J. Manhart, P.E., PTOE, PTP
Senior Traffic Engineer

Attachment

B. PROPOSED DEVELOPMENT AND STUDY AREA

1. Description of Proposed Development

Anderson-KM Builders, LLC plans to redevelop an existing Byerly's Store at 7171 France Avenue South in the City of Edina, Hennepin County, MN. The site is to be redeveloped to replace the existing grocery store with a new 52,500 square foot grocery store and additional 20,950 square feet of retail and 234 units of multi-family housing. It is anticipated that the new grocery would be open mid-August of 2014, and the multi-family housing and additional retail would open in phases between mid-November, 2014 and late 2015.

The purpose of this study is to determine the potential traffic and parking impacts of the proposed redevelopment.

2. Map of Site and Street Network

Figure 1 illustrates the development site and the study area street network. RLK analyzed traffic in the area bounded by France Avenue on the west, Hazelton Road on the north, York Avenue South on the east and 72nd Street on the south. RLK also looked at the interaction of trips between the proposed Byerly's redevelopment site and the Macy's Furniture Store to the south. Figure 2 shows the overall street network to be analyzed in the study area.

3. Intersections and Roadway Segments to be Analyzed

Existing intersections to be analyzed include:

- France Avenue (County State Aid Highway 17) and Hazelton Road
- France Avenue (County State Aid Highway 17) and W. 72nd Street
- York Avenue (County State Aid Highway 31) and Hazelton Road
- Byerly's Main Access and Hazelton Road
- Byerly's Service Access and Hazelton Road

This study will also focus on the following proposed driveway intersections:

- Byerly's new Truck/Secondary Access onto Hazelton Road (unsignalized)
- Byerly's new Main Access onto Hazelton Road (proposed as signalized)
- New Access onto France Avenue (proposed right-in/right-out)

C. EXISTING TRAFFIC CONDITIONS

1. Existing Traffic Statistics

To address the impacts of a development on the surrounding roadway system, it is necessary to first understand the existing traffic conditions prior to the area's redevelopment. The site exists today with a Byerly's grocery store encompassing 59,359 square feet. In addition, there are 448 parking stalls serving the grocery store.

PM peak hour and Saturday Midday peak hour turning movement counts were conducted during November 3 and 7, 2012. The two peak times analyzed – P.M. Peak Hour and Saturday Midday Peak Hour – correspond to the heaviest commercial traffic in the area.

Figure 3 shows an aerial photograph of the existing intersections and driveways serving the Byerly's site. Figure 4 shows the average daily traffic along the routes within the study area. Figure 5 shows the peak hour turning movements at the major intersections within the study area.

AM peak hour counts were previously conducted by Hennepin County at several nearby intersections along France Avenue. The AM peak hour counts were found to be 20% to 25% lower than the PM peak or Saturday peak counts. Therefore, only the PM and Saturday peak hours were analyzed with this study. These counts were used as the existing baseline conditions for the area.

According to recent traffic studies conducted for the City of Edina, it has been found that France Avenue traffic has decreased. In 2009, two-way traffic counts on France Avenue varied from 26,000 vehicles/day to 28,500 vehicles per day. In 2011, it was found that France Avenue daily traffic volumes have decreased to a range between 24,300 vehicles per day to 27,800 vehicles per day.¹

2. Existing Roadway Geometrics

Vehicular access to the site from the development area is expected to occur primarily on two roadways: France Avenue and Hazelton Road. The following text details the specifics of each of these roadways:

- **France Avenue S. (County State Aid Highway 17)** is functionally classified as an A-Minor Arterial-Reliever route. It exists as a six-lane divided roadway with separated left and right turn lanes in an urban cross section which travels primarily north/south through the City of Edina. In the area of the proposed site, France Avenue has a posted speed limit of 40 mph. Average daily traffic (ADT) just south of Gallagher Drive is 28,000 veh/day, as measured in 2011².
- **Hazelton Road** is functionally classified as a collector. It is a four-lane undivided roadway with an urban cross section which travels east/west. Hazelton Road, also classified as Municipal State

¹ PowerPoint presentation from France Avenue TE S.P. 120-020-037 Intersection Enhancements Stakeholders Meeting #1, May 31, 2012.

² 2011 Publication Traffic Volumes Metro Street Series – 4D, Minnesota Department of Transportation, Office of Transportation Data and Analysis, Traffic Volume Program, <http://www.dot.state.mn.us/traffic/data/index.html>

Aid System Route 166, has a posted speed limit of 30 mph. Hazelton has an ADT of 7,600 veh/day just east of the proposed site, as measured in 2009.

- **York Avenue S. (County State Aid Highway 31)** is functionally classified as a Major Collector, and is a four-lane divided County roadway with an urban cross section and 35 mph speeds. ADT on York Avenue south of Hazelton is 15,000 veh/day, as measured in 2011.

The following summary lists the numbers of lanes and types of traffic control at intersections, as well as posted speeds, no parking zones in the area:

- France Avenue at Hazelton Road - Traffic Signal control; On-Street Parking prohibited; Speed Limit 40 mph on France; Speed Limit 30 mph on Hazelton.
 - SB France Ave approaching Hazelton – one shared through/right, two through, one left;
 - NB France Ave approaching Hazelton – one free right, three through, one left;
 - EB Hazelton approaching France Ave – one shared left/through/right lane;
 - WB Hazelton approaching France Ave – one free right, one through, one left.
- Hazelton Road at westerly Byerly's Site Entrance – Side Street Stop Sign control; On-Street Parking prohibited; Speed Limit 30 mph on Hazelton.
 - SB Development Driveway (Rue de France) approaching Hazelton Road – one shared right/through/left lane;
 - NB Byerly's westerly driveway approaching Hazelton Road – one right lane, one shared through/lane;
 - WB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, one shared through/left lane;
 - EB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, one shared through/left lane
- Hazelton Road at easterly Byerly's Site Entrance – Side Street Stop Sign control; On-Street Parking prohibited; Speed Limit 30 mph on Hazelton.
 - SB Development Driveway (Rue De France) approaching Hazelton Road – one shared right/through/left lane;
 - NB Byerly's westerly driveway approaching Hazelton Road – one shared right/through/left lane (widened for truck movements);
 - WB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, one shared through/left lane;
 - EB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, one shared through/left lane
- York Avenue at Hazelton Road – Traffic Signal control; On-Street Parking prohibited; Speed Limit 35 mph on York; Speed Limit 30 mph on Hazelton.
 - SB York Ave approaching Hazelton – one right, two through, one left lane;
 - NB York Ave approaching Hazelton – one left, one through, one shared through/right;
 - EB Hazelton approaching York Ave – one left/through, one right;

- WB Development Driveway approaching York Ave – one shared left/through/right lane.
- France Avenue at 72nd Street – Side Street Stop Sign control; On-Street Parking prohibited; Speed Limit 40 mph on France; Speed Limit 30 mph on 72nd Street.
 - SB France Ave approaching 72nd St – one through/right, two through;
 - NB France Ave approaching 72nd St – three through lanes;
 - EB 72nd St approaching France Ave – one shared left/through/right lane.
- France Avenue at Gallagher Drive – Traffic Signal control; On-Street Parking prohibited; Speed Limit 40 mph on France; Speed Limit 30 mph on Gallagher.
 - EB Gallagher Drive approaching France Avenue – one through/right, one left;
 - WB Gallagher Drive approaching France Avenue – one through/right, one left;
 - SB France Avenue approaching Gallagher Drive – one shared through/right, two through, one left lane
 - NB France Avenue approaching Gallagher Drive – one shared through/right, two through, one left lane

Sidewalks are currently provided on one side of France Avenue in this study area. A six-foot wide sidewalk exists adjacent to the curb along the west side of France Avenue between 66th Street to 76th Street. On the east side of France, there is no sidewalk between Hazelton Road and Gallagher Drive. There are six-foot sidewalks along the north and south sides of Hazelton Road from France Avenue to York Avenue.

Metro Transit bus routes 6 and 587 currently serve this area. Bus stops are currently located along the both sides of France Avenue at Hazelton and at 72nd Street, as well as one located on the west side of France at Gallagher Drive. Along Hazelton, there are bus stops located on the north and south side of the street just west of the Target Store and just east of the Edina Promenade.

Existing rights-of-way vary within the study area. From Hazelton Road south to 72nd Street, the half-street right-of-way narrows from 75 to 63 feet (for the right turn lane). The westerly half-street right-of-way of France Avenue in this segment is 50 feet. On Hazelton Road, the half-street right-of-way on the south half of the street is 36 feet, while the north half street right-of-way varies from 36 to 44 feet (again for the right turn lane).

3. Operational Analysis Methodology

Operational analyses were conducted at the study intersections per the 2000 Highway Capacity Manual (HCM), which reports a level of service based on the delay experienced by vehicles at a particular intersection. The level of service concept indicates a measure of average operating conditions at intersections during an hour. Service levels range from A through F with each level defined by a range of control delay per vehicle. Table 1 lists the level of service criteria used to determine acceptable traffic operation.

Traffic operations for peak hour conditions within the study area were analyzed using the industry-standard Synchro/SimTraffic software package, which uses the data and methodology contained in the

HCM, published by the Transportation Research Board. The software model was calibrated using existing conditions before being used to assess future conditions.

The operating conditions of transportation facilities, such as traffic signals and stop-controlled intersections, are evaluated based on the relationship of the theoretical capacity of a facility to the actual traffic volumes on that facility. Various factors affect capacity, including travel speed, roadway geometry, grade, number and width of travel lanes, and intersection control. The current standards for evaluating capacity and operating conditions are contained in the HCM. The procedures describe operating conditions in terms of a Level of Service (LOS). Facilities are given letter designations from "A," representing the best operating conditions, to "F," representing the worst. Generally, Level of Service "D" represents the threshold for acceptable overall intersection operating conditions during a peak hour.

Table 1. LOS Criteria

Signalized Intersections		AWSC & TWSC Intersections	
LOS	Control Delay (sec/Veh)	LOS	Control Delay (sec/Veh)
A	≤ 10	A	0 - 10
B	> 10 - 20	B	> 10 - 15
C	> 20 - 35	C	> 15 - 25
D	> 35 - 55	D	> 25 - 35
E	> 55 - 80	E	> 35 - 50
F	> 80	F	> 50

Source: Highway Capacity Manual 2000³

The acceptable threshold for a particular movement at an intersection depends on both the priority assigned to that movement and its traffic volume. In general, the higher the priority and the higher the traffic volume, the more stringent the acceptable threshold will be. For example, the acceptable threshold for a high-priority/high-volume suburban movement might be "D," while LOS "F" on a low-priority/low-volume urban movement might be appropriate.

For side-street stop-controlled intersections, a key measure of operational effectiveness is the side-street LOS. Long delays and poor LOS can sometimes result on the side street, even if the overall intersection is functioning well, making it a valuable design criterion. Again, depending on priority and traffic volume, acceptable side-street LOS can range from "D" to "F."

A final fundamental component of operational analyses is a study of vehicular queuing, or the lineup of vehicles waiting to pass through an intersection. An intersection can operate with an acceptable level of service, but if queues from the intersection extend back to block entrances to turn lanes or accesses to adjacent land uses, unsafe operating conditions could result. The 95th percentile queue, or the length of queue with a 5% chance of occurring during the peak hour, is considered the standard for design purposes.

For the purposes of this analysis, however, LOS "D" or "E" or better is considered acceptable for low-volume movements at certain intersections.

For this analysis, the industry-standard Synchro and SimTraffic software packages were used to analyze intersection operation. All operational and queue reports are found in the Appendices of this report.

³ Highway Capacity Manual, Special Report 209, Transportation Research Board, Washington D.C

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4. Existing Trip Generation

Based on the trip generation rates found in the Institute of Transportation Engineers' Trip Generation Manual, 9th Edition, this supermarket will generate just over 6,000 vehicular trips each weekday and more than 10,500 trips on a Saturday (See Table 2).

Table 2. Trip Generation Estimates¹ – Existing Byerly's Site

Land Use	Size	Weekday PM		Daily	Saturday Midday		Saturday Daily
		Enter	Exit		Enter	Exit	
Supermarket	59,359 sq. ft.	258	239	6,069	322	310	10,542
Total Existing Trips		258	239	6,069	322	310	10,542
		497			632		

1. Per the data and methodologies in Trip Generation, 9th Edition, published by ITE.

Table 3, below, summarizes the existing LOS at the primary intersections in the study area based on the current lane geometry, traffic control and traffic volumes. The table shows that all intersection are operating at an overall LOS D or better during both the weekday PM and Saturday peak hours with most movements operating at LOS E or better. The northbound left turn from the westerly Byerly's intersection has significant delay in the Saturday midday period, while the eastbound left turn from the unsignalized intersection of 72nd to northbound France Avenue shows significant delay during both study periods (despite minimal volumes). Full reports showing detailed LOS and delays by approach are included in the Appendix.

Table 3 - Existing Level of Service

Intersection	PM Peak Hour		Saturday Midday Peak Hour	
	LOS	Int. Delay (sec/veh)	LOS	Int. Delay (Sec/veh)
France Ave & Hazelton Rd.	B (D)	18	C (D)	22
Hazelton Rd. & Westerly Byerly's Drive	a (e)	7	a (f)	22
Hazelton Rd. & Easterly Byerly's Drive	a (c)	1	a (c)	1
Hazelton Rd. & York Ave.	C (D)	24	C (D)	30
France Ave. & 72 nd St.	a (f)	2	a (f)	1
France Ave. & Gallagher Dr.	B (C)	20	B (D)	16

NOTES: a. Upper Case letters reflect LOS at signalized intersections; Lower case letter reflect LOS at unsignalized intersection.

b. First letter reflects Intersection LOS; letter inside parentheses reflects LOS of worst movement.

c. Int. Delay reflects overall average intersection delay, not necessarily worst movement delay.

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5. Existing Queue Lengths

RLK conducted a study of the existing traffic conditions for P.M. Peak Hour and Saturday Midday traffic conditions. A queuing analysis was conducted for the main intersections/driveways for the P.M. Peak Hour. Traffic engineers use a measure called 95th percentile queue. The measurement defines the queue length (in feet) that has only a 5-percent probability of being exceeded during the analysis time period. If the 95th percentile queue length approaches or exceeds the segment length, mediation may be required. Results are shown in Tables 4a and 4b:

**Table 4a. Selected 95th Percentile Queue Lengths – Weekday P.M. Peak Hour
– Existing Byerly’s Site**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	162
Northbound Through France at Hazelton	402	420
Eastbound Left Hazelton at Byerly’s Driveway	256	43
Northbound Byerly’s Driveway at Hazelton	500	110

**Table 4b. Selected 95th Percentile Queue Lengths – Saturday Midday Peak Hour
– Existing Byerly’s Site**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	196
Northbound Through France at Hazelton	402	302
Eastbound Left Hazelton at Byerly’s Driveway	256	74
Northbound Byerly’s Driveway at Hazelton	500	154

Westbound Hazelton approach has available capacity. Northbound France Avenue has a queue length that at times extends back through the 72nd Street intersection during the p.m. peak hour. Other critical movements have available capacity at intersections.

D. FUTURE PROJECTED TRAFFIC CONDITIONS WITHOUT DEVELOPMENT

In the testing of the future projected traffic conditions without the development (i.e., the “No-Build” condition), an annual growth rate of 1% was assumed for background traffic. Under this scenario, it is assumed that the Byerly’s site remains as-is. Therefore, traffic volumes are projected to be constant for traffic entering and exiting the Byerly’s site.

The Byerly’s France Avenue Redevelopment project has a completion date for the final phase in 2014. Therefore, the typical date for traffic analysis is one year after completion, so as to assess the traffic once the pattern for drivers has sufficiently developed in the area.

To accurately assess traffic conditions with or without the development, traffic conditions will be assessed for the year 2015 without the development, but with the normal background growth on the street system.

1. Changes in Road Network

The City of Edina was successful in securing Federal Transportation Enhancement funding for the construction of Pedestrian / Intersection Enhancements along France Avenue (CSAH 17) at 76th Street, 70th Street and 66th Street. While not finalized, the recommended improvements include:

- Reducing the vehicle lanes to the minimum State Aid requirements on northbound France Avenue the entire length from 76th Street to 66th Street and on southbound France Avenue and the side streets only through the intersections at 66th Street, 70th Street and 76th Street.
- Removing and relocation of the France Avenue northbound outside curb from 76th Street to 66th Street and southbound outside curb at the intersections of 66th Street, 70th Street and 76th Street.
- Removing free right turn islands in all quadrants at 76th Street, in the southeast quadrant at 70th Street, in the southeast quadrant at 69th Street, in the southeast quadrant of the Southdale entrance, in the northeast quadrant of the Southdale exit and in the southeast, southwest and northeast quadrants at 66th Street.
- Widening the center median on France Avenue and the side streets to a 10 foot width only at the intersections of 66th Street, 70th Street and 76th Street.
- Providing an 8 foot landscaped boulevard on the eastside of France Avenue from 76th Street to 66th Street.
- Providing an 8 foot sidewalk on the eastside of France Avenue from 76th Street to 66th Street and on the west side of France Avenue only at the intersections of 66th Street, 70th Street and 76th Street.

- Providing a minimum 6 foot landscaped boulevard on the side streets at 66th Street, 70th Street and 76th Street.
- Providing a 6 foot sidewalk on the side streets where sidewalks currently exist at 66th Street, 70th Street and 76th Street.
- Either a 5 foot on-street bike lane or a shared lane with “Sharrow” eastbound and westbound on 66th Street, 70th Street and 76th Street through France Avenue.
- ADA compliant pedestrian ramps at all intersections and driveways on the eastside of France Avenue from 76th Street to 66th Street and on the west side of France Avenue at 66th Street, 70th Street and 76th Street.
- Revised traffic signal systems at 66th Street, 70th Street and 76th Street including APS pedestrian push buttons, countdown pedestrian signal timers, median refuge island pedestrian push buttons and new vehicle and bicycle detection systems.
- Urban design feature including, landscaping, monuments, planter boxes, bollards and colored or stamped concrete at the intersection of 66th Street, 70th Street and 76th Street.

Where applicable, these enhancements have been incorporated into the proposed future-year conditions and models for France Avenue (-- both No-Build and Build scenarios).

2. Background Development Traffic Growth

Despite recent decreases in traffic volumes, future traffic growth must be assumed. This background growth must be included in future year traffic forecasts. In order to account for some background growth in traffic, a traffic growth projection factor of 1.0 was used to project traffic to the 2015 and 2016 study years.

In addition to the regional background traffic, other specific none development related traffic was determined and included with the overall background traffic.

- Southdale Residential Apartment Development - Based on information received from WSB & Associates, Inc., a 232-unit 10-story apartment development is proposed for the southeast corner of the Southdale Center property. The apartment building would be constructed in the northwest corner of the intersection of York Street and 69th Street. The analysis assumes that all units will be occupied by 2015 and that the background trip generation will be included in the background of the 2015 and 2016 analysis.
- Future Southdale Restaurant Development – The analysis of the Southdale Residential Development included the analysis of a future restaurant that is proposed in northeast quadrant of France Avenue and 69th Street in the Southdale Center Parking lot. The restaurant was assumed to be 8,000 s.f. in size with approximately 300 seats. The analysis assumes the restaurant will be developed by 2014 and that the background trip generation for the restaurant will be the same for the 2015 and 2016 analysis scenarios.

- Centennial Lakes Coffee Shop -- Based on information received from WSB & Associates, Inc., a free-standing 1,750 square foot coffee shop building is proposed in the southeast quadrant of the intersection of France Avenue and Plaza Drive. The site has other potential uses (fast food restaurant, bank, pharmacy, specialty retail, etc.), but the primary analysis of the site assumed a coffee shop. The analysis assumes that all units will be occupied by 2015 and that the background trip generation will be included in the background of the 2015 and 2016 analysis.
- Proposed Restaurant/Retail Site -- A proposed restaurant or retail site is being proposed for the site of the former Szechuan Star restaurant to the east side of the Byerly's redevelopment property site. This site will be added to the long-term background traffic impacts.

In each case, RLK looked at the traffic studies prepared for each development and compared the added trips to the background traffic for each study scenario. In the short-term build alternates of the Southdale Residential Project, the 2014 Build Alternate 1 traffic on northbound France Avenue was 53 fewer trips in the P.M. Peak Hour than in the 2014 No-Build scenario for the project.⁴ The 2014 Build Alternate generated 20 more trips on northbound York Avenue for the Saturday Midday peak hour than in the 2014 No-build scenario for the project.

In the Centennial Lakes Plaza Coffee Shop Traffic Study, 21 trips were added to northbound and southbound lanes of France Avenue north of Plaza Drive in the P.M. Peak Hour.⁵ Similarly, 32 trips were added to northbound France Avenue in the Saturday Midday Peak Hour, and 30 trip were added to the southbound approach on France Avenue at Plaza Drive.

In each development case, the associated trips were added (or subtracted, as the case may be) to either France Avenue or York Avenue through movements in the Byerly's redevelopment analysis to represent these changes to the background developments.

3. No-Build Trip Distribution

If the No-Build scenario were to occur, all site traffic will continue to enter from and exit onto Hazelton Road at the two access driveways. The westerly driveway will continue to be the primary driveway serving the site, while the easterly driveway will continue as the delivery driveway. It is assumed that the existing trip distribution pattern will remain.

Site-generated trips will be unchanged from the existing trip distribution patterns.

⁴ Southdale Residential Traffic and Parking Study, prepared for the City of Edina by WSB & Associates, 09/04/12.

⁵ Centennial Lakes Coffee Shop Traffic Study, prepared for the City of Edina by WSB & Associates, 06/18/12.

4. Projected No-Build Traffic Volumes

To accurately project No-Build traffic volumes in the study area, it is assumed the existing Byerly's traffic volumes will not change while the background traffic will increase by the annual growth rate. To model this scenario, the trip generation for the existing grocery store was calculated and stripped from the existing traffic pattern based on percentages at each intersection. The remaining traffic is considered the Background Traffic.

The Background Traffic volumes were then increased by the overall growth rate (1% annually) and the additional development background trips (described above) were added. Finally, the existing grocery trips were added back in to reflect the No-Build scenario.

Figure 5 shows the 2015 No-Build turning movement volumes for the PM Peak Hour and for the Saturday Midday Peak Hour.

5. 2015 No-Build Traffic Operations

Traffic operations were tested for the 2015 No-Build Scenario assuming the roadway enhancements along France Avenue have been implemented. In addition, the following geometric improvements are assumed along Hazelton:

- France Avenue at Hazelton Road - Traffic Signal control; On-Street Parking prohibited; Speed Limit 40 mph on France; Speed Limit 30 mph on Hazelton.
 - SB France Ave approaching Hazelton – one shared through/right, two through, one left;
 - NB France Ave approaching Hazelton – one free right, three through, one left;
 - EB Hazelton approaching France Ave – one shared left/through/right lane;
 - WB Hazelton approaching France Ave – one free right, one through, one left.

- Hazelton Road at westerly Byerly's Site Entrance – Side Street Stop Sign control; On-Street Parking prohibited; Speed Limit 30 mph on Hazelton. (Note: Modification to ¾ access not assumed under No-Build scenario.)
 - SB Development Driveway (Rue de France) approaching Hazelton Road – one shared right/through/left lane;
 - NB Byerly's westerly driveway approaching Hazelton Road – one right lane, one shared through/lane;
 - WB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, one left lane;
 - EB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, one left lane serving as the west end of the Two-Way Left Turn Lane (TWLTL).

- Hazelton Road at easterly Byerly's Site Entrance – Side Street Stop Sign control; On-Street Parking prohibited; Speed Limit 30 mph on Hazelton.
 - SB Development Driveway (Rue De France) approaching Hazelton Road – one shared right/through/left lane;

- NB Byerly's westerly driveway approaching Hazelton Road –one shared right/through/left lane (widened for truck movements);
 - WB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, left turn opportunity in the TWLTL;
 - EB Hazelton Road approaching westerly Byerly's Site Entrance – one shared through/right lane, left turn opportunity in the TWLTL
- York Avenue at Hazelton Road – Traffic Signal control; On-Street Parking prohibited; Speed Limit 35 mph on York; Speed Limit 30 mph on Hazelton.
 - SB York Ave approaching Hazelton – one right, two through, one left lane;
 - NB York Ave approaching Hazelton – one left, one through, one shared through/right;
 - EB Hazelton approaching York Ave – (Note: TWLTL assumed to end prior to York Avenue) one left/through, one right;
 - WB Development Driveway approaching York Ave – one shared left/through/right lane.

The levels of service for the study intersections are shown on Table 5.

**Table 5. Projected Levels of Service – 2015 No-Build Condition
(Assumes France Avenue Enhancements)**

Intersection	PM Peak Hour		Saturday Midday Peak Hour	
	LOS	Int. Delay (sec/veh)	LOS	Int. Delay (Sec/veh)
France Ave & Hazelton Rd.	B (F-SBLt)	17.7	C (F-SBLt)	33.3
Hazelton Rd. & Westerly Byerly's Drive	a (d)	5.4	b (f-NBLt))	53.9
Hazelton Rd. & Easterly Byerly's Drive (Signalized or unsignalized)	N.A.	N.A.	N.A.	N.A.
	a (c)	1.0	a (c)	1.7
Hazelton Rd. & York Ave.	C (F-NBLt)	31.6	C (F-NBLt)	31.4
France Ave. & 72 nd St.	a (f-EB)	2.0	a (f)	1.5
France Ave. & Gallagher Dr.	B (F-SBLt)	19.4	B (F-SBLt)	18.6

NOTES: a. Upper Case letters reflect LOS at signalized intersections; Lower case letter reflect LOS at unsignalized intersection.
 b. First letter reflects Intersection LOS; letter inside parentheses reflects LOS of worst movement.
 c. Int. Delay reflects overall average intersection delay, not necessarily worst movement delay.

The associated 95th percentile queue lengths for the 2015 No-Build condition are listed on Tables 6a and 6b.

**Table 6a. Selected 95th Percentile Queue Lengths' – Weekday P.M. Peak Hour
– 2015 No-Build Conditions**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	139
Southbound Left France at Hazelton	360	419
Northbound Right France at Hazelton	315	99
Westbound Right France at Hazelton	150	125
Eastbound Left Hazelton at Byerly's Driveway	259	41
Northbound Byerly's Driveway at Hazelton	270	78
Northbound Left York at Hazelton	160	181
Westbound Left Turn Gallagher at France	120	79
Southbound Left Turn France at Gallagher	230	171

**Table 6b. Selected 95th Percentile Queue Lengths' – Saturday Midday Peak Hour
– 2015 No-Build Conditions**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	201
Southbound Left France at Hazelton	360	394
Northbound Right France at Hazelton	315	119
Westbound Right France at Hazelton	150	114
Eastbound Left Hazelton at Byerly's Driveway	259	46
Northbound Byerly's Driveway at Hazelton	270	156
Northbound Left York at Hazelton	160	185
Westbound Left Turn Gallagher at France	120	105
Southbound Left Turn France at Gallagher	230	268

F. FUTURE PROJECTED TRAFFIC CONDITIONS WITH DEVELOPMENT

1. Proposed Development Phases

There are several phases proposed for the Byerly's Redevelopment Project:

- A. Construction of a new 52,500 square-foot grocery store – completed by mid-August 2014;
- B. Construction of a new 109-unit mid-rise apartment building – completed by mid-November 2014;
- C. Construction of a new 77-unit mid-rise apartment building with 10,450 square feet of retail space on the ground floor – completed by mid- to late-2015.
- D. Construction of a new 48-unit mid-rise apartment building with 10,500 square feet of retail space on the ground floor – completed by mid- to late-2015.

The staging of these phases is such as to allow the existing grocery store to remain open until the new store is completed, then the old store will close and the remaining apartment and retail will be constructed.

Therefore, two Build scenarios are to be tested:

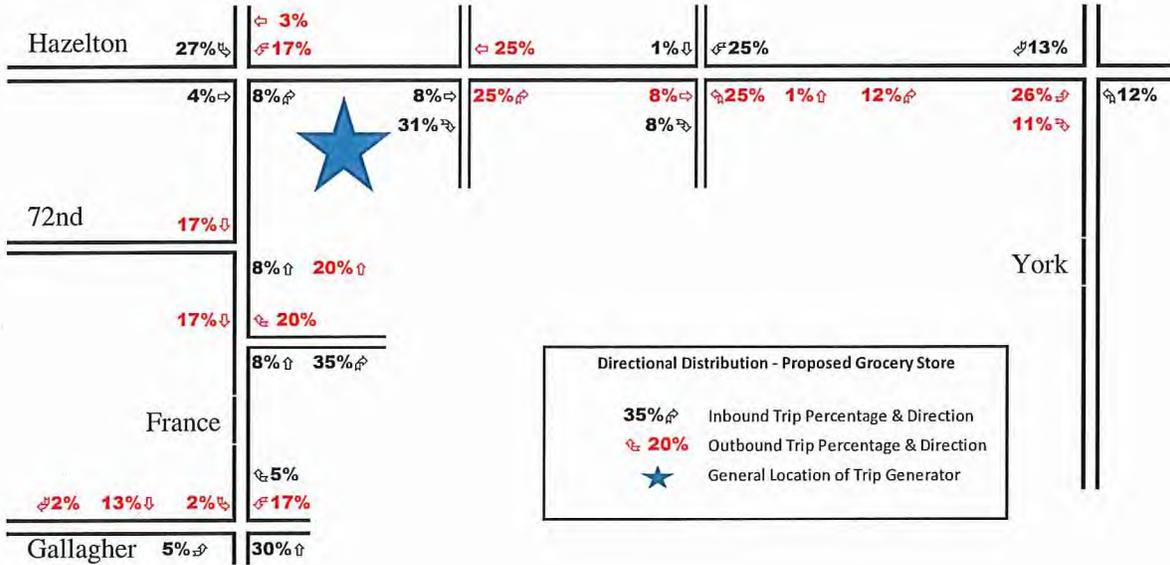
- 2015 Build – incorporating the new grocery store and the 109-unit mid-rise apartment building;
- 2016 Build – incorporating the 2015 Build land uses with the two remaining mid-rise apartments and ground floor retail.

2. Proposed Directional Distribution

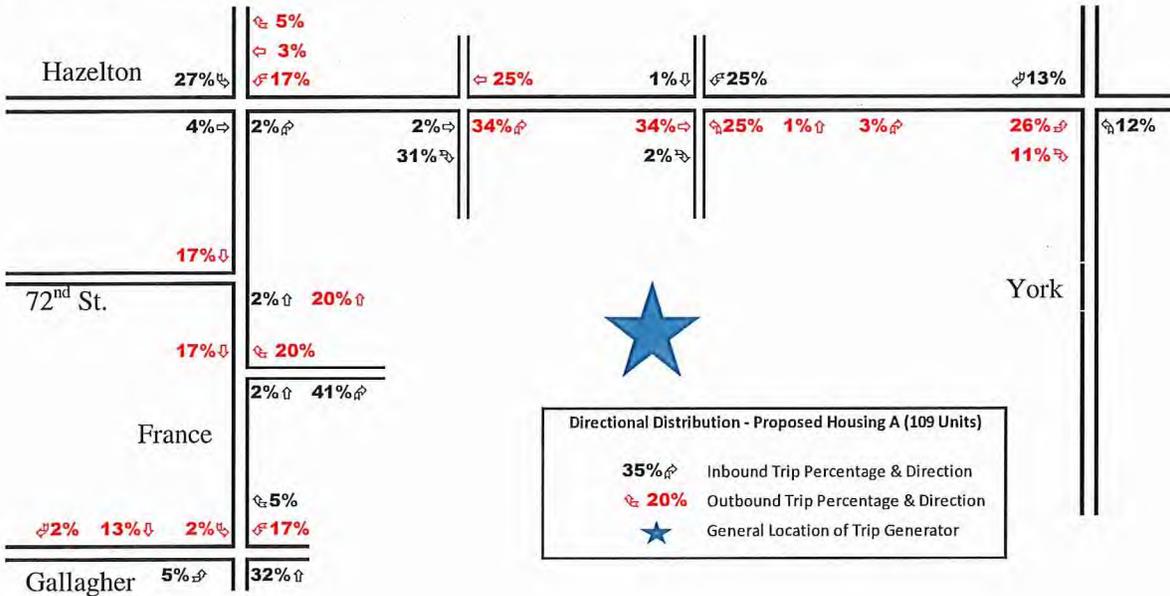
Because of the proposed site layout, access to the various buildings will be possible from more than one driveway. In addition, some of these driveways will be full access while others will have limited access. Generally, trips to and from most access points were distributed 80/20 with the closer access getting the majority of the entering/exiting traffic. Exiting traffic intending to head northbound on France from the grocery or from the apartments was split 50/50 between the new RI/RO access onto France and the Hazelton signal. Some traffic headed southbound is split between the Hazelton easterly intersection or down to Gallagher intersection.

The following diagrams illustrate the directional distribution of ingress and egress traffic into and out of each land use of the redevelopment project:

Grocery Store Traffic:

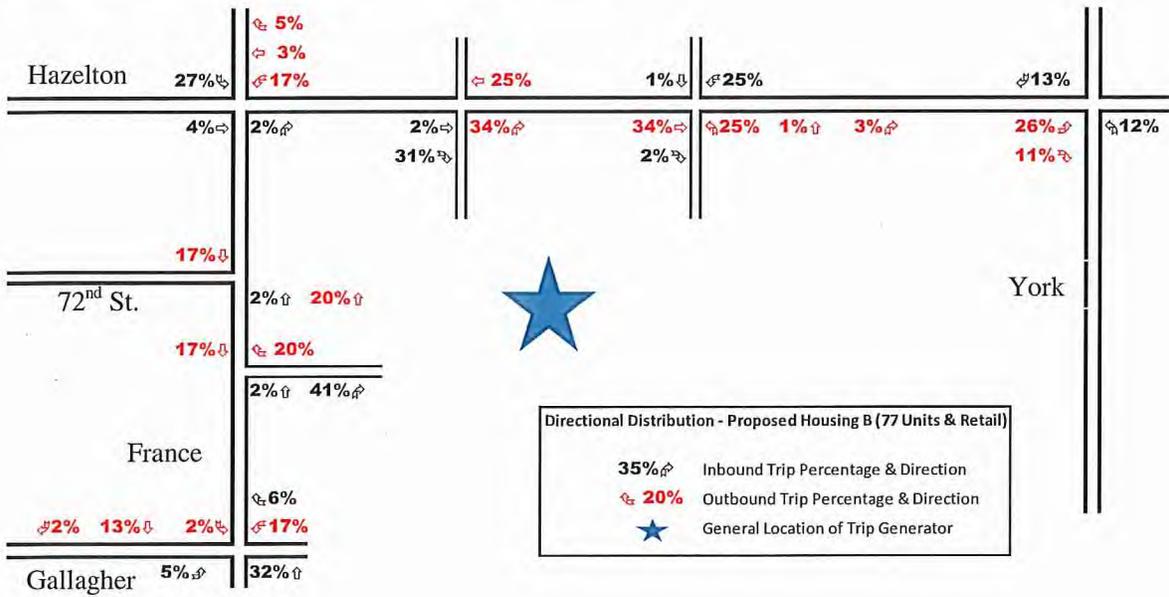


Proposed Housing A (109 Units):

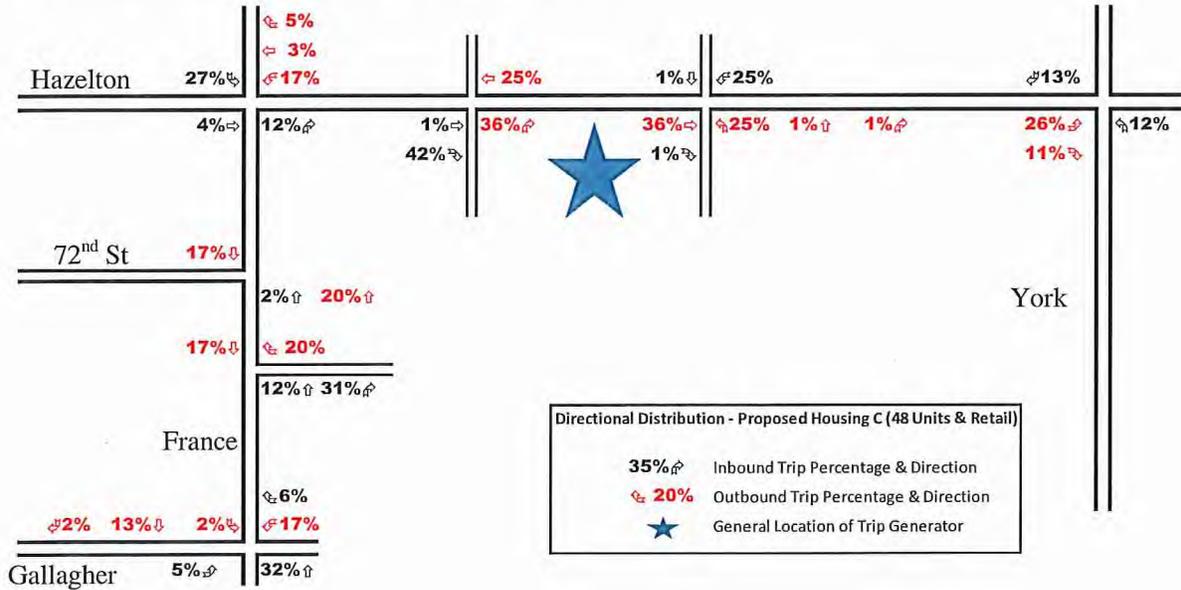


A75

Proposed Housing B (77 Units & 10,450 sq. ft. Retail):



Proposed Housing C (48 Unites & 10,500 sq. ft. Retail):



A76

3. Proposed Build Trip Generations

Tables 7 and 8 show the proposed trip generation estimates for each phase of the development – the 2015 Build (encompassing the new Byerly’s store and Housing A), and the 2016 Build (which adds the trip generation of Housing B and C to the trip generation from the 2015 Build analysis). Each is based on the trip generation rates found in the Institute of Transportation Engineers’ Trip Generation Manual, 9th Edition.

It should be noted that each scenario is adjusted to reflect shared trips and pass-by trips. According to the ITE Trip Generation Handbook, “Pass-by trips are made as intermediate stops on the way from an origin to a primary trip destination without a route diversion.” Further, “Pass-by trips are only applicable to trips that enter or exit the site, not internal trips.”⁶

According to Table 5.10 of the Handbook, the average pass-by trips percentage for supermarkets is 36%, which was used in this study.

The ITE Trip Generation Manual also outlines a procedure for calculating internal capture rates for trip origins within a multi-use development (aka, “Shared Trips”). The total Shared Trip assumption for this study was 20%.

Table 7. 2015 Build - Trip Generation Estimates¹ – Proposed Site

Land Use	Size	Weekday PM		Daily	Saturday MIDDAY		Saturday Daily
		Enter	Exit		Enter	Exit	
Supermarket	52,500 sq. ft.	254	244	5,368	285	274	9,323
Apartment (Building A)	109 Units	44	24	725	28	28	697
Total - Gross		298	268	6,093	313	302	8,626
		566			615		
Shared Trips		56	56	1,211	85	82	2,710
		112			167		
Pass-By Trips		72	72		0	0	
		144			0		
Total Proposed Trips		170	140	4,882	228	220	5,916
		310			448		

2. Per the data and methodologies in Trip Generation, 9th Edition, published by ITE.

⁶ Trip Generation Manual, 9th Edition, Volume 1: User’s Guide and Handbook, Institute of Transportation Engineers, Washington, DC, 2012.

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Table 8. 2016 Build - Trip Generation Estimates¹ – Proposed Site

Land Use	Size	Weekday PM		Daily	Saturday MIDDAY		Saturday Daily
		Enter	Exit		Enter	Exit	
Supermarket	52,500 sq. ft.	254	244	5,368	285	274	9,323
Apartments (Building A)	109 Units	44	24	725	28	28	697
Apartment (Building B)	77 Units	31	17	512	20	20	492
Specialty Retail	10,450 sq. ft.	19	20	449	26	26	522
Apartment (Building C)	48 Units	19	10	319	12	12	307
Specialty Retail	10,500 sq. ft.	19	20	451	26	26	525
Total - Gross		386	335	7,824	397	386	11,866
		721			783		
Shared Trips		73	73	1,576	105	102	3,134
		146			207		
Pass-By Trips		82	82		10	10	
		164			20		
Total Proposed Trips		231	180	6,248	282	274	8,732
		411			556		

3. Per the data and methodologies in *Trip Generation*, 9th Edition, published by ITE.

4. Projected Traffic Operations – 2015 Build

Table 9 summarizes the projected levels of service for the 2015 Build Condition. This reflects the traffic operation of the new Byerly’s Grocery Store and the first building of apartment units (aka, “Housing A”).

This analysis assumes the intersection enhancements along France Avenue. In addition, this analysis assumes that Hazelton has been reconfigured from a four-lane roadway (two lanes in each direction) to a three-lane roadway (one lane in each direction with a center Two-Way Left Turn Lane – a “TWLTL”). Further, the westerly driveway intersection along Hazelton between the Byerly’s redevelopment site and Rue de France shopping center has been configured into a three-quarter access – that is, eastbound Hazelton traffic can make left turns, make right turns or proceed straight, while westbound traffic can only turn right or proceed straight. No movement can make a left turn except the eastbound movement. That means that from the Byerly’s property, northbound movements can only turn right (eastbound) at Hazelton.

Full access is possible from the Byerly’s property easterly intersection with Hazelton. This intersection has been tested with either signalized or unsignalized operation. Both resulting levels of service are shown in the table below. Both operate acceptably.

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Table 9. Projected Levels of Service – 2015 Build Condition

Intersection	PM Peak Hour		Saturday Midday Peak Hour	
	LOS	Int. Delay (sec/veh)	LOS	Int. Delay (Sec/veh)
France Ave & Hazelton Rd.	B (F-SBLt)	17.3	C (F-SBLt)	31.4
Hazelton Rd. & Westerly Byerly's Drive	a (b)	2.1	b (b)	2.4
Hazelton Rd. & Easterly Byerly's Drive (Signalized or unsignalized)	B (B)	11.3	B (B)	11.6
	a (c)	2.1	a (c)	3.2
Hazelton Rd. & York Ave.	C (F-NBLt)	28.8	C (F-NBLt)	31.5
France Ave. & 72 nd St.	a (f-EB)	1.8	a (f-EB)	2
France Ave. & Gallagher Dr.	C (F-SBLt)	18.0	C (F-SBLt)	20.4
France Ave. & Byerly's Driveway	a (a)	n/a	a (a)	n/a

NOTES: a. Upper Case letters reflect LOS at signalized intersections; Lower case letter reflect LOS at unsignalized intersection.
 b. First letter reflects Intersection LOS; letter inside parentheses reflects LOS of worst movement.
 c. Int. Delay reflects overall average intersection delay, not necessarily worst movement delay.

Queue lengths for the 2015 Build conditions are listed on Tables 10a and 10b.

Table 10a. Selected 95th Percentile Queue Lengths – Weekday P.M. Peak Hour – 2015 Build Conditions

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	162
Southbound Left France at Hazelton	360	434
Northbound Right France at Hazelton	315	147
Westbound Right France at Hazelton	150	90
Eastbound Left Hazelton at Byerly's Driveway	259	45
Northbound Byerly's Driveway at Hazelton	270	43
Northbound Left York at Hazelton	160	182
Westbound Left Turn Gallagher at France	120	71
Southbound Left Turn France at Gallagher	230	196

Table 10b. Selected 95th Percentile Queue Lengths – Saturday Midday Peak Hour – 2015 Build Conditions

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	180
Southbound Left France at Hazelton	360	478
Northbound Right France at Hazelton	315	44
Westbound Right France at Hazelton	150	73
Eastbound Left Hazelton at Byerly's Driveway	259	55
Northbound Byerly's Driveway at Hazelton	270	37
Northbound Left York at Hazelton	160	186
Westbound Left Turn Gallagher at France	120	90
Southbound Left Turn France at Gallagher	230	318

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5. Projected Traffic Operations – 2016 Build

Table 11 summarizes the projected levels of service for the 2016 Build Condition. This reflects the traffic operation of the 2015 Build conditions combined with the two additional apartment buildings (aka, “Housing B and C”) and ground floor retail uses. (NOTE: This level of analysis did not include the proposed restaurant use to the east of the Byerly’s redevelopment site.)

This analysis assumes the same intersection enhancements along France Avenue, as well as the geometric and traffic control changes along Hazelton described under 2015 Build. Full access is again possible from the Byerly’s property easterly intersection with Hazelton. This intersection has been tested with either signalized or unsignalized operation. Both resulting levels of service are shown in the table below. Both continue to operate acceptably.

**Table 11. Projected Levels of Service – 2016 Build Condition
(Without Impact of Parcel to the East)**

Intersection	PM Peak Hour		Saturday Midday Peak Hour	
	LOS	Int. Delay (sec/veh)	LOS	Int. Delay (Sec/veh)
France Ave & Hazelton Rd.	C (F-SBLt)	20.9	D (F-SBLt)	36.4
Hazelton Rd. & Westerly Byerly’s Drive	a (b)	2.2	b (b)	2.5
Hazelton Rd. & Easterly Byerly’s Drive (Signalized or unsignalized)	B (B)	13.0	B (C)	13.3
	a (c)	2.5	a (d)	3.6
Hazelton Rd. & York Ave.	C (F-NBLt)	32.4	C (F-NBLt)	33.3
France Ave. & 72 nd St.	a (c)	0.5	a (d)	0.6
France Ave. & Gallagher Dr.	B (F-SBLt)	19.2	B (F-SBLt)	18.3
France Ave. & Byerly’s Driveway	a (a)	n/a	a (a)	n/a

NOTES: a. Upper Case letters reflect LOS at signalized intersections; Lower case letter reflect LOS at unsignalized intersection.
b. First letter reflects Intersection LOS; letter inside parentheses reflects LOS of worst movement.
c. Int. Delay reflects overall average intersection delay, not necessarily worst movement delay.

**Table 12a. Selected 95th Percentile Queue Lengths – Weekday P.M. Peak Hour
– 2016 Build Conditions**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	136
Southbound Left France at Hazelton	360	480
Northbound Right France at Hazelton	315	67
Westbound Right France at Hazelton	150	100
Eastbound Left Hazelton at Byerly’s Driveway	259	41
Northbound Byerly’s Driveway at Hazelton	270	41
Northbound Left York at Hazelton	160	176
Westbound Left Turn Gallagher at France	120	93
Southbound Left Turn France at Gallagher	230	146

180

**Table 12b. Selected 95th Percentile Queue Lengths' – Saturday Midday Peak Hour
– 2016 Build Conditions**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	177
Southbound Left France at Hazelton	360	440
Northbound Right France at Hazelton	315	89
Westbound Right France at Hazelton	150	92
Eastbound Left Hazelton at Byerly's Driveway	259	91
Northbound Byerly's Driveway at Hazelton	270	49
Northbound Left York at Hazelton	160	187
Westbound Left Turn Gallagher at France	120	124
Southbound Left Turn France at Gallagher	230	176

6. Projected Traffic Operations – 2016 Build with Restaurant Development to the East

Table 13 illustrates the trip generation added by the restaurant development to the east of the Byerly's Redevelopment. Applying these trip estimates, Table 14 summarizes the projected levels of service for the 2016 Build Condition assuming an outside developer were to develop the former Szechuan Star into another restaurant. Traffic operation of the 2016 Build conditions have been combined with the impacts of a possible 8,000 square foot quality sit-down restaurant that may be in operation at or after the completion of the 2016 Build scenario.

This analysis assumes the same intersection enhancements along France Avenue, as well as the geometric and traffic control changes along Hazelton described under 2015 Build. Full access is again possible from the Byerly's property easterly intersection with Hazelton. This intersection has been tested with either signalized or unsignalized operation. Both resulting levels of service are shown in the table below. Both continue to operate acceptably.

Table 13. 2016 Build with Impacts to the East - Trip Generation Estimates¹ – Proposed Site

Land Use	Size	Weekday PM		Daily	Saturday Midday		Saturday Daily
		Enter	Exit		Enter	Exit	
Supermarket	52,500 sq. ft.	254	244	5,368	285	274	9,323
Apartment (Building A)	109 Units	44	24	725	28	28	697
Apartment (Building B)	77 Units	31	17	512	20	20	492
Specialty Retail	10,450 sq. ft.	19	20	449	26	26	522
Apartment (Building C)	48 Units	19	10	319	12	12	307
Specialty Retail	10,500 sq. ft.	19	20	451	26	26	525
Quality Restaurant	8,000 sq. ft.	40	20	720	51	35	755
Total - Gross		426	354	8,544	448	421	11,621
		780			869		
Shared Trips		77	77	1,676	119	112	3,350
		154			231		
Pass-By Trips		93	93		10	10	
		186			20		
Total Proposed Trips		231	180	6,868	319	299	9,271
		411			618		

¹Per the data and methodologies in Trip Generation, 9th Edition, published by ITE.

Table 14. Projected Levels of Service – 2016 Build Condition (With Impact of Parcel to the East)

Intersection	PM Peak Hour		Saturday Midday Peak Hour	
	LOS	Int. Delay (sec/veh)	LOS	Int. Delay (Sec/veh)
France Ave & Hazelton Rd.	C (F-SBLt)	21.9	D (F-SBLt)	39.6
Hazelton Rd. & Westerly Byerly's Drive	a (b)	2.2	b (b)	2.7
Hazelton Rd. & Easterly Byerly's Drive (Signalized or unsignalized)	B (B)	13.0	B (C)	13.4
	a (c)	2.7	a (d)	4.4
Hazelton Rd. & York Ave.	C (F-NBLt)	33.0	C (F-NBLt)	34.5
France Ave. & 72 nd St.	a (c)	0.5	a (d)	0.7
France Ave. & Gallagher Dr.	B (F-SBLt)	19.3	B (F-SBLt)	18.7
France Ave. & Byerly's Driveway	a (a)	n/a	a (a)	n/a

NOTES: a. Upper Case letters reflect LOS at signalized intersections; Lower case letter reflect LOS at unsignalized intersection.
 b. First letter reflects Intersection LOS; letter inside parentheses reflects LOS of worst movement.
 c. Int. Delay reflects overall average intersection delay, not necessarily worst movement delay.

ASZ

Queuing is also assessed for this condition, and is listed in Tables 15a and 15b.

**Table 15a. Selected 95th Percentile Queue Lengths' – Weekday P.M. Peak Hour
– 2016 Build Conditions w/restaurant**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	175
Southbound Left France at Hazelton	360	381
Northbound Right France at Hazelton	315	84
Westbound Right France at Hazelton	150	69
Eastbound Left Hazelton at Byerly's Driveway	259	68
Northbound Byerly's Driveway at Hazelton	270	54
Northbound Left York at Hazelton	160	185
Westbound Left Turn Gallagher at France	120	90
Southbound Left Turn France at Gallagher	230	161

**Table 15b. Selected 95th Percentile Queue Lengths' – Saturday Midday Peak Hour
– 2016 Build Conditions w/restaurant**

Segment	Segment Length (ft.)	95 th Percentile Queue Length (ft.)
Westbound Left Turn Hazelton at France	256	93
Southbound Left France at Hazelton	360	442
Northbound Right France at Hazelton	315	90
Westbound Right France at Hazelton	150	93
Eastbound Left Hazelton at Byerly's Driveway	259	78
Northbound Byerly's Driveway at Hazelton	270	58
Northbound Left York at Hazelton	160	185
Westbound Left Turn Gallagher at France	120	119
Southbound Left Turn France at Gallagher	230	230

G. TRAFFIC SIGNAL WARRANT ANALYSIS

As part of the Edina Redevelopment Project, consideration has been given whether the easterly driveway intersection at Hazelton will be warranted for signalization as a result of this project. RLK has used the following methodology to assess the likelihood of signalization at the intersection.

The Minnesota Department of Transportation has prepared a document "Metro Traffic Signal Justification Report Methodology", to assist in determining whether an intersection is justified for signalization.⁷ The document outlines the current methodology in determining whether criteria are met for a signal to be considered justified at a particular intersection.

For a specific intersection to be considered for a traffic signal installation one of the following criteria must be met:

1. The intersection meets Warrant 1A, 1B or 7 of the current MN MUTCD.
2. Current traffic volumes do not meet Warrant 1A or 1B, but development in the area will occur such that the warrants will be met in a reasonable period of time and state funds are not used for construction.
4. Current traffic volumes do not meet Warrant 1A or 1B, but a significant crash problem exists (an average of at least three correctable crashes per year (any 12-month period) over the most recent 3-year period) and traffic volumes are likely to meet warrants within a reasonable period.
5. The intersection has significant amounts of pedestrian traffic, which can be documented.

A summary of the analysis of Warrants 1A and 1B are as follows:

Number of lanes for moving traffic on each approach:

- For Major Street = 2 (left turn lane is shared), Vehicles per hour on major street (total of both approaches) must be ≥ 600 for Condition A, 900 for Condition B
- For Minor Street = 2 (left lane + shared through/right lane), Vehicle per hour on higher-volume minor street approach (one direction only) must be ≥ 200 for Condition A, 100 for Condition B

For Condition A, the 2016 Build (with restaurant) Major Street approach = 679 trips per hour in PM Peak Hour; and 911 trips per hour in Saturday Midday Peak Hour. The Minor Street approach traffic = 64 trips

⁷ "Metro Traffic Signal Justification Report Methodology", as part of Minnesota Department of Transportation Engineering Services Technical Memorandum No. 07-02-T-01, Intersection Control Evaluation (ICE), March 20, 2007.

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per hour (one direction only) in PM Peak Hour; = 97 trips per hour in Saturday Midday Peak Hour. Warrant 1A is not met.

For Condition B, the 2016 Build (with restaurant) conditions are not met for PM Peak Hour conditions, but are very close to being met for the Saturday Midday Peak Hour.

The methodology also states that a combination of Conditions A and B may be used after adequate trial of other remedial measures is made. This allows for a reduction to levels that are 80% of the previous minimums for warrants to be made. Therefore:

- For Major Street = 1 (left turn lane is shared), Vehicles per hour on major street (total of both approaches) must be ≥ 480 for Condition A, 720 for Condition B
- For Minor Street = 2 (left lane + shared through/right lane), Vehicle per hour on higher-volume minor street approach (one direction only) must be ≥ 160 for Condition A, 80 for Condition B

Under these 80% conditions, the Saturday Midday Peak Hour does not meet Condition A but does meet Condition B.

The nearby pedestrian crossing of Hazelton at the Edina Promenade may be documented to provide additional justification to the signalization of this intersection.

H. PARKING IMPACT ANALYSIS

The Byerly's redevelopment has proposed the following on-site parking for each parcel of development:

Byerly's Store:	46,700 sq. ft. ground floor; therefore, at 5 stalls per 1000 sq. ft., = 235 stalls required; 236 stalls provided to south of store.
Housing A and B:	At 2.0 stalls/unit; 186 units x 2.0 stalls = 372 stalls, per code At 1.5 stalls/unit; 186 units x 1.5 stalls = 279 stalls Parking User Rate = 1 stall per bedroom (except 3 br) = 263 stalls 286 stalls provided in garage; 25 surface stalls = 311 stalls total
Housing C:	At 2.0 stalls/unit; 48 units x 2.0 stalls = 96 stalls, per code At 1.5 stalls/unit; 48 units x 1.5 stalls = 72 stalls Parking User Rate = 1 stall per bedroom (except 3 br) = 64 stalls 72 stalls provided in garage; 9 surface stalls = 81 stalls total

Overall Parking Summary for Housing/Retail Buildings

Parking at 2 stalls per unit	468
Parking at 1.5 stalls per unit	351
Parking User Rate	327
Garage Parking Provided	358
Surface Parking Provided	34
Total Provided	392

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I. CONCLUSIONS AND RECOMMENDATIONS

Based on the analysis documents in this study, RLK Incorporated has drawn the following conclusions and recommendations:

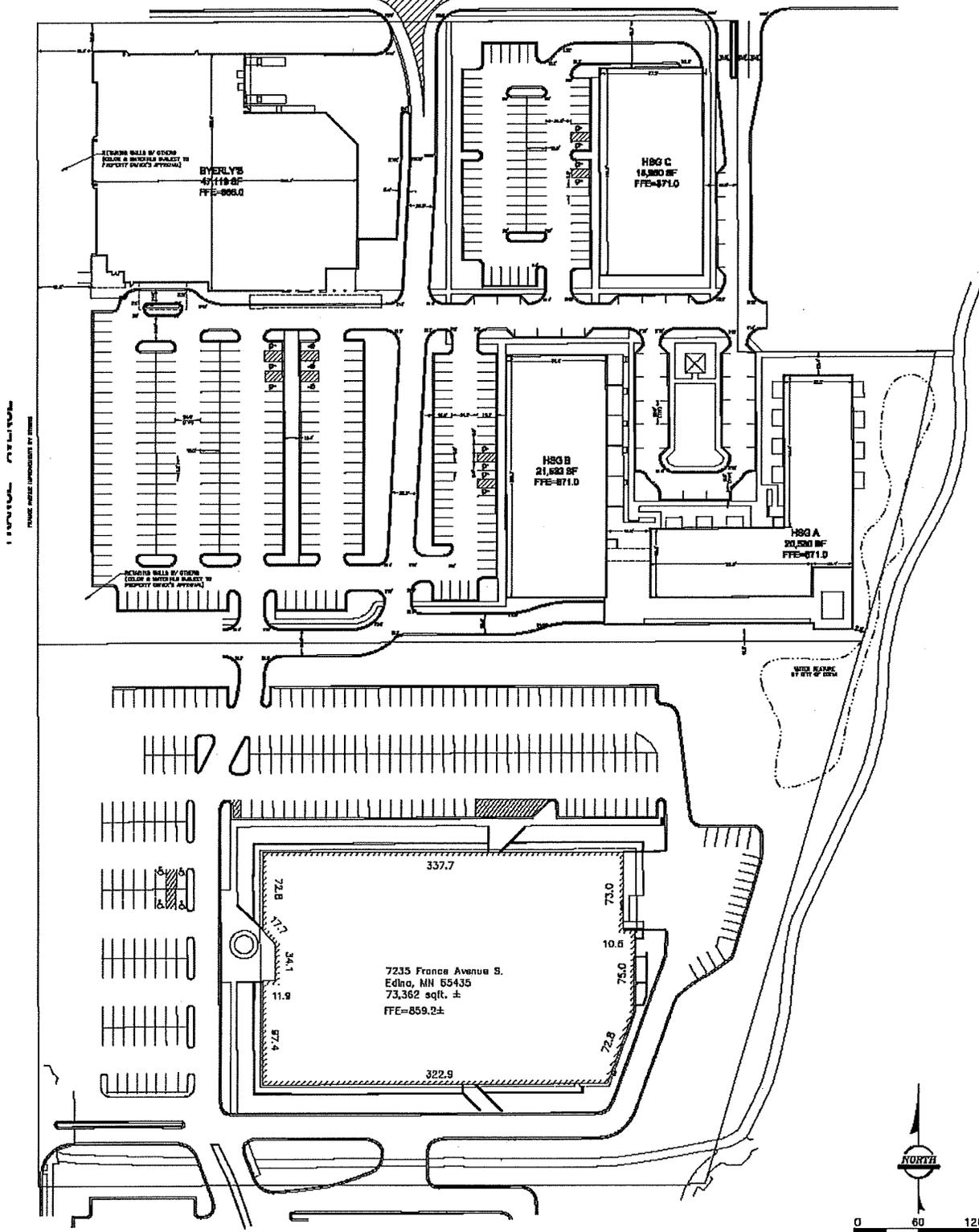
- The three-lane configuration of Hazelton appears to work well in both scenarios.
- The westerly Byerly's driveway operates well (LOS-A) as a right-in/right-out at Hazelton.
- At this point, the easterly driveway operates equally well as either a signalized or side street stop sign intersection (both LOS-A, with signal operating slightly better, especially for northbound movements). Left turn 95th percentile queues at the intersection are less than 100 feet in each approach. As a signal, the average control delay is 7.2 sec/veh. As an unsignalized intersection, the average delay is 4.1 sec/veh (due to free flow along Hazelton). Signal warrants may be met with further analysis outside the scope of this report.
- The right-in/right-out onto France operates well, and does not interfere with northbound traffic or weaving traffic approaching the right turn lane at Hazelton.
- The additional access points into and out of the development disperse the trips throughout the development, and thus reduce the overall traffic impacts onto the surrounding system.
- There is a reduction in overall traffic on the westbound approach of Hazelton at France. This is due to the redistribution of trips resulting from the alternate access points (right-in/right-out at France; cross access to Gallagher via the Macy's Furniture Store access, etc.). Westbound 95th percentile queues on Hazelton at France Avenue extend to the east 183 feet (total length to westerly driveway is approx. 260 feet).
- There are some queuing issues with the northbound left turn movement at York, the southbound left turn from France to eastbound Hazelton, and southbound France at Gallagher. These conditions seem to exist under the No-Build as well as the Build Conditions, and seem to be attributable to other development in the area (especially Target and Yorktown Square impacts at the York intersection).
- Off-street parking supply is met when considered at a 1.5 stall per unit or a parking user rate is considered. The mixed use development and availability of nearby transit may allow the parking supply to meet these lower rates.

It is clear that the additional access points to and from the Byerly's redevelopment site greatly benefit the traffic operations despite there being more trips generated than in the current configuration. As a result, there are no queuing impacts directly associated with the redevelopment, even with the "roadway diet" from four lanes on Hazelton to three. While a signal is not explicitly warranted based on traffic volumes, a case can be made that signalizing the easterly Byerly's intersection will facilitate the pedestrian crossing at the Promenade to the east (subject to further engineering study).

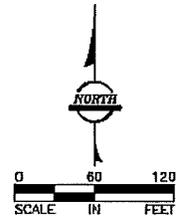
FIGURES

Figure 1	Project Site
Figure 2	Existing Geometrics
Figure 3	Aerial Photo of Existing Conditions
Figure 4	Existing Peak Hour Turning Movements
Figure 5	2015 No-Build Peak Hour Turning Movements
Figure 6	Proposed Geometrics
Figure 7	2015 Build Peak Hour Turning Movements
Figure 8	2016 Build Peak Hour Turning Movements
Figure 9	2016 Build with Restaurant to the East Peak Hour Turning Movements

HAZELTON ROAD



3821 Gallagher Drive
Edina, MN 55435
Miter Investment Group LP



Jan 17, 2013 3:37pm G:\Anderson K-M Builders\012-135-M\Q\Technical Data\Traffic\Exhibits\012\Site Plan.dwg



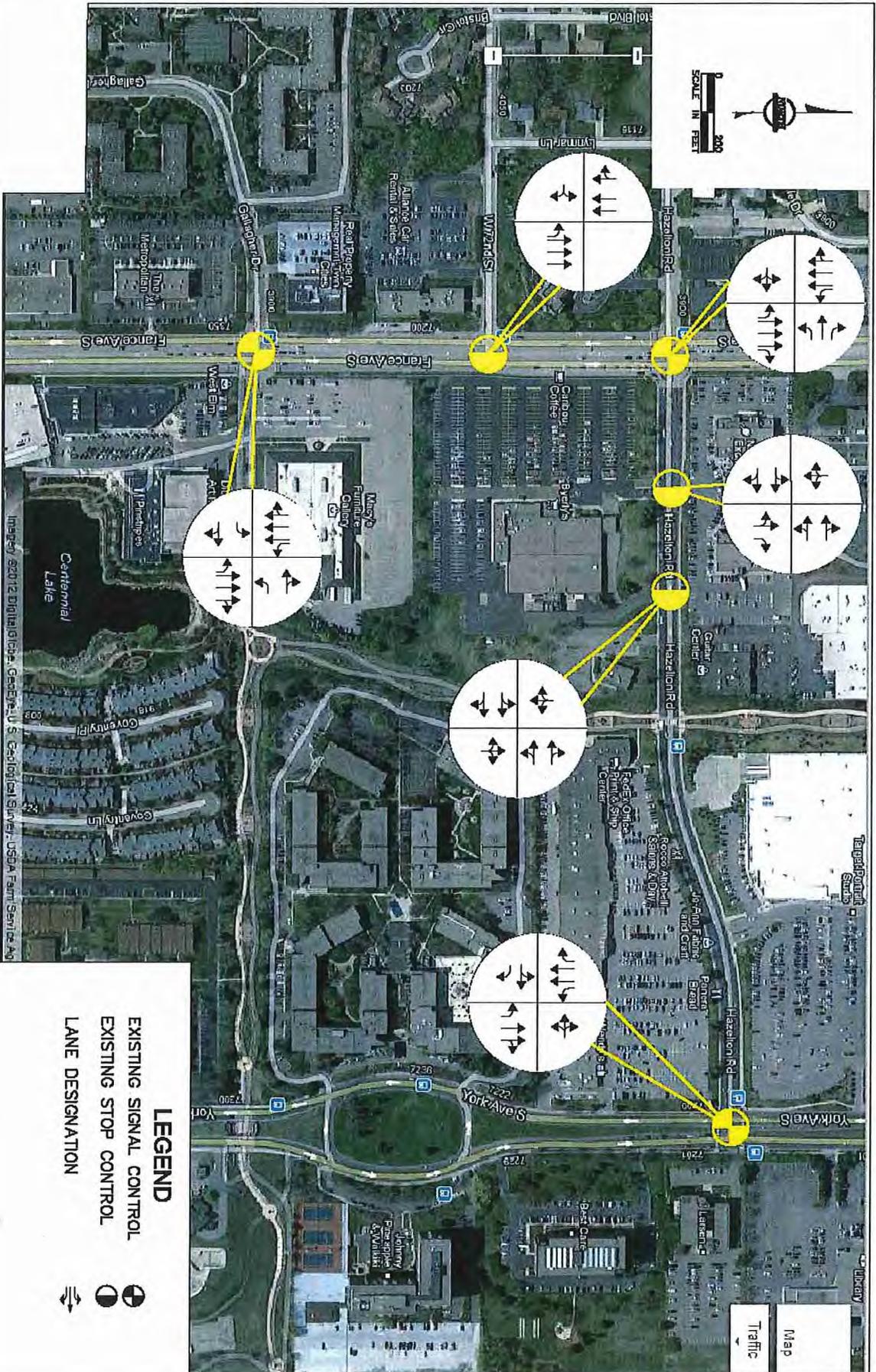
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EDINA BYERLY'S PROJECT SITE PLAN

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Figure # 1
Project # 2012135M



EDINA BYERLY'S

EXISTING GEOMETRICS

Figure 2

Project # 2012135M

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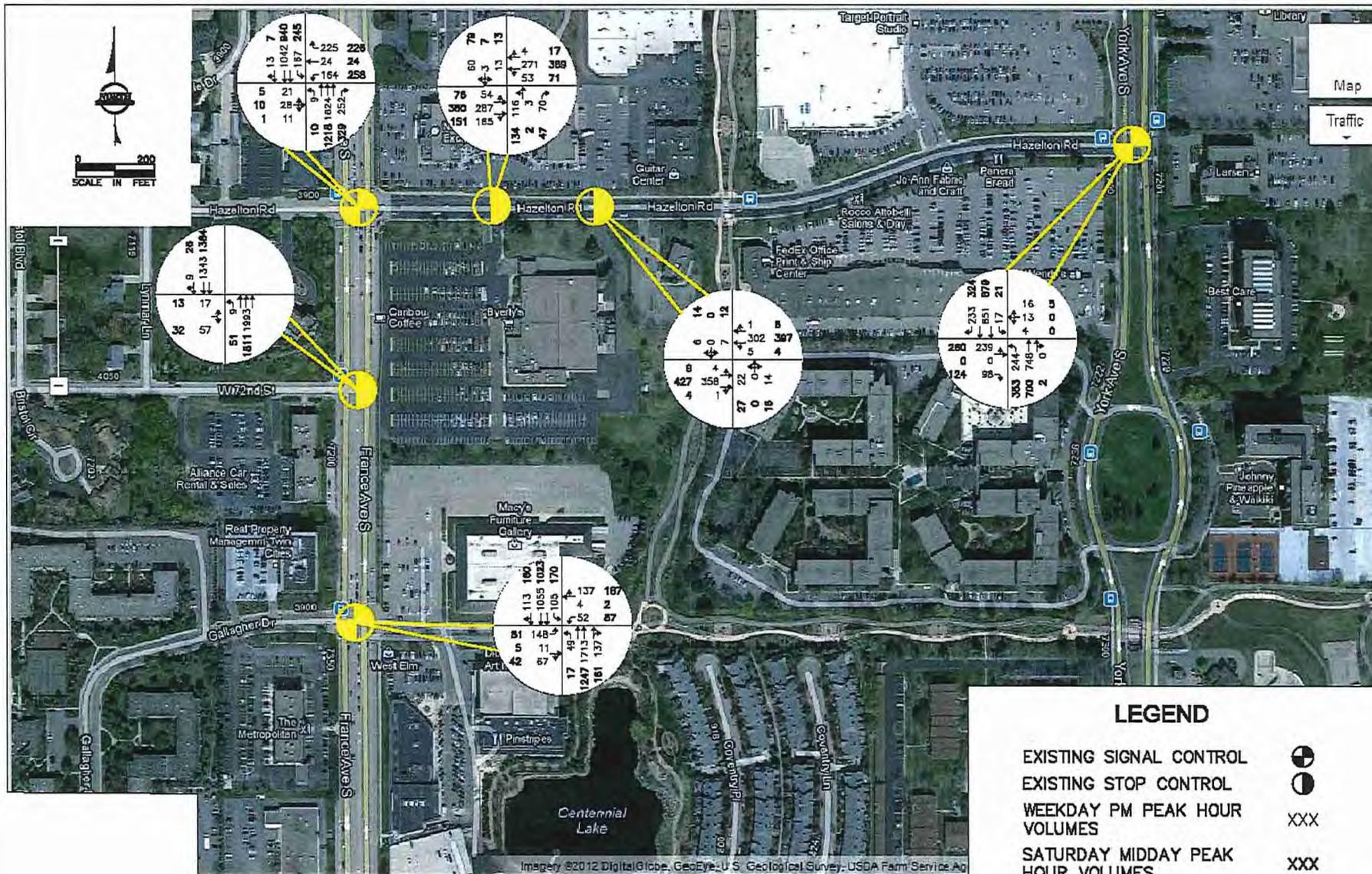
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Figure 3 – Aerial Photo of Existing Conditions



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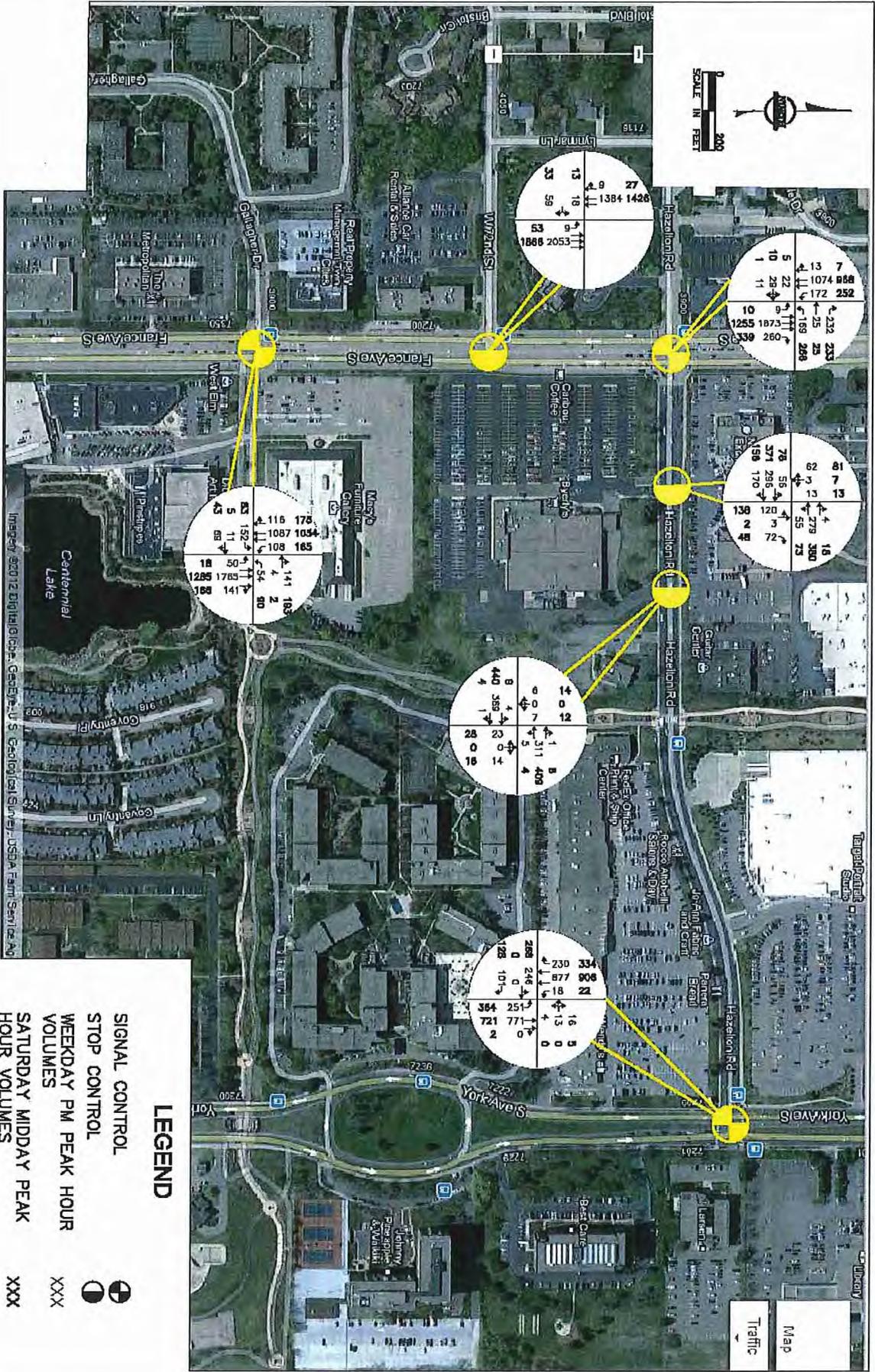
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EDINA BYERLY'S EXISTING VOLUMES

Figure
 4

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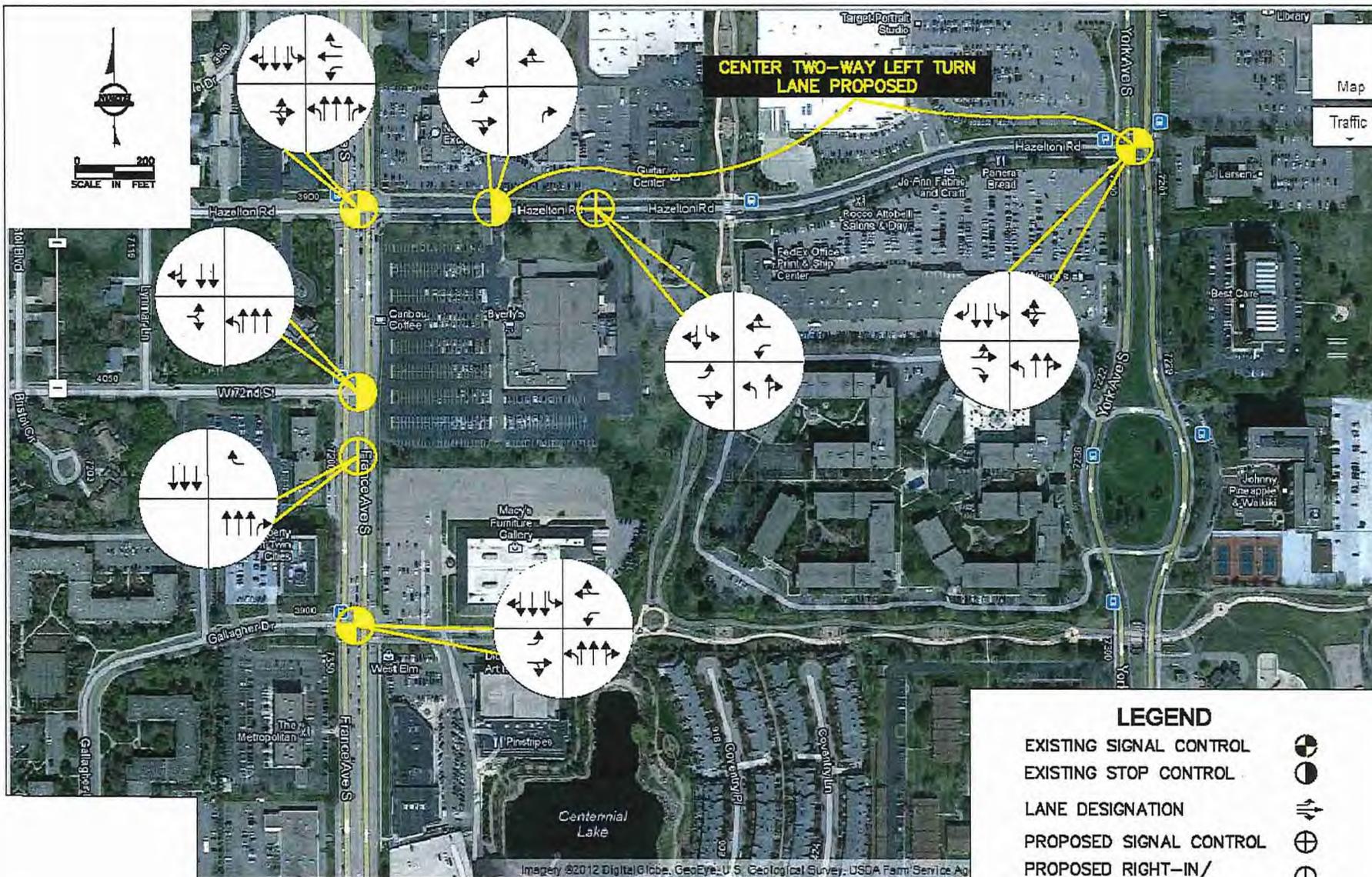
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EDINA BYERLY'S

2015 NO-BUILD PEAK HOUR TURN MOVEMENTS

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EDINA BYERLY'S PROPOSED GEOMETRICS

Figure
 6

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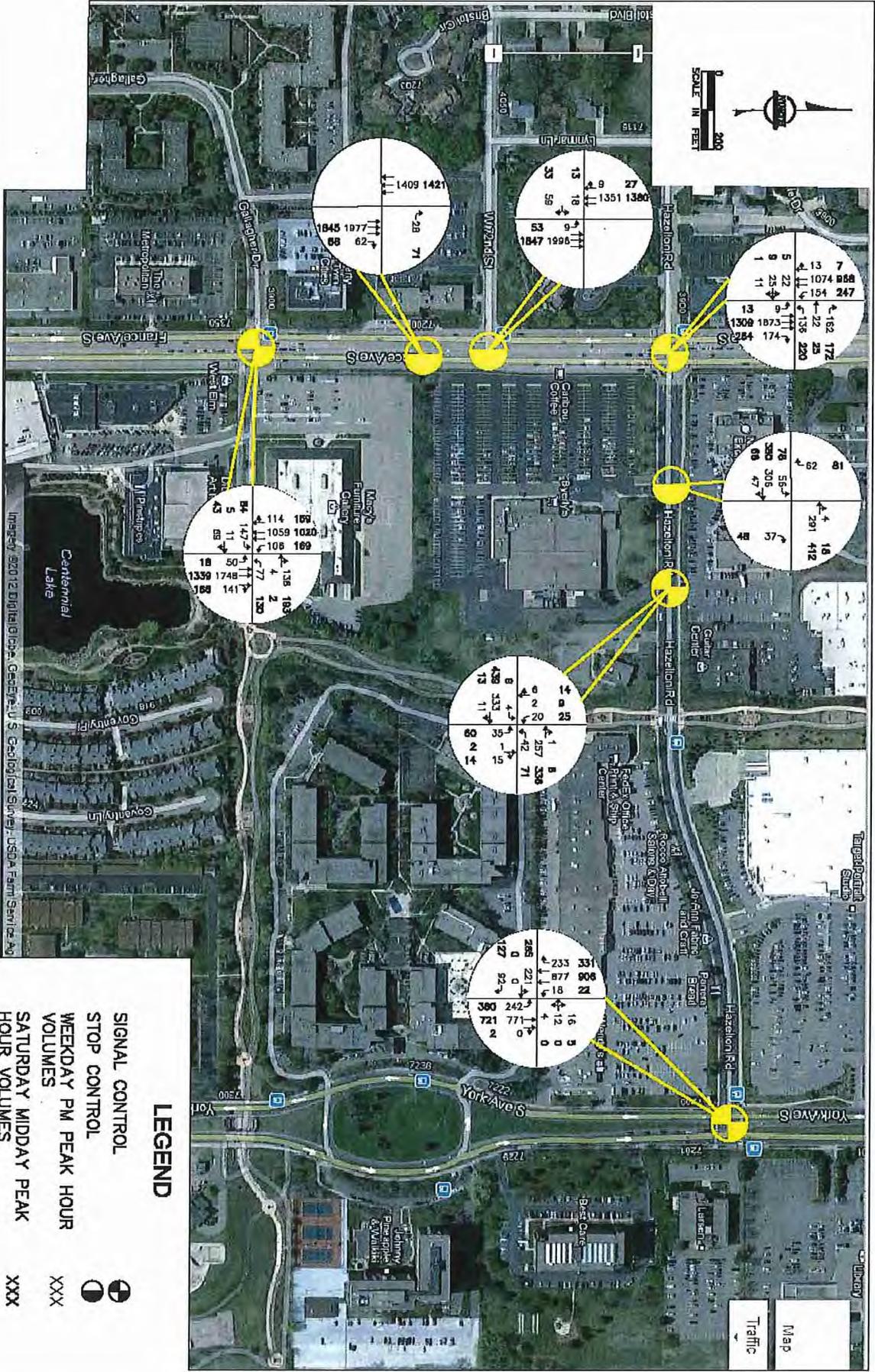


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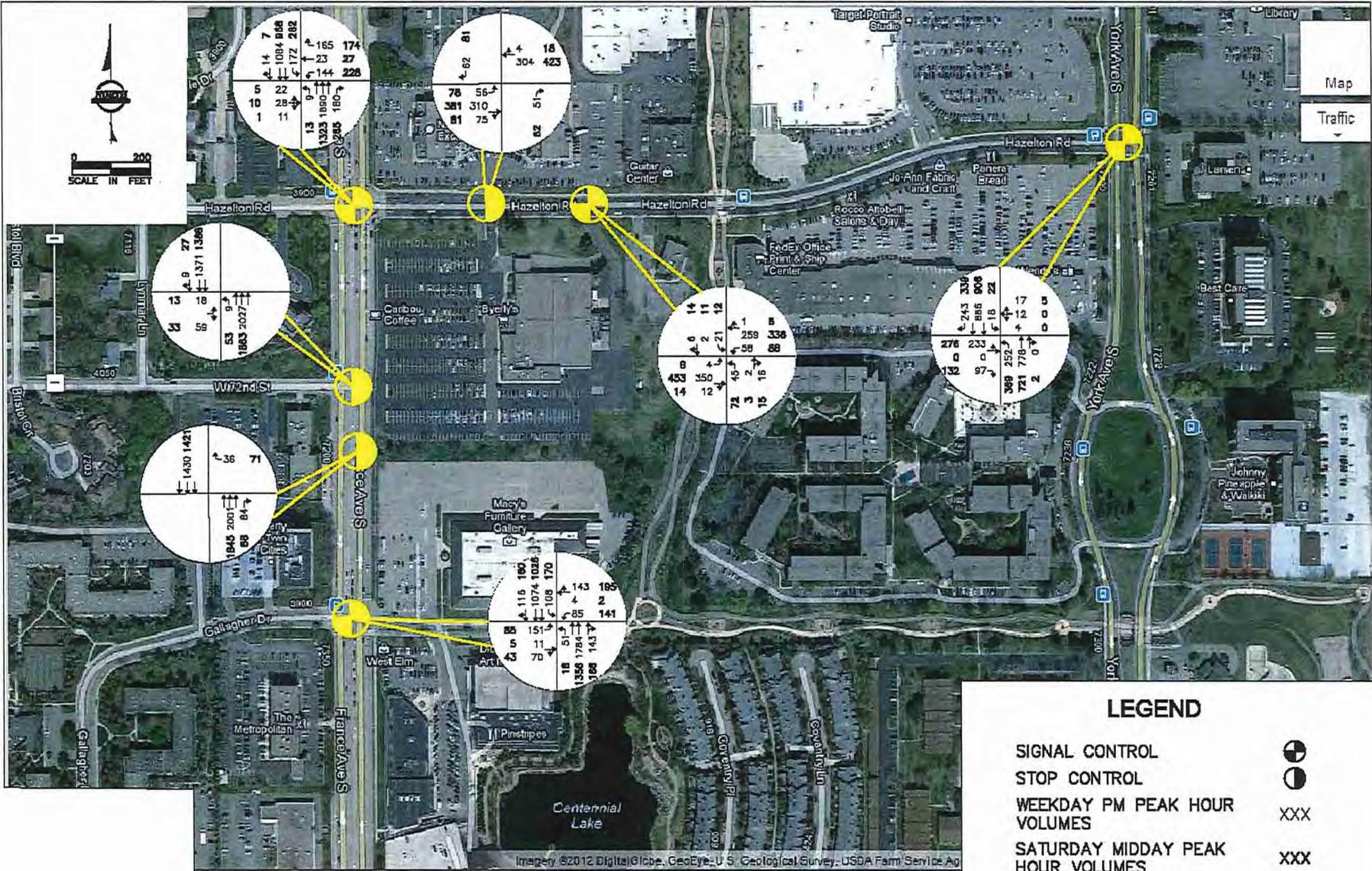


EDINA BYERLY'S

2015 BUILD PEAK HOUR TURN MOVEMENTS

LEGEND

○ SIGNAL CONTROL
● STOP CONTROL
XXX WEEKDAY PM PEAK HOUR VOLUMES
XXX SATURDAY MIDDAY PEAK HOUR VOLUMES
XXX



Handwritten initials: H



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2016 BUILD PEAK HOUR TURN MOVEMENTS

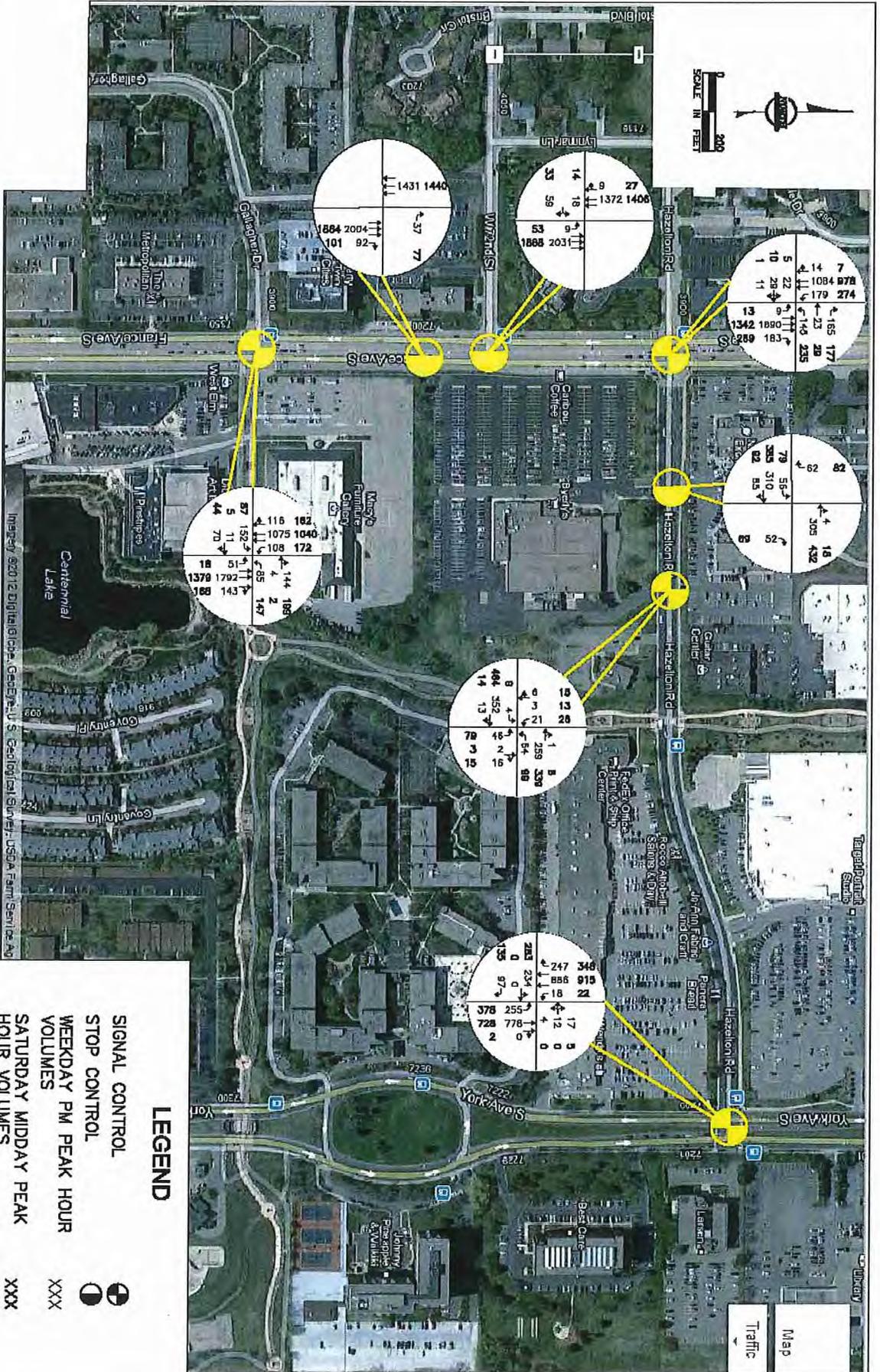
Figure
8

Project #
2012135M



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EDINA BYERLY'S

2016 BUILD PEAK HOUR TURN MOVEMENTS - WITH RESTAURANT

Figure 9

Project # 2012135M

APPENDICES

See Technical Appendices (bound separately) for Intersection LOS for signalized and unsignalized intersection operational performance reports and queuing analyses for each scenario.

Engineering Department • Phone 952-826-0371
Fax 952-826-0392 • www.CityofEdina.com



Date: May 17, 2013

To: Cary Teague – Community Development Director

From: Wayne Houle – Director of Engineering

**Re: Byerly's – France Avenue Re-Development
Dated April 26, 2013**

Engineering has reviewed the above stated proposed plan and offer the following comments:

- ⊙ A Nine Mile Creek Watershed permit will be required, along with other agency permits such as Hennepin County Public Works, MNDH, MPCA, MCES.
- ⊙ A developer's agreement will be required for the placement of the public water main and sanitary sewer and for any other public improvements. The developer agreement should also include cost participation for the France Avenue improvements and also should indicate that the watermain and sanitary sewer mainline are public. The agreement should also state that the City is responsible for the maintenance and operation of the watermain and sanitary sewer and in the event that the City needs to excavate for a repair of the system that the City is not responsible for restoring the surface, such as pavements and or landscaping.

Civil Plat:

- Sidewalk easements should be shown along France Avenue and also along Hazelton Road. Any easements needed for the roundabout should also be shown.

Civil Sheet 4:

- Show existing sanitary sewer trunk line located in southwest corner of the development.

Civil Sheet 5:

- Show updated roundabout design.
- Change the island of the right-in right-out on Hazelton Rd to be concrete versus paint.

Civil Sheet 6:

- Storm sewer details need to show separating fabric between uncompacted subgrade and rock.
- Provide additional storm sewer at entrance to underground garage of HSG A.

Civil Sheet 9:

- Show existing sanitary sewer trunk line located in southwest corner of development.
- Re-align westerly north-south water main to eliminate bends in watermain including where the mainline connects to the watermain on Hazelton Rd.
- Indicate on plans what utilities are private versus public by noting that on the pipes.
- Move gate-valves on services closer to mainline. Engineering will indicate where to place the gate-valves.
- Provide additional storm sewer at entrance to underground garage of HSG A.

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Civil Sheet 10:

- Update all City of Edina details.

Staff will require a more detail review of the Civil Plans if this project is approved by the City Council. Please contact me if you have any questions regarding this review.

A100